



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

DESIGN REVIEW COMMITTEE AGENDA

WEDNESDAY, MAY 9, 2018
3:00 P.M.

ESQUIMALT COUNCIL CHAMBERS

- I. CALL TO ORDER
- II. LATE ITEMS
- III. ADOPTION OF AGENDA
- IV. ADOPTION OF MINUTES – April 11, 2018
- V. STAFF REPORT

- 1) **Official Community Plan Amendment And Rezoning Application**
1379 Esquimalt Road / 520 Foster Street (St. Peter and St. Paul's Church) [PID 024-848-905; Lot A (DD EP75276), Suburban Lot 27, Esquimalt District, Plan 5092]

PURPOSE OF APPLICATION:

CitySpaces Consulting on behalf of the owner of St. Peter and St. Paul's Church (The Anglican Synod of the Diocese of British Columbia), is requesting an amendment to the Official Community Plan Bylaw No. 2646, and a change in zoning to facilitate the subdivision of the subject property into four separate parcels (three land parcels and one air space parcel). The 'Hermitage', an existing multiple unit residential building would occupy one land parcel, a small cemetery would be on a second land parcel, and the existing heritage church with a new 'ministry centre' would occupy the third land parcel. Twenty-four new, affordable rental housing units for seniors would be located above the new ministry centre in an air space parcel. The proposed development would require a Heritage Alteration Permit as the connection of the new ministry centre to the heritage designated church would require an alteration to this protected building.

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] provide Council and the Director of Development Services with comments on the Official Community Plan Amendment and Rezoning Application authorizing a new 5 storey, 24 unit, multiple family residential building with a new 'ministry centre' on the ground floor sited in accordance with the BCLS Site Plan provided by Wey Mayenburg Land Surveying Inc., and incorporating height and massing consistent with the architectural plans provided by Number Ten Architectural Group, stamped "Received April 24, 2018" detailing the development proposal, to be located at 1379 Esquimalt Road / 520 Foster Street (St. Peter and St. Paul's Church) [PID 024-848-905; Lot A (DD EP75276), Suburban Lot 27, Esquimalt District, Plan 5092], and **make a recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

2) REVIEW OF THE DRAFT OFFICIAL COMMUNITY PLAN

PURPOSE OF APPLICATION:

To review the Draft Official Community Plan and provide Council with any recommended changes along with reasons for the proposed changes.

VI. NEXT REGULAR MEETING

June 13, 2018

VII. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

ADVISORY DESIGN REVIEW COMMITTEE MINUTES OF APRIL 11, 2018 ESQUIMALT COUNCIL CHAMBERS

PRESENT:	Bev Windjack, Acting Chair Jill Singleton	Wendy Kay Robert Schindelka
ABSENT:	Roger Wheelock, Graeme Verhulst, Ally Dewji and Cst. Rae Robirtis	
STAFF:	Bill Brown, Director of Development Services, Staff Liaison Alex Tang, Planner Pearl Barnard, Recording Secretary	

I. CALL TO ORDER

Present Committee members nominated Bev Windjack to assume the Chair.

Acting Chair Bev Windjack called the meeting to order at 3:20 p.m.

II. LATE ITEMS

(1) Pertaining to Agenda Item (V)

- Letter received April 11, 2018, re: Pacific House Arts Committee

III. APPROVAL OF AGENDA

Moved by Robert Schindelka, seconded by Wendy Kay: That the agenda be approved as amended with the inclusion of the late item. **Carried Unanimously**

IV. ADOPTION OF MINUTES – March 14, 2018

Moved by Wendy Kay, seconded by Jill Singleton: That the minutes of March 14, 2018, be adopted as circulated. **Carried Unanimously**

V. STAFF REPORTS

OFFICIAL COMMUNITY PLAN (OCP) AMENDMENT and REZONING APPLICATION 899 Esquimalt Road

Bob Heaslip, Development Planning Strategy, Farzin Yadegari, Farzin Yadegari Architect Inc., Caelan Griffiths, PMG Landscape Architects and Robert Evans, provided an overview of the OCP amendment and rezoning applications for 899 Esquimalt Road, presented a PowerPoint presentation and responded to questions from the Committee.

Committee comments included (*response in italics*):

- The height of the proposed development is not acceptable under the current OCP designation.
- The proposed building is not attractive on all sides. As this is important gateway building to Esquimalt, the aesthetics of the east side of the building needs to be improved.
- The proposed wall art is not a compelling addition to the façade.
- Concerns that the overflow parking could have an impact on residential streets.
- Like the look of the “Green over Grey” green wall system. Concerns with the maintenance of the wall, a dead or failing wall will not be acceptable and the maintenance costs could be a financial burden on the future strata.

- Building looks like 15 storeys from Wollaston Street. *Staff clarified that height is measured from the average grade; hence, this building is considered to be 13 storeys.*
- Community consultation held for the project? *Concerns were expressed about height, traffic and parking.*
- Explored housing opportunities including rental housing agreement options. *Building will be strata titled with rental option.*

RECOMMENDATION:

Moved by Bev Windjack, seconded by Wendy Kay: The Esquimalt Design Review Committee recommends that the application for an amendment to the Official Community Plan and rezoning, authorizing a 13 storey, commercial mixed-use building consisting 2 retail commercial space and 62 residential units, sited in accordance with the BCLS Site Plan provided by Wey Massenburg Land Surveying Inc., stamped “Received November 30, 2017”, and incorporating height and massing consistent with the architectural plans prepared by Farzin Yadegari Architect Inc., stamped “Received March 7, 2018”, detailing the development proposed to be located at 899 Esquimalt Road [PID 030-151-562, Lot A, Section 11, Esquimalt District, Plan EPP69557] **be forwarded to Council with a recommendation for approval with the following conditions:**

1. That the proposed building height conforms to the current Official Community Plan;
2. That the aesthetics of the east face of the building be enhanced to be as attractive as the north, south and west elevations; and
3. The financial responsibility for the maintenance of the green wall needs to be detailed.

Reason: The overall design of the project generally met the intent of the OCP for that location, that it provided an attractive north elevation along Esquimalt Road, and, by stepping down on the south side, was sensitive to the adjacent residential neighbourhood. **Carried Unanimously**

VIII. NEXT REGULAR MEETING

Wednesday May 9, 2018

IX. ADJOURNMENT

The meeting adjourned at 4:15 p.m.

CERTIFIED CORRECT

CHAIR, DESIGN REVIEW COMMITTEE
THIS 9th DAY OF MAY, 2018

ANJA NURVO,
CORPORATE OFFICER



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: May 9, 2018

STAFF REPORT

DATE: May 4, 2018

TO: Chair and Members of the Design Review Committee

FROM: Karen Hay, Planner
Bill Brown, Director of Development Services

SUBJECT: **Official Community Plan Amendment And Rezoning Application**
1379 Esquimalt Road / 520 Foster Street (St. Peter and St. Paul's Church)
[PID 024-848-905; Lot A (DD EP75276), Suburban Lot 27, Esquimalt District,
Plan 5092]

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] provide Council and the Director of Development Services with comments on the Official Community Plan Amendment and Rezoning Application authorizing a new 5 storey, 24 unit, multiple family residential building with a new 'ministry centre' on the ground floor sited in accordance with the BCLS Site Plan provided by Wey Mayenburg Land Surveying Inc., and incorporating height and massing consistent with the architectural plans provided by Number Ten Architectural Group, stamped "Received April 24, 2018" detailing the development proposal, to be located at 1379 Esquimalt Road / 520 Foster Street (St. Peter and St. Paul's Church) [PID 024-848-905; Lot A (DD EP75276), Suburban Lot 27, Esquimalt District, Plan 5092], and **make a recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application:

CitySpaces Consulting on behalf of the owner of St. Peter and St. Paul's Church (The Anglican Synod of the Diocese of British Columbia), is requesting an amendment to the Official Community Plan Bylaw No. 2646, and a change in zoning to facilitate the subdivision of the subject property into four separate parcels (three land parcels and one air space parcel). The 'Hermitage', an existing multiple unit residential building would occupy one land parcel, a small cemetery would be on a second land parcel, and the existing heritage church with a new 'ministry centre' would occupy the third land parcel. Twenty-four new, affordable rental housing units for seniors would be located above the new ministry centre in an air space parcel. The proposed development would require a Heritage Alteration Permit as the connection of the new ministry centre to the heritage designated church would require an alteration to this

protected building.

Staff request the Design Review Committee review this proposal with regards to the proposed siting, height, mass, density, lot coverage, usable open space and parking and provide comments for staff and the applicant to consider as well as a recommendation to Council.

Context:

Applicant: Deane Strongitharm, CitySpaces Consulting

Owner: The Anglican Synod of the Diocese of British Columbia

Architect: Barry Cosgrave, Number Ten Architectural Group

Property Size: Metric: 17653 m² Imperial: 4.36 acres

Existing Land Use: Church, Parish Hall, Senior's Multiple Family Residence, Cemetery

Surrounding Land Uses:

North: Multiple Family Residential

South: Multiple Family Residential

West: Multiple Family Residential

East: Multiple Family Residential

Existing Zoning: Comprehensive Development District No. 23 [CD-23]

Existing OCP Designation: Institutional

Official Community Plan [OCP]

The OCP's current designation for the property is "Institutional", which does not support an option for standalone residential uses. Therefore, the proposed amendment to the Official Community Plan would consist of an amendment to Schedule 'A' of the Official Community Plan, Bylaw No. 2646, being the Land Use Designation Map, by creating a site specific Land Use Designation for the subject site that would allow: exclusive medium density residential uses, and institutional uses (church, ministry centre and cemetery).

OCP Section 2 - Managed Growth – Land Use and Development states that the objectives and policies in this section are designed to promote sustainable land use and development in the community.

OCP 2.0.1(a) states the Township should encourage high quality development that enhances and benefits the community as a whole.

OCP 2.0.1(e) states the Township should support increased residential density and higher buildings along the Esquimalt Road corridor, particularly in the areas within walking distance of Esquimalt Village and the Vic West border.

OCP 2.0.2(a) states Esquimalt's future new development, infill and redevelopment will be in accordance with the land use designations shown on OCP Schedule A, together with the guidelines set out in Development Permit Areas (OCP Section 9).

OCP Section 2.2 - Residential Land Use of the Official Community Plan recognizes that modest growth is likely to occur through the infilling of vacant or under-utilized parcels, redevelopment of existing residential properties to higher densities (such as townhouses, apartment buildings and mixed commercial-residential uses) and the replacement of existing buildings.

Section 2.2.1(a) states the Township should work toward a more complete community by maintaining a healthy mixture of housing types, accommodating people with a wide range of income levels.

Section 2.2.1(b) states the Township should encourage new residential development with high design standards for building and landscaping and which enhance existing and new neighbourhoods.

OCP Section 2.2.4.1 Multi-Unit Residential Policies are intended to provide more predictability for residents and give direction to design teams preparing development proposals. This proposal for a 24 unit residential building is consistent with many policies contained in this section. It proposed to be an affordable rental building for seniors located near a regular bus route.

OCP Section 2.2.4.4 Multi-Unit, High-Rise Residential states that in areas designated Multi-Unit, High-Rise Residential on Schedule A, building heights of up to 12 storeys are acceptable with a Floor Area Ratio of up to 3.0. Buildings with shallow setbacks must step down to no more than three storeys at street level in order to provide appropriate human scale along the sidewalk. The proposed building at five storeys does not step back at the third storey, has 3.0 metre setbacks on two frontages and is therefore inconsistent with this policy.

OCP Section 2.5.2 (a) Institutional Land Use Policy states that Institutional uses are generally considered to be appropriate within any land use designation and need not be specifically designated on "Schedule A". However, any proposal for new institutional zoning will be reviewed in detail and assessed on its own merits.

OCP Section 3.2 Heritage recognizes that Heritage features, including buildings, trees, First Nations cultural features, natural areas and viewpoints are important community amenities that give the community a "sense of place".

OCP Section 3.2.1 (a) states that the Township encourages the recognition and adaptive re-use of heritage structures, provided that the essential character of the building or site can be maintained.

OCP Section 3.2.1 (d) states that when redeveloping heritage sites or buildings, the owners are encouraged to do so in a manner that is respectful of the character and significance of the site.

OCP Section 3.2.1(e) Encourages the municipality to work with the development community and owners of heritage structures to find innovative solutions that will permit change to occur while minimizing its impacts upon sites' heritage value.

OCP Section 3.3.1(a) Affordable Housing Objectives states that the Township should encourage a range of housing by type, tenure, and price to ensure that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

OCP Section 9 Provides Development Permit Guidelines for Multiple Unit Residential Buildings.

As a Development Permit is not being considered at this time it would be inappropriate to address many of the guidelines, though the following that are relevant to the discussion of zoning and parking issues for multiple family buildings can be useful in the evaluation of this proposal.

Section 9.3.5(b) states, in part, that new buildings should be designed and sited to minimize visual intrusion onto the privacy of surrounding homes and minimize the casting of shadows onto the private outdoor space of adjacent residential units. The proposed building has a height of 15.8 metres (5 storeys). The location of the new building at the north-west corner of the site will mean the majority of shadowing will be over public roadways.

Section 9.3.5(c) states that high density multi-unit residential buildings should be designed so that the upper storeys are stepped back from the building footprint with lower building heights along the street front. The proposed building does not step back.

Section 9.3.5(f) states that underground parking will be provided for any multi-unit residential building exceeding four storeys. This proposal is for affordable housing and as a means to keep construction costs of the building low, only surface parking is proposed to be used primarily for church and ministry centre functions.

Zoning

Density, Lot Coverage, Height and Setbacks: The following chart compares the floor area ratios, lot coverage, setbacks, and height for this proposal with the requirements of the existing zone [Comprehensive Development District No. 23]:

	Proposed Comprehensive Development Zone [Existing plus proposed Building with 24 new Residential Units]		CD-23 [Existing CD Zone]
	Hermitage Site (26 units) (after subdivision)	New Building (24 units) (after subdivision)	
Floor Area Ratio	0.88	0.56	1.0
Lot Coverage	31%	30%	30%
Setbacks • Esquimalt Rd. [North] • Grafton St. [West] • Foster St. [East] • Interior Side [South]	12.0 m (new lot line) 8.9 m 9.3 m 4.30 m	3.0 m 3.0 m 17.2 (to existing church) 9.8 m (new lot line)	17 m (residential) 8.5 m 12.0 m 4.5 m
Building Height	14 m [4 storeys]	15.8 m [5 storeys]	20 m
Off Street Parking	28 spaces [1.07/unit] (17 under & 7 east of building, 4 in the new lot)	23 spaces in new lot, primarily for church use + 4 new public spaces on Foster St. boulevard	[0.5/unit] 45 spaces total [including church use]

The Floor Area Ratio of this proposal complies with the current CD-23 zone, and there is only a slight increase in the lot coverage with the additional building. The CD-23 zone was written to accommodate a building that was never built on the site, which would have been located directly north of the Hermitage residential building and would have replaced the existing church hall. The height of the proposed building does not exceed the height permitted in the CD-23 zone.

The siting of the proposed building creates some fairly shallow setbacks from Esquimalt Road and in the north-west corner adjacent to Grafton Street. This will cause shadowing on the sidewalks in this area but having the building located close to the sidewalk also provides for potential interaction between the activities in the ministry centre building and the public on the sidewalk.

Parking is provided at four locations on the site: under the Hermitage building (17 spaces), at east end of Hermitage building (7 spaces), 27 spaces in surface parking between buildings, and four new spaces are proposed on the municipal boulevard off of Foster Street. The total number of spaces on the site would be 51, plus the 4 new public spaces. The new building is intended to house low income seniors and it is reasonable to assume that these tenants will have little need for parking.

This location is served by two BC Transit routes, #15 and #26 and both run fairly frequently, with service every 10 – 15 minutes at bus stops within 50 metres of the subject property.

The rezoning would add Cemetery as a permitted use on this site. The cemetery has existed for years but without formal recognition. The cemetery would exist on its own lot, in order to comply with provincial government regulations.

Heritage:

The details of the alterations to the heritage designated building are still to be provided. The proposed new ministry centre would attach to the west end of the church and will require raising the stained glass windows on this wall, about two feet above their current location. The Heritage Alteration Permit would be processed at a later date, if the rezoning is successful.

The owners/applicant have sited the proposed building and parking in order to protect the natural heritage features of the site. Parking is restricted in order to maintain the rock outcropping with remnant Garry Oak ecosystem species and also to protect the mature native (Douglas-fir, Big Leaf Maple, Red Cedar and Arbutus) trees that give this location much of its character.

Note: All projects are subject to compliance with the BC Building Code, Esquimalt Subdivision and Servicing Bylaw, Esquimalt Zoning Bylaw and other Regulations and Policies set by Council.

ALTERNATIVES:

1. Forward the application to Council with a recommendation of **approval**.
2. Forward the application to Council with a recommendation of **approval including specific conditions**.
3. Forward the application to Council with a recommendation of **denial**.



1390

1386

St Pauls Pl

1360

Esquimalt Rd

1357

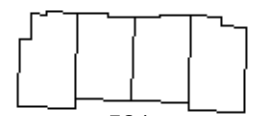
1405

506

Grafton St

1379

Foster St



521

515

1406

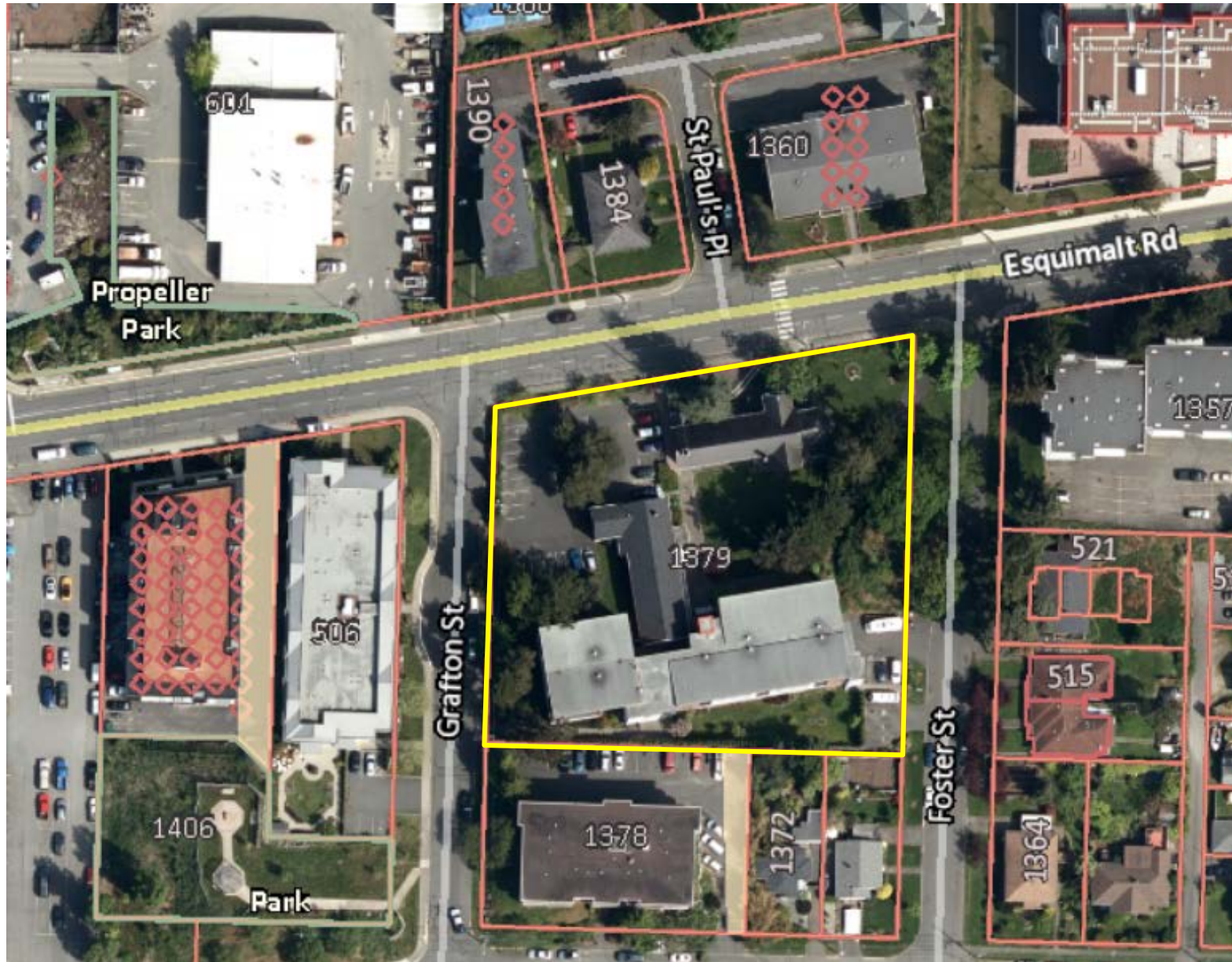
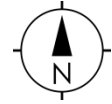
1378

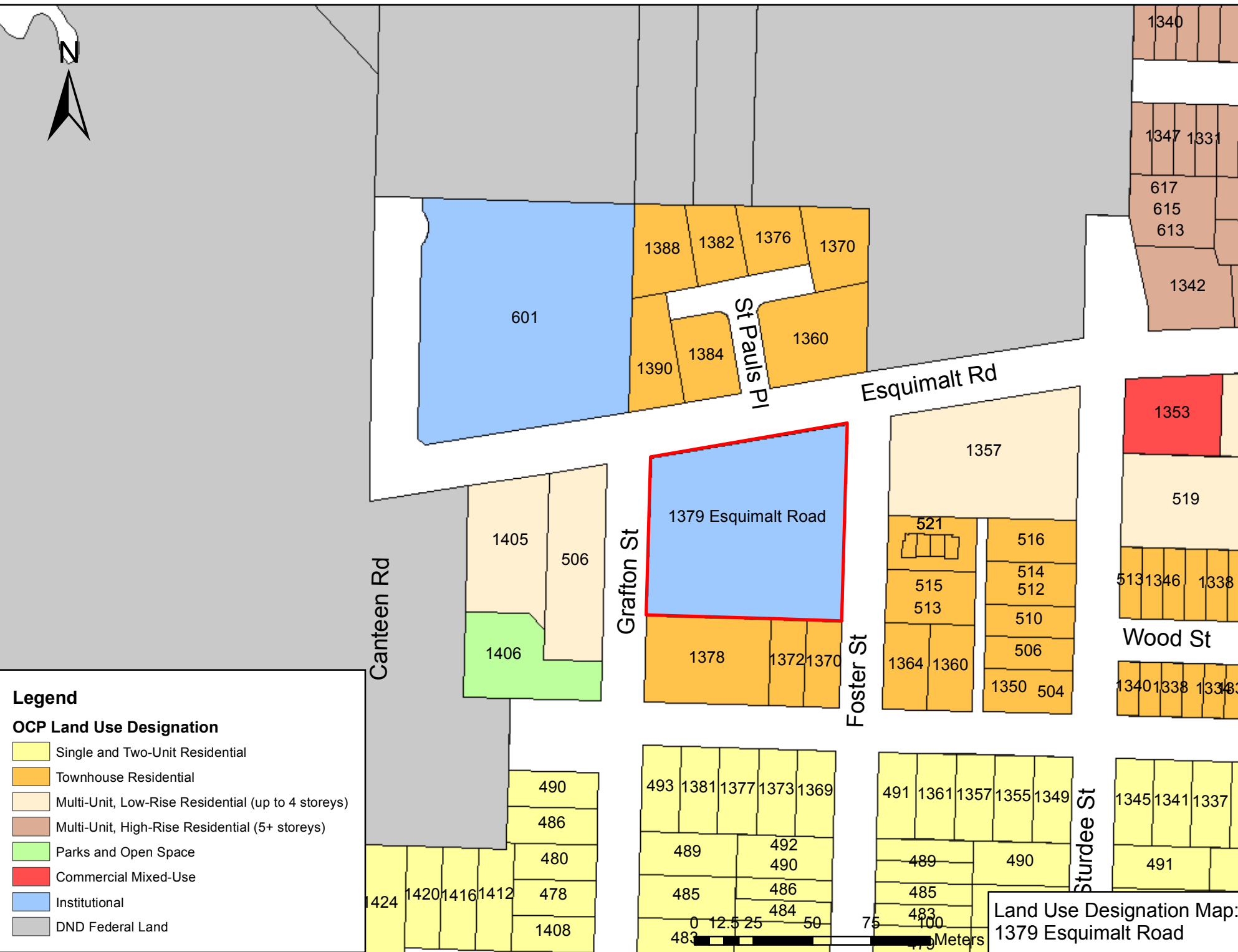
1372

1370

Subject Property Map:
1379 Esquimalt Road

1379 Esquimalt Road - 520 Foster Street - air photo

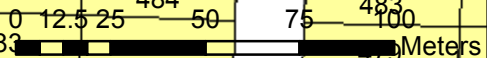




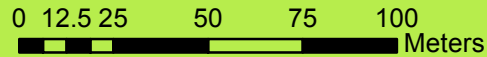
Legend

OCP Land Use Designation

- Single and Two-Unit Residential
- Townhouse Residential
- Multi-Unit, Low-Rise Residential (up to 4 storeys)
- Multi-Unit, High-Rise Residential (5+ storeys)
- Parks and Open Space
- Commercial Mixed-Use
- Institutional
- DND Federal Land



Land Use Designation Map:
1379 Esquimalt Road



Canteen Rd

Grafton St

St Pauls Pl

Foster St

Sturdee St

Esquimalt Rd

Wood St



67.15 COMPREHENSIVE DEVELOPMENT DISTRICT NO. 23 [CD NO. 23]

In that zone designated as CD No. 23 (Comprehensive Development District No. 23), no Building or Structure or part thereof shall be erected, constructed, placed, maintained, or used and no land shall be used except in accordance with and subject to the regulations contained in or incorporated by reference into this bylaw.

(1) Permitted Uses

The following Uses and no others shall be permitted:

- (a) Multiple Family residential
- (b) Church
- (c) Parish hall
- (d) Meeting rooms
- (e) Guest suite
- (f) Home Occupation

(2) Parcel Size

The minimum Parcel size of fee simple Parcels created by subdivision shall be 6,284 square metres.

(3) Density

The number of Dwelling Units permitted on Lots 1, 2, 3, 4 and 5, Suburban Lot 27, Section 339MA, Esquimalt District, Plan 5092 shall be limited to thirty-nine (39) plus one (1) guest suite for a density of one (1) unit per 157 square metres.

(4) Building Height

- (a) No residential Building shall exceed a Height of 20 metres.
- (b) No Institutional Building shall exceed a Height of 20 metres.
- (c) No Accessory Building shall exceed a Height of 3.6 metres.

(5) Lot Coverage

All Principal Buildings, Accessory Buildings and Structures combined shall not cover more than 30% of the combined area of Lots 1, 2, 3, 4 and 5, Suburban Lot 27, Section 339MA, Esquimalt District, Plan 5092.

(6) Floor Area Ratio

The total combined Floor Area Ratio for the development of Lots 1, 2, 3, 4 and 5, Suburban Lot 27, Section 339MA, Esquimalt District, Plan 5092 shall not exceed 1.00.

(7) **Siting Requirements**

(a) **Residential Buildings**

- (i) No residential Building shall be located within 17 metres of Esquimalt Road.
- (ii) No residential Building shall be located within 8.5 metres of Grafton Street.
- (iii) No residential Building shall be located within 4.5 metres of the south property line or within 12 metres of the east property line.
- (iv) Building Separation: No residential Building shall be located within 4.5 metres of any other residential Building.

(b) **Accessory Buildings**

- (i) No Accessory Building shall be located in front of the Buildings fronting onto Grafton Street or Esquimalt Road.
- (ii) No Accessory Building shall be located within 1.5 metres of the east or south property lines.
- (iii) Building Separation: No Accessory Building shall be located within 2.5 metres of a Principal Building.

(8) **Fencing**

Subject to Section 22, no fence shall exceed a Height of 1.2 metres along the Grafton Street or Esquimalt Road frontages. No fence shall exceed a Height of 2 metres along the east and south property lines.

(9) **Off Street Parking**

The number of off street parking stalls shall be provided in the ratio of 0.5 stalls for the residential units and one (1) space per ten (10) seats within the church for a total of forty-five (45) parking stalls.

October 31, 2017

Mayor Barbara Desjardins & Council
Township of Esquimalt
1229 Esquimalt Road
Esquimalt BC V9A 3P1



**RE: ROGERS COURT SENIORS APARTMENTS (1379 ESQUIMALT ROAD)
REZONING APPLICATION**

Dear Mayor Desjardins & Council:

CitySpaces Consulting, on behalf of the Rogers Court Society (agent of the Anglican Diocese of British Columbia), is pleased to submit this application to rezone the Diocese's property at 1379 Esquimalt Road.

THE PROPOSAL

This application proposes rezoning the property from its current CD-23 zone to a new site-specific zone to address the following proposed changes to the site:

- Subdivision of the property into four separate parcels – three land parcels and one air space parcel (Note: one of the land parcels is for the existing small cemetery);
- Accommodation of the existing Church, a new Church Hall, and new affordable rental housing units for seniors.
- Amendment to the permitted residential density to permit the addition of 24 affordable seniors housing units.

The first land parcel would contain the existing designated heritage St. Peter & St. Paul's Church, and the existing Church Hall – to be replaced by a new Church Hall, and surrounding property. The second land parcel would contain the existing Hermitage (a seniors "life lease" apartment building with 26 units). The air parcel would be situated above the proposed new Church Hall and contain four floors of seniors affordable rental apartments (24 units). The fourth parcel would create a separate lot for the existing cemetery located between the Church and Esquimalt Road.

BACKGROUND

Some 17 years ago, the St. Peter & St. Paul's Parish made arrangements with the Diocese to lease the 1379 Esquimalt Road property to the St. Paul's Housing Society for 99 years for a two-phased development of "life lease" seniors housing units.

**CitySpaces
Consulting Ltd.**

Suite 101
848 Courtney St.
Victoria BC
V8W 1C4

250.383.0304 Tel
866.383.0304 Toll-free
250.383.7273 Fax
www.cityspaces.ca

Victoria

Vancouver



C
I
T
Y
S
P
A
C
E
S

The Hermitage was the first phase of this project with 26 "life lease" units. Unfortunately, the project went over budget, and required additional capital investment. A four-storey second phase was planned to include a Ministry Centre on the main floor with three levels of housing above. A Development Permit was issued, but the building was never constructed. This application replaces that original project.

In 2010, the Parish of St. Peter & St. Paul's was created as a "hub" church for the consolidation of six existing parishes. The original Church Hall was supposed to have been demolished 15 years ago, but it still stands today, and is non-conforming. This building is old and tired, and does not provide suitable space for this amalgamated Parish or community activities.

ABOUT ROGER'S COURT SOCIETY

Roger's Court Society (the agent for the Diocese and this project) is a not-for-profit charitable society formed from the amalgamation of the St. Matthias Garden Court Society and the St. Matthias Housing Society in 2008. Since 1972, the purpose of the Society now, and in its previous incarnations, has been to provide the community with affordable rental housing for seniors.

The Diocese has several seniors and charitable housing societies on Vancouver Island. Each is its own separate legal entity with its own constitution and bylaws. Collectively, the Diocese's housing societies provide more than 300 affordable homes on the Island.

PROJECT OVERVIEW

This project proposes to:

- Provide 24 units of much needed affordable seniors rental housing (with the support of BC Housing) within a new five-storey building. These housing units will include 16 one-bedrooms, and eight studios;
- Provide a new Church Hall and community space for the Parish and the neighbourhood on the main floor of the new residential building. The Hall will comprise modern, useable spaces that can accommodate Parish-related activities, as well as a variety of community groups and gatherings;
- Locate the new building closer to Esquimalt Road than the previously approved project in order to be able to use the existing Parish Hall during the construction period, and to ultimately create more animation of the street frontage with a street level plaza area. The old non-conforming building will be removed from the site at the completion of the project;
- Provide the existing Hermitage residential (St. Paul's Housing Society) with a new lease on a newly created land parcel;
- Encourage greater Parish and community use of all the Church's space on the site. The exterior of the Church is designated heritage, and throughout the planning process there has been careful consideration about how to limit impacts on the historic Church structure, while connecting and incorporating it with new project.



- Reorganize and improve the parking layout to create a generous separation between the new building and The Hermitage (when compared to the originally approved second building), while minimizing impacts on the existing trees as much as possible (see attached Tree Management Plan);
- Orient the new building to maximize daylighting into the residential units;
- Implement several green features, including the extensive application of rain gardens. Key evergreen trees will also be preserved, and the large paved area at the northeast portion of the site will be replaced with a new pedestrian-friendly pathway;
- Create new landscape focal prints, including a new gazebo and seating area, brick paver entrance features, and a public realm patio area that tiers down to the sidewalk;
- Install a total of 16 bicycle parking stalls in two separate locations on the site; and
- Extend the sidewalk along Foster Street.

SITE & BUILDING CHARACTERISTICS

CHARACTERISTIC		EXISTING	PROPOSED
1	Legal Description	Parcel Identifier: 024-848-905 Lot A (DD EP75276) Suburban Lot 27, Esquimalt District Plan 5092	See title document for additional Housing Agreement, Heritage Status notice, covenants, leases, sub-leases, and statutory right-of-way information.
2	Address	1379 Esquimalt Road	—
3	Zoning	CD-23	New site-specific zone to accommodate the existing Hermitage on its own titled property, and a new Church Hall and affordable seniors rental units with the existing heritage Church on a separate parcel.
4	Property Size	6,270 m ² (67,492 sf)	Divided into three fee simple lots: 1) The Hermitage 3,407 m ² (36,673 sf) 2) Church, hall, housing 2,743 m ² (29,525 sf) 3) Cemetery 120 m ² (1,292 sf)



CHARACTERISTIC		EXISTING	PROPOSED
5	Buildings & Sizes (total floor space)	1) The Hermitage 2987 m ² (35,152 sf) 2) The Church Building 280 m ² (3,104 sf) 3) Existing Church Hall 292.5 m ² (3,148 sf)	1) The Hermitage 2987 m ² (35,152 sf) 2) The Church Building 280 m ² (3,104 sf) 3) New Church Hall 472.4 m ² (5,085 sf) 4) New Residences 1,558 m ² (16,770 sf)
6	FSR		1) The Hermitage 0.88:1 2) Church, hall, housing 0.67:1
7	Heritage	Designated heritage	Designated heritage

LAND USE POLICY

The proposed project meets many policy objectives of the Township's *Official Community Plan*, including:



2.0.1 General Land Use and Development Objectives

- (a) To encourage high quality development that enhances and benefits the community as a whole.
- (d) To support increased residential density and higher buildings along the Esquimalt Road corridor, particularly in the areas within walking distance of Esquimalt Village and the Vic West border.
- (g) To facilitate moderate densification in accordance with the overall objectives and statements of the Regional Growth Strategy and which will meet the municipality's anticipated housing needs for the life of this Plan.

2.2 Residential Land Use

- (a) To work toward a more "complete community" by maintaining a healthy mixture of housing types, accommodating people with a wide range of income levels.
- (b) To encourage new residential development with high design standards for building and landscaping and which enhance existing and new neighbourhoods.

2.2.4.1 Multi-Unit Residential Policies

- (c) Wherever practical, multi-unit residential housing will be located near a Major Road as shown on "Schedule B". This supports transit service and also helps maintain the integrity of single-unit and two-unit housing neighbourhoods;
- (d) Wherever feasible, major multi-unit residential projects will be located within reasonable distance of one of Esquimalt's commercial areas in order to encourage walking and cycling;
- (f) Wherever desirable and achievable, consideration will be given for special needs and assisted housing, including seniors, disabled persons and families.
- (g) Within the areas designated on "Schedule A" as Townhouse Residential, Multi-Unit, Low-Rise Residential and Multi-Unit, High-Rise Residential, the following criteria will be used to evaluate development proposals requiring an application for rezoning:
 - The massing and height of the project will respond sensitively to the prevailing character of the immediate neighbourhood. This will vary by location;
 - The project will relate to the street. Its exterior finishes, scale, treatment of parking areas, and landscaping, will enhance the appearance of the neighbourhood and contribute positively to the streetscape;
 - The proponent will demonstrate that the neighbourhood has been consulted in a fair and meaningful way, and that residents' concerns have been appropriately responded to in the proposal; and
 - Where new multi-unit residential projects are proposed, they should not "land-lock", otherwise isolate, or negatively affect the development potential of adjacent parcels.
- (h) Development proposals with heights and /or densities greater than those set out in policies 2.2.4.2 to 2.2.4.4 may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor-space where new affordable, accessible or special needs housing units or amenities are provided for the benefit of the community.



3.1 Healthy Community General Objectives

To work towards achieving a healthy community that provides:

- Quality housing that meets the accessibility and affordability needs of the community;
- Age diversity and the ability of seniors to age in place;

3.2.1 Heritage Policies

- (a) The Township encourages the recognition and adaptive re-use of heritage structures, provided that the essential character of the building or site can be maintained.
- (c) The preservation and adaptive reuse of historic sites, including military heritage sites, buildings, features, trees, natural areas and viewpoints, is considered to be an amenity to the community and, as a result, the municipality may consider the use of density bonus zoning, bylaw variances and tax credits to encourage the retention of such amenities.

- (d) When redeveloping heritage sites or buildings, the owners are encouraged to do so in a manner that is respectful of the character and significance of the site.

3.3 Housing

3.3.1 Affordable Housing Objectives

- (a) Encourage a range of housing by type, tenure and price to ensure that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.
- (b) Involve the real estate community, non-market housing providers and other community interests in helping to develop and maintain well-managed, attractive, affordable housing in Esquimalt.

3.3.2 Affordable Housing Policies

- (a) Affordable housing will be provided by the private market and the non-profit housing sector. Partnerships between private, public or non-market housing providers will be encouraged. These might include innovative approaches such as limited equity, rent-to-own, and mixed market and non-market projects.
- (b) Special needs housing and non-market housing are integral components of residential areas and are encouraged. This housing should be dispersed throughout Esquimalt, rather than focused within a single area.
- (e) In order to enable citizens to “age in place”, the Township supports the provision of a range of seniors’ housing and innovative care options in mature and new residential areas.

3.7.6 Places of Worship Policies

- (a) Esquimalt recognizes the valuable role places of worship play in the community and continues to support the development of facilities in accordance with local community needs.



BUILDING DESIGN APPROACH

St. Peter & St. Paul’s Church is a heritage building that is an important part of the social and visual identification of the neighbourhood. Surrounded by green space, the Church’s steeple and stained glass window on the west façade are character defining elements that are visible from various locations, especially when travelling east/west along Esquimalt Road.

Early in the design phase, the Parish and its leaders emphasized the importance of the new Church Hall envisioned for the project. The Hall is to provide new opportunities to embrace the community at large, as well as flexibility in how Church spaces can be used. This direction required a physical link to the existing Church, and the opportunity to open out to the community along the Esquimalt Road frontage.

The design challenges for this project included a physical link at the west end of the existing Church while still allowing visibility of the building, its steeple, and the stained glass window to passersby along Esquimalt Road. The Design Team investigated four and five-storey options for

the new building. It was determined that the four-storey option resulted in a larger building footprint that obscured the Church's west façade. The five-storey building, with its smaller footprint, resulted in keeping the critical views of the steeple and stained glass window along Esquimalt Road.

The building's design incorporates a low one-storey link to the Church, and visibly demarcates the Church Hall space along the base with a white façade. The upper floors residential floors are stepped to lessen the visual bulk of the building. The fifth floor is also rendered in a white façade material to reduce the visual effect of the building's height. The exterior design uses geometric patterns found in the Church for window breakup, guard rail design, and façade ornamentation. The building is designed to be a sympathetic new neighbour to the heritage Church.

INCORPORATION & PRESERVATION OF HERITAGE DESIGNATED CHURCH

Designed and built in 1866 by Architect Thomas Trounce, the Church was originally called St. Paul's and located at the foot of Signal Hill. Later known as St. Paul's Naval and Garrison Church, the building was dismantled and moved up the hill to its present location in 1904. The Township designated the building as heritage in 1999, and added it to the Heritage Register in 2011.

John Dam & Associates has been actively involved with the development documents for this project, providing input on the siting and orientation of the new building, including its impact on and connection to the historic Church (see attached John Dam & Association letter).

Noting the west elevation with the stained glass windows as an important character defining element, the new building has been pulled back from obscuring this wall by creating a low-rise, 'opaque' corridor between the two buildings. The corridor will feature curtain wall glazing, subtly incorporating a strong Gothic tenet of the arched window within its framing, while keeping the two buildings visually separate. This corridor will allow for comfortable access into and between the two buildings for both the residents of the new building and the general public. This project provides the opportunity to continue the story of this historic building by revitalizing its function as a place of worship, and enhancing its contemporary potential with the development of Rogers Court.



SUBDIVISION

This application involves a relatively complicated subdivision strategy, which will also require reciprocal access easements for the parking. The intention is to place the existing Hermitage residential building on a separate titled lot for the benefit of its life lease residents. Effectively, the subdivision will split the current lot in half, and an access easement will be required because about half of the parking for the residents of the new building and the Church will be located on the new parcel occupied by The Hermitage.

An air space parcel will be created for the new affordable seniors rental residences located above the first floor of the proposed new building. The land and the first floor of the new

structure will be owned by the Diocese. The residential entrance and the floors of the air space parcel above will be controlled and managed by the Rogers Court Society. The airspace parcel will then be split into 24 strata units for BC Housing financing purposes.

The final subdivision is for the small cemetery that fronts on to Esquimalt Road. This subdivision is required because of the Provincial government's legislative restrictions on "places of internment", and the consequent need to separate the cemetery from the new development. It is noted that this subdivision may raise the need for variances that will need to be discussed with Township staff.

Until the project is completed with all of the works are in place, and the subdivisions described above are registered, certain interim easements and covenants may be required to allow for temporary encroachments

PUBLIC ENGAGEMENT

Two main public engagement sessions for this project were held on June 27, 2017. The first session involved meeting with the residents of The Hermitage to review plans to date, and to get their feedback and preferences for two proposed options - a) four-storeys or b) five-storeys. Later that day, an open house was held for residents of the neighbourhood in general. Attendees were also asked for their feedback and preferences. Transcriptions of the sign-in sheets and comment forms from both events are included with this application. While there were a variety of views, the predominant preference was the five-storey, smaller footprint building, which also allows for improved floorspace layouts of the residential units.

An update meeting was also held with the parishioners of St. Peter & St. Paul's Church on Sunday, August 13, 2017.



VEHICLE & BICYCLE PARKING ANALYSIS

A vehicle parking study was recently commissioned from Watt Consulting, and confirms that the proposed 36 parking stalls are appropriate for this project. Expected parking demand was generated based on vehicle ownership information, observations, surveys, and research. Results suggest resident parking demand will be 14 vehicles, visitor parking demand will be two vehicles (typically casual), and typical Church demand will be 10 vehicles for a total typical parking demand of 26 stalls. Visitor and Church parking will be accommodated on-site during a typical day, with no impacts to on-street parking. During a Sunday Church Service (8:30 to 11:30 a.m.), 39 vehicles are anticipated, and approximately 21 vehicles are expected to seek nearby on-street parking. These vehicles can be easily accommodated while retaining sufficient on-street parking for area residents.

Four parking stalls, located in the existing Church parking area, are allocated to the residents of The Hermitage. Those stalls have been relocated to The Hermitage's driveway area off Foster Street to maintain the parking standard for the premises.

Located near the Esquimalt Road entrance and the parking lot entrance to the proposed new building, a total of sixteen bicycle parking stalls will be provided.

PROPOSED PROJECT TIMELINE

- Fall/Winter 2017/18 Rezoning
- Spring/Summer 2018 Working Drawings
- Fall 2018 Construction Starts
- Fall 2019 Completion & Occupancy

CLOSING

This application represents a unique opportunity to maintain a special historical building, while creating a new and improved community space for the Parish and the neighbourhood, and responding to the increasing demand for affordable seniors rental apartments in Esquimalt. We look forward to presenting this proposal to Council and committees, and demonstrating its many positive features.

Should you require any further information, please do not hesitate to contact the undersigned (250-383-0304 x 122, dstrongitharm@cityspaces.ca).

Sincerely,



Deane Strongitharm, MCIP, RPP



Attachs.

cc: Stephen Martin, Executive Officer, The Anglican Diocese of British Columbia
Peter Daniel, Asset Manager, The Anglican Diocese of British Columbia



October 31st, 2017



Township of Esquimalt
1229 Esquimalt Road
Esquimalt, British Columbia
V9A 3P1

Re: Roger’s Court Development & the Integration of St. Paul’s Historic Church

Dear Mayor and Council:

Originally constructed in 1866, St. Paul’s Church was built on the shores of Esquimalt Harbour with the intention of providing worship space for the village of Esquimalt and the military personnel stationed at the nearby base. After the addition of the transepts in 1879 the church was relocated in 1904 to its current location on Esquimalt Road and extended by an additional bay. In 1928, the east apse and vestry were added. Today, there is the opportunity to continue the story of this historic building by revitalizing its function as a place of worship and enhancing its contemporary potential with the development of Roger’s Court.

John Dam & Associates has been actively involved with the development documents, providing input on the siting and orientation of the new tower including its impact on and connection to the historic church.

Noting the west elevation with the stained glass windows as an important character defining element, the new building has been pulled back from obscuring this wall by creating a low-rise, ‘opaque’ corridor between the two buildings. The corridor will feature curtain wall glazing, subtly incorporating a strong Gothic tenet of the arched window within its framing while keeping the two buildings visually separate. A key importance of this corridor, despite effectively separating the two buildings, will allow for comfortable access into and between the two buildings for both the residents of the new building and the general public. This is intended to revitalize the church for both traditional and contemporary use – an important aspect for the continued existence this historic building.

The new development will both renew the purpose and use of the historic church space while remaining mindfully separate from it and minimally impacting the view lines of this historic building.

Sincerely,


John Dam, Principal
Building Conservation Engineer
B.A.Sc., M.Sc., P.Eng., CAHP, LEED AP





1379 Esquimalt Road Development

Parking Study



Prepared for: **number TEN architectural group**

Prepared by: **Watt Consulting Group**

Our File: **2244**

Date: **October 24, 2017**

TABLE OF CONTENTS

1.0	INTRODUCTION	1
1.1	Subject Site.....	1
1.2	Site Characteristics	2
2.0	PROPOSED DEVELOPMENT	3
2.1	Proposed Parking Supply.....	3
3.0	PARKING REQUIREMENT	3
4.0	EXPECTED PARKING DEMAND	4
4.1	Senior's Housing.....	4
4.2	Church/Ministry Centre	5
4.3	Visitor Parking Demand	7
4.4	Summary of Expected Parking Demand	8
5.0	ON-STREET PARKING ASSESSMENT.....	8
6.0	ON-SITE PARKING MANAGEMENT	10
7.0	SUMMARY	10
7.1	Recommendations.....	11

1.0 INTRODUCTION

Watt Consulting Group was retained by number TEN architectural group to prepare a parking study for the proposed rezoning of the 1379 Esquimalt Road property in the Township of Esquimalt. The purpose of this study is to determine if the proposed parking supply will accommodate parking demand by considering demand at representative sites and in consideration of parking management approaches.

1.1 SUBJECT SITE

The proposed development site is located at 1379 Esquimalt Road in the Township of Esquimalt. The site is currently zoned CD-23 Comprehensive Development District No. 23. See [Figure 1](#).

FIGURE 1. SUBJECT SITE



1.2 SITE CHARACTERISTICS

The following provides details regarding transportation options and services that are located in close proximity to the site.



Transit

The closest bus stops to the site are 85m west of the site (eastbound service), and adjacent the site on the north side of Esquimalt Road (westbound service). Routes that serve the bus stops are Route 15 | Uvic/Esquimalt, Route 25 | Maplewood/Admirals Walk/Colwood, and Route 26 | Dockyard/Uvic. These routes provide service and/or connection to the majority of destinations in Greater Victoria including UVic, Camosun, Downtown Victoria, and the Western Communities. Route 15 is a Regional Route with a service frequency of 15 to 60 minutes with limited stops; Routes 25 and 26 are local routes with service frequency of 20 to 120 minutes.

BC Transit's Transit Future Plan has identified Esquimalt Road as a "Frequent Transit Corridor"¹ that will provide frequent service (15 minutes or better between 7am and 10pm, 7 days per week) with improved transit travel times achieved by fewer stops and transit priority measures and enhanced bus stop infrastructure. With the Frequent Transit Network projected to carry a large share of the future transit system's total ridership, the subject site will benefit from frequent, reliable and convenient transit service.



Walking

There are adequate sidewalks on both sides of the road on Esquimalt Road. There are also crosswalks at major intersections, and a mid-block crosswalk on Esquimalt Road directly adjacent the site connecting to the north side of Esquimalt Road. The site has a Walkscore² of 60 which suggests the site is somewhat walkable and some errands can be accomplished on foot.



Services

At the intersection of Admirals Road and Esquimalt Road (less than 500m from the site) there is a grocery store, liquor store, and a few small-scale restaurants and retail shops. Esquimalt Village is less than 1km from the site and has a library, medical services, postal services, and various commercial uses that will accommodate the daily needs of site residents.

¹ More information on the Victoria Transit Future Plan is available online at: <http://bctransit.com/victoria/transit-future/victoria-transit-future-plan>

² Walkscore. <https://www.walkscore.com/score/1379-esquimalt-rd-victoria-bc-canada>

2.0 PROPOSED DEVELOPMENT

The proposed development is for 24 units of non-profit senior's housing with a Ministry Centre on the main floor of the building. The existing St. Paul's & St. Peter's Church will be retained and the existing Community Hall will be demolished. See **Table 1**.

The Ministry Centre will consist of meeting spaces, limited offices, a kitchen, and a multi-purpose space with capacity for 48 people, seated. As the Ministry Centre is located on the ground floor, and is connected with the Church, they will be utilized in conjunction with each other. The proposed Ministry Centre is expected to function similarly to the existing Community Hall and exhibit similar parking demand characteristics.

TABLE 1. SUMMARY OF PROPOSED DEVELOPMENT

Land Use	Quantity		
	Units	Sq.ft.	m ²
Senior's Housing (new)	24		
Ministry Centre (new)		4,857	451
Church (existing)	110		

2.1 PROPOSED PARKING SUPPLY

The proposed parking supply for the site is 36 parking spaces; a combined 28 spaces for the Senior's Housing and Ministry Centre, four spaces for the Church, and four spaces for the Hermitage building.

The Hermitage (a condominium building located at 520 Foster Street) has an agreement that 4 of the site's existing 21 parking spaces are for their use. The proposal includes adding 4 parking spaces off of Foster Street to fulfill the Hermitage's agreement.

3.0 PARKING REQUIREMENT

The Township of Esquimalt Parking Bylaw No. 2011³ determines the minimum parking supply requirement for all land uses. The site is subject to a total parking requirement of 53 parking spaces. See **Table 2**.

TABLE 2. SUMMARY OF PROPOSED DEVELOPMENT

Land Use	Quantity	Parking Requirement		
		Use	Rate	Req't
Senior's Housing (new)	24	Senior Citizens Apartments	0.5 / unit	12
Ministry Centre	426m ²	Entertainment	1/5 seats with a minimum of 1 / 14m ² GFA	30

https://www.esquimalt.ca/sites/default/files/docs/municipal-hall/bylaws/parking_bylaw_2011_july.pdf

Land Use	Quantity	Parking Requirement		
		Use	Rate	Req't
Church	110	Churches	1 / 10 seats	11
Total Parking Requirement				53

4.0 EXPECTED PARKING DEMAND

Expected parking demand is estimated in the following sections to determine if the proposed parking supply will accommodate site parking needs. Expected parking demand is based on vehicle ownership information from ICBC, observations, surveys and research.

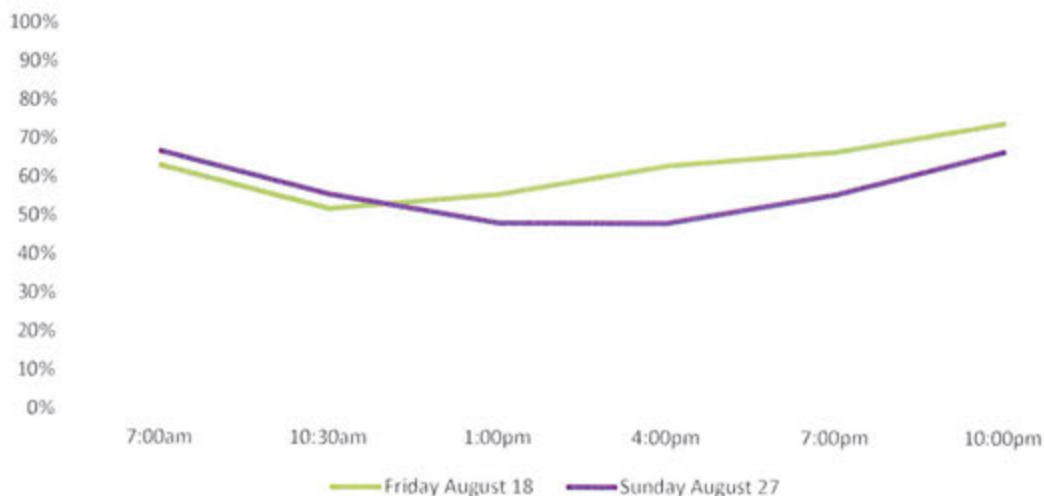
4.1 SENIOR'S HOUSING

There are two existing multi-family buildings on the same block as the proposed site. Parking demand information was obtained for both buildings as follows:

Hermitage, 520 Foster Street. The Hermitage building has 26 residential units, subject to strata title ownership. Based on conversations with site manager⁴, it is understood that there are 31 vehicles associated with the building; a parking demand rate of 1.19 vehicles per unit.

Vista Del Mar Apartments, 1378 Lyall Street. This building has 26 residential units that are all adult-oriented, accessible, apartment rentals. Parking observations were conducted on site, and concluded a peak parking occupancy of 74% (20 vehicles), which occurred on Friday at 10:00pm. See **Figure 2**. This results in a peak parking demand of 0.69 vehicles per unit.

FIGURE 2. PARKING OCCUPANCY AT NEARBY RENTAL APARTMENT



⁴ Email correspondence occurred on August 29, 2017 via email.

There is significant research to suggest that parking demand varies depending on tenure type (i.e., condominium vs. rental apartment)⁵. This is evident in the parking demand rates generated for these two buildings that found the condominium site has a 72% higher parking demand than the rental apartment. As the proposed seniors units on site will be rental apartment, the parking demand rate for Vista Del Mar (1378 Lyall Street) – 0.69 vehicles per unit - is considered more representative than the Hermitage (520 Foster Street).

Parking demand at representative seniors housing sites has been obtained at sites throughout the CRD using vehicle ownership information provided by the Insurance Corporation of British Columbia ("ICBC") and through on-site observations. Results conclude an average vehicle ownership rate of 0.48 vehicles per unit, and range from 0.41 vehicles per unit to 0.56 vehicles per unit. See **Table 3**.

TABLE 3. VEHICLE OWNERSHIP AT REPRESENTATIVE SITES

Site	Tenure	Parking Demand		Other Notes
		Rate (vehicles/unit)	Source	
1550 Arrow Road Mount Douglas	Apartment	0.49	Observations ICBC	Seniors 55+ Independent
		0.41		
3221 Cedar Hill Road Marguerite Court	Apartment	0.48	ICBC	Adults 55+ Subsidized based on Income
1780 Townley Street Townley Lodge	Apartment	0.41	ICBC	Seniors Low-Income
2840 Gillie Place Castanea Place	Apartment	0.55	ICBC	Seniors
3812 Carey Road Carey Place	Apartment	0.56	ICBC	Seniors Affordable
Average Parking Demand		0.48		

The expected parking demand rate for the Senior's Housing is 0.6 vehicles per unit. This takes into consideration the parking demand rate at the adjacent 1378 Lyall Street (Vista Del Mar) rental apartment site (0.69 vehicles per unit) and the average parking demand from the five Seniors sites elsewhere in the Capital Region (0.48 vehicles per unit).

4.2 CHURCH / MINISTRY CENTRE

The existing Church and Community Hall function simultaneously with each other. The Community Hall may hold pre- and post-Sunday Church service events, and events throughout

⁵ Based on findings from two research studies:

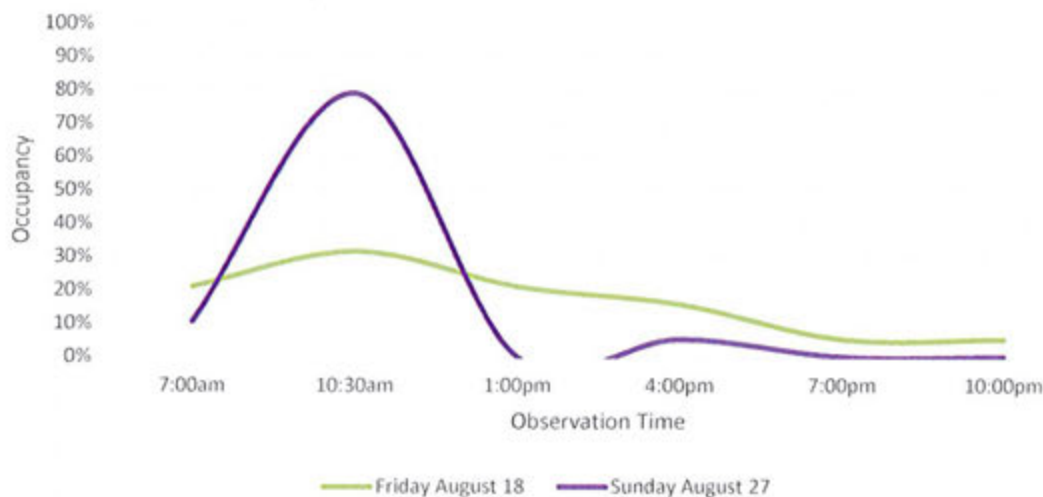
- City of Toronto, Parking Standards Review, February 2007, p16, Figure 3.1. Available at: www1.toronto.ca/city_of_toronto/city_planning/zoning_environment/files/pdf/cansult_final_apart_stds.pdf
- Metro Vancouver, The Metro Vancouver Apartment Parking Study – Technical Report, 2010, p44, Table 21. Available at: www.metrovancouver.org/planning/development/strategy/RGSDocs/Apartment_Parking_Study_TechnicalReport.pdf

the week. Sunday Church Service occurs every Sunday at 8:30am and 10:30am. Typical attendance at the 8:30am service is 15-20 people and the 10:30am service attracts approximately 60-65 people. The size of the congregation is not expected to change in size as a result of the redevelopment. The majority of congregation members live in Esquimalt, View Royal and a portion of Saanich that borders Gorge Road.

In future, the Church and the proposed Ministry Centre are expected to function similarly to the existing Community Hall and Church. Existing parking demand is therefore considered a good forecast of future parking demand. Observations were conducted at the site during a weekday and a Sunday to understand parking demand characteristics throughout the day. See **Figure 3**. The Church parking lot has a supply of 19 unreserved parking spaces. Observations were conducted during the following periods:

- Friday August 18, 2017 at 7:00am, 10:30am, 1:00pm, 4:00pm, 7:00pm and 10:00pm
- Sunday August 27, 2017 at 7:00am, 10:30am, 1:00pm, 4:00pm, 7:00pm, and 10:00pm

FIGURE 3. SUMMARY OF CHURCH OFF-STREET PARKING UTILIZATION



Peak parking demand was observed on Sunday at 10:30am during Church service when 15 vehicles were observed on-site. An additional five vehicles were observed on the south side of Esquimalt Road directly adjacent the site (during all other observations there were no vehicles parked here), and are assumed to be vehicles related to the Church service. There were four vehicles observed on Grafton Street in the 2 hour parking area adjacent the site that are also assumed to be attributed to the Church function. This suggests there are a total of 24 vehicles attributed to the Church. There were 37 individuals in attendance at the Church on the day of the observations⁶, which results in a parking demand rate of 0.65 vehicles per person. Observations were conducted during the summer when attendance is lower than a typical service during the year. Attendance for a typical service during the rest of the year is

⁶ Attendance figure provided by Church representative

approximately 60 people. Using the observed parking demand rate, it is assumed that Church service throughout much of the year results in approximately 39 vehicles.

Peak parking demand for a church occurs for a limited time during the week (every Sunday during church service), and it is inefficient to supply parking that will accommodate parking demand during this time, as it will significantly oversupply parking at all other times. On-street parking observations were undertaken (see Section 5.0) that concluded there is sufficient on-street parking available surrounding the site to accommodate Sunday church parking demand without preventing area residents from accessing on-street parking.

Off-peak Church parking demand was assessed to determine church parking demand when service is not offered. Site observations determined an average of two vehicles associated with the church use, with a peak demand of six vehicles. Based on the assumed increase in congregation during a typical period in the year (non-summer), this suggests that average parking demand will be increased to 3 vehicles and peak parking demand is increased to 10 vehicles.

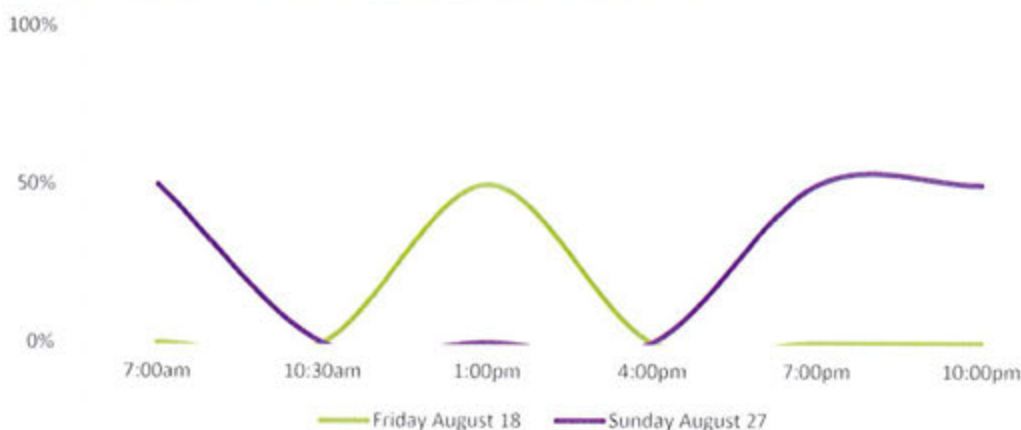
4.3 VISITOR PARKING DEMAND

Visitor parking observations were conducted at the designated visitor parking for the Hermitage building at 520 Foster Street. There are two designated visitor parking spaces – a visitor parking supply rate of 0.08 spaces per unit. Observations were conducted during the following periods:

- Friday August 18, 2017 at 7:00am, 10:00am, 1:00pm, 4:00pm, 7:00pm, 10:00pm
- Sunday August 26, 2017 at 7:00am, 10:00am, 1:00pm, 4:00pm, 7:00pm, 10:00pm

These visitor parking spaces were most commonly observed empty – the peak visitor parking demand occurred on various periods during observations. See **Figure 4**. Peak visitor parking demand was 1 vehicle; a parking demand rate of 0.04 vehicles per unit.

FIGURE 4. SUMMARY OF VISITOR PARKING OCCUPANCY



Observations were conducted as part of a study by Metro Vancouver⁷ that concluded typical visitor parking demand is less than 0.1 vehicles per unit. This is similar to observations that were conducted for parking studies in the City of Langford and the City of Victoria, and suggests that visitor parking demand is not strongly influenced by location.

Observations were conducted in the summer and it is expected that visitor parking demand will be higher during other times of the year. It is therefore suggested that a visitor parking demand will be no more than 0.1 vehicles per unit.

4.4 SUMMARY OF EXPECTED PARKING DEMAND

Based on analysis of each proposed land use, total expected parking demand for the site is 26-55 vehicles (26 vehicles represents a typical day at the site, and 55 vehicles represents when a Sunday Church Service is in session). See **Table 4**. Parking for the existing Community Hall and Church were assessed together – the future Ministry Centre and Church will share parking, similar to their current function. This suggests parking demand will be accommodated during a typical non-church service period. When a Church service is in session it is estimated that approximately 23 vehicles will seek parking on-street nearby the site.

TABLE 4. SUMMARY OF EXPECTED PARKING DEMAND

Land Use	Quantity	Expected Parking Demand Rate	Expected Parking Demand
Senior's Housing (new)	Resident	0.6 vehicles / unit	14
	Visitor	0.1 vehicles / unit	2
Ministry Centre/Church		10-39 vehicles	10-39
Total Expected Parking Demand			26-55

5.0 ON-STREET PARKING ASSESSMENT

On-street parking conditions were assessed on streets surrounding the site bounded by Esquimalt Road to the north, Lyall Street to the south, Canteen Road to the east, and Sturdee Road to the west. Observations were conducted during the following periods:

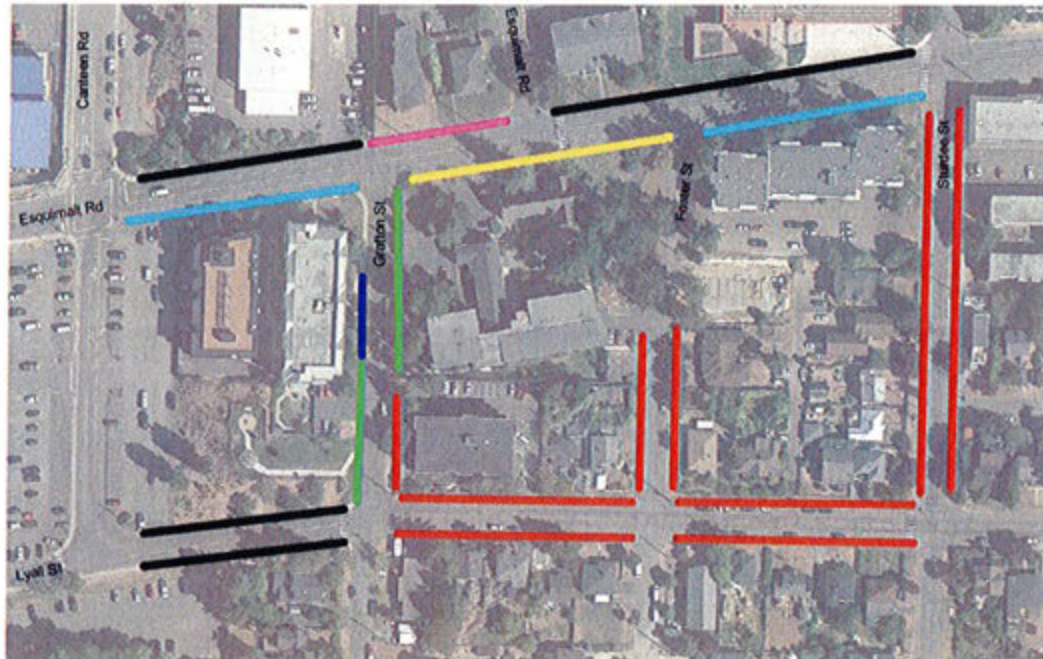
- Friday August 18, 2017 at 7:00am, 10:00am, 1:00pm, 4:00pm, 7:00pm, 10:00pm
- Sunday August 26, 2017 at 7:00am, 10:00am, 1:00pm, 4:00pm, 7:00pm, 10:00pm

These periods were selected to determine on-street parking occupancy during a typical weekday, and compare it to a Sunday when church is in session.

⁷ Metro Vancouver Apartment Parking Study, Technical Report, 2012. Available online at: http://www.metrovancouver.org/services/regional-planning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf

On-street parking restrictions adjacent the site limits the ability for site residents and visitors to park on-street. Parking directly adjacent the site on Grafton Street are restricted to 2 hours, 7am – 6pm, Monday – Friday. Parking on Esquimalt Road is restricted to 2 hours and no stopping from 3pm to 5pm, Monday to Friday. This is due to heavy traffic leaving the Navy Base in the afternoon. This restriction is reversed in the morning, with no stopping from 7am to 9am on the north side. Parking on Foster Street is restricted to Residential Parking Only. See **Figure 5**.

FIGURE 5. SUMMARY OF ON-STREET PARKING RESTRICTIONS



- Legend
- No Stopping, 3pm-5pm, Monday-Friday
 - 2 Hour, No Stopping, 3pm-5pm, Monday-Friday
 - No Stopping, 7am-9am, Monday-Friday
 - Residential Parking Only
 - 2 Hour, 7am-6pm, Monday-Friday
 - Passenger Zone
 - No Parking

There are a total of 18 parking spaces that are restricted to 2 hour, 7am-6pm, Monday to Friday on Grafton Street and 36 parking spaces on Esquimalt Road that restrict No Stopping from 3pm-5pm, Monday to Friday, eastbound, and 7am-9am, Monday to Friday, westbound. Total parking available within one block of the site is 54 parking spaces. During the peak period (Sunday August 27 at 10:30pm), total occupancy of these spaces was 20% with 43 spaces still available. On-street parking that is located on the site frontage has a total parking supply of 25 parking spaces; suggesting spill-over from the site can be accommodated.

6.0 ON-SITE PARKING MANAGEMENT

Site parking demand will vary depending on whether there is an event at the Church. Church events vary in size and frequency throughout the year, however Sunday service is considered peak parking demand. See **Table 5**. These results suggest that total parking demand during a typical day at the site is 26 vehicles and during Church service is 55 vehicles. Parking demand during a typical day will be accommodated on-site, however approximately 23 vehicles are expected to seek parking in nearby on-street parking during Sunday morning Church service.

There is sufficient on-street parking available adjacent the site to accommodate this additional demand. Of the on-street parking that is available to site visitors, this additional demand will increase occupancy to 43%. Given the limited timeframe of Church Service and low frequencies, this additional on-street parking occupancy is seen as acceptable.

TABLE 5. SUMMARY OF EXPECTED PARKING DEMAND

	Parking Demand	
	Typical	Church Service
Senior's Housing(Resident)	14	14
Senior's Housing (Visitor)	2	2
Church	10	39
Total Expected Parking Demand	26	55

Regardless of an event occurring at the Church, all resident parking should be reserved at all times of the day. Typically, resident parking demand is significantly lower during the day than in the evening, however, since residents are seniors and more likely to be retired, it is still expected to have a substantial amount of parking demand present during the day.

All other parking should be unassigned and available to residential visitors, church patrons, and church employees.

7.0 SUMMARY

The proposed development is for 24 senior's housing units and a Ministry Centre that will be connected to the existing church. The proposed parking supply for the site is 36 parking spaces.

Expected parking demand was generated based on vehicle ownership information, observations, surveys and research. Results suggest resident parking demand will be 14 vehicles, residential visitor parking demand will be 2 vehicles, typical church parking demand will be 10 vehicles, and 39 vehicles during a Sunday Service.

14 parking spaces should be reserved for residents. Residential visitor and church parking will be accommodated on-site during a typical day with no impact on on-street parking. Approximately 23 vehicles are expected to seek park in nearby on-street parking during Sunday Church service (approximately 8:30am to 11:30am), which can be accommodated in nearby on-street parking areas while retaining sufficient on-street parking for area residents.

7.1 RECOMMENDATIONS

1. Proposed parking supply is supported for the site
2. 14 spaces should be assigned to residents



GREEN BUILDING CHECKLIST

The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of **Green Technologies** in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project .

As you review this checklist, if you have any questions please contact **Development Services** at 250.414.7108 for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt

Adopted on January 10th, 2011



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level? <u>BC Energy Step Code – Step 2</u>	Yes ✓	No
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features?	Yes ✓	No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them. <u>Building envelope will be designed with a rainscreen & interior materials will be durable, including vinyl flooring in the rental suites & marmoleum flooring in the Church Hall.</u>	Yes ✓	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building? <u>100</u> %		
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project. <u>The second to fifth floors will be wood frame construction using lumber products from BC.</u>		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings?	Yes ✓	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? <u>No</u> For which parts of the building (e.g. framing, roof, sheathing etc.)? <u>NA</u>		
8	Can alternatives to Chlorofluorocarbon’s and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents] be used in this project? If so, please describe these. <u>See attached.</u>	Yes ✓	No
9	List any products you are proposing that are produced using lower energy levels in manufacturing. <u>Concrete foundations, main floor structure, second floor concrete slab & upper floor concrete toppings. Cellulose insulation in exterior walls. Wood frame construction from second to fifth floors.</u>		
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]?	Yes ✓	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde?	Yes	No ✓

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

12	Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs?	Yes ✓	No
13	For commercial buildings, do flushes for urinals exceed BC Building Code requirements?	Yes	No ✓
14	Does your project use dual flush toilets and do these exceed the BC Building Code requirements?	Yes	No ✓
15	Does your project exceed the BC Building Code requirements for maximum flow rates for private showers?	Yes	No ✓
16	Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets?	Yes	No ✓

Storm Water

17	If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.]	Yes	No	N/A ✓
18	Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property?	Yes ✓	No	N/A
19	Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. <u>The intent is to create a rain garden along the front of Esquimalt Road & a second rain garden along the western property line along Grafton Street.</u>	Yes ✓	No	N/A
20	Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses?	Yes	No ✓	N/A
21	Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. <u>New surface parking lot will drain toward new building & a strip of permeable pavers to allow infiltration of runoff prior to conveyance to rain garden along Grafton St.</u>	Yes ✓	No	N/A
22	Will this project have an engineered green roof system or has the structure been designed for a future green roof installation?	Yes	No ✓	N/A
23	What percentage of the site will be maintained as naturally permeable surfaces?	39.8 %		

Waste water

24	For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these. _____	Yes	No	N/A ✓
----	--	-----	----	----------

Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

25	Are any healthy trees being removed? If so, how many and what species? <u>Three Scotch Pine are being removed from the existing Church parking lot. (See Tree Management Plan)</u>	Yes ✓	No	N/A
	Could your site design be altered to save these trees?		✓	
	Have you consulted with our Parks Department regarding their removal?		✓	

26	Will this project add new trees to the site and increase our urban forest? If so, how many and what species? <u>Five Crimson King Maples, six Sweet Gum & three Austrian Pines</u>	Yes ✓	No	N/A
27	Are trees [existing or new] being used to provide shade in summer or to buffer winds?	Yes ✓	No	N/A
28	Will any existing native vegetation on this site be protected? If so, please describe where and how. <u>See attached.</u>	Yes ✓	No	N/A
29	Will new landscaped areas incorporate any plant species native to southern Vancouver Island? <u>Native plant species will be used in the rain gardens for stormwater treatment.</u>	Yes ✓	No	N/A
30	Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas? <u>Lily of the Valley, California Lilac, Pink Azalea, Shirobana Spirea & Japanese False Holly</u>	Yes ✓	No	N/A
31	Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)?	Yes ✓	No	N/A
32	Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property?	Yes ✓	No	N/A
33	Will topsoil will be protected and reused on the site?	Yes ✓	No	N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

34	Will the building design be certified by an independent energy auditor/analyst? If so, what will the rating be? <u>BC Energy Step Code – Step 2</u>	Yes ✓	No	N/A
35	Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting?	Yes ✓	No	N/A
36	Does the design and siting of buildings maximize exposure to natural light? What percentage of interior spaces will be illuminated by sunlight? <u>60 %</u>	Yes ✓	No	N/A
37	Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.). If so, please describe. <u>See attached.</u> If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? <u>See attached.</u>	Yes ✓	No	N/A
38	Has the building been designed to be solar ready?	Yes ✓	No	N/A
39	Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity?	Yes ✓	No	N/A
40	Do windows exceed the BC Building Code heat transfer coefficient standards?	Yes	No ✓	N/A
41	Are energy efficient appliances being installed in this project? If so, please describe. <u>EnergyStar appliances (fridges, stoves, washer, dryers) will be installed.</u>	✓		
42	Will high efficiency light fixtures be used in this project? If so, please describe. <u>All light fixtures will be LED or Compact Fluorescent (CFL).</u>	Yes ✓	No	N/A
43	Will building occupants have control over thermal, ventilation and light levels?	Yes ✓	No	N/A
44	Will outdoor areas have automatic lighting [i.e. motion sensors or time set]?	Yes ✓	No	N/A
45	Will underground parking areas have automatic lighting?	Yes	No	N/A ✓

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	Yes ✓	No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. <u>Low VOC paints, primers, varnishes and flooring will be used throughout.</u>	Yes ✓	No	N/A
48	Will the building have windows that occupants can open?	Yes ✓	No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area?	Yes ✓	No	N/A
50	Will fresh air intakes be located away from air pollution sources?	Yes ✓	No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. <u>Reuse of dimensional lumber.</u>	Yes ✓	No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. <u>Reuse of forming material.</u>	Yes ✓	No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	Yes ✓	No	N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	Yes	No	N/A ✓

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	Yes ✓	No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	Yes	No	N/A ✓
57	Is access provided for those with assisted mobility devices?	Yes ✓	No	N/A
58	Are accessible bike racks provided for visitors?	Yes ✓	No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees?	Yes	No ✓	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input type="checkbox"/> transit passes <input type="checkbox"/> car share memberships <input type="checkbox"/> shared bicycles for short term use <input type="checkbox"/> weather protected bus shelters <input type="checkbox"/> plug-ins for electric vehicles <input checked="" type="checkbox"/> Secured and enclosed scooter parking			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.

GREEN BUILDING CHECKLIST

Green Building Standards

8. Any refrigerant required for this project will be hydro-fluorocarbon (HFC). Mechanical and building materials will be fibreglass rather than foamed insulation. There is no intent to use CFCs or HCFCs on this project.

Natural Features/Landscaping

28. The Project Team includes an arborist who will provide guidance and supervise work within or near critical root zones of trees to be retained on and off-site. The work will include, but is not limited to:
 - Pruning, root pruning, excavation, etc.
 - Arranging for specified growing medium to be placed in excavations.
 - Ensuring barriers are installed or re-installed according to Township specifications after work in critical work zones is completed prior to Township inspection/re-inspection.
 - Coordinating service installers for excavation of utility serving, particularly when using a common trench.

Energy Efficiency

37. An air source heat pump may be used where single application cooling is required (common lounge area). There is currently no intent to air condition the entire building. Condensing units, if required, will be small (less noise), located in parking areas away from residential units, and screened with plantings. Efficiencies in heating will be gained through heat recovery of the ventilation systems rather than a primary heating plant.



South Elevation
REV

1
VINYL WINDOW
BLACK

2
VINYL WINDOW
WARM WHITE

3
ALUMINIUM RAILING
WARM WHITE

4
STUCCO SMOOTH PAINTED
Sherwin Williams
'Dromedary Camel' (WARM TAN)

5
STUCCO SMOOTH PAINTED
Sherwin Williams
'Oak Creek' (WARM BROWN)

6
CEMENTITIOUS FIBRE BOARD PAINTED
Sherwin Williams
'Shoji White' (WARM WHITE)

7
GLASS
CLEAR

8
SPANDREL GLASS
TO MATCH GLASS

numberTEN architectural group
200 - 1419 Dune Street
Victoria, BC
Canada V8W 2G3
T 250.360.2106
F 250.360.2106
www.number10.ca
info@number10.ca



2017-10-25

**St Peter's and St Paul's
Housing and
Ministry Centre**

Proposed Colour Board

DATE	SPVDS	CREATED BY	BC
PROJECT NO.	XXXXX		
REV. PROJECT NO.	2017505		

**ST PETER'S AND ST PAUL'S HOUSING AND MINISTRY CENTRE
REZONING APPLICATION**

1379 ESQUIMALT ROAD

LOT A (DD EP75276), SUBURBAN LOT 27, ESQUIMALT DISTRICT, PLAN 5092, PID 024-848-905



1. APPROVED REZONING APPLICATION	APR 24 2018
2. REZONING APPLICATION SUBMISSION	
3. REZONING APPLICATION SUBMISSION	
4. REZONING APPLICATION SUBMISSION	

numberTEN architectural group
200 - 1618 Stone Street
Victoria, BC
Canada V8W 2K3
T 250.360.2700
F 250.360.2746
www.number10.com
info@number10.com

ARCHITECTURAL	
CONTRACTOR	
DATE	NTS 2018-04-20

PROJECT	St Peter's and St Paul's Housing and Ministry Centre
DESCRIPTION	Cover Sheet, Project Data, and List of Drawings
DRAWN BY	SP/DS
CHECKED BY	BC
DATE PROJECT NO.	XXXXX
DATE PROJECT NO.	2017505

**St Peter's and St Paul's
Housing and
Ministry Centre**

Cover Sheet, Project Data,
and List of Drawings

A-00

PROJECT DATA

	Current Zoning Requirements	Existing Site (Pre Subdivision) Data	Proposed Subdivided Site Data	Proposed Subdivided Site Data	Proposed Subdivided Site Data	Setback Zoning Exemptions
Zoning	CD-23 Zone	CD-23 Zone	Existing 'Hermitage' Lot (Area A)	New Lot and New Zone (CO) (Area B)	Subdivided Cemetery Lot (Area C)	/
Site Area	min. 6,284 sq.m (67,640 sf)	6,270 sq.m (67,492 sf)	3,407 sq.m (36,677.4 sf)	2,766 sq.m (29,775.1 sf)	97 sq.m (1,044 sf)	/
Building Footprint	/	1,679.3 sq.m (18,077 sf)	1,067.6 sq.m (11,491.5 sf)	Proposed 500, Church 300, Total 850 sq.m (9,149 sf)	/	/
Total Building Floor Area	/	3,286.7 sq.m (35,377.7 sf)	2,087.1 sq.m (22,552.9 sf)	Proposed 1,558, Church 280, 1,838 sq.m (19,784 sf)	/	/
Number of Storeys	/	The Hermitage - 4, Church - 1.5	The Hermitage - 4	Proposed - 5, Church - 1.5	/	/
Height	20m (65.6 ft)	The Hermitage - 14m (45.9 ft), Church - 10m (32.8 ft)	The Hermitage - 14m (45.9 ft)	(From Grade) Proposed - 15.8m (50.9 ft), Church - 10m (32.8 ft)	/	/
Site Coverage	30%	1,679.3 / 6,270 = 26.7%	1,067.6 / 3,407 = 31%	850 / 2,766 = 30%	/	/
Floor Area Ratio	1	0.51	0.881	Proposed 0.561, Church 0.101, Total 0.661	/	/
Number of Units	39 plus 1 guest per 157 sq.m	26	26	24	/	/
Number of Parking Stalls	45	28 Church stalls, 7 on-site and 24 off-site = 35 total	24 U/B stalls, 7 on-site stalls = 31 total	27 on-site stalls, 4 off-site stalls, = 31 total	/	/
South Setback	4.5m (14.8 ft)	4.4m (14.5 ft)	4.4m (14.5 ft)	9.9m (32.5 ft)	/	/
North Setback	1.7m (5.6 ft)	46m (150.9 ft)	12m (39.4 ft)	3.1m (10.1 ft)	/	From Church Stairs to Prop. Line = 1.12m
East Setback	1.2m (3.9 ft)	9.4m (30.7 ft)	9.4m (30.7 ft)	17.4m (57.1 ft)	/	Church to Cemetery lot line = 0.5m
West Setback	8.5m (27.9 ft)	9.1m (29.9 ft)	9.1m (29.9 ft)	3.1m (10.1 ft)	/	From Upper Balcony to Prop. Line = 2.64m
Green Space	/	2,907 sq.m (31,292 sf) 46%	1,318 sq.m (14,187 sf) 39%	977 sq.m (10,516 sf) 35%	/	/
Impervious Surface Space	/	1,163 sq.m (12,500 sf) 18%	2,089 sq.m (22,496 sf) 61%	1,789 sq.m (19,257 sf) 65%	97 sq.m (1,044 sf)	/

LIST OF DRAWINGS

- | | | |
|--|-------------------------------------|-------------------------------|
| ARCHITECTURAL | LANDSCAPING | SITE SERVICING (CIVIL) |
| A-00- COVER SHEET, PROJECT DATA AND LIST OF DRAWINGS | L-01- LANDSCAPE CONCEPT PLAN | C-01- SITE PLAN |
| A-01- EXISTING SITE PLAN | L-02- TREES, SHRUBS AND HARDSCAPING | C-02- GRADING PLAN |
| A-02- EXISTING SITE SURVEY | | |
| A-03- SITE SUBDIVISION PLAN | | |
| A-04- PROPOSED SITE PLAN | | |
| A-05- PROPOSED MAIN AND SECOND TO FOURTH FLOOR PLAN | | |
| A-06- PROPOSED FIFTH AND ROOF PLAN | | |
| A-07- PROPOSED ELEVATIONS | | |
| A-08- PROPOSED ELEVATIONS | | |
| A-09- PROPOSED CROSS SECTION | | |
| A-10- 3D MODEL IMAGES | | |
| A-11- 3D MODEL IMAGES | | |
| A-12- 3D MODEL IMAGES | | |



RECEIVED
APR 24 2018
 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

1. PRELIMINARY APPLICATION SUBMISSION	APR 10 2018
2. PRELIMINARY APPLICATION REVIEW	APR 10 2018
3. PRELIMINARY APPLICATION SUBMISSION	APR 10 2018
4. PRELIMINARY APPLICATION REVIEW	APR 10 2018

numberTEN architectural group
 200 - 1810 Howe Street
 Victoria, BC
 Canada V8W 3K3
 T 250.380.2166
 F 250.380.2165
 www.number10.com
 info@number10.com

number 10

ARCHITECTURAL	
DESIGNER	
DATE	1:200
DATE	2018-04-20

**St Peter's and St Paul's
 Housing and
 Ministry Centre**

Existing Site Plan

DATE	SP/DS	CHECKED BY	BC
DATE	XXXXX		
DATE	2017505		

A-01

Existing Site Plan
 SCALE 1:200

Site Plan Of:
 Lot A (OD EP75276), Suburban Lot 27,
 Esquimalt District, Plan 5092.

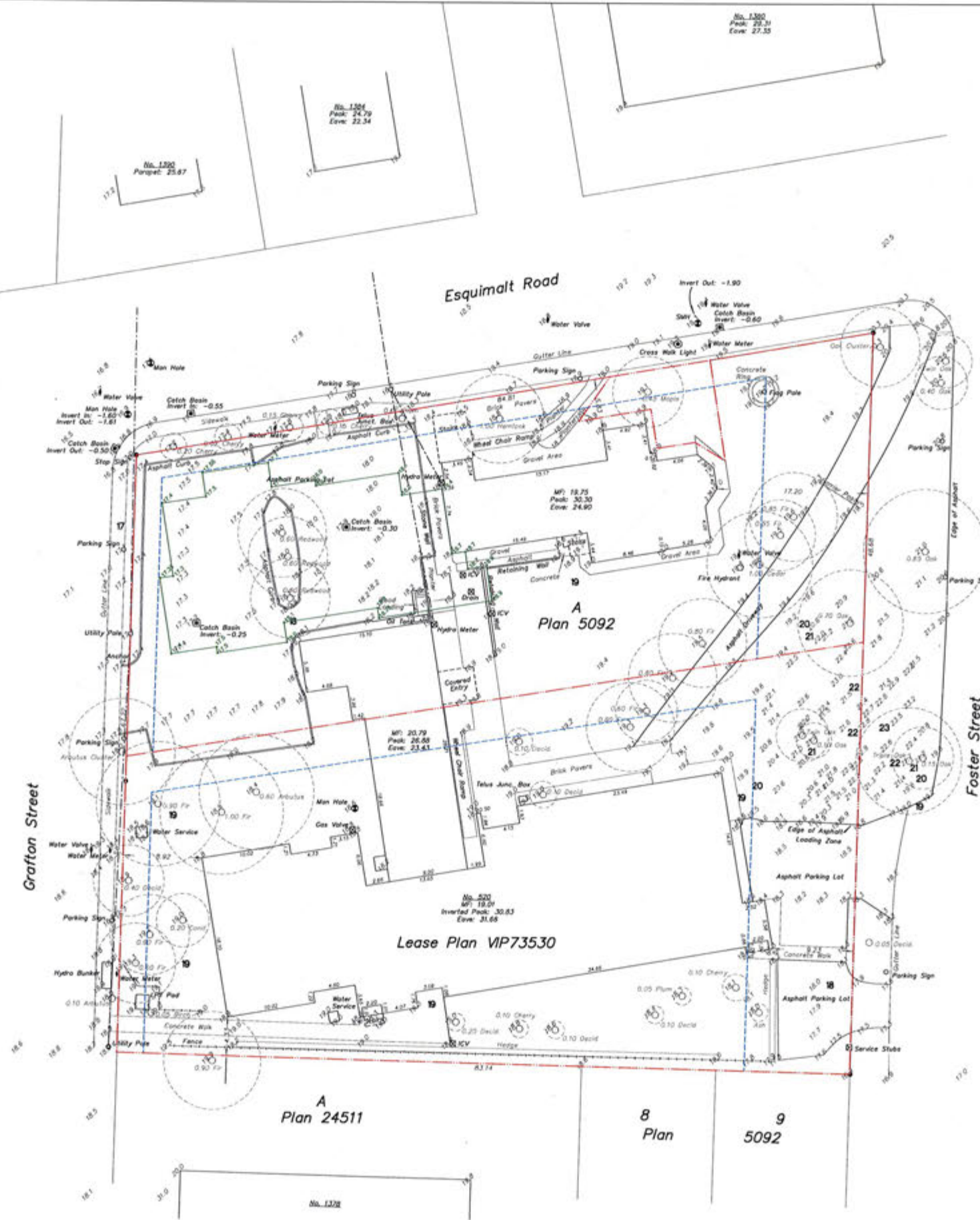


Scale = 1:250

Dated this 25th day of February, 2015.

Elevations are based upon geodetic datum and referenced to OCM 8410188

Elevations & distances shown are in metres.



Lines and grade points added by number 10
 LEGEND

Property Line	---
Set backs	---
Utility Strata	---
Average Building	---
Grade Points	○



1. JENNIFER KENNEDY	REGISTERED SURVEYOR
2. JENNIFER KENNEDY	REGISTERED SURVEYOR
3. JENNIFER KENNEDY	REGISTERED SURVEYOR
4. JENNIFER KENNEDY	REGISTERED SURVEYOR

numberTEN architectural group
 200 - 1618 Store Street
 Victoria, BC
 Canada V8W 2K3
 T 250 360 2106
 F 250 360 2146
 www.number10.com
 info@number10.com

SCALE: 1:250 DATE: 2018-04-20

St Peter's and St Paul's
 Housing and
 Ministry Centre

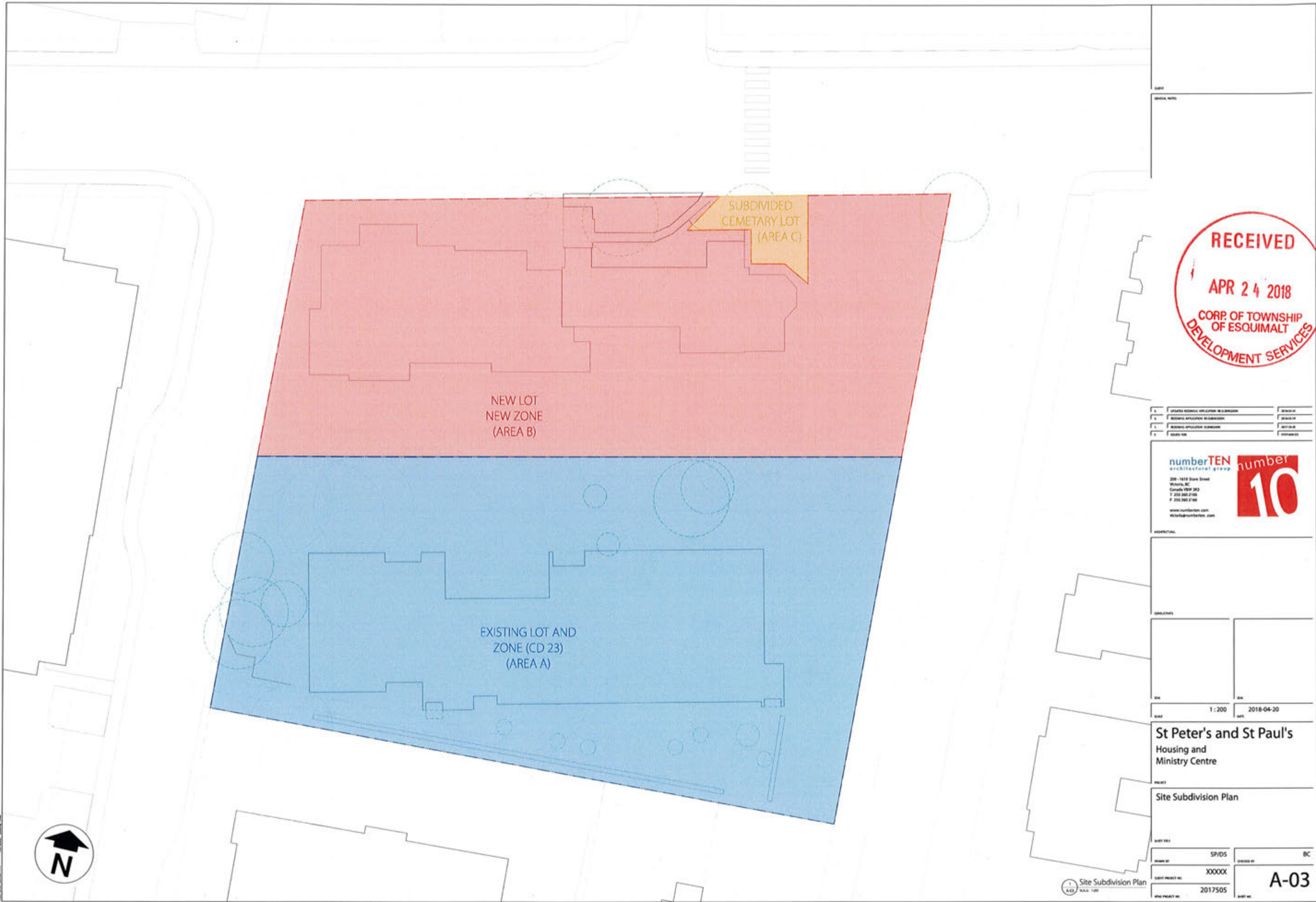
Existing Site Survey

PROJECT NO.	SP/DS	CHECKED BY	BC
DATE PROJECT NO.	XXXXX		
DATE PROJECT NO.	2017505		

A-02

The subject property is affected by the following registered documents: ES21302, ET26643, ET30925.

Way Mayenburg Land Surveying Inc.
 www.waylandsurveying.com
 #1-2227 James White Boulevard
 Sidney, BC V8L 1Z3
 Telephone (250) 656-5155
 Fax: 1500411hp112



1	ISSUED REVISION APPLICATION NO. 1000000000	DATE: 01/01/18
2	ISSUED REVISION APPLICATION NO. 1000000000	DATE: 01/01/18
3	ISSUED REVISION APPLICATION NO. 1000000000	DATE: 01/01/18
4	ISSUED REVISION APPLICATION NO. 1000000000	DATE: 01/01/18

numberTEN architectural group
 200 - 1810 Howe Street
 Victoria, BC
 Canada V8W 2K3
 T 250.360.2100
 F 250.360.2100
 www.number10.com
 info@number10.com



SCALE	1:200
DATE	2018-04-20

**St Peter's and St Paul's
 Housing and
 Ministry Centre**

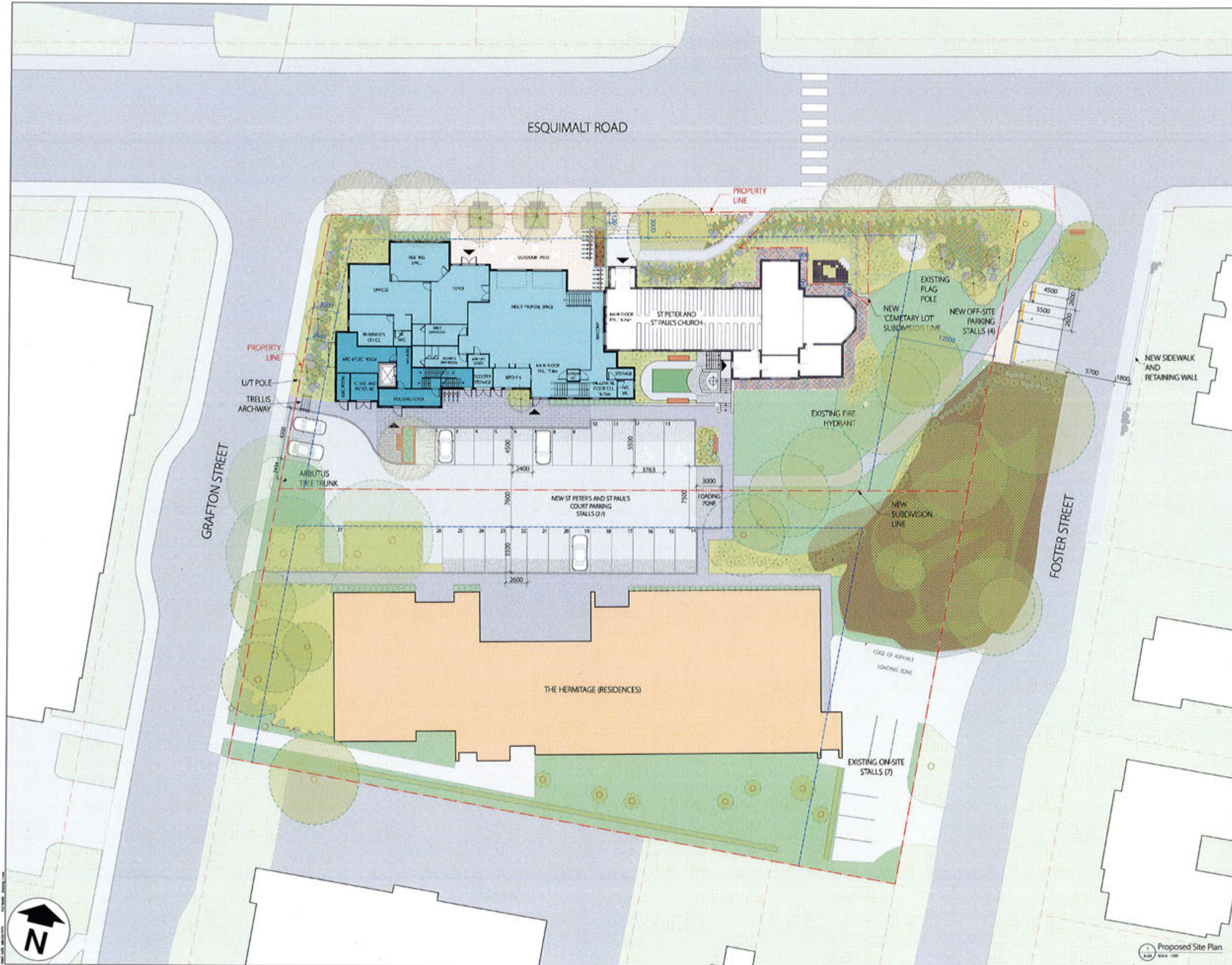
Site Subdivision Plan

DATE: 01/01/18	SP/D/S	DATE: 01/01/18	BC
SCALE: 1:200	XXXXX	SCALE: 1:200	XXXXX
PROJECT NO.:	2017505	PROJECT NO.:	2017505

A-03



1000000000 - 1000000000 - 1000000000



RECEIVED
APR 24 2018
 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT SERVICES

1	SP/DS	2018-04-20
2	SP/DS	2018-04-20
3	SP/DS	2018-04-20
4	SP/DS	2018-04-20

numberTEN architectural group
 200 - 1410 Stone Street
 Victoria, BC
 Canada V8W 3K2
 T 250 360 2100
 F 250 360 2100
 www.number10.com
 info@number10.com

number 10

DATE	1:200	2018-04-20
SCALE		
PROJECT		
CLIENT		
DESIGNER		
DATE		

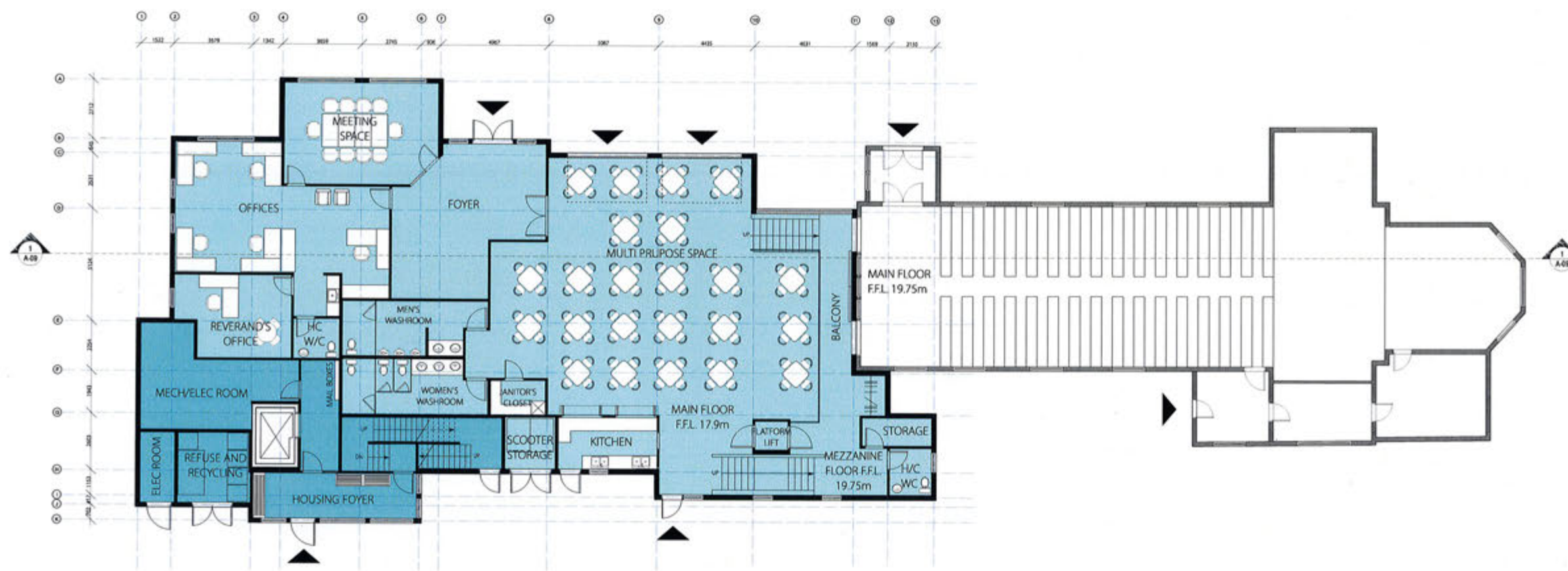
**St Peter's and St Paul's
 Housing and
 Ministry Centre**

PROJ: [blank]
 TITLE: **Proposed Site Plan**

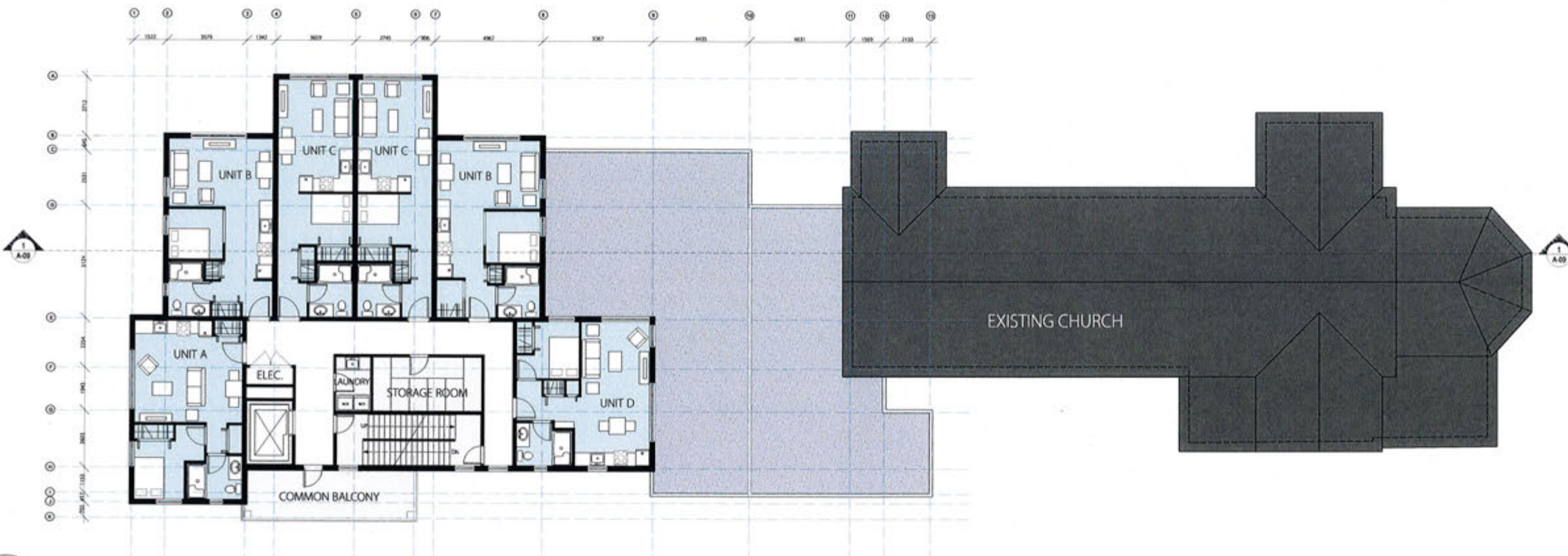
DATE		
BY	SP/DS	BC
CHECKED BY	XXXXX	
PROJECT NO.	2017505	
SHEET NO.		A-04



Proposed Site Plan
 SCALE: 1:200



Main Floor Plan
SCALE 1:100



Second to Fourth Floor Plan
SCALE 1:100

UNIT TYPE	
UNIT A - ONE BEDROOM	NET AREA = 40 sq.m (430 sf) TOTAL COUNT = 4
UNIT B - ONE BEDROOM	NET AREA = 39 sq.m (415 sf) TOTAL COUNT = 8
UNIT C - STUDIO	NET AREA = 37 sq.m (395 sf) TOTAL COUNT = 8
UNIT D - ONE BEDROOM	NET AREA = 40 sq.m (430 sf) TOTAL COUNT = 4

1	UPDATE SCHEMATIC APPLICATION SUBMISSION	2018-04-20
1	REVISION APPLICATION SUBMISSION	2018-03-19
1	REVISION APPLICATION SUBMISSION	2017-10-25
1	SCHEMATIC	2017-09-20

numberTEN architectural group
208 - 1610 Howe Street
Victoria, BC
Canada V8W 3K3
T 250.360.2106
F 250.360.2105
www.number10.com
info@number10.com



SCALE 1:100 DATE 2018-04-20

St Peter's and St Paul's
Housing and
Ministry Centre

Proposed Main and Second
to Fourth Floor Plan

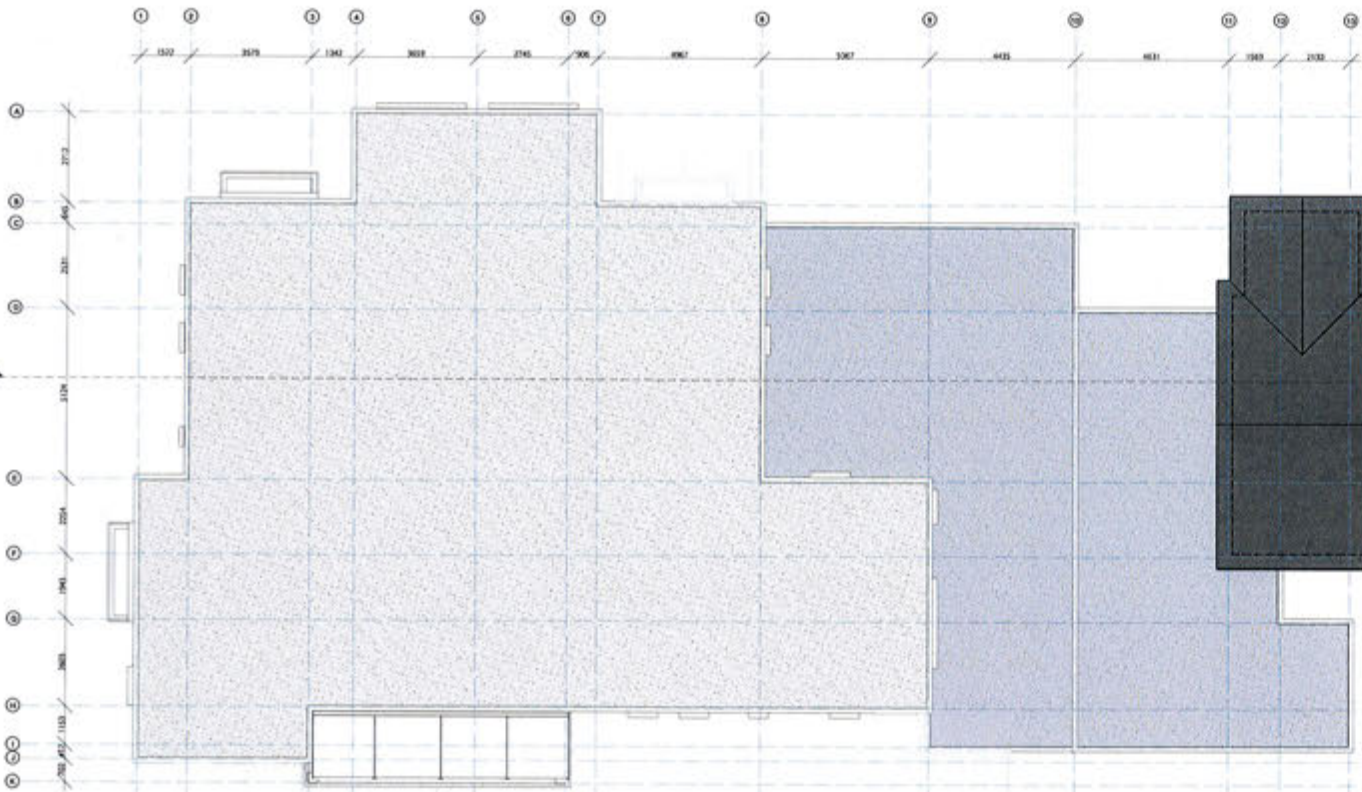
DESIGNED BY	SP/DS	CHECKED BY	BC
CLIENT PROJECT NO.	XXXXX		
DATE PROJECT NO.	2017505		

A-05





1 Fifth Floor Plan
SCALE 1:100



2 Roof Plan
SCALE 1:100

UNIT TYPE	
UNIT A - ONE BEDROOM	NET AREA = 40 sq.m (430 sf) TOTAL COUNT = 4
UNIT B - ONE BEDROOM	NET AREA = 39 sq.m (415 sf) TOTAL COUNT = 8
UNIT C - STUDIO	NET AREA = 37 sq.m (395 sf) TOTAL COUNT = 8
UNIT D - ONE BEDROOM	NET AREA = 40 sq.m (430 sf) TOTAL COUNT = 4

1. SITE PLAN	REVISION
2. CONCEPT PLAN	REVISION
3. PRELIMINARY PLAN	REVISION
4. FINAL PLAN	REVISION

numberTEN architectural group
 201 - 1018 Stone Street
 Victoria, BC
 Canada V8W 3K3
 T 250 360 2100
 F 250 360 2100
 www.number10.ca
 info@number10.ca



SCALE 1:100 DATE 2018-04-20

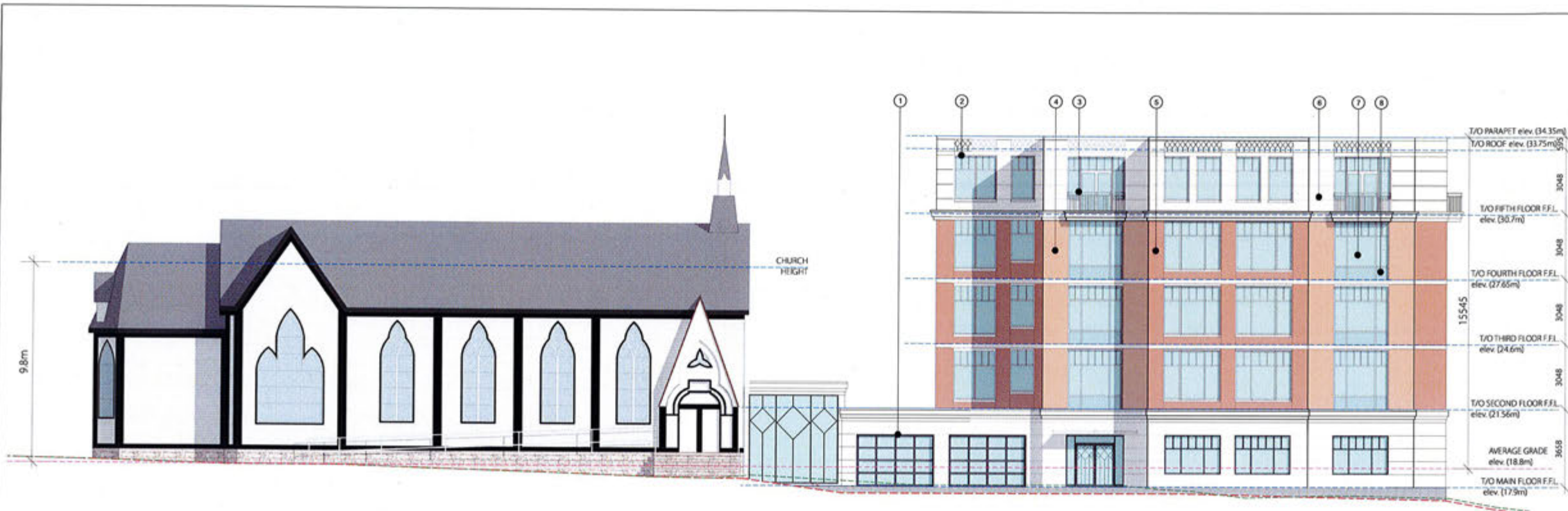
St Peter's and St Paul's
 Housing and Ministry Centre

Proposed Fifth And Roof Plan

DRAWN BY	SPVDS	CHECKED BY	BC
DATE PROJECT NO.	XXXXX		
FIELD PROJECT NO.	2017505		

A-06





1 South Elevation
SCALE 1:10

LEGEND:
 NATURAL EXISTING GRADE: - - - - -
 PROPOSED GRADE: - - - - -

- 1 Window Frame - 'Black'
- 2 Window Frame - 'Warm White'
- 3 Aluminium Flashing - 'Warm White'
- 4 Stucco - Sherwin Williams 'Dorchester Camel' (Warm Tan)
- 5 Stucco - Sherwin Williams 'Oak Creek' (Warm Brown)
- 6 Cementitious Fiber Board - Sherwin Williams 'Shoji White'
- 7 Glass 'Clear'
- 8 Spandrel Glass

1	DATE REVISION	REVISION
2	DATE REVISION	REVISION
3	DATE REVISION	REVISION
4	DATE REVISION	REVISION

numberTEN architectural group
 200 - 1018 St. Paul Street
 Victoria, BC
 Canada V8W 3K3
 T 250.360.2100
 F 250.360.2100
 www.number10.com
 info@number10.com




SCALE 1:100 DATE 2018-04-20

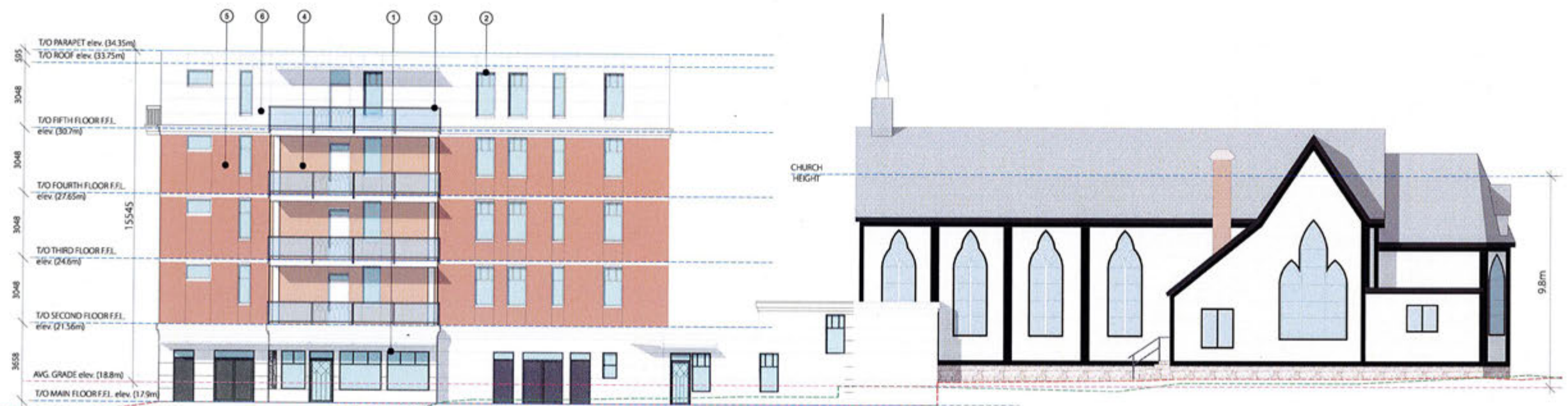
St Peter's and St Paul's
 Housing and
 Ministry Centre

Proposed Elevations

DATE PLOT	SP/D5	BC
DRAWN BY	XXXXX	
CHECK PROJECT NO.	2017505	
DATE PLOT		A-07



2 East Elevation
SCALE 1:10



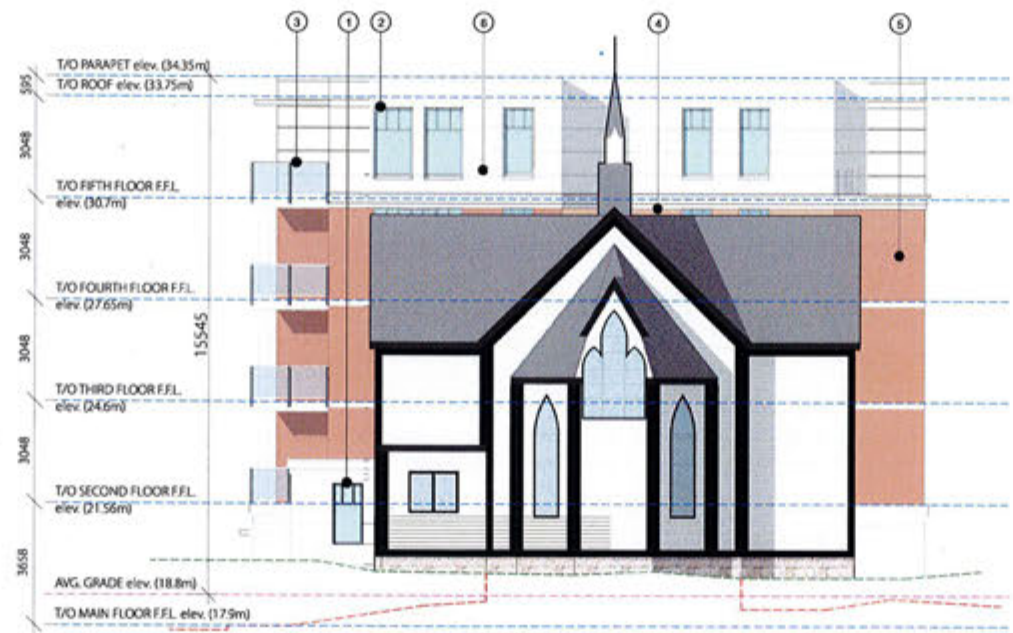
1 North Elevation
SCALE 1:100

LEGEND:
 NATURAL (EXISTING) GRADE: - - - - -
 PROPOSED GRADE: - - - - -

- 1 Window Frame - 'Black'
- 2 Window Frame - 'Warm White'
- 3 Aluminium Flashing - 'Warm White'
- 4 Stucco - Sherwin Williams 'Dromedary Camel' (Warm Tan)
- 5 Stucco - Sherwin Williams 'Oak Creek' (Warm Brown)
- 6 Cementitious fibre Board - Sherwin Williams 'Shoji White'
- 7 Glass 'Clear'
- 8 Spandrel Glass

1	UPDATES	DATE
2	REVISIONS	DATE
3	REVISIONS	DATE
4	REVISIONS	DATE

numberTEN architectural group
 200 - 1510 Howe Street
 Victoria, BC
 Canada V8W 3K3
 T 250 380 2100
 F 250 380 2100
 www.numberten.com
 info@numberten.com



2 West Elevation
SCALE 1:100

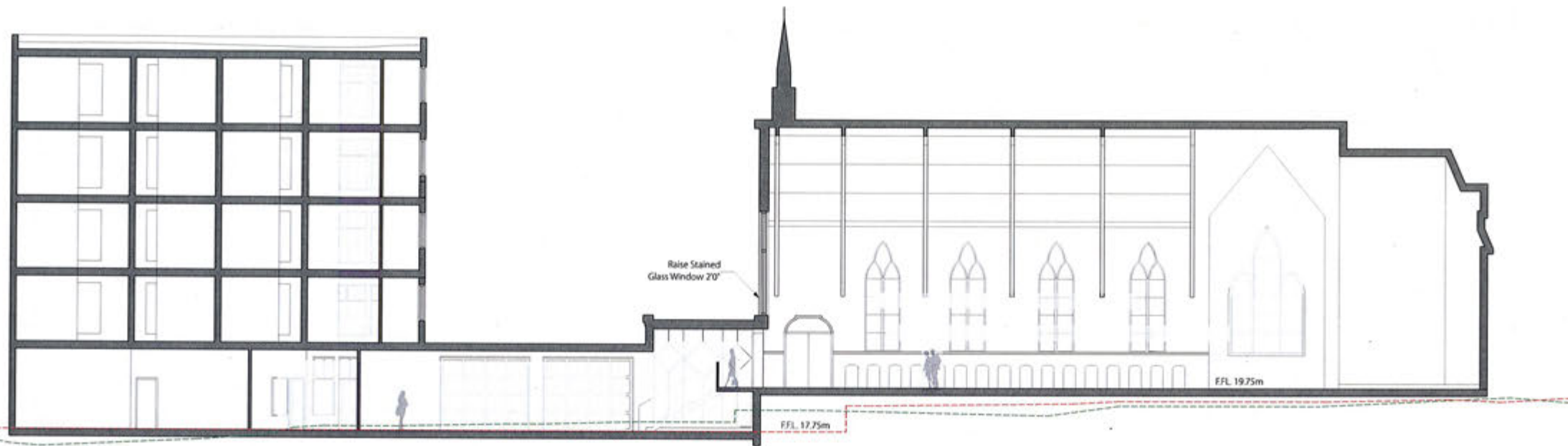
RECEIVED
 APR 24 2018
 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

SCALE 1:100 DATE 2018-04-20

St Peter's and St Paul's
 Housing and
 Ministry Centre

Proposed Elevations

DATE PLOTTED	SP/DS	BC
DRAWN BY	XXXXX	CHECKED BY
CLIENT PROJECT NO.	2017505	A-08
DATE PROJECT NO.		



Section Through Building and Church
SCALE 1/8"

- LEGEND:
- NATURAL (EXISTING) GRADE: - - - - -
- PROPOSED GRADE: - - - - -
- 1 Window Frame - 'Black'
 - 2 Window Frame - 'Warm White'
 - 3 Aluminium Flashing - 'Warm White'
 - 4 Stucco - Sherwin Williams 'Dromedary Camel' (Warm Tan)
 - 5 Stucco - Sherwin Williams 'Oak Creek' (Warm Brown)
 - 6 Cementitious Fibre Board - Sherwin Williams 'Shoji White'
 - 7 Glass 'Clear'
 - 8 Spandrel Glass

1	SPURD KING APPLICATOR MOUNTING	1/8" x 1/8"
2	SPURD KING APPLICATOR MOUNTING	1/8" x 1/8"
3	SPURD KING APPLICATOR MOUNTING	1/8" x 1/8"
4	SPURD KING	1/8" x 1/8"

numberTEN architectural group

200 - 1619 Stone Street
Victoria, BC
Canada V8W 3K3
T 250.360.2100
F 250.360.2100
www.numberten.com
info@numberten.com




SCALE: 1:100 DATE: 2018-04-20

St Peter's and St Paul's
Housing and
Ministry Centre

Proposed Cross Section

DRWN BY	SP/DS	CHECKED BY	BC
CLIENT PROJECT NO.	XXXXX		
WORK PROJECT NO.	2017505		

A-09



1 South Elevation
SCALE



2 View of Esquimalt Road Patio
SCALE



3 View Looking East Up Esquimalt Road
SCALE

DATE
 REVISIONS

1	SPURTS KENNEDY APPLICATOR SUBMISSION	2018-04
1	REVISION APPLICATOR SUBMISSION	2018-04
1	REVISION APPLICATOR SUBMISSION	2018-04
1	ISSUE FOR	2018-04

numberTEN architectural group
 200 - 1628 Shore Street
 Victoria, BC
 Canada V8W 3K3
 T 250 360 2196
 F 250 360 2198
 www.number10.com
 info@number10.com



RECEIVED
 APR 24 2018
 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT SERVICES

DATE: 2018-04-20

PROJECT: St Peter's and St Paul's Housing and Ministry Centre

3D Model Images

DATE PLOTTED	SP/D/S	CHECKED BY	BC
DRAWN BY	XXXXX		
CLIENT PROJECT NO.	2017505		
FINAL PROJECT NO.			A-10



1 Aerial View
SCALE



2 View of Front Patio
SCALE



3 View of Patio Entry
SCALE



4 View of Housing Entry From Grafton St.
SCALE

1	SPURDIS HOUSING APPLICATION REVISIONS	2018-04-20
2	REVISION APPLICATION REVISIONS	2018-04-20
3	REVISION APPLICATION SUBMISSION	2017-04-20
4	REVISION	2017-04-20

numberTEN architectural group
 208 - 1818 Shore Street
 Victoria, BC
 Canada V8W 3K3
 T 250.360.2706
 F 250.360.2706
 www.numberten.com
 info@numberten.com



RECEIVED

APR 24 2018

CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT SERVICES

DATE: 2018-04-20

PROJECT: St Peter's and St Paul's Housing and Ministry Centre

DESCRIPTION: 3D Model Images

DATE PLOTTED: SP/DS	DESIGNED BY: BC
DRAWN BY: XXXXX	
CLIENT PROJECT NO.: 2017505	SHEET NO.: A-11



1 West Side View From Across Grafton St.
A-12 RNL



2 View of Housing Entrance
A-13 RNL



3 South Elevation From Parking
A-14 RNL



4 View From Backyard
A-15 RNL

1. SITE PLAN	DATE: 2018-04-20
2. GENERAL NOTES	DATE: 2018-04-20
3. EXISTING APPLICABLE SUBMISSION	DATE: 2018-04-20
4. EXISTING APPLICABLE SUBMISSION	DATE: 2018-04-20
5. EXISTING APPLICABLE SUBMISSION	DATE: 2018-04-20

numberTEN architectural group
200 - 1628 Stave Street
Vancouver, BC
Canada V6M 3K3
T 206 360 2706
F 206 360 2708
www.numberten.com
info@numberten.com

number 10

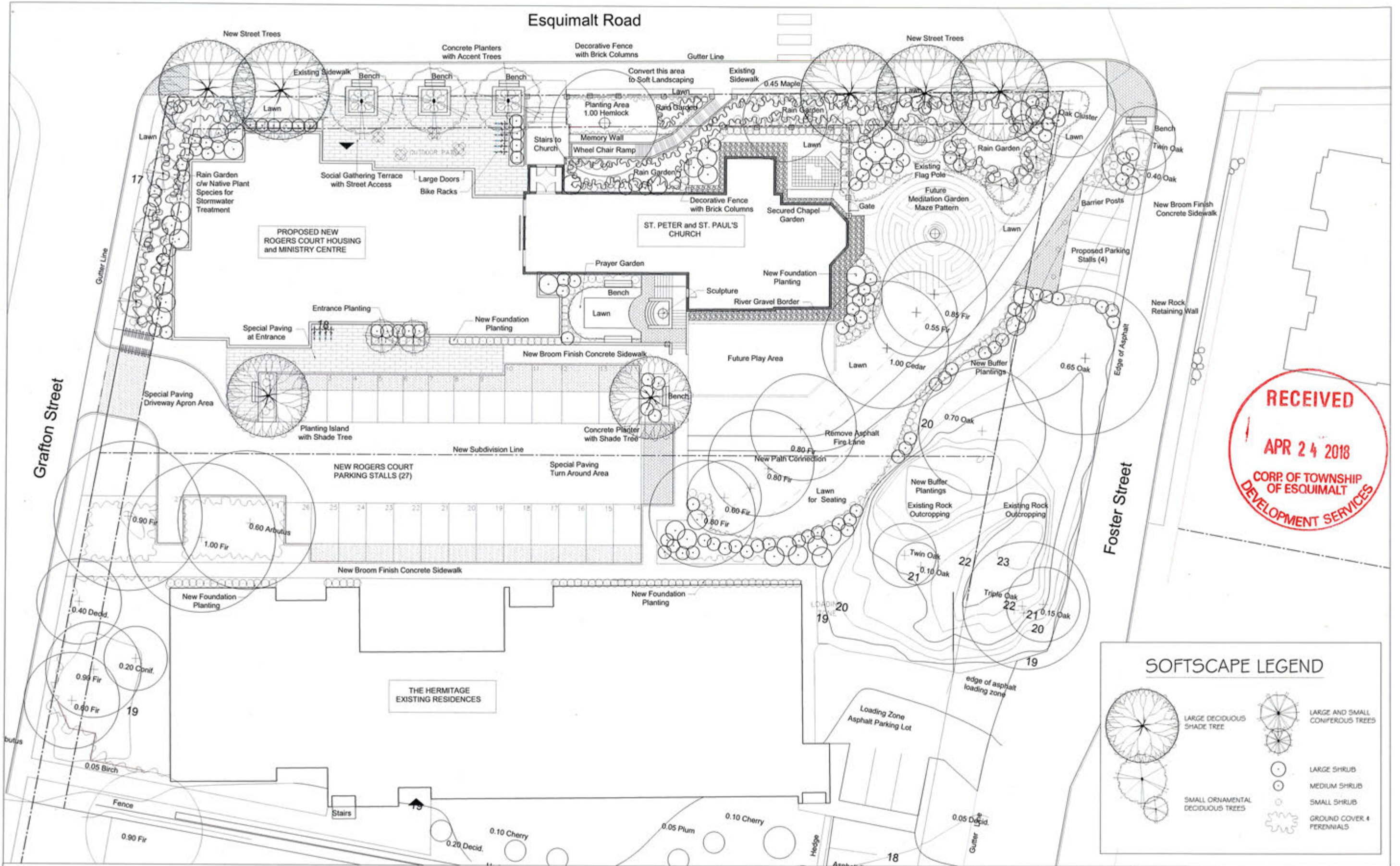
RECEIVED
APR 24 2018
CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES

DATE: 2018-04-20

St Peter's and St Paul's
Housing and
Ministry Centre

3D Model Images

DATE: 2018-04-20	SP/D5	BC
DRAWN BY: XXXXX	CHECKED BY:	
CLIENT PROJECT NO: 2017505		
FINAL PROJECT NO:		



St. Peter's & St. Paul's Housing and Ministry Centre

LANDSCAPE CONCEPT PLAN

SCALE: 1:150 Metric

DATE: April 19, 2018

4★SITE
 LANDSCAPE ARCHITECTURE
 AND SITE PLANNING 250.906.7885

St. Peter's & St. Pauls

Trees & Shrubs



Crimson King Maple



Vine Maple



Lily of the Valley



California Lilac



Austrian Pine



Sweet Gum



Shirobana Spiraea



Pink Azalea



Japanese False Holly

St. Peter's & St. Pauls

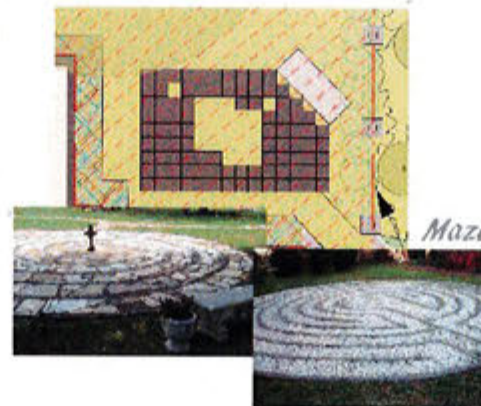
Hardscape Features



Prayer Gardens



Memorial Garden



Maze



Rain Gardens



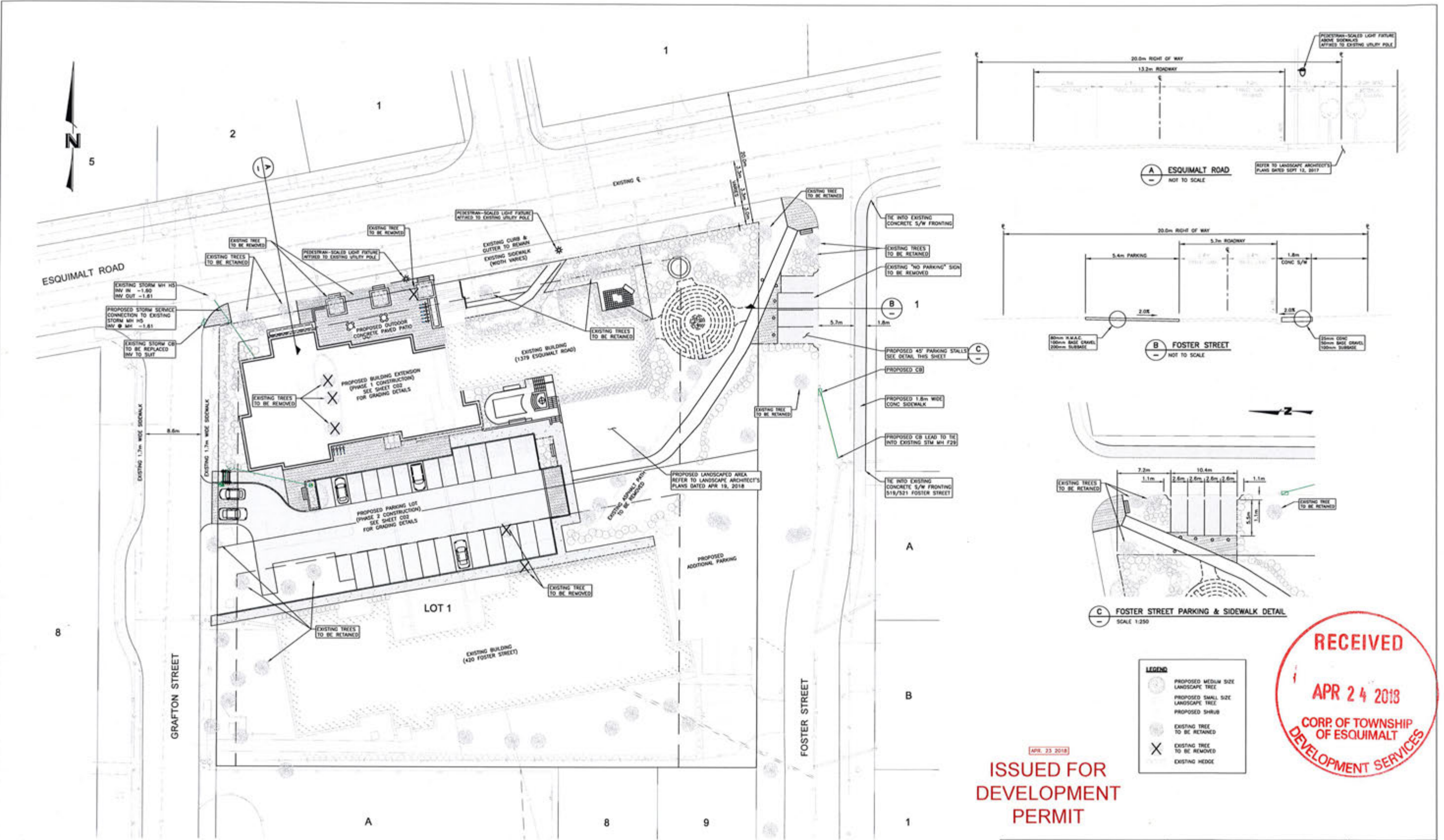
1	UPDATES/REVISED APPLICATION NUMBER	1	APPROVAL
2	REVISION APPLICATION NUMBER	2	APPROVAL
3	REVISION APPLICATION NUMBER	3	APPROVAL
4	REVISION APPLICATION NUMBER	4	APPROVAL

numberTEN architectural group
 200 - 1628 Stone Street
 Victoria, BC
 Canada V8W 3K3
 T 250.360.2786
 F 250.360.2788
 www.number10.com
 info@number10.com

4★SITE
 LANDSCAPE ARCHITECTURE
 AND SITE PLANNING SINCE 1988

DATE	2018-04-20
SCALE	
PROJECT	St Peter's and St Paul's Housing and Ministry Centre
PROJECT	Trees, Shrubs and Hardscaping

DATE	4 SITE	DATE	
DRAWN BY	XXXXX	CHECKED BY	
CLIENT PROJECT NO.	2017505	SHEET NO.	L-02



APR 23 2018
ISSUED FOR DEVELOPMENT PERMIT



LEGEND

- PROPOSED MEDIUM SIZE LANDSCAPE TREE
- PROPOSED SMALL SIZE LANDSCAPE TREE
- PROPOSED SHRUB
- EXISTING TREE TO BE RETAINED
- ✕ EXISTING TREE TO BE REMOVED
- EXISTING HEDGE



PLAN
 SCALE 1:250

LEGEND - PROPOSED SERVICES SHOWN DASHED

WATER	W	CURB	C	EXISTING U/O UTIL	---	MANHOLE	○	HYDRANT	◇	REDUCER	→	MONUMENT	⊙
SEWER	S	EDGE PAVT	SW	PROPOSED U/E UTIL	---	CLEANOUT	□	VALVE	◇	CAP	→	LOT PIN	•
GAS	G	BUSHLINE	SW	PROPOSED U/E UTIL	---	CATCHBASIN	⊞	FLUSH VALVE	◇	CULVERT	→	LEAD PLUG	•
		TREE	○	POWER POLE	○	SILTTRAP	⊞	AIR VALVE	◇	HEADWALL	→		
				ANCHOR	→	CONC.BOX	⊞	WATER	○	ROAD SIGN	→		

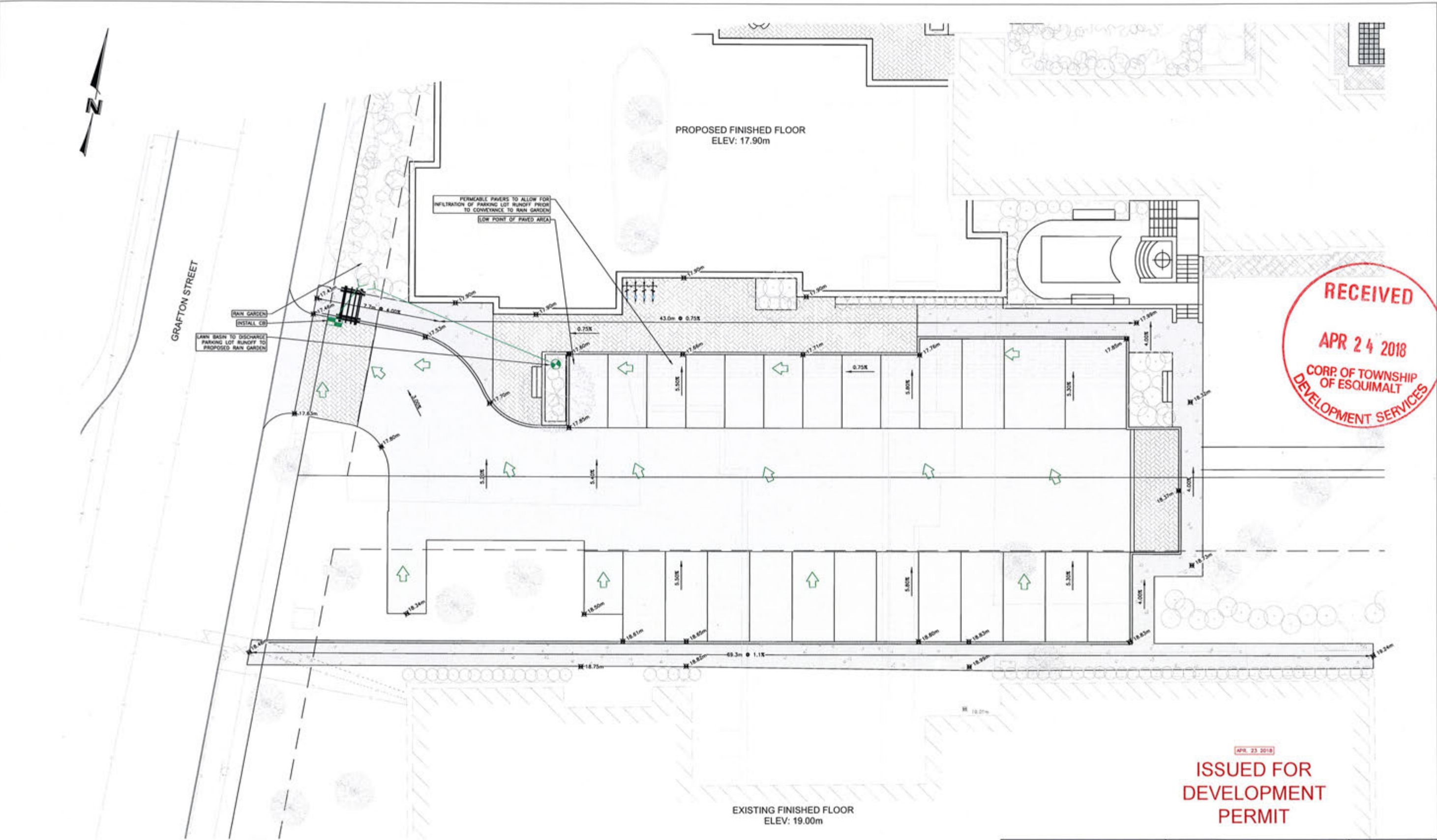
REVISIONS

NO.	DESCRIPTION	DATE	BY
1			
2			
3			
4			

DESIGN APPROVED

APPROVED BY	DATE	SIGN
DESIGN ENG.		
ASSIST. ENG.		
MUNICIPAL ENG.		

ISLANDER ENGINEERING		ROGERS HOUSING SOCIETY		ISLANDER PROJECT No. 2079	
		ROGERS COURT HOUSING & MINISTRY CENTRE		SHEET 1 OF 2	
		ESQUIMALT, BC		REV.	
		S.M.		FILE No. C01	
		DESIGN		DRAWN PAV/JA	
		CHECKED MJA		DATE APR 2018	
		SCALE: Hor. 1:250		Vertical	



APR 23 2018
 ISSUED FOR
 DEVELOPMENT
 PERMIT

PLAN
SCALE 1:250



THE LOCATION AND ELEVATION OF EXISTING UNDERGROUND SERVICES ON THIS DRAWING MAY NOT BE ACCURATE OR COMPLETE. THE ACTUAL HORIZONTAL AND VERTICAL LOCATIONS MUST BE CONFIRMED BY UTILITY COMPANIES AND THE CONTRACTOR PRIOR TO THE TO THE START OF ANY EXCAVATIONS

LEGEND - PROPOSED SERVICES SHOWN DASHED			
WATER	W	CURB	C
SEWER	S	SIDEWALK	SW
GAS	G	EDGE PAVL	EP
		SUSPLINE	SP
		TREE	T
		ANCHOR	A
		EXISTING U/S UTIL	---
		PROPOSED U/S UTIL	- - - -
		LIGHT STANDARD	MS
		POWER POLE	PP
		MANHOLE	MO
		CLEANOUT	CO
		CATCHBASIN	CB
		SILTTRAP	ST
		CONC. BOX	CB
		HYDRANT	HY
		VALVE	V
		FLUSH VALVE	FV
		AIR VALVE	AV
		METER	M
		REDUCER	R
		GAP	G
		CULVERT	CU
		HEADWALL	HW
		ROAD SIGN	RS
		MONUMENT	MO
		LOT PIN	LP
		LEAD PLUG	LP

REVISIONS	
NO.	DESCRIPTION

DESIGN APPROVED		
APPROVED BY	DATE	SIGN

ISLANDER PROJECT No. 2079		SHEET 2 OF 2		REV.
ROGERS HOUSING SOCIETY		ESQUIMALT, BC		
ROGERS COURT HOUSING & MINISTRY CENTRE				



ROGERS HOUSING SOCIETY
 ROGERS COURT HOUSING & MINISTRY CENTRE
 ESQUIMALT, BC

GRADING PLAN			
B.M.	DESIGN	DRAWN	CHECKED



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: May 9, 2018

STAFF REPORT

DATE: May 4, 2018
TO: Chair and Members of the Design Review Committee
FROM: Bill Brown, Director of Development Services
SUBJECT: Review of the Draft Official Community Plan

PURPOSE:

To review the Draft Official Community Plan and provide Council with any recommended changes along with reasons for the proposed changes.

BACKGROUND:

At their April 16, 2018 Committee of the Whole meeting, Council requested that staff refer the proposed Official Community Plan to the Design Review Committee for review and recommendations.

The review process began on September 28, 2015, when Council approved the Project Charter for the Review of the Official Community Plan. From the time the project began until now staff have guided the review process through numerous public consultation phases involving a variety of means and methods (open houses, surveys (online and paper), public workshops, Council workshops, Facebook®, studies (e.g. Esquimalt Age-friendly Assessment), stakeholder consultations (e.g. School Districts number 61 and 93), statutory referrals, booth at Buccaneer Days, and interdepartmental consultation). The following timeline provides a chronological overview of the process to date:

September 28, 2015	Council authorizes the commencement of the review of the Official Community Plan.
October 13, 2015	Council reviews the draft Official Community Plan survey.
January 11, 2016	Staff presents the Official Community Plan survey results to Council.
February 15, 2016	Council approves the Phase 2 consultation plan.
April 1, 2016	Council reviews the Township's Vision statement.

May 4, 2016	Looking Forward Forum held at the Recreation Centre.
December 12, 2016	Staff gives Council an Official Community Plan update.
February 20, 2018	Staff gives Council an Official Community Plan update.
May 5, 2017	Affordable Housing Workshop held at the Archie Browning Sports Centre.
June 7, 2017	Official Community Plan Open House held at the Archie Browning Sports Centre.
June 19, 2017	Staff present the Affordable Housing Workshop report to Council.
July 10, 2017	Staff present the Open House Report to Council.
October 3, 2017	Council holds a Special Committee of the Whole meeting to discuss housing policy.
September 2017 to February 2018	Staff draft policies and development permit guidelines.
March 26, 2018	Council gives first reading to the OCP Bylaw at a special meeting of Council.

For the past several months, the information that has been collected throughout the process has been collated and analyzed to develop the Draft Official Community Plan policies and Design Guidelines (Draft Plan) attached as Schedule "A".

The intent of the review process was not to rewrite the entire Official Community Plan but rather to update it to reflect emerging community values, changes to the law, and changes or new interpretations of the law. To this end, a fair amount of the 2007 Official Community Plan has been carried forward into the proposed Official Community Plan. On the other hand, the 2007 Official Community Plan also contained material that was either difficult to understand and interpret or should not have been in the Official Community Plan. These portions have been either removed from the proposed Plan or revised. There has also been substantial content added to the proposed Plan based on information received during the review process as well as changes to legislation. An overview of the major changes to the proposed Plan is outlined below:

- 1) The look and feel of the proposed Plan has been updated to reflect contemporary legislative drafting practices and graphic design.
- 2) The introduction has been revised and updated, and now contains the Township's Vision, Mission, and Values statements.
- 3) The community profile has been updated using the results of the 2016 Federal Census. This section will be enhanced in the version of the Draft Plan that is presented to Council for first and second readings.

- 4) This Draft Plan follows the requirements of the Local Government Act in a more precise manner. This is manifested in a number of ways including the addition of another land use designation map. As per the requirements of the Local Government Act, there are now two land use designation maps, one for the “Present” and one for the “Proposed” land use designations.
- 5) A substantial portion of land adjacent to Esquimalt Road has been redesignated from High Density Residential and Commercial/Commercial Mixed-use to Neighbourhood Commercial Mixed-use.
- 6) Most of the land north of Esquimalt Road between Admirals Road and CFB Esquimalt has been redesignated from High Density Residential to Medium Density Residential or Neighbourhood Commercial Mixed-use.
- 7) The Temporary Use Permit section has been made more robust and brought into line with the existing legislation. Under the 2007 Plan, Temporary Use Permits could only be issued for up to two years. Under the new Plan, they can be issued for up to three years in accordance with amendments made to the Local Government Act since the 2007 Plan was adopted.
- 8) The Development Approval Information Area section has also been significantly enhanced in order to better comply with the enabling provisions of the Local Government Act and recent case law.
- 9) The Housing and Residential Land Use Section has been significantly revised with the addition of a quantitative analysis of trends in housing based on data from recent Federal Censuses. The analysis concludes that the municipality has sufficient land to accommodate its housing needs for the next five years.
- 10) Policy has been added that would allow Council to amend the Zoning Bylaw to allow suites in duplexes.
- 11) Policy has been added that would allow Council to amend the Zoning Bylaw to allow “Detached Accessory Dwelling Units” (aka laneway houses, granny flats, garden suites, coach houses) in low density residential areas.
- 12) Policy has been added that would allow Council to amend the Zoning Bylaw in order to allow short-term accommodation in residential areas (e.g. Airbnb ®).
- 13) A section has been added on “Age-friendly Housing”.
- 14) Rather than differentiate multi-family housing based primarily on height as is the case in the 2007 Plan, the Draft Plan differentiates it based on density by referring to two classes of multi-family housing as either medium density with a floor area ratio of up to 2.0 and a height of up to 6-storeys, or a high density with a floor area ratio of up to 3.0 and a height of up to 12-storeys. Note that in the 2007 Plan, “Multi-Unit Low Rise Residential” was considered to be buildings up to 4-storeys with a floor area ratio of up to 1.5. Since the 2007 Plan was adopted, the BC Building Code has been amended in order to allow wood frame buildings up to 6-storeys. In order to ensure concurrence between the Draft Plan and the BC Building Code, the height of medium density buildings has been set at 6-storeys. In addition, the floor area ratio has been set at 2.0 in concert with the Plan’s overall goal of allowing for a moderate increase in density.

- 15) Policy has been added that encourages the use of the Revitalization Tax Exemption as an incentive for new or renovated purpose-built multi-unit rental buildings.
- 16) Policy has been added related to encouraging housing agreements that ensure that all strata units have the opportunity to be used as long-term rental.
- 17) The Parks, Recreation, and Trails section has been updated based on discussions with the Parks and Recreation Department's senior management team.
- 18) The Transportation and Infrastructure sections have been updated based on discussions with the Engineering and Public Works senior management team.
- 19) A number of seminal policies are carried forward from the 2007 Plan into the Draft Plan. These include the policy to pursue the acquisition of dedicated parkland along Esquimalt's waterfronts at the time of subdivision.
- 20) The target for the reduction of the community's greenhouse gas by 38% has been moved from 2020 (less than two years from now) to 2030.
- 21) The greenhouse gas emissions target for 2050 has been increased from a reduction of 83% to a target of "zero net community greenhouse gas emissions". This target would be consistent with targets set by the City of Victoria and the District of Saanich.
- 22) An Urban Food Production section has been added to the Draft Plan.
- 23) The Economic Development section has been updated to reflect the Economic Development Strategy and recent Economic Development initiatives. Specific policies have been added related to the "clean technology" sector and encouraging an advanced educational institute to locate in Esquimalt.
- 24) The Regional Context Statement has not been included in the Draft Plan as staff are waiting for guidance from the Capital Regional District. A Regional Context Statement will be included in the Plan that is presented to Council for first and second readings.
- 25) The Development Permit Areas section of the Draft Plan has been significantly updated by both removing material that was outside of the realm of development permit guidelines and adding new material in response to amendments to the Local Government Act.
- 26) The following four new "environmental" Development Permit Areas have been added to the Plan:
 - a. Protection of the Natural Environment;
 - b. Protection of development from hazardous conditions;
 - c. Establishment of objectives to promote energy conservation and promote the reduction of greenhouse gas emissions; and
 - d. Establishment of objectives to promote water conservation.

All areas except for the "protection of development from hazardous conditions" encompass the entire municipality. This means that many properties will be subject to four or more development permit areas. There

are provisions in the Plan to ensure that only one development permit application will be required even if a property is subject to more than one development permit area.

- 27) Also new to this section is the West Bay development permit area. It is based on the West Bay Neighbourhood Design Guidelines that were completed several years ago. It was always envisioned that the guidelines in the West Bay Neighbourhood Design Guidelines would be incorporated into the Official Community Plan when it was updated. Although the West Bay Neighbourhood Design Guidelines appear as an appendix in the Draft Plan, they will be slotted into their appropriate position in the Plan that is submitted to Council for first and second readings.

Overall, the Draft Plan represents a significant updating of the 2007 Plan as well as a clean up and partial reorganization in order to make it more user-friendly. Changes have also been made in order to comply with existing legislation and recent case law. Most importantly, the Draft Plan incorporates the results of an extensive community consultation process. It is important that an Official Community Plan reflects the dreams and aspirations of its residents, employers, and business owners. In addition, the Plan incorporates the latest information available including: data from the 2016 Federal Census, the results of numerous studies and reports (e.g. Age Friendly Assessment, Healthy Housing Workshop, Affordability Housing Workshop, Economic Development round tables and resulting strategy, greenhouse gas emissions data, and published literature.) It is imperative that Council is comfortable with the content of the Draft Plan since it is the statutory document that will guide Council's land use decision making processes for the next five to ten years.



Township of Esquimalt

Official Community Plan

First Reading Council March 26, 2018

Working Copy April 2018

Council April 23, 2018

Second Reading Council May 7, 2018

Township of
ESQUIMALT

Township of
ESQUIMALT



CONTENTS

PART 1 INTRODUCTION.....	5	PART 2 POLICIES	17
1. Introduction.....	6	4. Land Use Planning.....	18
1.1 Overview	6	4.1 Land Use Designations.....	18
1.2 Purpose of an Official Community Plan	6	4.2 General Development	18
1.3 Plan Area	7	4.3 Temporary Use Permits	19
2. Township of Esquimalt.....	8	4.4 Development Approval Information Area	20
2.1 General	8	4.5 Sand and Gravel	22
2.2 Vision, Mission and Values.....	8	4.6 Restriction on Land that is Subject to Hazardous Conditions.....	22
2.3 Community Profile	9	5. Housing & Residential Land Use	23
2.4 Regional Profile.....	11	5.1 General: Anticipated Housing Needs in the Next Five Years	23
2.5 Planning Context.....	12	5.2 Low Density Residential Redevelopment	26
3. Regional Context Statement	13	5.3 Medium and High Density Residential Development.....	26
3.1 General	13	5.4 Affordable Housing	28
3.2 Housing	13	5.5 Age-friendly Housing.....	29
3.3 Transportation.....	13	6. Commercial & Commercial Mixed-Use Land Use.....	30
3.4 Regional District Services	13	6.1 General	30
3.5 Parks and Natural Areas	14	6.2 Revitalization.....	32
3.6 Economic Development.....	14	6.3 Esquimalt Town Centre.....	32
3.7 Greenhouse Gas Emissions Reduction..	14	6.4 Neighbourhood Commercial Mixed-Use	33
3.8 Population Projections.....	14		
3.9 Additional Matters.....	15		
3.10 Summary	15		

7. Business Land Use.....	37	12. Infrastructure.....	52
7.1 General	37	12.1 General	52
7.2 Esquimalt Business Park	37	12.2 Sanitary Sewer Collection Systems.....	52
7.3 Public Services and Procurement Canada Land.....	38	12.3 Stormwater Collection Systems	53
7.4 McLoughlin Point Special Use	38	12.4 Street Lighting Systems.....	54
8. Federal Lands		12.5 Regional Sanitary Collection System).....	54
8.1 General	39	12.6 Potable Water Distribution System....	54
8.2 Federal Land.....	39	12.7 Utilities Distribution Systems	55
8.3 Public Services and Procurement Canada Land.....	40	13. Environment, Energy & Climate Change	56
9. institutional & Public Facilities Land Use	41	13.1 Natural Areas.....	56
9.1 General	41	13.2 Solid Waste Management	56
9.2 Federal Land.....	41	13.3 Reduction of Greenhouse Gas Emissions.....	57
9.3 Public and Private Service Facilities.....	42	13.4 Climate Change Adaptation	60
9.4 Schools	43	14. Urban Agriculture	61
10. Parks, Recreation & Trails.....	44	14.1 Agricultural Land Use	61
10.1 General	44	14.2 Urban Food Production	61
10.2 Parks	44	15. Arts, Culture & Heritage	63
10.3 Recreation.....	45	15.1 Arts and Culture	63
10.4 Trails and Waterfront Walkways.....	46	15.2 Heritage.....	64
11. Transportation.....	48	16. Economic Development.....	67
11.1 General	48	16.1 General	67
11.2 Walking.....	48	16.2 Commercial & Industrial	68
11.3 Cycling.....	49	16.3 West Bay	68
11.4 Public Transit	49	16.4 Tourism	69
11.5 E&N Rail Corridor	50		
11.6 Roads	50		
11.7 Public Parking	51		

**PART 3 DEVELOPMENT
PERMIT AREAS..... 71**

17. Development Permit Areas..... 72

17.1 General72

17.2 Regulations.....72

17.3 General Development Permit Exemptions73

**18 Development Permit Area No. 1
-Natural Environment 75**

18.1 Area75

18.2 Designation75

18.3 Justification75

18.4 Exemptions76

18.5 Guidelines78

**19. Development Permit Area No. 2 -
Protection of Development from
Hazardous Conditions..... 82**

19.1 Area82

19.2 Designation82

19.3 Justification82

19.4 Exemptions82

19.5 Guidelines83

**20. Development Permit Area No. 3 -
Enhanced Design Control
Residential 84**

20.1 Area84

20.2 Designation84

20.3 Justification84

20.4 Exemptions84

20.5 Duplex Guidelines85

20.6 Guidelines Single-Unit Infill.....86

**21 Development Permit Area No. 4 -
Commercial..... 88**

21.1 Area88

21.2 Designation88

21.3 Justification88

21.4 Exemptions88

21.5 Guidelines89

**22. Development Permit Area No. 5 -
Business..... 90**

22.1 Area90

22.2 Designations90

22.3 Justification90

22.4 Exemptions90

22.5 Guidelines91

**23. Development Permit Area No. 6 -
Multi- Family Residential 92**

23.1 Area92

23.2 Designation92

23.3 Justification92

23.4 Exemptions92

23.5 Guidelines93



24. Development Permit Area No. 7 - Energy Conservation and Greenhouse Gas Reduction.....	94	28. Development Permit Area No. 11 - West Bay	113
24.1 Area:	94	28.1 Area	113
24.2 Designation:	94	28.2 Designations.....	113
24.3 Justification:	94	28.3 Justification	113
24.4 Exemptions	95	28.4 Exemptions	114
24.5 Guidelines.....	95	28.5 Guidelines.....	114
25. Development Permit Area No. 8 - Water Conservation	99	29. Development Permit Area No. 12 - McLoughlin Point.....	141
25.1 Area:	99	29.1 Area	141
25.2 Designation	99	29.2 Designation	141
25.3 Justification	99	29.3 Justification	141
25.4 Exemptions	100	29.4 Exemptions	142
25.5 Guidelines.....	100	29.5 Guidelines.....	142
26. Development Permit Area No. 9 - English Inn	102	PART 4 MAPS	153
26.1 Area	102	Schedule A (Present Land Use Designations).....	155
26.2 Designations.....	102	Schedule B (Proposed Land Use Designations).....	156
26.3 Justification	102	Schedule C (Road Network).....	157
26.4 Exemptions	103	Schedule D (Sewer Network).....	158
26.5 Guidelines.....	103	Schedule E (Water Network)	159
 		Schedule F (Public Parks & Facilities).....	160
27. Development Permit Area No. 10 - Esquimalt Town Square	107	Schedule G (Public View Corridors).....	161
27.1 Area	107	Schedule H (Development Permit Areas..)	162
27.2 Designations.....	107	 	
27.3 Justification	107	PART 5 APPENDICES	163
27.4 Exemptions	108	Township of Esquimalt Heritage Values ..	165
27.5 Design Guidelines.....	108		



PART 1 INTRODUCTION



1 INTRODUCTION



1.1 Overview

This Official Community Plan [OCP or the Plan] provides a framework to guide future growth and change in Esquimalt in a sustainable manner. The Plan's statements, goals, objectives, policies, targets, actions, and map designations promote the long-term economic, environmental, social and cultural sustainability of the community.

The Plan is a statement of objectives and policies that guide Council's decision making process respecting planning and land use management, within Esquimalt's municipal boundaries. It also delineates development permit areas, which together with the associated development permit guidelines, guide the form and character of development. In addition, the development permit guidelines promote the conservation of energy and resources along with the protection of the natural environment, its ecosystems and biological diversity. Finally, the Plan contains development permit guidelines for the protection of development from hazardous conditions. In addition to guiding Council's decision making, the Plan also provides residents, land and business owners, and developers with a vision of the future of the community and a framework for their own decision making.

An OCP is not a static document, it should respond to changes in the community, including its values and social, economic and environmental conditions and changing regulations and authorities. Plan changes are required to be undertaken through bylaw amendments after consultation, as required under the Local Government Act which includes a Public Hearing process, to ensure that the public's input on significant land use decisions and policy changes are considered for approval.

1.2 Purpose of an Official Community Plan

The Local Government Act sets out mandatory and optional topics to be included in an OCP and provides a range of tools that allow municipalities to implement their plans. Where a topic is not within the jurisdiction of the local government, an OCP may only state the broad objectives related to that matter. Once adopted, the OCP does not commit the Township to carry out the projects in the Plan but it does require that works undertaken and bylaws enacted by Council are consistent with it.

In addition to local interests, the OCP takes into account the policies and regulations of the Capital Region, provincial and federal ministries and regulating bodies that are involved in the management of land and natural resources. Land that is owned by the Federal Government is immune from municipal land use regulations. Furthermore, the Provincial Government is not bound by municipal land

use regulations in relation to the use or development of land, or in the planning, construction, alteration, servicing, maintenance or use of improvements – although that immunity does not extend to tenants or occupiers of Provincial Crown land.

1.3 Plan Area

The area covered by this OCP includes all water and land areas within the boundaries of the Township of Esquimalt, as shown on the “Present Land Use Designation Map.”

Section 1.4 Plan Highlights

The Plan contains a number of highlights which are summarized as follows:

- The Regional Context Statement has been updated to reflect the 2018 Regional Growth Strategy.
- The Plan contains both a “Present Land Use Designation” map and a “Proposed Land Use Designation” map. Owners of land may choose to redevelop their land in accordance with either designation as long as the zoning of the land allows the proposed development.
- The Temporary Use Permit section has been updated to reflect amendments to the Local Government Act.
- The Development Approval Information Area section has been significantly updated to reflect the requirements of the Local Government Act and decisions by the courts.
- A detailed analysis of proposed housing is included.
- A policy framework is set to allow Council to develop a regulatory regime to allow suites in duplexes.
- A policy framework is set to allow Council to develop a regulatory regime to allow detached accessory dwelling units on appropriate parcels of land.
- The Plan sets a target of at least a 38 percent reduction in community greenhouse gas emissions by 2030.
- The Plan sets a target of zero net community greenhouse gas emission by 2050.
- Four development permit areas now cover the entire municipality. They are:
 - Protection of the natural environment, its ecosystems, and biological diversity;
 - Establishment of objectives to promote energy conservation;
 - Establishment of objectives to promote water conservation; and
 - Establishment of objectives to promote the reduction of greenhouse gas emissions.
 - Inclusion of a section on urban agriculture.

These highlights represent a significant evolution of the Plan as it responds to contemporary issues facing Esquimalt.

2 TOWNSHIP OF ESQUIMALT



2.1 General

Section 2 provides highlights of the Township of Esquimalt as it is in the present day and where it is envisioned to go in the future. In this section you will find the community's vision, mission and values statements, as well as community and regional profiles.

2.2 Vision, Mission and Values

The Township's vision, mission and values statements provide the context for this Official Community Plan and guide the decision making of Council and staff in the provision and delivery of works and services.

Our Vision

- Esquimalt is a caring and welcoming community that has strong relationships with our neighbours, the Esquimalt and Songhees Nations, regional municipalities, and the naval base.
- Our vibrant economy is supported by an engaged community.
- As a coastal community, we are stewards of our environment and value our past, present and future.

Our Mission

Focusing on community priorities, the Township of Esquimalt works to make our community and environment a better place for today and the future.

Our Values

- **Accountability** – we acknowledge and take responsibility for our actions, decisions and policies.
- **Integrity** – we practice high standards of ethical behaviour and open communication that inspire trust.
- **Respect** – we foster an environment of fairness where people are valued and treated with dignity.
- **Service** – we strive to meet community needs and achieve high-quality results through teamwork, innovation and creativity.

2.3 Community Profile

The Township of Esquimalt is a closely knit caring community whose citizens represent a wide socio-economic and demographic spectrum. It is also a community that embraces newcomers to Canada and strives to be more diverse and inclusive by providing a range of multicultural services and programs. A key goal of the OCP is to put into place a land use policy framework that will promote a vibrant community for all of its residents.

2.3.1 Population and Age Profile

After years of virtually no growth, Esquimalt’s population showed an 8.9 percent increase in growth between 2006 and 2011 (Figure 1).

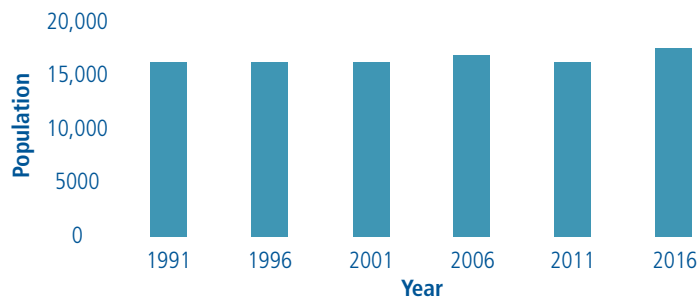


Figure 1. In 2016, Esquimalt had a population of 17,655, an increase of 8.9 percent from its 2011 population of 16,209.

Esquimalt’s population exhibits a classic demographic distribution with a large bulge of baby boomers between the ages of 50 and 69 and another bulge corresponding to the children of baby boomers, also known as Generation Y, between the ages of 25 and 39 (Figure 2). Sandwiched between the two bulges is Generation X. Also evident from the data is an upward trend in the young cohorts representing perhaps the grandchildren of the baby boomers. This has implications for day cares and schools as the current 0-4 cohort is the biggest such cohort in the past 20 years. This may signal a school capacity issue starting with the elementary school and working its way up as the cohort ages. The data also suggests that there will be a need for more housing that can accommodate families with young children.

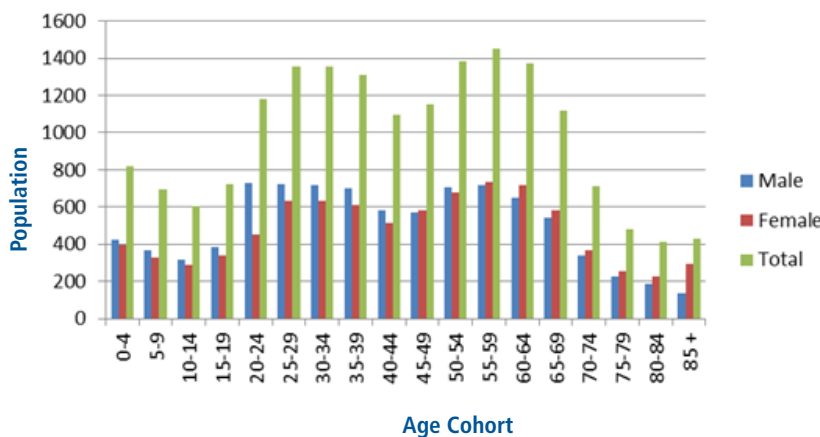


Figure 2. Population by age cohort based on 2016 Federal Census data. Draft figure.

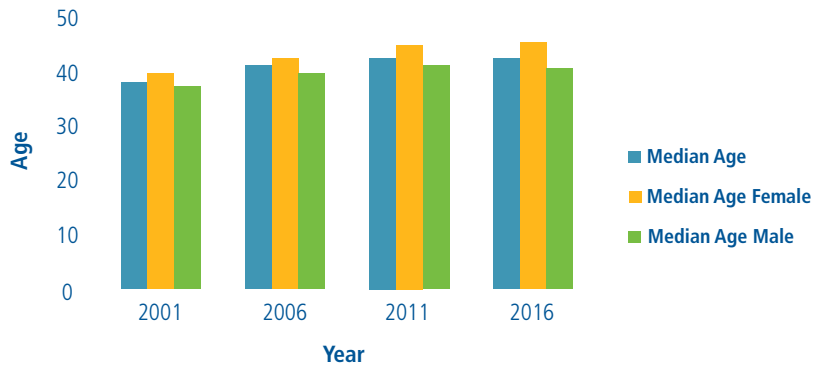


Figure 3. Median age of Esquimalt's population based on Federal Census data.

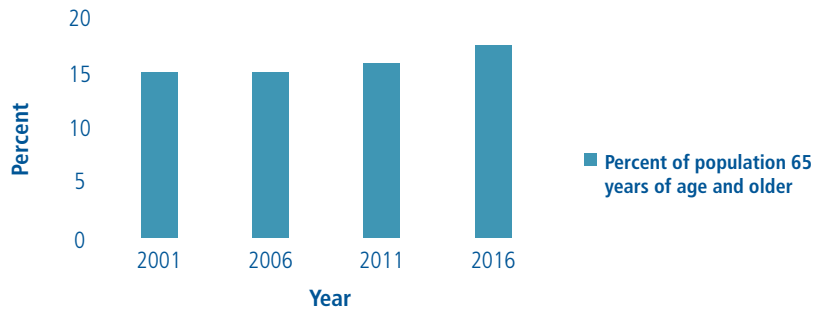


Figure 4. Percent of Esquimalt's population over 65 years of age from 2001 to 2016 based on Federal Census data.

Figures 3 and 4 demonstrate the trend in Esquimalt's aging population as both the median age and the percent of the population over 65 years of age slowly increase. This has implications in terms of everything from the types of housing required to the design of streets.

2.3.2 Income

The total household income by income bracket for the approximately 8480 households in Esquimalt is shown in Figure 5. The largest number of households is found in the \$100,000 to \$124,999 income bracket. The median total income of households in 2015 for Esquimalt was \$60,424 compared to \$69,665 for the Capital Regional District. The average total income of households in 2015 for Esquimalt was \$72,453 compared to \$88,438 for the Capital Regional District.

An important issue addressed by this plan relates to affordable housing. Figure 6 shows the benchmark housing values for Esquimalt as of February, 2018. There are a myriad of variables that affect the ability of an individual or a household to purchase a home, however, under normal circumstances it would not be possible for a household earning the average household income in Esquimalt to purchase a single family home. They could however, probably purchase a condo apartment. This has direct policy implications related to the type of housing that should be built in order to allow residents to purchase a home.

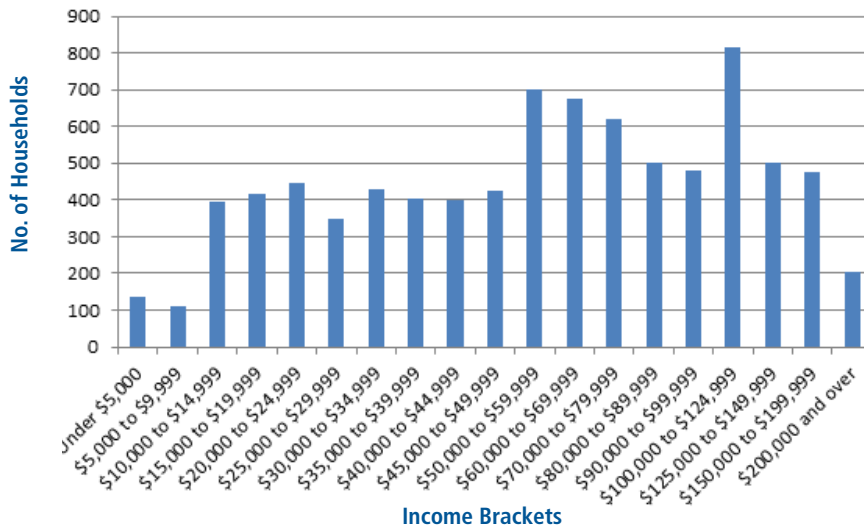


Figure 5. Total household income in 2015 by income bracket. Draft figure..

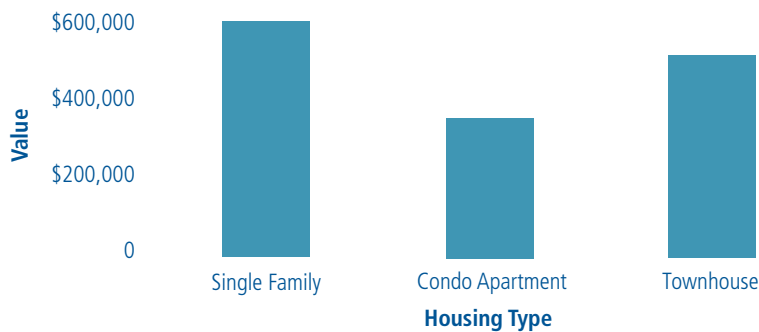


Figure 6. Benchmark housing values for Esquimalt, February 2018 (source: Victoria Real Estate Board)



2.4 Regional Profile

Esquimalt, with a population of approximately 17,655, is one of four “core municipalities” within the Capital Region. The municipality has one of the highest ratios of jobs to residents. CFB Esquimalt, located primarily in Esquimalt, is one of the region’s largest employers.

Through the Capital Regional District, residents of Esquimalt have access to services such as regional parks, recycling, an affordable housing portfolio, water supply and waste disposal systems.

2.5 Planning Context

The area that forms the Township of Esquimalt was settled by two First Nations — the Esquimalt and the Songhees. Members of these Nations reside on lands adjacent to Esquimalt’s north-western boundary.

The name Esquimalt reflects its maritime setting. In the language of the native Coast Salish peoples, Es-woy-malth means “place of shoaling waters.”

Looking into the foreseeable future, Esquimalt is likely to become a focus of renewed growth and redevelopment. The reasons for this include:

- Proximity and easy access to the main employment areas in the Capital Region;
- Established and attractive community amenities — schools, parks, trails, services, shopping;
- More than ten kilometres of saltwater shoreline and expansive water and mountain views from higher elevations;
- Affordable housing in comparison with many other areas of Greater Victoria, both rental and home ownership;
- Significant areas for potential redevelopment, particularly areas that may be surplus to the Department of National Defence. (Almost 30 percent of Esquimalt’s land base is under the jurisdiction of the Federal Government); and
- Mature residential character areas and many features of significant heritage interest.



3 REGIONAL CONTEXT STATEMENT



3.1 General

The Regional Context Statement (RCS) specifically identifies the relationship between the Official Community Plan (OCP) and the Regional Growth Strategy (RGS). The RGS places the Township completely within the Urban Containment Policy Area, therefore, those portions of the RGS that pertain to rural communities such as Section 1.2 are not applicable to the Township and are not addressed by this RCS. Likewise, any RGS content solely related to the West Shore or the Saanich Peninsula are not addressed by this RCS. On the other hand, housing, transportation, regional district services, parks and natural areas, and economic development within the Township are inextricably linked to the same matters in the region. Through its acceptance of the Regional Growth Strategy (RGS), Council acknowledges its responsibility to support the goals and aspirations of the RGS through its OCP. To this end, the following relationships between the RGS and the OCP are specifically identified as being mutually supportive:

3.2 Environment and Infrastructure

Potable water for the Township is provided by the City of Victoria which obtains its raw water from the CRD. The RGS addresses the need to use this resource in a cost-efficient and effective manner. In response, the OCP contains policies promoting densification which provides for a more efficient system use of potable water. In addition, the OCP contains a development permit area designated for water conservation. It and other development permit areas include the following design guidelines that will help reduce per-capita demand thereby allowing for future growth with reduced increases in supply:

1. Use drought resistant vegetation in landscaping;
2. Use stormwater for irrigating landscaping;
3. Use high-efficiency irrigations systems for landscaping;
4. Use recirculated water systems for pools and fountains; and
5. Reuse water where possible (the new wastewater treatment plant is a recent example of a development that will reuse waste water for irrigation purposed).

In addition to containing a development permit area designated for the conservation of water, the OCP also contains a development permit area designated for the protection of the natural environment, its ecosystems and biological diversity. This specifically supports policy #4 in section 2.1 of the RGS.

3.3 Housing and Community

The RGS seeks to provide for an adequate, accessible and diverse supply of affordable housing across the housing spectrum. The OCP responds to this policy by providing for increased densification through provisions for suites in duplexes, detached accessory dwelling units, and increased height and density for medium density residential housing and local commercial mixed-use designated land. Specifically, the OCP identifies the following lands as areas for residential densification:

1. Esquimalt Town Square,
2. West Bay Triangle,
3. Esquimalt Road corridor,
4. Land in the vicinity of the intersection of Admirals Road and the E&N Rail line, and
5. Land lying north of Esquimalt Road between Admirals Road and CFB Esquimalt.

The Plan also supports the provision a variety of housing types and tenures delivered by both market and non-market developers to support the housing needs of a diverse demographic.

The RGS also seeks to avoid locating new development in areas with high seismic hazard. In response, the OCP contains a provision that may require a land owner to submit a Tsunami Report prior to rezoning, subdividing or developing any lands abutting any body of water connected to the Pacific Ocean. In addition, lands adjacent to any body of water are designated as a development permit area for protection of development from hazardous conditions, particularly Tsunami, flooding, landslip, erosion, and other natural hazards.

Finally, the housing and community section of the RGS contains a policy related to climate change adaptation. In response, the OCP contains policies requiring the community to be prepared to be adaptable and resilient to the impacts of climate change. Specifically, the policies require the Township to consider including climate change projections, risks, and measures for dealing with more frequent and severe weather events in its strategies, plans and policies. In addition, the Township is to consider information on climate change risk and vulnerabilities when evaluating new development proposals.

3.4 Transportation

The major road network identified in Schedule "C" is consistent with regional multi-modal network identified in the July 2014 Capital Regional District Regional Transportation Plan that is referred to in Section 4.1 of the RGS. This is supported by policy under Sections 10.6.1. and 10.6.2. In addition, the OCP recognizes roads as a community resource that enhance community conviviality and social bonds. Furthermore, the OCP responds to the RGS related to transportation by encouraging denser development along transit routes, encouraging the provision of bicycle related facilities in new developments, providing for the provision of more bicycle infrastructure on the public road right of way where it can be demonstrated through proper transportation studies that it is justified, by encouraging development adjacent to public transit routes to incorporate bus shelters into their design, and by supporting parking reductions where supported by a parking study. In addition, the OCP encourages multi-modal street design.

3.5 Regional District Services

The Township is host to the Core Area Waste Water Treatment Plant and the Macaulay Point Pump Station, two vital components to the Core Area's waste water treatment system. The OCP identifies McLoughlin Point as the location of the Core Area Waste Water Treatment Plant and Macaulay Point as the location of the Macaulay Point Pump Station.

3.6 Parks and Natural Areas

Congruent with the RGS, the protection of the natural environment is embedded in the Township's Vision Statement which is located at the beginning of the OCP. Although bereft of any regionally significant parks, the Township is blessed with abundant parkland, some of which contains significant natural areas. In addition, the OCP supports the continued development of Esquimalt's network of parks and open spaces to meet the needs of users. It also promotes the development of a multi-functional network of greenways.

Further, the Plan promotes a balanced approach between recreational use and protection and enhancement of areas of natural foreshore habitat for fish, birds and other wildlife.

3.7 Economic Development

As the home of two of the region's major employers, the Township is cognizant of the importance that it plays in the region's economy as outlined in the RGS. These employers have attracted global corporations which have located offices in the region. Through its densification policies, the OCP strives to house employees closer to the employment centres thereby helping to create compact communities and reducing greenhouse gas emissions by reducing the need to commute. In addition, the OCP supports the attraction of a wide range of business types including advanced-technology, green economy, and knowledge based.

3.8 Greenhouse Gas Emissions Reduction

The RGS contains targets to reduce community greenhouse gas emissions by 33 percent (from 2007 levels) by 2020, and by 61 percent by 2038. The OCP contains a target to reduce greenhouse gas emissions by at least 38 percent by 2030 and to become carbon neutral by realizing zero net community greenhouse gas emissions by 2050. The Plan encourages better infrastructure for walking and cycling and promotes the use of best practices such as:

1. Seeking partners in piloting innovative approaches to greenhouse gas emission reduction;
2. Developing transportation demand management plans with major employers;
3. Promoting local agriculture;
4. Encouraging the use of car sharing;
5. Increasing density along transit corridors; and
6. Designating extensive areas a neighbourhood commercial mixed use.

The Plan also designates the entire Township as a development permit area for "Energy Conservation and Greenhouse Gas Reduction" and it supports Section 7.1 of the RGS to significantly reduce community-based greenhouse gas emissions.

3.9 Population Projections

The RGS projects that by 2038, Esquimalt will have a population of 18,300, an increase of 10.2 percent from 2011. Between 2011 and 2016 Esquimalt's population increased by 8.9 percent to 17,655. Based on past trends, the Township's population could exceed the 2038 projection by the year 2021. This reflects that the Township is working well to achieve regional interests of densification and being a complete compact community. It is noted that the RGS states that the CRD will update the projections using the most recent census data at the time of the first update to the 2018 RGS. Pursuant to Section 447 (1) (b), the Township will make the OCP consistent with the Regional Growth Strategy at the time that the Regional Growth Strategy is amended to reflect the revised projections.

3.10 Compact Complete Communities

As per the direction of the RGS, the OCP is replete with policies supporting the development of Esquimalt as a compact complete community. These include recognizing the Esquimalt Town Centre as the principle commercial area within Esquimalt as well as the main civic and recreational area. The Esquimalt Town Square is supported by a series of Neighbourhood Commercial Mixed – Use areas spread throughout the community. In addition, multi-modal transportation is supported as is a range of housing types. Esquimalt strives to have one hundred percent of its population living in a complete community.

3.11 Food and Agricultural Systems

Although the Township has only one parcel of Agricultural Land Reserve, namely the Gorge Vale Golf Club, it recognizes the importance of urban agriculture and supports the relevant principles and policies of Section 6.1 of the RGS through the inclusion of a number of objectives and policies in the OCP related to urban agriculture. The policies cover a broad spectrum of urban agriculture from support for community gardens to support for farmer's markets. In addition, a number of development permit guidelines are related to the promotion of urban agriculture.

3.12 Summary

The OCP brings the land use and transportation aspirations of the Regional Growth Strategy to the local scale. To the degree to which the criteria, principles, and policies of the Regional Growth Strategy apply to the Township, the Official Community Plan efficiently and effectively translates them to a local governance scale which will guide the decisions of Council with respect to the matters covered by both documents.



PART 2 POLICIES



4

LAND USE PLANNING



4.1 Land Use Designations

The existing land uses and development patterns are shown on the “Present Land Use Designation Map.” This map provides a framework for the detailed regulations contained in the Zoning Bylaw. Future land use, development and redevelopment will be in accordance with the land use designations shown on the “Proposed Land Use Designation Map,” together with the guidelines set out in Development Permit Areas. Owners of land may choose to develop or redevelop their land in accordance with either the “Present Land Use Designation Map” or the “Future Land Use Designation Map” as long as the zoning of the land allows for the proposed development or redevelopment.

4.2 General Development

OBJECTIVE: To knit together urban pattern, use, and form into a cohesive land use fabric that enhances community and protects the environment.

Policy

To consider and implement (as appropriate) the full breadth of statutory planning tools under the Local Government Act and Community Charter, as well as any other authority or agreements, to further enhance development within Esquimalt, including to achieve high quality design for the community.

Policy

Encourage development proponents to demonstrate how the design of a new development addresses crime prevention principles.

Policy

Encourage universal design in all new developments.

Policy

Encourage owners to locate new buildings in order to retain protected trees, as well as healthy and mature trees, existing on the property.

Policy

Encourage new developments that are sited and designed to maintain or enhance public view corridors. Public view corridors are shown on the “Public Views Corridors Map” (Schedule “G”).

4.3 Temporary Use Permits

OBJECTIVE: To allow for the temporary use of a parcel of land for a use that is not permitted on the parcel under current zoning.

Policy

The entire Township is designated as an area where temporary uses may be allowed.

Policy

Temporary Use Permits may be approved and issued for residential, commercial, industrial, recreational, public utility, agricultural, and institutional uses, including any combination of the foregoing uses, that do not conform to the permitted uses for a parcel as prescribed in the Zoning Bylaw.

Policy

Temporary Use Permits may be issued for a period in accordance with the Local Government Act, presently of up to three-years and may be renewed for up to an additional three-year period.

Policy

In considering an application for a Temporary Use Permit, Council may consider the following:

1. Suitability of the site for the proposed use;
2. Compatibility with adjacent land uses;
3. Potential environmental impacts, and
4. Potential economic impacts.

Policy

Temporary Use Permits may be issued with conditions, including with respect to the following:

1. Requirement for security deposits in order to ensure removal of the use and related buildings and structures on termination or expiry of the permit, rectification of unsafe conditions and/or damage to the environment as a result of the temporary use and/or compliance with any conditions associated with the approval and issuance of a Temporary Use Permit;
2. Restrictions on duration and timing of use;
3. Restrictions on the intensity of use;
4. Provision of adequate parking;
5. Provision of landscaping and buffering;
6. Provisions related to the structures and buildings associated with the temporary use;
7. Provisions for traffic management;
8. Provisions for control of nuisances including but not limited to: noise, dust, fumes, odour, glare, vibration, and light; and
9. To allow for the better integration of the temporary use with the surrounding neighbourhood and the community of Esquimalt as a whole.

4.4 Development Approval Information Area

4.4.1 Background

Pursuant to Section 485 of the Local Government Act, Statutes of BC, an OCP may do one or more of the following:

1. Specify circumstance in which development approval information may be required;
2. Designate areas for which development approval information may be required;
3. Designate areas for which, in specified circumstances, development approval information may be required.

The purpose of Development Approval Information is to provide information on the anticipated impact of a proposed activity or development on the community, including, without limiting this, information regarding impact on such matters as the following:

1. Transportation patterns including traffic flow;
2. Local infrastructure;
3. Public facilities including schools and parks;
4. Community services; and
5. The natural environment of the area affected.

4.4.2 Designated Area

The entire area within the Township of Esquimalt municipal boundary is designated as a Development Approval Information Area.

4.4.3 Justification – Special Conditions and Objectives

Special Conditions

As outlined throughout this Plan, there are many special conditions and features of the Township to justify the designation of the entire Township for which development approval information may be required. These include but are not limited to the following:

- The Township is surrounded by sensitive shoreline with high ecological values and biological diversity, abuts a National Migratory Bird Sanctuary, is located in a rare Garry Oak Ecosystem, is a home of rare plant and animal species, and has other important environmental values and considerations.
- On September 1st, 2012, Historic Esquimalt celebrated 100 years as an incorporated Township. First Nations people of the Coast Salish linguistic group had used the area now known as Esquimalt for approximately 4000 years before the advent of European settlement. Cultural and Heritage resources are a significant component of the Township's history and urban fabric, with many examples of high aesthetic values and other significance.
- Esquimalt possesses two of the finest natural harbours on the west coast of the Americas: Esquimalt Harbour is the headquarters of the Royal Navy's Pacific Squadron, complete with graving dock and ship repair and fitting, and West Bay is a thriving, attractive and walkable harbour front neighbourhood.

- The Township is largely developed and/or urbanized with few (if any) greenfield development sites, and undergoing significant redevelopment and renewal. Infrastructure is, in some cases, old, and assessing and allocating capacity for new development and redevelopment can be challenging without appropriate assessment information including with respect to highways, fire protection systems, water supply systems, sewage systems, solid waste disposal and recycling facilities, energy and communications utilities, and local parking facilities. Municipal infrastructure represents a fundamental component of the efficient and effective functioning of the Township.
- The Township has numerous local facilities, including schools, parks, health care services, local transportation services including public transit, that may be impacted by medium and high density residential development.
- The Township is located in one of the most earthquake prone areas in Canada, and must contend with tsunami, land slip, erosion and other natural hazardous conditions.

Township objectives

- To protect the ecological integrity of Esquimalt's shorelines.
- To protect the habitat of Migratory Birds from disturbances related to development.
- To protect the ecological integrity of Esquimalt's remaining Garry Oak Ecosystem.
- To conserve energy and water.
- To protect existing municipal infrastructure and properly plan for future infrastructure needs.
- To understand, mitigate and otherwise plan for potential impacts of traffic generated by proposed developments and redevelopments on the Township's road system. Understanding potential traffic impacts of development that involves significant vehicle traffic is crucial to making sound land use and development decisions.
- To ensure that existing local facilities can adequately service new residents and properly plan for future service needs.
- To protect the integrity of the Township's economy, which is critical to the well being of its residents.
- To protect the integrity of the Township's tax base, as that base is fundamental to ensuring that the Township can provide the necessary services and infrastructure to its residents.
- To plan for earthquake preparedness and resiliency, including to preserve life and reduce property damage.
- To avoid approving land uses and developments that may negatively impact near-by developments or land uses or may be negatively impacted by existing land uses or developments, considering that nuisances such as noise, dust, glare, and odour can negatively impact the quality of life for the Township's residents and business owners.
- To protect, where feasible, the Township's significant cultural and heritage resources.
- To maintain a high quality built environment, including aesthetic values, as this contributes to the vibrancy of the community.
- To facilitate the implementation of Development Permit purposes and guidelines.

In accordance with the Local Government Act, the Township identifies additional policies and procedures in a separate bylaw for the implementation of Development Approval Information requirements.

4.5 Sand and Gravel

There are no lands within the Township suitable for sand and gravel extraction.

4.6 Restriction on Land that is Subject to Hazardous Conditions

All lands abutting any body of water connected to the Pacific Ocean are in a potential Tsunami Hazard Area. Prior to rezoning, subdividing, or developing these lands a landowner may be required to submit a Tsunami Report with their application.

5

HOUSING & RESIDENTIAL LAND USE



5.1 General: Anticipated Housing Needs in the Next Five Years

Table 1 and Figure 7 show the housing statistics for Esquimalt based on the 2011 and 2016 Federal Censuses. Note that there are significant differences in the trends for each housing type. Single detached houses decreased by 1.7 percent while apartments in buildings that have five or more stories increased by 23.1 percent. Table 1 clearly shows a trend toward multi-family dwellings and away from single detached dwellings.

TABLE 1. PERCENT CHANGE IN HOUSING TYPES BETWEEN 2011 AND 2016.

Dwelling Type	Number of Dwelling Units - 2011 Census	Number of Dwelling Units - 2016 Census	Percent Change 2011 to 2016
Single Detached House	2030	1995	-1.7
Apartment in building that has five or more stories	455	560	23.1
Semi-detached house	665	690	3.8
Row house	385	405	5.2
Apartment or flat in duplex	795	910	14.5
Apartment in a building that has fewer than five storeys	3695	3910	5.8
Other single - attached house	15	15	0.0
Total	8040	8485	5.5

Table 2 contains simple arithmetic population projection estimates based on various growth rate scenarios. The net increase in population under each scenario is then used to estimate the number of new housing units that will be required in the next five years based on an average household size of 2.0 as per the 2016 Federal Census.

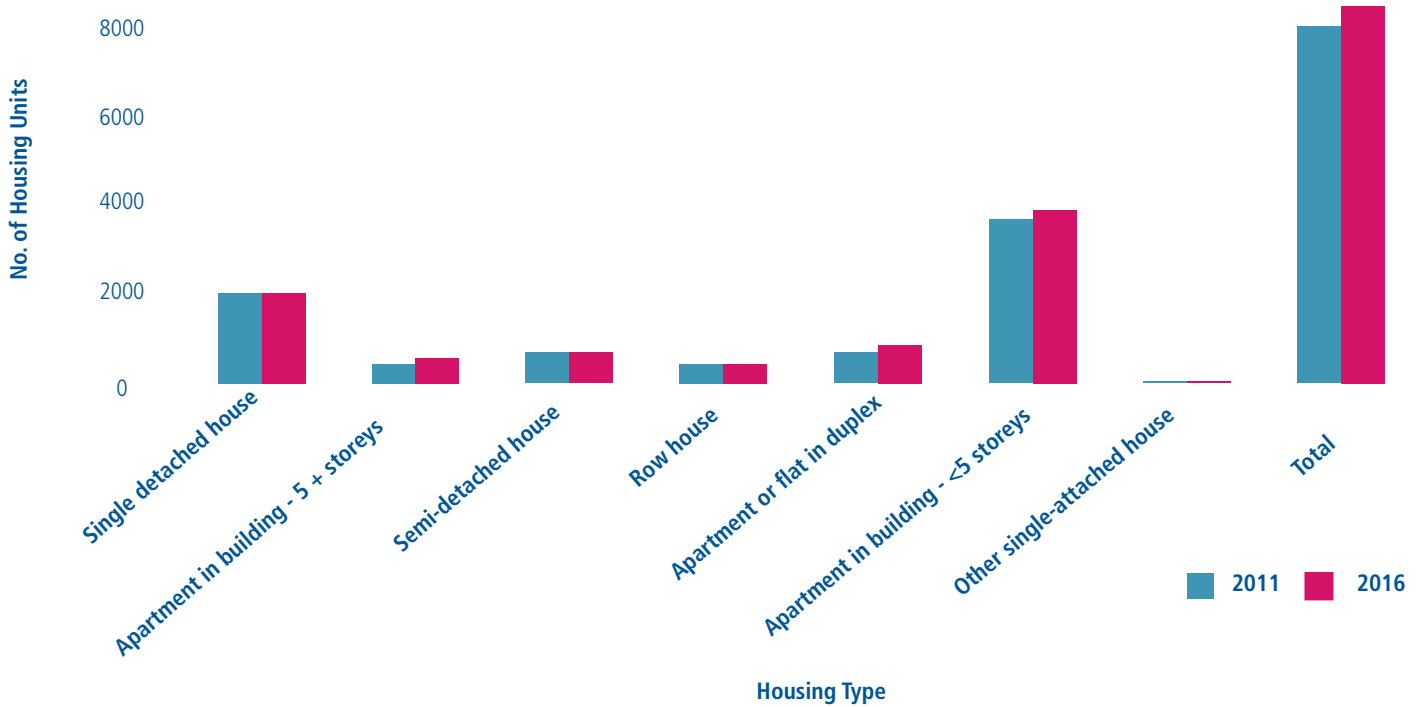


Figure 7. Percent change in housing types between 2011 and 2016

TABLE 2 POPULATION GROWTH SCENARIOS FOR THE NEXT FIVE YEARS AND ESTIMATED NUMBER OF NEW HOUSING UNITS REQUIRED.

2016 Population	Rate of Growth Over Five Years (%)	Estimated 2021 Population	Increase in Population	Estimated Number of New Housing Units Required at an Average of 2.0 People Per Unit
17,665	2.5	18107	442	221
17,665	5	18548	883	442
17,665	7.5	18990	1325	662
17,665	10	19432	1767	883

Table 3 summarizes the number of units in all developments where Council has approved the zoning but the developments have not received an occupancy permit. The data in the table indicates that 664 housing units of various forms and tenures have been approved but have not yet receive an occupancy permit. The current approvals would be sufficient to accommodate a population increase of approximately 7.5 percent over the next five years based on an average household size of 2.0. It should be noted that in order to maintain a balanced housing market the supply of housing should exceed the demand for housing. Given the amount of land designated for multi-unit development as indicated in the Future Land Use Designation Map that is not yet yielding it maximum potential number of dwelling units, the Township has more than adequate land to accommodate its housing needs for the next five years at growth rates beyond 10 percent, assuming that zoning approvals coincide with the maximum development potential allowed by the OCP.

TABLE 3. ESTIMATED NUMBER OF HOUSING UNITS THAT HAVE RECEIVED ZONING APPROVAL BUT NOT AN OCCUPANCY PERMIT.

Dwelling Type	Location	Number of Proposed Units - Rental	Number of Proposed Units - Owner	Density (units per net ha.)	Status
Single Detached House (Infill)	Various (infill)		7	21.6	
Apartment in building that has five or more stories	Esquimalt Town Square	34	68	130	Development permit approved
	Legion	152		415	Zoning approved
	English Inn		173	165	Development permit approved
	856 Esquimalt Road		168	230	Zoning approved
Semi-detached house	Various		4	29.5	Ongoing
	English Inn		6	40	Development permit approved
Apartment or flat in duplex	Various				
Apartment in a building that has fewer than five storeys	615 Fernhill		10	130	Zoning approved
	Triangle Lands			160	Zoning approved
	826 Esquimalt Road		30	330	Under construction
Other single - attached house	West Bay				
Total		186	478		

OBJECTIVE: Support expansion of housing types within Esquimalt while addressing concerns such as tree protection, parking, traffic, noise, effects on neighbouring properties, and neighbourhood character.

Policy

Support the inclusion of secondary suites within present and proposed low density residential land use designated areas.

Policy

Support the inclusion of detached accessory dwelling units on appropriate low density residential land use designated areas where only one principal dwelling unit exists.

Policy

Consider new townhouse residential proposals with a Floor Area Ratio of up to 0.70, and up to three storeys in height, in areas designated Townhouse Residential on the “Proposed Land Use Designation Map,” provided the design responds effectively to both its site and surrounding land uses.

Policy

Recognize that float homes provide an alternative housing choice, and support the continued existence of Esquimalt’s only float home community located in West Bay, as designated on the “Present Land Use Designation Map” and “Proposed Land Use Designation Map.”

Policy

Support the inclusion of live/work units on residential parcels located outside of areas designated commercial on the “Proposed Land Use Designation Map” and on Major Roads as designated on the “Road Network Map” (Schedule “C”).

Policy

Recognize short-term accommodation use as appropriate within designated residential land use areas only where appropriate zoning has been sought and secured.

Policy

Support the development of a variety of housing types and designs to meet the anticipated housing needs of residents. This may include non-market and market housing options that are designed to accommodate young and multi-generational families, the local workforce, as well as middle and high income households.

Policy

Encourage the development of rental accommodation designed for a variety of demographic household types, including young families.

5.2 Low Density Residential Redevelopment

OBJECTIVE: Strive for redevelopment and infill development that improves and enhances the appearance and livability of neighbourhoods and the community as a whole.

Policy

Proposed redevelopment or infill within present low density residential land use designated areas should be built to high quality design and landscaping standards and respond sensitively to existing neighbourhood amenities.

Policy

Consider the inclusion of secondary suites in infill developments where it is demonstrated that neighbourhood impacts can be mitigated.

Policy

Discourage new applications for infill housing, including rezoning and subdivision, for panhandle lots in the 1100 and 1200 blocks of Old Esquimalt Road and the 600 block of Fernhill Road.

5.3 Medium and High Density Residential Development

OBJECTIVE: Support compact, efficient medium density and high density residential development that integrates with existing and proposed adjacent uses.

Policy

Encourage new medium density and high density residential development with high quality design standards for building and landscaping and which enhance existing neighbourhoods.

Policy

Prioritize medium density and high density residential development in proposed land use designated areas that:

1. reduce single occupancy vehicle use;
2. support transit service; or
3. are located within close proximity to employment centres.

Policy

Consider new medium density residential development proposals with a Floor Area Ratio of up to 2.0, and up to six storeys in height, in areas designated on the “Proposed Land Use Designation Map.”

Policy

Consider new high density residential development proposals with a Floor Area Ratio of up to 3.0, and up to 12 storeys in height, in areas designated on the “Proposed Land Use Designation Map.”

Policy

Notwithstanding other policies set out in this OCP, maximum heights in medium density and high density residential land use designated areas in West Bay are limited to those parcel heights identified in the West Bay Development Permit Area.

Policy

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy

Recognize, for the purposes of density bonuses, “amenities” may include but are not limited to:

1. Privately-owned, publicly-accessible open space;
2. Public art;
3. Contributions towards the enhancement of public recreation facilities;
4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
6. Group daycare and respite facilities for children and adults;
7. Preservation of heritage structures, features or assets;
8. Affordable housing units;
9. Special needs housing units;
10. Community gardens;
11. Enhanced green family play space for residents;
12. Public space improvement supporting and surrounding transit stations; and
13. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council.

Policy

A mix of dwelling unit sizes should be provided in medium density and high-rise residential land use designated areas in order to meet the varying housing needs of Esquimalt residents.

Policy

Buildings with shallow setbacks must step down to no more than three storeys at street level in order to provide an appropriate human scale along the sidewalk.

Policy

Encourage the incorporation of spaces designed to foster social interaction.

Policy

Encourage the installation of electric vehicle charging infrastructure in medium and high density residential developments.

5.4 Affordable Housing

OBJECTIVE: To encourage a range of housing by type, tenure and price so that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

Policy

Encourage the provision of affordable housing by the private market and the non-profit housing sector. Partnerships between private, public or non-market housing providers may be supported. These might include innovative approaches such as limited equity, rent-to-own, co-op, mixed market and non-market projects.

Policy

Encourage the placement of new rental, affordable, special needs, and seniors housing in accordance with designated residential land use areas as they are integral components of inclusive neighbourhoods.

Policy

Avoid the spatial concentration of affordable and special needs housing in neighbourhoods.

Policy

Promote housing agreements to ensure that all strata units have the opportunity to be used as long-term residential rentals within strata buildings.

Policy

Consider offering a Revitalization Tax Exemption to all new or renovated purpose-built multi-unit rental buildings within the Township.

Policy

Consider bonus density, parking relaxations or other development variances where a development proposal includes affordable, special needs or seniors housing. This may apply to both market and non-market housing, and mixed-use proposals. A housing agreement may be entered into between the Township and the owner.

Policy

Work in co-operation with the provincial and federal governments, the real estate community, social service agencies, faith-based organizations, service clubs and other community resources

to provide affordable housing and support services for seniors, the frail elderly and other special needs groups.

Policy

Encourage the provision of missing middle housing types such as two-unit dwellings (duplexes), townhouses and small lot infill as one avenue to address housing affordability.

5.5 Age-friendly Housing

OBJECTIVE: To expand and protect seniors housing in Esquimalt to enable citizens to “age in place.”

Policy

Support and facilitate development of multi-generational housing, including in medium and high density residential developments.

Policy

Encourage child friendly developments that provide appropriate amenities such as outdoor play areas for young children that are well-separated from traffic circulation and parking areas.

Policy

Encourage adaptable design for all dwellings created through rezoning.

Policy

Encourage more accessible housing for people with mobility limitations on the ground floor of medium and high density residential buildings.

Policy

Support aging-at-home through means such as low or no fee permits for modifications.

Policy

Encourage development of more assisted living residences.

Policy

Promote the development of a complex care facility within Esquimalt.

Policy

Promote modifying homes to enable shared housing for seniors.

Policy

Encourage the development of seniors housing that is within close proximity and accessible to services and amenities.

Policy

Recognize that the provision of bicycle storage may be waived or varied if, in the opinion of Council, there is no demonstrated need, such as in a congregate care facility.

6

COMMERCIAL & COMMERCIAL MIXED-USE LAND USE



6.1 General

The neighbourhood commercial mixed use and Commercial/Commercial Mixed-use Areas are designated on the “Present and Proposed Land Use Designation Maps” (Schedules “A” and “B”).

OBJECTIVE: Esquimalt is a complete community where commercial enterprises serve the needs of area residents, local businesses, and visitors.

Policy

Encourage a mix of uses in the commercial/commercial mixed-use areas.

Policy

Exclusive commercial development is appropriate in commercial/commercial mixed-use areas.

Policy

All commercial/commercial mixed-use areas are designated Development Permit Areas, in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.

Policy

Lands outside the Commercial/Commercial Mixed-use designation on the “Proposed Land Use Designation Map” should not be considered for commercial zoning unless the following criteria are met:

1. The project is desirable to the community and cannot be appropriately located within established commercial areas;
2. The density and scale of the project is sensitive to the prevailing character of surrounding lands;
3. The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
4. The project’s parking requirements can be satisfied on-site; and
5. The project’s parking requirements should not unduly affect neighbouring land uses.

Policy

In commercial mixed-use areas, buildings with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.

Policy:

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy

Recognize, for the purposes of density bonuses, “amenities” may include but are not limited to:

1. Privately-owned, publicly-accessible open space;
2. Public art;
3. Contributions towards the enhancement of public recreation facilities;
4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
6. Group daycare and respite facilities for children and adults;
7. Preservation of heritage structures, features or assets;
8. Affordable housing units;
9. Special needs housing units;
10. Community gardens;
11. Enhanced green family play space for residents;
12. Public space improvements supporting and surrounding transit stations; and
13. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council.

Policy

The majority of the frontage for commercial mixed-use buildings at the ground floor, should be commercial.

Policy

On corner sites, the ground floor commercial use in mixed-use buildings should wrap around the corner.

Policy

In commercial/commercial mixed-use developments, parking should be located behind or under the building.

Policy

The installation of electric vehicle charging infrastructure in commercial/commercial mixed-use developments is encouraged.

Policy

Encourage the provision of amenities such as mini parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas.

Policy

To encourage the use of bicycles, provision should be made in all commercial/commercial mixed-use developments for bicycle parking for employees and visitors.

6.2 Revitalization

OBJECTIVE: To encourage growth through revitalization and redevelopment of commercial areas.

Policy

Encourage public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.

Policy

Design and install entrance signage at key street locations, including entrances to Esquimalt.

6.3 Esquimalt Town Centre

OBJECTIVE: To foster the creation of an Esquimalt Town Centre that successfully integrates commercial, institutional and residential activity.

Policy

Esquimalt Town Centre is located along Esquimalt Road, between Constance Avenue and Joffre Street.

Policy

Esquimalt Town Centre will continue to be the principal commercial area within Esquimalt.

Policy

Encourage a range of retail stores, services, professional offices, community services, recreation, institutional, educational, and arts / cultural uses that encourage activity throughout the day and evening.

Policy

Encourage high quality development that supports and reinforces Esquimalt Town Centre as the main commercial, civic and recreational service centre. Designs are encouraged which incorporate a pedestrian orientation and include such features as sidewalk cafes, attractive landscaping, public art and other amenities.

Policy

Encourage high density mixed-use commercial/residential complexes in Esquimalt Town Centre.



Policy

Esquimalt Town Centre should continue to provide opportunities for Esquimalt's young people, including places for activities and programs.

Policy

Support the development of a transit hub and supporting infrastructure in the Esquimalt Town Centre.

Policy

Improvements to the Esquimalt Road frontage within Esquimalt Town Centre should encourage pedestrian use and facilitate connections to nearby public facilities and open spaces.

Policy

Create a clearly defined, well-landscaped, and well-lit pedestrian network throughout the Esquimalt Town Centre.

6.4 Neighbourhood Commercial Mixed-Use

OBJECTIVE: To encourage the development of a series of neighbourhood commercial mixed-use nodes throughout Esquimalt that will primarily serve the daily needs of the surrounding neighbourhood but may also include destination uses.

Policy

The Neighbourhood Commercial Mixed-Use areas are shown on the "Present and Proposed Land Use Designation Maps" (Schedules "A" and "B").

Policy

In areas designated as Neighbourhood Commercial Mixed-Use on the "Present and Proposed Land Use Designation Maps" consider proposals with a height up to six storeys and a Floor Area Ratio of up to 1.5 for the residential portion.

Policy

In areas designated as Neighbourhood Commercial Mixed-Use on the “Present and Proposed Land Use Designation Maps,” developments containing exclusively residential uses are acceptable.

Policy

Encourage retail outlets that sell healthy fresh foods.

Policy

In areas designated as “Neighbourhood Commercial Mixed-Use” on the “Present and Proposed Land Use Designation Maps” a building height of up to six storeys and a Floor Area Ratio of up to 2.0 is acceptable when the use of the building is exclusively residential.

Policy

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy

For the purposes of density bonuses, “amenities” may include but are not limited to:

1. Privately-owned, publicly-accessible open space;
2. Public art;
3. Contributions towards the enhancement of public recreation facilities;
4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
6. Group daycare and respite facilities for children and adults;
7. Preservation of heritage structures, features or assets;
8. Affordable housing units;
9. Special needs housing units;
10. Community gardens;
11. Enhanced green family play space for residents;
12. Public space improvements supporting and surrounding transit stations; and
13. Other as may be appropriate to the development proposal or surrounded community as deemed appropriate by Council.

6.4.1 Head Street and Esquimalt Road

OBJECTIVE: To create a vibrant commercial mixed-use node centred around the intersection of Esquimalt Road and Head Street.

Policy

Esquimalt's secondary commercial area is located along Esquimalt Road at Head Street.

Policy

Encourage redevelopment in the Head Street/Esquimalt Road Neighbourhood Commercial Mixed-use Area.

Policy

Redevelopment should occur at a neighbourhood scale.

6.4.2 West Bay

West Bay is a thriving, attractive and walkable harbour front neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbour front recreation, and sunny open spaces with strong connections to the harbour, make this a truly livable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and up-land ecology.

Policy

Encourage the West Bay commercial area be oriented to pedestrians and boat traffic.

Policy

Notwithstanding other policies set out in this OCP, maximum heights in Neighbourhood Commercial/Neighbourhood Commercial Mixed-Use land use designated areas in West Bay are limited to those heights identified in the West Bay development permit area.



6.4.3 Tillicum Road/Craigflower Road

OBJECTIVE: To encourage the development of new services that meet the needs of the residents of the Panhandle, Gorge Vale Golf Course and the area east of Tillicum Road.

Policy

As the nearby residential areas intensify and population grows, redevelopment of this commercial area is encouraged as a mix of small shops and services, potentially with residential above.

6.4.4 Mixed-Use: English Inn

The English Inn consists of a 1.8-hectare parcel of land as shown on the “Present and Proposed Land Use Designation Maps” (Schedules “A” and “B”). The English Inn Mixed-Use land use designation is a variation of the Commercial/Commercial Mixed-Use designation intended to achieve heritage protection of the significant features and trees on the property and the main building.

OBJECTIVE: To create a vibrant mixed use development that integrates within the existing neighbourhood.

Policy

The English Inn Mixed-Use designation permits and encourages a mixture of commercial (tourist accommodation), residential and institutional uses (congregate care senior citizens apartments and similar), but it also allows the possibility of only residential uses.

Policy

Subdivision, covenants and variances to parking and zoning regulations may be considered to further facilitate protection and maintenance of the main Maclure House building concomitant with redevelopment of the remainder of the property.

Policy:

Given the land contours and existing trees and vegetation, the uses in the main heritage building itself may require some parking spaces to be provided on adjacent lands if/when the original parcel is subdivided.

7 BUSINESS LAND USE



7.1 General

Lands designated Business on the “Present and Proposed Land Use Designation Maps” (Schedules “A” and “B”) will be used for industrial and accessory uses. The lands are very valuable as employment and wealth generating lands and should be protected as such.

All business areas are designated a Development Permit Area.

7.2 Esquimalt Business Park

OBJECTIVE: To encourage the maintenance and viability of the business sector within Esquimalt to provide for local employment and a diversified tax base.

Policy

Consider innovative proposals that intensify the light industrial uses.

Policy

Industrial and business uses associated with clean technology are strongly encouraged to locate in the industrial park in order to take advantage of the proximity to the region’s high tech sector.

Policy

Industrial and business uses associated with ship repair and fitting are strongly encouraged to locate in the industrial park in order to take advantage of its proximity to the federal graving dock.

Policy

Support public and private initiatives that enhance the aesthetics and identity of the Esquimalt Business Park.

Policy

Encourage the use of bicycles in new industrial and business developments with the provision of secure bicycle parking and shower facilities for employees and visitors.

Policy:

Reductions in off-street parking requirements may be considered in new industrial and business developments where the following are provided:

1. an appropriate number of secure bicycle storage spaces;
2. shower and change rooms;
3. visitor bicycle parking spaces; and
4. building location within short walking distance of a regional bus route.

7.3 Public Services and Procurement Canada Land

OBJECTIVE: To support the economic viability of the graving dock through the provision of work-force housing and the provision of land in the industrial park for allied industries.

Policy

Encourage the continued use of the Public Services and Procurement Canada's drydock for industrial purposes.

Note: In the event that Public Services and Procurement Canada lands are disposed of or privatized, the lands will remain industrial and be subject to local government bylaws and development permit guidelines. Until such time, the Township respects the Federal prerogative to use its lands for federal purposes and respectfully requests the Government of Canada and its departments and agencies to respect the OCP if the property is not used for Federal purposes.

7.4 McLoughlin Point Special Use

The Capital Regional District included McLoughlin Point in the regional Liquid Waste Management Plan for purposes of a sewage treatment plant, a public facility as shown on Schedule F. This Plan recognizes the provincial prerogative under the Environmental Management Act, and seeks to facilitate a variety of high technology, industrial, institutional, business and professional mixed-uses of impeccable design and cohesive operation, preferably with public access primarily for educational purposes, to showcase the development and proposed sewage treatment plant to the community and world, particularly given the prominence of McLoughlin Point as the entrance to Victoria Harbour.

OBJECTIVE: To encourage the development of the site for a variety of uses in a manner that reflects its prominent location and the ecology of the area.

Policy

At the time of redevelopment for other uses that do not include the sewage treatment plant, any plan should be prepared in consideration of the surrounding DND lands, and to alternative uses for this site that complement and support a mixed-use (including high-tech, clean industrial) redevelopment plan of impeccable design with public access.

8 FEDERAL LANDS



8.1 General

A significant proportion of the Township's land base is owned and regulated by the federal crown (Schedule "A"). In addition to CFB Esquimalt and the Federal Graving Dock, the federal crown also owns and regulates the bed and shore of Esquimalt Harbour and West Bay. Although the Township cannot regulate land use owned by the federal crown when it is used for federal government purposes, the Township recognizes that the federal crown land is inextricably linked to other lands within the Township and it is therefore critical to coordinate the planning of land use between the two regulators to the greatest extent possible.

8.2 Federal Land

8.2.1 Federal Land: General

The Township acknowledges that the Federal Government's divestment policy generally gives priority to other government departments and First Nations when land is divested.

OBJECTIVE: To ensure that any divested federal lands are subject to appropriate land use planning prior to development.

Policy

Future land use on any lands divested by the Federal Government will be consistent with the objectives of this or any future OCP.

Policy

A comprehensive area redevelopment plan will be developed for any lands divested by the Federal Government. The plan is to be sympathetic to adjacent neighbourhoods and respectful of the heritage assets and values of the site.

8.2.2 Federal Land: CFB Esquimalt – Work Point

OBJECTIVE: To encourage future owners of any divested federal lands in the Work Point area to appropriately integrate new developments into adjacent neighbourhoods in a manner that is compatible with nearby land uses.

Policy

If divested by the Federal Government, it is envisaged that parts of the CFB Esquimalt - Work Point area could be redeveloped to a mix of townhouse and medium density residential developments surrounded by greenspace.

Policy

Continuous public waterfront access linking Macaulay Point Park with the West Bay Walkway and downtown Victoria will be a key planning principle in any future concept plan.

8.2.3 Federal Land: CFB Esquimalt – North Naden

OBJECTIVE: To ensure that any divested federal lands in the North Naden area are appropriately integrated into adjacent neighbourhoods and compatible with nearby land uses.

Policy

If the CFB Esquimalt – North Naden area is developed, natural greenspaces and a local neighbourhood park should be negotiated as part of any development proposal.

8.3 Public Services and Procurement Canada Land

OBJECTIVE: To support the economic viability of the graving dock through the provision of workforce housing and the provision of land in the industrial park for allied industries.

Policy

Encourage the continued use of the Public Services and Procurement Canada's dry dock for industrial purposes.

Note: In the event that Public Services and Procurement Canada lands are disposed of or privatized, the lands will remain industrial and be subject to local government bylaws and development permit guidelines. Until such time, the Township respects the Federal prerogative to use its lands for federal purposes and respectfully requests the Government of Canada and its departments and agencies to respect the OCP if the property is not used for Federal purposes.

9 INSTITUTIONAL & PUBLIC FACILITIES LAND USE



9.1 General

Institutional lands are identified on the “Present and Proposed Land Use Designation Maps” (Schedules “A” and “B”).

OBJECTIVE: To encourage the retention of Esquimalt’s civic, faith-based and educational institutions as important components of the social fabric of the community.

Policy

Institutional uses are generally considered to be appropriate within any land use designation and need not be specifically designated on the “Proposed Land Use Designation Map” (Schedule “B”). However, any proposal for new institutional zoning will be reviewed in detail and assessed on its own merits.

9.2 Federal Land

9.2.1 Federal Land: General

The Township acknowledges that the Federal Government’s divestment policy generally gives priority to other government departments and First Nations when land is divested.

OBJECTIVE: To ensure that any divested federal lands are subject to appropriate land use planning prior to development.

Policy

Future land use on any lands divested by the Federal Government will be consistent with the objectives of this or any future OCP.

Policy

A comprehensive area redevelopment plan will be developed for any lands divested by the Federal Government. The plan is to be sympathetic to adjacent neighbourhoods and respectful of the heritage assets and values of the site.

9.2.2 Federal Land: CFB Esquimalt – Work Point

OBJECTIVE: To encourage future owners of any divested federal lands in the Work Point area to appropriately integrate new developments into adjacent neighbourhoods in a manner that is compatible with nearby land uses.

Policy

If divested by the Federal Government, it is envisaged that parts of the CFB Esquimalt - Work Point area could be redeveloped to a mix of townhouse and medium density residential developments surrounded by greenspace.

Policy

Continuous public waterfront access linking Macaulay Point Park with the West Bay Walkway and downtown Victoria will be a key planning principle in any future concept plan.

9.2.3 Federal Land: CFB Esquimalt – North Naden

OBJECTIVE: To ensure that any divested federal lands in the North Naden area are appropriately integrated into adjacent neighbourhoods and compatible with nearby land uses.

Policy

If the CFB Esquimalt – North Naden area is developed, natural greenspaces and a local neighbourhood park should be negotiated as part of any development proposal.

9.3 Public and Private Service Facilities

There is a range of public and private service-oriented institutional facilities, such as health services, schools and child care facilities, seniors and youth services and emergency services, library services and places of worship and civic uses that contribute to the amenities and social fabric of Esquimalt. Established public facilities are shown on the “Public Parks and Facilities Map” (Schedule “F”).

OBJECTIVE: To encourage public and private service facilities that serve the needs of residents to locate in Esquimalt.

Policy

Encourage health services and wellness-related businesses to locate in Esquimalt.

Policy

Encourage a senior-friendly community environment, including medical and support services, pedestrian system, public transit service and facilities, and recreational opportunities that are designed with accessibility in-mind.

Policy

Recognize the value of maintaining the Esquimalt Municipal Hall as the primary emergency operations centre and Esquimalt Public Works Yard as a secondary site.

Policy

Recognize the important role the library plays in the community to provide education and learning opportunities and as a community gathering space.

Policy

Support the development of library services within the community as needed in a fiscally responsible manner.

Policy

Recognize the valuable role places of worship play in the community and continue to support the development of facilities in accordance with local community needs.

Policy

Subject to Section 220 of the Community Charter, encourage multiple uses in places of worship, including but not limited to, daycares, emergency centres, non-profit organizational and assembly uses.

9.4 Schools

OBJECTIVE: To encourage the integration of schools into the community.

Policy

During the next five years it is not anticipated that additional school sites will be needed by School District No. 61 within Esquimalt. Esquimalt would support an expansion of School District No. 61 school facilities if a need were identified in the future.

Policy

Support opportunities for L'école Victor-Brodeur to expand their school facility in Esquimalt.

Policy

Recognize L'école Victor-Brodeur as a centre for the francophone community in Esquimalt and the region.

Policy

Support multiple uses in schools, including but not limited to, daycares, emergency centres, non-profit organizational and assembly uses.

Policy

Consider partnering with local schools to provide improved and new community facilities such as sports fields and theatre space.

Policy

Encourage walking and cycling by improving the safety of walking and cycling routes to schools.

Policy

Encourage our family of schools to become community schools.

10

PARKS, RECREATION & TRAILS



10.1 General

The delivery of parks and recreation services in Esquimalt will aim to provide benefits, either direct or indirect, to all citizens in the community. There will be a strong focus on promoting health and wellness, arts, culture and preserving and enhancing the Township's parks, open spaces and public facilities.

10.2 Parks

OBJECTIVE: To balance the multiple purposes and uses of parks, such as cultural events, recreation, sports, ecosystem services, commemoration and aesthetic enjoyment, so that they contribute positively to the quality of life of residents.

Policy

Continue to strategically manage and enhance our urban forest, creating a thriving and sustainable mix of tree species and ages, resulting in a healthy ecosystem that is valued and cared for by the Township and its residents.

Policy

Continue to develop a network of parks and open spaces that meet Township wide and local area user needs.

Policy

Develop a well-connected, clearly identifiable, and multi-functional network of greenways.

Policy

Consider including universal accessibility principles in the construction of new park facilities.

Policy

Investigate the feasibility of artificial turf playing fields in Esquimalt.

Policy

Actively plan for the integration of passive recreational opportunities throughout the urban forest in the public realm.

Policy

Develop plans for infrastructure upgrades to enhance access and public use at Saxe Point Park.

Policy

Develop plans for new facilities and park infrastructure at Esquimalt Gorge Park.

Policy

Balance recreational use of the waterfront with the protection and enhancement of areas of natural foreshore habitat for fish, birds and other wildlife.

Policy

Protect the sensitive habitat found in the Gorge Park estuary.

10.3 Recreation

10.3.1 Facilities

OBJECTIVE: To ensure that recreation and wellness services, programs, spaces and places meet diverse community needs

Policy

Support existing and future demographics by incorporating flexibility into the design of recreation, wellness, and cultural facilities.

Policy

Design new spaces to eliminate physical access barriers, encourage programmed and unstructured activity, support flexibility of use and space that can be altered at a later date to accommodate changing demands from users.

10.3.2 Active Living

OBJECTIVE: To encourage people to make healthy, active lifestyle choices throughout their lives.

Policy

Develop facilities so that children and youth have an opportunity to build healthy habits through physical activity – both through unstructured play and organized activities and programs.

Policy

Reduce design barriers to living a physically active life.

Policy

Utilize design solutions to support active lifestyles for aging populations and programs.

Policy

Design the built environment to support and promote opportunities for people of all ages to be active for life and achieve one's highest potential.

10.3.3 Community Connections

OBJECTIVE: To provide opportunities to connect with neighbours and feel a sense of belonging.

Policy

Promote opportunities for people to connect with the physical environment through the design of parks and public spaces.

Policy

Continue to maintain and enhance our multi-use trail network to provide connections and access to waterfront areas, parks and open space, regional trails and residential neighbourhoods.

Policy

Create a network of trail and sidewalk infrastructure connecting Saxe Point Park to Esquimalt Gorge Park.

10.3.4 Partnerships

OBJECTIVE: To collaborate with others so that there is a coordinated efficient response to quality of life, recreation, and community wellness needs.

Policy

Support the co-planning and co-delivery of sport hosting, festivals, community and neighborhood events within the Township.

Policy

In consultation with stakeholders and residents, guide the future planning of recreation and wellness facilities to meet community needs.

10.4 Trails and Waterfront Walkways

OBJECTIVE: Increase public waterfront access throughout the Township while protecting our vital ecosystem.

Policy

When land is subdivided adjacent to a waterfront, the Township will require the dedication of parkland.

Policy

Enhance physical and visual public access to our marine waterfront throughout the Township while minimizing disturbance to waterfront vegetation and environmental features.

Policy

Consider construction of a networked waterfront trail along the Gorge Waterway from Admirals Road to Tillicum Road.

Policy

Endeavour to obtain right-of-way agreements with landowners to allow for public access to the waterfront.

Policy

Waterfront lands will be considered in future park land acquisition.

Policy

Maintain or create, as the case may be, waterfront access in the design of waterfront development where possible.

Policy

Enhance public access points to the Gorge Waterway.

Policy

Investigate marine access facilities for recreational users including kayaks, rowboats, kite surfers, paddle boarders, canoes and other water sports.

Policy

As opportunities arise, the Township will seek to develop a continuous walkway around West Bay and connecting to Head Street.



11 TRANSPORTATION



11.1 General

Esquimalt's transportation system provides the means that allows residents and visitors to move between their homes, places of work, commercial and recreation activities. It connects the community with itself and other communities.

11.2 Walking

Walking is an alternative mode of transportation that shares the road corridor cross section with motor vehicle and cycling traffic.

OBJECTIVE: To develop the pedestrian network into the road corridor infrastructure that provides a safe, enjoyable and continuous network to promote its use.

Policy

Encourage the inclusion of pathways and laneways that promote safe pedestrian travel, in new private developments and in the public realm.

Policy

Improve existing sidewalks, street furniture, crosswalks and other street amenities to make walking a safer and more enjoyable choice for people of all ages and abilities.

Policy

Develop pedestrian movement plans that will encourage pedestrians to use transit from their homes and major facilities in a manner that encourages this movement.

Policy

Plant trees along the public boulevards to reinforce the role and value of sidewalks as well as provide shade.

Policy

Sidewalks should be well lit, free from obstructions and wide enough for their busiest use.

11.3 Cycling

Cycling is an alternative mode of transportation that shares the road cross section with motor vehicles and pedestrians.

11.3.1 Public Cycling Infrastructure

OBJECTIVE: To encourage increased use of cycling for recreational and commuting purposes.

Policy

Consider providing a variety of end of trip facilities by transit stops, parks and institutional facilities. Encourage the provision of end of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers to encourage cycling as a viable form of transportation.

Policy

Where feasible and supported by reliable data, enhance infrastructure to encourage cycling as a healthy form of transportation.

Policy

Consider best management practices to provide guidance on decisions regarding the expansion and improvement of Esquimalt's cycling infrastructure.

11.3.2 New Development

OBJECTIVE: To encourage the inclusion of bicycle facilities in new developments.

Policy

Encourage developers to provide a variety of end of trip facilities for active transportation.

Policy

Encourage bike lockers in multi-unit residential and commercial/commercial mixed-use developments.

11.4 Public Transit

OBJECTIVE: To encourage transit orientated development that takes advantage of the transit system and increases use of the transit system.

Policy

Strive to increase the number of shelters, street furniture and pedestrian lighting to encourage the use of public transit.

Policy

Encourage the incorporation of transit shelters into new developments along transit routes.

Policy

Consider enhancing universal accessibility at transit stops and pedestrian connections to them.



Policy

Support the designation of Esquimalt Road as a future rapid bus route.

Policy

Consider incorporating Lampson Street and Admirals Road as part of the frequent transit network.

Policy

Consider including transit priority measures including transit signal priority and queue jump lanes along Esquimalt Road as a way to ensure the transition from frequent transit to rapid transit can occur and transit is prioritized through the corridor.

Policy

Encourage the provision of handyDART accessible transit service for people with disabilities.

Policy

Support densification along frequent and regional transit routes.



11.5 E&N Rail Corridor

The Esquimalt and Nanaimo (E&N) Railway is a railway between Victoria and Courtenay. In its past life the corridor was utilized to a great degree. With additional transportation corridors and mechanisms this usage has declined. The Island Corridor Foundation (ICF) has been assigned control of the right-of-way.

OBJECTIVE: To encourage multi-use of the E&N rail right-of-way.

Policy

Support the use of the rail line right-of-way for multi-modal transportation use and technology.

Policy

Continue to support further enhancements to the multi-use of the rail line right-of-way as part of the regional system to accommodate walking and cycling.

Policy

Where appropriate, consider residential densification in areas in close proximity to the E&N rail right-of-way.

11.6 Roads

Roads function to provide a surface that allows for the safe and smooth movement of pedestrians, bicycles and motor vehicles from origin to destination. In addition roads are places providing social space that allows for convivial interaction. The major road network is shown on "Roads Networks Map" (Schedule "C"). No new major phasing of extensions to the existing road system are planned.

11.6.1 Major Road System

OBJECTIVE: To improve mobility and access for local and regional travel and reduce intrusion on local streets.

Policy

Admirals Road, Craigflower Road, Esquimalt Road, Lampson Street, and Tillicum Road, will continue to serve as the municipality's principal through-traffic routes. Through-traffic will be encouraged to use these routes rather than local streets.

Policy

Work with DND and Shipyards/Graving Dock on developing a Transportation Demand Management Strategy that promotes alternatives to single occupancy vehicles .

11.6.2 Roadway Upgrades

OBJECTIVE: To maintain road infrastructure to an acceptable level of service.

Policy

Encourage multi-modal street design and accessibility for pedestrians, cyclists, transit users, goods and services providers, and motorists where supported by research based analysis.

Policy

Recognize roads as a community resource benefiting all users. Roads increase opportunities for personal communication and interaction among residents and businesses of the community and are not simply conduits for motor vehicle travel.

Policy

Pedestrians' and cyclists' needs and neighbourhood amenities will be taken into consideration in designing road improvements and road safety.

Policy

When rebuilding or improving roads, consideration will be given to accommodating pedestrians, cyclists, and motorists in the interest of road safety and community needs.

Policy

Consider development of a transportation management plan if a neighbourhood or corridor develops significant traffic issues. This plan will be neighbourhood or stakeholder driven and include their input in its formation.

Policy

To encourage the design and maintenance of a road network that provides, where possible, a satisfactory level of service to all users.

11.7 Public Parking

OBJECTIVE: To encourage on-site parking with all land use.

Policy

New developments should meet the needs of the land use designation and bylaws to achieve onsite parking or have variances supported by a parking study.

Policy

Where appropriate, consider using public parking areas for parking of bicycles and electric vehicles.

12 INFRASTRUCTURE



12.1 General

Within the Township a number of different types of infrastructure are maintained. There are three primary groups of providers of infrastructure. They are:

- The Township, responsible for:
 - Local sanitary sewer collection system
 - Stormwater collection system
 - Street lighting systems
 - Traffic signals and signage
 - Bus shelters
- Other local governments, responsible for:
 - Local potable water distribution network (City of Victoria)
 - Trunk potable water distribution network (CRD)
 - Sanitary sewer trunk collection system (CRD)
 - Waste water treatment plant (CRD)
- Other providers: Crown corporations (electricity) or private service providers (gas, fibre optic cable, cable)

The various types of infrastructure are provided throughout the Township and are located within the road right-of-way, utility lanes or rights-of-way on private property, and on Federal property.

12.2 Sanitary Sewer Collection Systems

The sanitary sewer collection system collects and transports sanitary flows. It is an older system that has had upgrades performed on it periodically. The Township is actively making improvements to the system to address issues of cross connection of services and inflow/infiltration (II) that reduces the system's capacity and ability to manage flows. The capacity of the system is sufficient for today's needs but may require modification as densification of the land types occur in the future. The approximate

location of the sanitary sewer system is shown on the Sewer Network Map (Schedule “D”). A new trunk main to transport sludge from the sewage treatment plant to the Hartland Landfill is planned to start construction in 2018. The main will be built and owned by the Capital Regional District (CRD).

OBJECTIVE: To provide a sanitary sewer collection system that can respond to the demands of growth and manage the system to minimize disruptions due to failure in the system.

Policy

Monitor capacity of the collection system vis a vis the capacity to address the requirements of new development.

Policy

Continue as a partner in the CRD Core Area Liquid Waste Management Plan to address issues of source control, watershed management, harbours health and the wastewater treatment plant.

12.3 Stormwater Collection Systems

The stormwater collection system provides the means for the collection of stormwater through the Township and transports the stormwater to receiving water bodies. The Township is actively making improvements to the system to address issues of water quality and cross connections. The capacity of the system is sufficient for today’s needs but may require modification as changing weather conditions evolve and densification occurs.

OBJECTIVE: To provide a stormwater collection system that can respond to the requirements of the environment and to manage the system to minimize disruptions due to failures in the system.

Policy

Endeavour to provide a stormwater collection system that has the capacity to service new development and anticipated climate change.

Policy

Consider the review and development of best management practices related to stormwater management to reduce the potential for pollution into the regions receiving water bodies.

Policy

Strive for the reduction of soil, hydrocarbons, and other pollutants through the use of mechanical and natural based stormwater treatment systems.

Policy

Where appropriate, support rain water collection systems on private property for rain water harvesting as a strategy to deal with increased rainfall due to climate change.

Policy

Encourage developers and home owners to use stormwater management techniques to maximize the management and control of rainfall on their property in order to slow and treat stormwater before it enters local waterways.

12.4 Street Lighting Systems

The Township owns the street light system with it being located on either stand-alone poles or BC Hydro poles. This system provides light to road right of way to assist in the movement of people, motor vehicles, bicycles and goods throughout the Township and the region.

OBJECTIVE: To promote the use of energy efficient street lights that enhance public safety and street-scape aesthetics.

Policy

Incorporate best management practices for street lights.

Policy

Street light design for new developments should consider pedestrian safety.

Policy

Where required, street lights should incorporate cut-offs to prevent light spillage on to private property, waterways, and into the night sky.

Policy

Where practical, consider the use of appropriately designed street light stanchions.

12.5 Regional Sanitary Collection System

The Township owns and operates the local sanitary collection system which feeds into the regional sanitary collection system which is owned and operated by the CRD. The CRD will be constructing a new main to carry sludge from the new wastewater treatment plant to the Hartland Landfill.

OBJECTIVE: To work with the CRD owners of these systems to encourage upgrading of the system.

Policy

Coordinate with the CRD in the planning and execution of projects on the collection system in order to minimize the impact on the road right of ways and existing infrastructure.

OBJECTIVE: To protect property owners and residents from incompatible uses.

Policy

A regional wastewater treatment facility at McLoughlin Point is supported. Sludge treatment in proximity to residential uses is strongly discouraged, as an affront to an established residential community, nearby businesses and schools, due to the potential negative health, safety, risk, nuisance and other considerations

12.6 Potable Water Distribution System

The City of Victoria owns and operates the water distribution system within the Township. In the next five years the City of Victoria expects to do a major upgrade of the water distribution system along Esquimalt Road.

OBJECTIVE: To work with owners of these systems to encourage upgrading of the system.

Policy

Coordinate with the City of Victoria in the planning and execution of projects on the distribution system in order to minimize the impact on the road right-of-ways and existing infrastructure.

12.7 Utilities Distribution Systems

The ownership and management of several distribution systems are operated within the Township by a variety of private corporations or a crown corporation. These systems deal with telecommunications, fibre optics, gas and power.

OBJECTIVE: To work with owners of the systems to encourage upgrading.

Policy

Support coordination with utility owners for the planning and execution of projects on the various systems in order to minimize the impact on the road right of ways and existing infrastructure.

Policy

Encourage various utilities owners to locate utilities be underground for new developments and upgrade projects.

13 ENVIRONMENT, ENERGY & CLIMATE CHANGE



13.1 Natural Areas

OBJECTIVE: To protect and enhance natural areas.

Policy

Encourage the protection of natural areas when properties are developed or redeveloped.

Policy

As an estuarine environment, the Gorge Waterway is recognized as a valuable natural asset providing specialized habitat and ecological functions for a variety of species.

Policy

The naturalization of the riparian area of the Gorge Waterway is recognized as being critical to maintaining its proper ecological functioning.

13.2 Solid Waste Management

The collection and management of solid waste has evolved from a single stream activity to four major streams. The four streams are: solid waste, kitchen scraps, recycling and yard and garden waste. For solid waste and kitchen scraps the Township is responsible for the collection of these streams. The CRD manages the Hartland Landfill where they are processed and disposed of. Recycling is a region wide initiative that is managed by the CRD. Yard and garden waste is a joint venture that the Township and Town of View Royal which together operate the collection and disposal of this stream.

13.2.1 Landfill Waste

OBJECTIVE: To reduce tonnage entering the garbage waste stream.

Policy

Work with the CRD to extend the capacity of Hartland Landfill by the identification and implementation of alternative processing technologies.

13.2.2 Waste Reduction

OBJECTIVE: To increase tonnage associated with recycling, kitchen scraps and yard and garden waste streams.

Policy

Promote an increase in residents' usage of the recycling, kitchen scraps, and yard and garden streams through education.

Policy

Work with the CRD for the identification and implementation of alternative processing technology for all three streams.

Policy

Work with the CRD to increase the number of material types that can be recycled and reused.

Policy

Encourage new and redeveloped properties to incorporate facilities to divert solid waste collection into the following streams: kitchen scraps, recycling, and yard and garden waste.

Policy

Encourage the use of on-site composting through building and landscape design.

Policy

Consider the use of integrated resource management technologies to reduce the volumes of waste transported to the Hartland Landfill and utilize the by-products of such technology within the built environment.

13.3 Reduction of Greenhouse Gas Emissions

OBJECTIVE: To continue to take action on stabilizing GHG concentrations in the atmosphere by reducing emissions.

13.3.1 Targets

- Take action to reduce community greenhouse gas (GHG) emissions by at least 38% by 2030, compared to 2007 levels.
- Strive for the ultimate objective of carbon neutrality by realizing zero net community GHG emissions by 2050.

13.3.2 Actions

1. Continue as a signatory to the Climate Action Charter and continue to make progress towards corporate carbon neutrality.
2. Develop plans to reduce both corporate and community-wide energy consumption and GHG emissions.
3. Encourage contractors who deliver municipal services to work towards reducing the energy use and GHG emissions associated with those operations.
4. Where feasible, adopt fuel-efficient and low carbon vehicles and equipment in the municipal fleet.

5. Implement a corporate green buildings policy that sets an energy efficiency standard for new corporate buildings and for improving the energy efficiency of existing buildings.
6. Protect and enhance the carbon sequestration value of natural systems by enhancing the urban forest and protecting local soil.
7. Report annually on corporate energy and emissions, as well as actions taken to reduce both corporate and community-wide energy consumption and GHG emissions.
8. Identify an appropriate site for the installation of a public Level 3 fast charging station.
9. Educate developers and home owners on the future need for shading and cooling of buildings in the summer.

13.3.3 Building Energy Efficiency

OBJECTIVE: To reduce building emissions by increasing energy efficiency in new buildings.

Policy

Adopt best practices based on evolving building technologies and materials.

Policy

Encourage the adoption of passive, efficient, and renewable energy systems in new buildings and during building retrofits.

Policy

Investigate options for encouraging developers to achieve high energy performance in new developments through such tools as density bonusing, expedited permit approval process, rebate of development fees, revitalization tax exemption, and other incentives.

Policy

Pursue higher energy-efficiency performance in new developments, through the achievement of higher steps in the BC Energy Step Code as an amenity associated with rezoning.

Policy

Encourage heritage preservation and the re-use of existing buildings where possible.

13.3.4 Low Carbon Transportation

OBJECTIVE: To increase the number of trips made by walking, cycling, or transit.

Policy

Adopt best practices based on evolving transportation technologies.

Policy:

Prioritize walking, cycling and public transit as preferred modes of transportation in infrastructure improvements.

Policy

Develop a multi-modal transportation plan to identify future policy and infrastructure changes to increase mode share in active transportation.

Policy

Where feasible, plan for cycling-only routes, or cycling routes on less-travelled local roads with traffic-calming devices, which are proven to be the safest and most popular types of routes for cyclists.

Policy

Consider prioritizing transit along frequent and regional transit corridors.

Policy

Where feasible, improve the continuity of the bike network by linking existing and future bike-ways and trails.

Policy

Where feasible, increase the amount and diversity of infrastructure available to cyclists, such as public bike racks and public bike repair stations.

Policy

Invest in public bike parking in public parks and facilities, such as parks and beach access points, to encourage cycling.

Policy

Increase the minimum requirements, and set design guidelines, for bicycle parking facilities in all new developments for residents, workers, and visitors.

13.3.5 Ferries

OBJECTIVE: To promote the use of ferries where they can be used to reduce traffic congestion.

Policy

Encourage dialogue between the major employers to ensure that, should a ferry become viable between the West Shore and CFB Esquimalt, that there are provisions to allow non-military personnel to transit through the base with limited security requirements.

13.3.6 Passenger Vehicle Alternatives

OBJECTIVE: To reduce impact of motor vehicles that derive energy from fossil fuels by increasing capacity for alternative fueling and sharing.

Policy

Investigate the potential for increasing the capacity for alternative fuelling in the Township, including but not limited to electric and bio-diesel.

Policy

Encourage the installation of electric vehicle charging infrastructure in all new multi-unit developments.

Policy

Pursue the installation of electric vehicle charging capacity in new developments during the re-zoning process.

Policy

Encourage the inclusion of car share in new multi-unit residential developments.

Policy

Support the reservation of on-street parking for car share vehicles.

13.3.7 Advocacy and Partnerships

OBJECTIVE: To strive to reduce energy and emissions through partnerships and advocacy.

Policy

Seek partners in piloting innovative approaches and technologies to reduce energy consumption and GHG emissions.

Policy

Collaborate with major employers on developing transportation demand management strategies for workers.

Policy

Promote energy saving programs and initiatives administered by third parties.

13.4 Climate Change Adaptation

OBJECTIVE: To prepare the community to be adaptable and resilient to the impacts of climate change.

Policy

Consider including climate change projections, risks, and measures for dealing with more frequent and severe weather events in Township strategies, plans and policies.

Policy

Consider information on climate change risk and vulnerabilities when evaluating new development proposals.



14 URBAN AGRICULTURE



14.1 Agricultural Land Use

Agricultural Land Reserve (ALR) are shown on the “Present and Proposed Land Use Designation Maps.” (Schedules “A” and “B”)

OBJECTIVE: To support the essence of the ALR in order to enhance food security.

Policy

Support innovative ideas and opportunities that promote and enhance community well-being.

Policy

Support modern urban food production and distribution methods that enhance regional food security.

14.2 Urban Food Production

OBJECTIVE: To support the establishment of allotment gardens, community gardens, edible landscapes, food bearing trees and other types of food production activities.

Policy

Allow for community gardens as an accessory use in all zones.

Policy

Support expansion of community gardens on public, private and institutional lands.

Policy

Encourage developers to consider the provision of garden space in commercial developments and in particular food services.

Policy

Encourage developments in food production gardens in all multi-unit residential developments including rooftop gardens.



OBJECTIVE: To promote local food production as a means to mitigate climate change and support food resiliency in an emergency.

Policy

Reduce restrictions to encourage placement of food producing plants on private property.

Policy

Support small scale greenhouses, aquaponics, and aquaculture as an accessory use on private property.

Policy

Continue to support a public launch to the marine environment at Fleming Beach for food harvesting.

Policy

Support the development of commissary kitchens that provide space for small scale commercial food production.

Policy

Support urban food production and related sales as a home based business.

Policy

Support farmers markets in appropriate park, institutional, commercial, and industrial land use designated areas.

15 ARTS, CULTURE & HERITAGE



15.1 Arts and Culture

OBJECTIVE: To encourage and develop a mosaic of appealing, lively and distinctive areas, vibrant public spaces, festivals, events and activities.

Policy

Promote an animated public realm where people can gather, connect and celebrate through art friendly public spaces and facilities.

Policy

Build on the strengths of public art so that it is a key element in shaping, animating and enriching the public realm, civic pride and community identity.

Policy

Recognize the importance of community festivals in the community's cultural development.

Policy

Encourage new developments to incorporate public art into their design.

Policy

Create opportunities for people to experience art in everyday life and for artists and communities to participate in the design, look and feel of the Township through public art initiatives.

Policy

Promote opportunities to interpret Esquimalt's foreshore and natural environment through the arts.



15.2 Heritage

15.2.1 Heritage Values

OBJECTIVE: To ensure that community heritage values are considered in all land use decisions.

Policy

Use the Heritage Values provided in Appendix 1 as a guiding document when considering the impact of land use decisions on the community's heritage.

Policy

Properties identified with community heritage value or character will be considered for addition to Esquimalt's Community Heritage Register.

Policy

Consider Statements of Significance for formally recognized heritage properties when rendering land use decisions.

Policy

Consideration will be given to the 'Significant Tree' list, [Schedule 'D' of Tree Protection Bylaw, 2015, No. 2837 as amended from time to time], when making land use decisions.

Policy

Consider impacts to potential heritage resources when making land use decisions.

Policy

Identify places within the community for placement of signage that celebrates local heritage and history.



Celebrate Esquimalt's Heritage

OBJECTIVE: To respect the Township's proud history by celebrating and thereby enhancing community member's connections to the past.

Policy

Work with local First Nations through a meaningful dialogue about shared heritage values.

Policy

Enter into dialogue with First Nations to enhance understanding of cultural safety.

Policy

Support educational efforts and community events that recognize and celebrate the community's heritage including navy, army, ship building and repair industry, and connection to nature and sea shore.

Policy

Develop signage that celebrates Esquimalt history and heritage at key locations.

Policy

Work with local First Nations to incorporate more Lekwungen place names within the Township of Esquimalt.

Policy

Continue to support well maintained private properties, having heritage value or character through the application of various land use tools.

Policy

Continue to use historic photos to wrap utility boxes, reflecting the interesting history and beauty of this area.

Policy

Find bold, innovative means to celebrate the community's heritage assets.

Policy

Support the relocation of the Municipal Archives to the first floor of Municipal Hall when the library relocates.

15.2.2 Heritage Preservation and Conservation

OBJECTIVE: To create a highly desired urban environment and strong sense of place.

Policy

Maintain or enhance public view corridors, focal points, pedestrian links, view points, and landmarks at a community wide and neighbourhood level. Public view corridors are shown on the "Public View Corridors Map" (Schedule "G").

Policy

Wherever possible, preserve character buildings in their original location. Where this is not possible for recognized heritage buildings, encourage relocation and restoration on-site, or on an appropriate alternative site.

Policy

Support new additions to heritage property that conserve and enhance that property, as consistent with the Standards and Guidelines for the Conservation of Historic Places in Canada.

Policy

Work with property owners to develop Heritage Designation Bylaws, Conservation Covenants and Heritage Revitalization Agreements during redevelopment.

Policy

Consider using the provisions of the Community Charter and Local Government Act to promote the conservation, rehabilitation and maintenance of heritage resources.

15.2.3 Neighbourhood Design

OBJECTIVE: To ensure that opportunities exist for new developments to build on existing neighbourhood and street features, to foster the sense of consistency over time and celebrate areas with unique character.

Policy

Determine neighbourhood character defining elements through a consultation process that identifies values of a neighbourhood.

Policy

Where character defining elements have been identified through a neighbourhood consultation process, consider developing new or revised design guidelines.

Policy

Reinforce landscaping as a character defining element. Retain and augment mature landscaping and trees where possible.



16 ECONOMIC DEVELOPMENT



16.1 General

OBJECTIVE: To encourage a diversity of knowledge based businesses to locate in Esquimalt.

Policy

Support economic development that:

1. Fosters investment and growth in local businesses, services and facilities that cater to the community;
2. Encourages a wide range of marine-oriented businesses (e.g., ship fitting, repair and maintenance, marine-related science and technology);
3. Encourages businesses that provide goods and services to the military sector;
4. Attracts a wider range of business types, including advanced-technology, green economy and knowledge-based businesses;
5. Expands tourism-related businesses that promote Esquimalt's unique regional offerings, including sports facilities, First Nations, military history, and saltwater recreational opportunities;
6. Expands the sport, arts and culture sector, promoting Esquimalt's sports facilities, arts and cultural heritage;
7. Continues to build relationships with the film industry and use of Esquimalt locations to attract more film productions; and
8. Works towards creating a complete community for families and seniors, including banking, medical and support services, daycare, an accessible trail system, public transit and recreational opportunities.

Policy

Pursue opportunities to bring advanced educational institutions into Esquimalt.

Policy

Support local food production, including local markets in Esquimalt and home based businesses.

16.2 Commercial & Industrial

OBJECTIVE: To encourage the upgrading of commercial and industrial areas.

Policy

Encourage revitalization of underdeveloped commercial and industrial areas that have been in decline.

Policy

Encourage the creation of new office space, cafes, restaurants and pubs in commercial areas.

Policy

Foster the revitalization of Esquimalt Road to give it more of a vibrant and pedestrian friendly downtown core with welcoming public spaces for residents and visitors.

Policy

Encourage revitalization of commercial areas along Admirals Road, Esquimalt Road, corner of Tillicum and Craigflower Roads, and Head Street.

Policy

Encourage the redevelopment and expansion of commercial areas along Craigflower Road at Dominion, Tillicum, and Old Island Highway.

Policy

Recognize the value of the E&N Rail Trail as a competitive advantage to Esquimalt's Business Park and Dockyard. The railway can move people and goods to the area. The rail trail provides alternative travel options for employees.

Policy

Encourage enhancements to the public realm in the business area, such as the development of public spaces or publicly available private spaces, for the local workforce, residents and users of the E&N Rail Trail.

16.3 West Bay

OBJECTIVE: To support land use initiatives that contribute to the economic development of West Bay.

Policy

Support expansion and commercial waterfront development around West Bay, provided it is planned to be compatible with the adjacent residential areas.

Policy

Continue to work with the Greater Victoria Harbour Authority to promote economic opportunities for West Bay.

Policy

Support the development of new tourism opportunities at West Bay.

Policy

Support the continued usage of the West Bay (Westsong) Walkway as a scenic waterfront trail that links downtown Victoria via Victoria West to West Bay.



16.4 Tourism

OBJECTIVE: To consider land use decisions that help promote tourism.

Policy

Working with other organizations, the Township will continue to promote Esquimalt's visitor attractions, including community parks, the Gorge Waterway, West Bay and Esquimalt Harbour, the CFB Esquimalt Naval Base and Military Museum including Fort Macaulay and Work Point, sports facilities, and community events.

Policy

Continue to support sports facilities as an important component of Esquimalt's economic diversity.

Policy

Encourage development of compatible commercial opportunities along the E&N Rail Trail.

Policy

Consider short-term vacation rentals within legal dwelling units, where the dwelling unit is an owner or tenant's principal residence, for up to 60 days per year.



**PART 3
DEVELOPMENT PERMIT
AREAS**



17 DEVELOPMENT PERMIT AREAS



17.1 General

Development permit areas are designated in order to promote appropriate development through the use of guidelines.

17.2 Regulations

17.2.1 Designated Development Permit Areas

Development permits are required for development proposals in the areas designated in “Development Permit Areas Map” (Schedule “H”), or as otherwise designated in narrative form in specific Development Permit Areas. This plan designates Development Permit Areas for:

- Protection of the natural environment, its ecosystems and biological diversity;
 - Protection of development from hazardous conditions;
 - Revitalization of an area in which a commercial use is permitted;
 - Establishment of objectives for the form and character of intensive residential development;
 - Establishment of objectives for the form and character of commercial, industrial or multi-family residential development;
 - Establishment of objectives to promote energy conservation;
 - Establishment of objectives to promote water conservation; and
 - Establishment of objectives to promote the reduction of greenhouse gas emissions.
1. Where land is subject to more than one Development Permit Area, development will be subject to the requirements of all applicable development permit guidelines. It may be possible to deal with all guidelines under one permit. The feasibility of this will be determined on a case-by-case basis.
 2. Although compliance with any specific guideline may not necessarily be required, general and substantial compliance with the intent of all guidelines is expected by the Township.

17.3 General Development Permit Exemptions

Except for development within 20 m of the Gorge Waterway high watermark, and within the Hazardous Conditions Development Permit Area, a development permit is not required in the case of:

1. An internal alteration to an existing building that does not increase its footprint;
2. A minor alteration to the exterior of a building that does not change the architectural character of the development. For the purpose of this section, “minor” is defined as a change which does not do any of the following:
 - Increase site coverage more than 5% of the approved coverage;
 - Increase any bylaw non-conformities; or
 - Comprise more than 10 m² of floor area as defined in the Zoning Bylaw.
3. A minor alteration to the exterior of a building for building envelope maintenance purposes (e.g. replacement of siding and/or windows), that does not change the form and character.
4. Temporary buildings or structures that are erected either for offices for construction or marketing purposes for a period that does not exceed the duration of such construction, or two years, whichever is less. The building or structure must be located on previously disturbed land.
5. A fence less than 2.0 m in height;
6. The following types of subdivision:
 - Subdivisions resulting in three or fewer fee simple or bare land strata parcels;
 - Subdivisions for lot line adjustments;
 - Strata subdivision of buildings; and
 - Subdivisions as a result of leases.

For greater certainty, buildings and land alterations associated with subdivision continue to require a development permit.

7. Park or trail improvements within a dedicated park excluding new buildings with a floor area of greater than 50 m²;
8. Road or utility works within a dedicated highway;
9. Minor changes to design and finish of buildings, or landscaping.
10. Removal of trees with a valid tree cutting permit.
11. Emergency repairs to property where a safety hazard exists subject however to a post repair development permit if temporary measures exceed three months.
12. Replacement of sign faces with no change in the location, size or type of signage.
13. Signage approved by the Director of Engineering and Public Works.
14. Erection of temporary tent structures, provided that:
 - A building permit has been issued with respect to the tent structure;
 - The tent structure is accessory to a commercial or business park use occurring on the same property;
 - The structure does not remain in use for a period of more than 14 days.

15. Utility buildings (e.g. pump stations, electrical and telephone kiosks) with a floor area of less than 10 m².
16. Accessory buildings with a floor area of less than 10 m².
17. A suite in a residential building provided that a building permit has been issued.
18. Alterations to landscaping where there is an approved landscape plan provided that there is no net loss of landscaping area and function as per the approved landscape plan.
19. Single detached dwellings except infill welling.
20. Municipal public works, undertaken or authorized by the Township of Esquimalt, provided that the activity does not disturb natural areas.
21. Agricultural uses within the Agricultural Land Reserve.
22. Removal of invasive alien plants (noxious weeds), as identified by the *BC Forest, Lands and Natural Resource Operations' Invasive Plant Program*, where extensive disturbance of the soil is avoided.

18 DPA NO. 1: NATURAL ENVIRONMENT



18.1 Area

Land within the municipal boundaries of the Corporation of the Township of Esquimalt.

18.2 Designation

Development Permit Area No. 1 is designated for the purpose of establishing objectives for:

- Section 488 (1) (a) – protection of the natural environment, its ecosystems and biological diversity.

18.3 Justification

- The gradual erosion of the natural environment incrementally degrades the value and function of local and regional ecosystem services. The average person owns a parcel of land for a relatively short time period of time (10-15 years); care can be taken to ensure larger trees and natural areas are available for the next generation.
- Landscapes and ecosystems are composed of patches of diverse habitats that may include 'species at risk', 'environmentally sensitive areas', and micro-ecosystems containing 'ecological memory'. Even small patches of native soil and vegetation support indigenous species; therefore, all areas can contribute to regional biodiversity and support ecosystem services.
- Protecting Esquimalt's natural environmental features [including but not limited to: local Garry Oak and Douglas-fir ecosystems, rock outcrops, hilly terrain, and rugged clean shorelines] contributes to natural ecosystem functioning and protection of biodiversity.
- Esquimalt has over 20 kilometres of shoreline that serves industrial, commercial and residential purposes, recreation enjoyment, and is vital habitat for numerous plants and animals (e.g. otters, whales, seal, oyster catchers, gulls, various species of waterfowl, and eagles). Shoreline ecology and fish habitat can be protected and enhanced by regulating development near shorelines, and by mitigating the impacts of stormwater entering local waterways. Keeping the urban environment absorbent helps lessen marine ecosystem damage from: introduced pollutants, sudden changes in water salinity and temperature, and shoreline erosion from surges in volume at stormwater pipe outfalls.

- The Gorge waterway is a sensitive, tidal-influenced watercourse that connects the fish-bearing fresh water of Craigflower and Colquitz Creeks with the salt waters of Victoria Harbour. Over the past few decades, significant public expenditures and efforts have gone into removing sources of pollution and contamination that once plagued this waterway. However, the removal of native shoreline vegetation and the construction of extensive seawalls have substantially diminished the quality of the shoreline as supportive habitat for fish and wildlife.
- Both private and public landowners are responsible for this loss of habitat, often through well meaning and beneficial projects such as shoreline walkways.
- Protecting and maintaining current wave energy patterns and natural patterns of erosion along Esquimalt's shorelines will contribute to the protection of natural features and dynamic processes.
- Invasive alien plant species pose a significant threat to regional biodiversity. Managing and reducing the introduction and spread of these species protects local ecosystem structure and function and biodiversity.
- Esquimalt has several natural area parks of varying sizes distributed across the municipality. These pieces of fragmented natural habitat can be better connected through the use of native plant landscaping along roadways and in private yards; thereby supporting regional biodiversity.
- Biodiversity can be enhanced in an urban region through the thoughtful selection of building methods and landscape design. Siting buildings to provide space for trees of varying species and sizes provides vertical habitat for birds, pollinators and other creatures. In addition, the plants of a traditional Garry Oak meadow ecosystem are well adapted for the seasonally dry conditions found on local building roofs, and therefore can be effectively used in a 'living roof' system (a green roof with enhanced ecosystem services). A living roof will moderate stormwater discharge while providing habitat for indigenous plants, invertebrates, and ground feeding and nesting birds.
- Native birds are an important component of urban biodiversity and provide important ecosystem services that contribute to human health. Esquimalt is located within the 'Pacific Flyway' (A chain of habitats used by at least one billion birds biannually as their migratory route along the west coast of North and South America, from Alaska to Patagonia.) and much of Esquimalt's shoreline is part of the 'Victoria Harbour Migratory Bird Sanctuary'. Reasonable actions can be taken to enhance bird habitat in an urban setting; including providing vertical habitat through protection and enhancement of the urban forest, and protecting local shorelines and waterways.
- Habitat corridors for pollinators and other wildlife will be enhanced; linking natural areas within and through the urban matrix to support biodiversity and local food gardens

18.4 Exemptions

18.4.1 Properties

For all properties:

1. Interior renovations or alterations of existing buildings where residential density is not being increased.
2. Ecological restoration projects undertaken or approved by the Township of Esquimalt.
3. Installation of unpaved paths or walking trails that are less than 1 m in width and covered in naturally permeable materials [wood chips, bark mulch, sand or loose gravel] where the soil remains undisturbed.

18.4.2 Gorge Waterway

For all lands located more than 7.5 m from the high watermark of the Gorge Waterway:

1. Repair, maintenance or reconstruction, on the same footprint, of existing legal or legally non-conforming buildings, patios, driveways, parking areas and utilities, provided there is no alteration to natural soil or native vegetation.
2. Construction of fencing where no native trees are removed and disturbance to native vegetation is negligible.
3. The addition of small temporary landscape amenities including benches, tables, garden ornaments, playground equipment, and raised garden beds (not including retaining walls).



18.4.3 High Watermark

For lands located more than 20 m from the high watermark of the Gorge Waterway, and more than 15 m from the high watermark of the Strait of Juan de Fuca:

1. Minor additions [less than 10 m² in area] to an existing legal or legally non-conforming building or structure.
2. Construction of buildings and structures less than 10 m² in area.
3. Installation of seasonal recreation equipment such as children's play equipment, patio furniture, temporary above natural ground level pools/hot tubs.
4. Temporary tent/carport structures.

18.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain circumstances.

18.5.1 Lands Free of Development

Lands to remain free of development, with conditions:

1. Lands within 7.5 m of the high watermark of the Gorge Waterway shall be retained in as natural a state as possible. Where the land has been previously altered, the area shall be restored with native trees and plants.
2. New buildings/ structures shall not be located within 20 m of the high watermark of the Gorge Waterway.
3. New buildings/ structures shall not be located within 10 m the high watermark of the Strait of Juan de Fuca.
4. Replacement of, expansion of, densification and intensification of the use of existing buildings within 20 m of the high watermark of the Gorge Waterway is discouraged; detached accessory dwelling units are strongly discouraged in this location.
5. Replacement of, expansion of, densification and intensification of the use of existing buildings within 15 m of the high watermark of the Strait of Juan de Fuca is discouraged and detached accessory dwelling units are strongly discouraged in this location.
6. Variances to 'Building Height' and 'Siting Requirements' will be considered where natural areas and trees are being protected.
7. Consider the use of conservation covenants for areas having high ecosystem conservation values. Property owners are encouraged to work with local land trusts to protect natural features and valuable habitat areas through land covenants.

18.5.2 Natural Features

Natural features and areas to be preserved, protected, restored, and enhanced where feasible:

1. Retain existing healthy native trees, vegetation, rock outcrops and soil wherever possible.
2. Preserve and enhance native tree and shrub clusters that overhang the waters edge as these provide shade, protection and feeding habitat for fish and wildlife.
3. Preservation of natural topography is favoured over blasting or building of retaining walls.
4. Narrower manoeuvring aisles, fewer and smaller parking spaces can be considered where natural areas are being conserved.
5. Design new development and landscaping to frame rather than block public views.
6. Avoid disturbing, compacting and removing areas of natural soil as this can lead to invasion by unwanted plant species, poor water absorption and poor establishment of new plantings. Use of local natural soil in disturbed and restored areas will support re-establishment of ecosystem functions.

18.5.3 Biodiversity

Landscaping features that will protect, restore and enhance biodiversity. Where feasible:

1. New landscaping shall consist predominantly of native plant and tree species. Plants that are native to the Coastal Douglas-fir biogeoclimatic zone are preferred in landscape treatments as they provide habitat for threatened indigenous flora and fauna. Drought tolerant plants native to western North America, that are known to be non-invasive, are a good alternative choice for landscaped areas.
2. In residential locations plan for 'nature out front'; for new landscaping in front and exterior side yards use a variety of site-appropriate, native species; thereby contributing positively to pedestrian friendly urban streets, future greenways and habitat enhanced corridors.
3. Choose trees and plants for site conditions; consider shade, sunlight, heat, wind-exposure, sea spray tolerance, and year round moisture requirements in their placement.
4. Consider the habitat and food needs of birds, pollinators, and humans in tree and plant species selection and placement; native plantings and food gardens compliment each other.
5. Encourage native plant and food gardens to spill from private land into boulevards.
6. Avoid monoculture plantings, especially expanses of turf grass outside of playing field sites.
7. Snags, logs, driftwood and rock cairns may be used as interesting landscaping features that also provide habitat for native flora and fauna.
8. Include a thick layer of natural leaf mulch in landscaped areas to help re-establish natural soil structure [layers] in disturbed areas.
9. Avoid using fast-growing non-native plants to cover and retain soils as they may become invasive and a constraint to the establishment of other plants.
10. Locate civil servicing pipes/lines under driveways or other paved areas to minimize tree root damage. (Note that the majority of trees have their roots in the top 0.6 m of the soil).
11. Design retaining wall spacing and landscape planting areas of sufficient width and depth to support plantings (eg. provide larger spaces for trees).
12. Support the daylighting of portions of the stormwater system for enhanced habitat.
13. Aim to meet the Canadian Landscape Standards in all landscaping installations.

18.5.4 Natural Environment

Measures to protect, restore and enhance the natural environment (limit noise, light and air pollution). Where it is reasonable:

1. Strategically locate leafy trees/ hedges and water features to mask urban noises such as traffic, garbage collection and delivery locations. Consider that leafy rough barked trees, vine covered walls and natural ground cover materials (mulch, soil) will help dampen urban noise.
2. Use International Dark-Sky Association approved lighting fixtures in outdoor locations. Outdoor lighting shall be no brighter than necessary, be fully shielded (directed downward and designed to serve pedestrian needs), have minimal blue light emissions and only be on when needed. Avoid vanity lighting, and lighting directed into the night sky and trees tops.
3. Light spillage on to waterways is strongly discouraged.

4. Place trees and vegetation near sources of air pollution including busy roadways, to assist in reduction of air pollution through the collection of particulate matter on leaves and needles, and absorption of toxic gases, including but not limited to: ozone, nitrogen dioxide, sulfur dioxide, carbon monoxide, carbon dioxide, cadmium, chromium, nickel and lead.

18.5.5 Drainage and Erosion

Measures to control drainage and shoreline erosion. Where it is reasonable:

1. Preserve, restore and enhance treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Reduce the impact of surges in stormwater on shorelines by designing on-site stormwater retention systems to contain the first 3 centimetres [1.25 inches] of precipitation on site, per precipitation event; and incorporating rainwater collection systems into roof design and landscaping.
3. Consider using shared private/ public rain gardens. Direct a portion of stormwater to adjacent public open spaces, when deemed appropriate by the Director of Engineering and Public Works.
4. Maximize the ratio of planted and pervious surfaces to unplanted surfaces, and design paved areas to direct water towards vegetated areas, to help reduce surface run off. Where paved surfaces are needed, intersperse with drought resistant vegetation and trees, to help absorb stormwater, provide shade and reduce the local heat island effect.
5. Use porous surfaces to enhance stormwater infiltration, permeable paving is preferable for all open air parking areas. Ensure installation methods contribute to sustained permeability and retention of stormwater on the site.
6. Choose absorbent landscaping materials; leaf mulches, wood chips and good quality top soil, over gravel, pavers and concrete. Provide mulch of organic, locally derived materials; leaf mulch from local tree leaves is most desirable.
7. Incorporation of rain gardens, bio-swales, rain barrels, and even small depressions (puddles) into landscaping will help reduce surges of stormwater entering local waterways.
8. Planting densities should ensure that vegetated areas will have near 100% plant coverage after two full growing seasons.

18.5.6 Protect, Restore and Enhance Shorelines

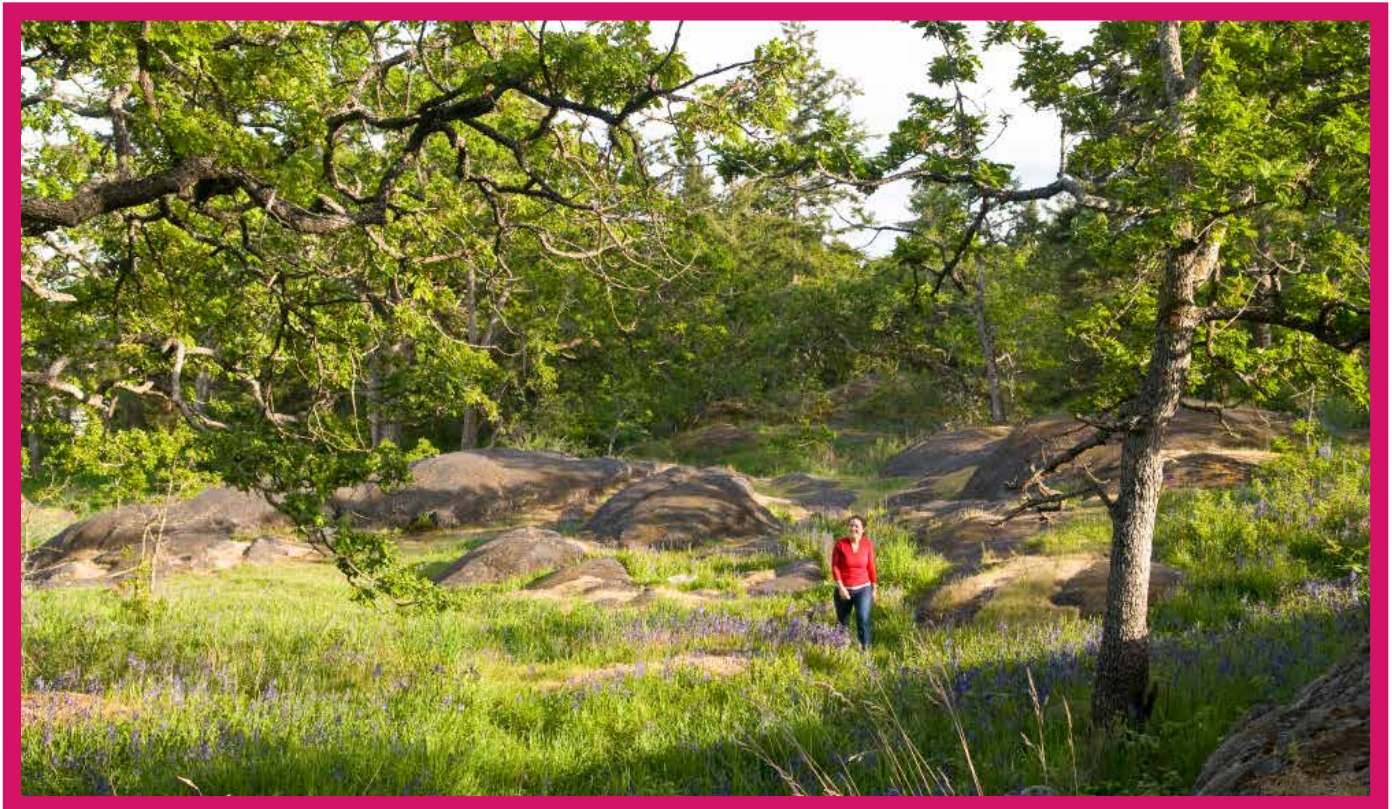
Measures to protect, restore and enhance local shorelines (reducing shoreline hardening and dock development). When it is feasible:

1. Waterfront property owners are encouraged to become familiar with and adopt a 'soft shore' restoration approach to the care of their foreshore property (i.e. Green Shores for Homes).
2. Avoid the expansion of dock area, bulkheads, groins or other shoreline hardening structures. Removal or reductions in the surface area of existing private docks is encouraged.
3. Where shoring methods are required to prevent erosion or the sloughing of the shoreline, choose bio-engineering methods over the use of sea-walls or retaining walls. Where sea-walls or retaining walls are the only means of effectively preventing erosion, design in consultation with qualified environmental professionals, as well as engineering professionals.

18.5.7 Native Bird Biodiversity

Measures to protect, restore and enhance native bird biodiversity. Where it is reasonable:

1. Protect and enhance habitat features like mature trees, shrub clusters, native fruit bearing shrubs, fresh water ponds and ephemeral damp areas (puddles).
2. Encourage increased front yard habitat along quieter streets to reduce bird vehicle conflicts and enhance the pedestrian experience through native plantings.
3. Sustain a mix of habitat types; including forest, shrub-land, meadow, riparian wetland and coastal shoreline ecosystems in landscaping.
4. Incorporate a vertical vegetation structure [vertical habitat] including layers of ground cover, shrub, understorey and canopy in landscape design.
5. Choose a range of native plant species and sizes; a mix of coniferous and deciduous trees will enhance bird species diversity.
6. Incorporate architectural features that limit collisions between birds and windows including patterned, frosted or tinted glass, exterior louvers, blinds, sun shades and canopies.
7. Cap and screen all ventilation pipes and grates, avoid openings greater than 2.0 x 2.0 cm.



19 DPA NO. 2: PROTECTION OF DEVELOPMENT FROM HAZARDOUS CONDITIONS



19.1 Area

All lands located within the inundation area as calculated by the most recent Tsunami modeling program are designated as part of Development Permit Area No. 2 – Protection of Development from Hazardous Conditions.

19.2 Designation

Development Permit Area No. 2 is designated for the purpose of establishing objectives for:

- Section 488 (1) (b) protection of development from hazardous conditions.

19.3 Justification

The Township is located in one of the most seismically active areas in Canada. As such it is imperative that land use and development decisions take into consideration the potential dangers associated with tsunamis.

A tsunami is a series of long surge-like waves and is usually caused by an underwater earthquake, landslide, or volcanic eruption. The most dangerous tsunami threat in the Capital Region will follow a major earthquake in the Cascadia subduction zone (CSZ), located about 100 km off Vancouver Island. Such an earthquake will cause ground shaking lasting between one and four minutes and will result in significant damage throughout the region (<https://www.crd.bc.ca/about/news/article/2013/04/19/modelling-tsunami>).

These guidelines are justified based on the thousands of people across the globe who have been killed due to tsunamis.

19.4 Exemptions

There are no exemptions.

19.5 Guidelines

1. No building intended for the occupation of people shall be built within an area directly impacted by a tsunami.
2. Tsunami walls, retaining walls, sea walls, and other similar structures located in an area directly impacted by a Tsunami shall be designed to absorb wave energy and deflect residual wave energy away from locations likely to be occupied by people.
3. Use of board form design, landscaping, breaking up large expanses of flat surfaces, and other techniques to add interest in Tsunami walls, sea walls, and other similar structures is encouraged.
4. The use of construction materials that may leach toxic chemicals over time into the land or water should be avoided.
5. Incorporating wildlife habitat such as marine pools, nesting ledges, rough surfaces, sheltered coves, and similar design elements into tsunami walls, retaining walls, and sea walls is encouraged.

20 DPA NO. 3: ENHANCED DESIGN CONTROL RESIDENTIAL



20.1 Area

All lands zoned for two-unit dwellings or zoned as Comprehensive Development Districts for residential developments only are designated as part of Development Permit Area No. 3 — Enhanced Design Control — Residential as shown on “Development Permit Areas Map” (Schedule “H”) of this Plan.

20.2 Designation

Development Permit Area No. 3 is designated for the purpose of establishing objectives for:

- Section 488(1) (e) – establishment of objectives for the form and character of intensive residential development.

20.3 Justification

The following guidelines were developed to allow for the better utilization and redevelopment of parcels within residential neighbourhoods and ensure that development occurs in a manner that retains the desirable physical characteristics of a neighbourhood.

Infill housing represents an important component of the housing spectrum. Because infill housing represents new development that is located within the context of an existing residential neighbourhood, it is important that there is harmonious compatibility between the form and character of the new development and the existing development

20.4 Exemptions

The following do not require a development permit:

1. Additions or renovations to one single-unit dwelling (excluding infill units) situated on a fee simple parcel where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling; and
2. Additions or renovations to any two-unit dwelling where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling.

20.5 Guidelines - Duplex Housing

1. Design to be consistent with and preserve the proportions and patterns of existing residential buildings in the immediate vicinity, including the wall-to-window area ratios and the amount and type of open space provided.
2. Where it is not possible to achieve buildings of similar size and proportion to the surrounding residential buildings, the fronts of the buildings should be designed to create the appearance of smaller structures either by staggering the dwelling units or visually breaking up the façade with architectural detailing.
3. Innovative and creative site-specific two-unit dwellings are encouraged where usable open space is maintained either on the ground (yard) or as rooftop gardens. Setbacks to the street may be reduced to maximize property use.
4. Front to back duplexes are generally discouraged unless they can be designed to eliminate negative impacts to the immediate neighbours such as shading of gardens, overlook of outdoor amenity areas and violation of privacy.
5. Side by side, up and down, or staggered unit configurations are preferred as these result in a greater number of units facing the street, less disruption of privacy, and a more equitable division of outdoor amenity areas between the two dwelling units.
6. The use of exterior building materials similar to those used in older residential neighbourhoods (i.e. combinations of wood, brick, stucco, and stone) is encouraged.
7. Rooflines of new development should relate in height, shape and pitch to existing residential buildings in the immediate area. For corner sites, the building design should avoid having large unbroken sloped roof areas facing the street.
8. To create interest in the façade of the buildings facing the street, the incorporation of architectural elements such as bay windows, covered porches, verandas and prominent front doors is encouraged.
9. Buildings should be designed to minimize visual intrusion on to the privacy of surrounding homes. Some overlook of adjoining yards and neighbouring decks may be unavoidable; however, additional privacy should be achieved by inseting balconies, decks and patios into the building or by screening them with latticework or landscaping. Windows should be spaced so that they do not align directly with those of other buildings.
10. The height and massing of new two-unit dwellings should be designed to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings
11. A landscaping plan showing ground cover areas, planting beds, shrubbery and trees (both existing and proposed) is required. Landscaping should add to the aesthetic appeal of the streetscape as well as provide privacy between dwelling units.
12. The provision of private open space should be part of an overall site development and landscape plan and should take into consideration general site circulation patterns (including parking), existing landscape features, sun access, privacy and usability.
13. Retention and protection of trees and the natural habitat is encouraged where possible.
14. Parking areas, garages and driveways should appear as a minor component of the site when viewed from the street. The building of curving access roads and driveways helps to avoid views from the street of large expanses of paving. The use of shared driveways is encouraged.

15. The use of permeable and decorative surfacing materials, such as brick, concrete pavers, textured concrete, coloured paving or grasscrete is encouraged in place of solid expanses of asphalt or concrete.
16. Where possible, hydro meters should not be placed on the front façade of a building and, if placed in a manner which is visible from the street, will be appropriately screened by the owner in a manner not inconsistent with BC Hydro requirements.
17. Where an existing single-unit dwelling is being converted to a two-unit dwelling both the original structure and the addition shall be in the same architectural style and constructed of the same exterior finishes including roofing material, window treatments, exterior finishes, door styles and trims.
18. Roof styles and pitches of the original and new portions of the building must be complimentary.
19. For new or converted two unit dwellings, garages and parking areas are encouraged to be located in the rear yard. Shared driveways are preferred to access the rear yard.
20. Where two single lane driveways serve a two unit lot, landscaping features are encouraged between the driveways.

20.6 Guidelines - Single-unit Infill Housing

20.6.1 Relationship to Existing Houses

1. Where an existing single-unit residence is to be retained and a second residence placed on the parcel, the existing dwelling is to be upgraded and made to blend with the new construction.
2. Where two or more new separate dwellings are situated on a parcel or within a comprehensive development zone, the buildings shall be designed as part of a comprehensive scheme with all buildings being finished in complementary materials and incorporating similar architectural details.
3. Where new infill single houses are proposed, the design of the new houses should be complementary in scale, size, exterior finishes, rooflines, and colours to the predominant styles of housing in the neighbourhood. It is important to ensure that the new construction fits with the overall scale and character of existing houses.

The intent of this guideline is not to encourage the replication or imitation of surrounding buildings but rather the design of structures that complement the streetscape.

20.6.2 Massing

1. New structures should be designed so that the overall massing is in keeping with other single-unit residences in the immediate area. New structures for lots other than corner or double frontage lots should be limited to one and one half storeys.
2. New structures, which are two storeys in height, should be designed so that the second storey is partially concealed within the slope of the roof to minimize the height of the building. The use of dormers set into the roof is preferred to a flat roof or a peaked roof set over the second storey.

20.6.3 Privacy/Screening/Shadowing

1. Proposed infill dwellings should have only a minimal impact on adjacent homes and be separated from neighbouring residences by vegetation, screening, natural elevation differences, or a combination of these features.
2. Windows, decks and patios should be located so as to minimize intrusion on to the privacy of adjacent properties.
3. Infill dwellings should be sited to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings.

20.6.4 Landscaping

1. Proposals for single-unit infill housing must include a landscape plan showing hard landscaping (i.e., parking areas, fences, and patios) as well as lawns, trees, shrubs, planting areas and proposed plant species.
2. Retention and protection of trees and the natural habitat is encouraged wherever possible.

20.6.5 Private Open/Yard Space

1. Any proposal for single-unit infill housing should provide for usable, private outdoor areas for each dwelling, at grade.

21 DPA NO. 4: COMMERCIAL



21.1 Area

All lands designated Commercial on “Development Permit Areas Map” (Schedule “H”) are part of DPA No. 4.

21.2 Designation

Development Permit Area No. 4 is designated for the purpose of establishing objectives for:

- Section 488 (1)(d) – revitalization of an area in which commercial use is permitted; and
- Section 488(1)(f) – form and character of commercial development.

21.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

21.4 Exemptions

The following do not require a development permit:

1. Construction of buildings or structures less than 10 m² in area; and
2. Minor additions to existing buildings and structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the building or structure.

21.5 Guidelines

1. Facades should be appropriate to a pedestrian-oriented shopping area with windows facing the street and doors opening on to the street rather than on to a courtyard or laneway.
2. Ornamental lighting that not only highlights the building but also increases the amount of light falling on to pedestrian areas should be used wherever possible. However, lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
3. Buildings should be designed and sited to minimize the creation of shadows on public spaces.
4. Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
5. Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
6. The design of new commercial buildings, including areas used for parking, should incorporate Crime Prevention Through Environmental Design (CPTED) principles.
7. Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets where setbacks are required for wider sidewalks, boulevard trees, bus stops and street furniture.
8. Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
9. Retention and protection of trees and the natural habitat is encouraged wherever possible.
10. Where new development is to occur within Esquimalt's commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials.



22 DPA NO. 5: BUSINESS



22.1 Area

All lands designated Industrial on “Development Permit Areas Map” (Schedule “H”) are part of DPA No. 5.

22.2 Designations

Development Permit Area No. 5 is designated for the purpose of establishing objectives for:

- Section 488.1(1)(f) – form and character of industrial development.

22.3 Justification

The Devonshire/Viewfield Road Business area is surrounded by residential neighbourhoods. In order to ensure that any future development in this area is undertaken in a manner that will be compatible with the surrounding residential uses, it is desirable that new development incorporates a sympathetic form and character.

1. To encourage environmentally friendly light industry in the Devonshire/Viewfield business park; and
2. To encourage revitalization and enhancement of the existing business areas.

22.4 Exemptions

The following do not require a development permit:

1. Construction of buildings or structures less than 10 square meters in area; and
2. Minor additions to existing buildings where the floor area of the addition does not exceed 10 percent of the ground floor area of the building.

22.5 Guidelines

1. Buildings should be designed to minimize the intrusion into the privacy of existing surrounding homes.
2. Buildings should be located to avoid casting shadows on to adjacent residential properties.
3. Outdoor storage and parking areas will be screened by berms, fences, landscaping or solid noise-absorbing barriers or a combination of these methods. Landscaping should also be incorporated within the parking areas to “break up” large expanses of pavement.
4. Retention and protection of trees and the natural habitat is encouraged wherever possible.

23 DPA NO. 6: MULTI-FAMILY RESIDENTIAL



23.1 Area

All land designated Multi-Unit Residential on “Development Permit Areas Map” (Schedule “H”) are part of DPA No. 6

23.2 Designation

Development Permit Area No. 6 is designated for the purpose of:

- Section 488(1)(f) – establishment of objectives for the form and character of multi-family residential development.

23.3 Justification

This Plan emphasizes the importance of protecting residential neighbourhoods and encouraging a high quality of construction for new developments. It is essential that new multi-unit residential development not have a negative impact on, or be out of character with, existing residential neighbourhoods. The primary objective of Development Permit Area No. 6 is to ensure that the development of multi-unit residential sites is compatible with surrounding uses.

23.4 Exemptions

The following do not require a development permit:

1. Construction of buildings or structures less than 10 m²;
2. Minor additions to existing dwellings where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling; and
3. Placement of signs less than 1.5 m² in size.

23.5 Guidelines

1. The size and siting of buildings that abut existing single- and two-unit and townhouse dwellings should reflect the size and scale of adjacent development and complement the surrounding uses. To achieve this, height and setback restrictions may be imposed as a condition of the development permit.
2. New buildings should be designed and sited to minimize visual intrusion on to the privacy of surrounding homes and minimize the casting of shadows on to the private outdoor space of adjacent residential units.
3. High-density multi-unit residential buildings or mixed commercial/residential buildings in commercial areas should be designed so that the upper storeys are stepped back from the building footprint, with lower building heights along the street front.
4. Landscaping should emphasize the creation of an attractive streetscape, as well as provide privacy between individual buildings and dwellings, screen parking areas and break up large expanses of paving.
5. Surface parking areas in developments less than five storeys in height, will be situated away from the street and screened by berms, landscaping or solid fencing or a combination of these three.
6. Underground parking will be provided for any multi-unit residential buildings exceeding four storeys.
7. The retention of public view corridors, particularly views to the water, should be encouraged wherever possible.
8. To preserve view corridors and complement natural topography, stepped-down building designs are encouraged for sloping sites.
9. Retention and protection of trees and the natural habitat is encouraged wherever possible.
10. Townhouses will be designed such that the habitable space of one dwelling unit abuts the habitable space of another unit and the common wall overlap between adjoining dwellings shall be at least 50 percent.
11. Site lighting should provide personal safety for residents and visitors and be of the type that reduces glare and does not cause the spillover of light on to adjacent residential sites.

24 DPA NO. 7: ENERGY CONSERVATION & GREENHOUSE GAS REDUCTION



24.1 Area

Land within the municipal boundaries of the Corporation of the Township of Esquimalt.

24.2 Designation

Development Permit Area No. 7 – is designated for:

- Section 488 (1)(h) – energy conservation; and
- Section 488 (1)(j) – GHG emissions reduction.

24.3 Justification

The Province of British Columbia has legislated greenhouse gas targets and requires the cooperation of local governments to achieve them. As part of its pledge to the Community Climate Action Charter, Esquimalt set its own target to reduce greenhouse gas emissions by 38% of 2007 levels by 2030; with the eventual goal of progressing towards carbon neutrality.

The objectives in this DPA include:

- Encourage a shift in practice and behavior to accelerate a reduction in fossil fuel dependence;
- Support reductions in energy consumption in buildings, and reduced maintenance costs through the use of durable building materials; support the best use of existing infrastructure and minimizing the need for system capacity expansion and extension;
- Encourage and support innovation in redevelopment, siting and design;
- Deliver neighbourhoods that support residents physical and mental health with long-term livability, including walkability;
- Consider the long-term comfort of building occupants in design decisions;
- Create neighbourhoods and buildings that respect, serve, and support the needs of all economic classes;
- Build neighbourhoods that support transit, walking and other forms of active transportation;

- Support the construction of new buildings that contribute to those neighbourhoods where people are delighted to live, work, walk and play;
- Support development to have a positive impact on the biosphere, community resilience and residents' health.

24.4 Exemptions

1. Minor alteration/ addition to the exterior of a building. For the purpose of this section, "minor" is defined as a change which does not:
 - Increase the lot coverage by the lessor of 5% of the parcel or 50 m² (based on the site coverage of all buildings and structures);
 - Increase any bylaw non-conformities;
 - Comprise an addition of more than 50 m² of gross floor area; or
 - Require a Development Permit for 'Form and Character.'
2. Landscaping.
3. Installation of temporary tent/carport structures intended to be used for less than one year.

24.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants) is strongly encouraged and may be required in certain circumstances.

24.5.1 Siting of buildings and structures

Where it is feasible:

1. Orient buildings to take advantage of site specific climate conditions, in terms of solar access and wind flow; design massing and solar orientation for optimum passive performance.
2. Build new developments compactly, considering the solar penetration and passive performance provided for neighbouring sites, and avoid shading adjacent to usable outdoor open spaces.
3. In commercial, residential or commercial mixed-use designated areas with taller developments, vary building heights to strategically reduce the shading on to adjacent buildings.
4. Provide space for pleasant pedestrian pathways between buildings.
5. Strategically site buildings to sustain and increase the community's urban forest tree canopy cover.
6. Provide space for significant landscaping including varying heights of trees, shrubs and ground covers.
7. Provide intuitive pedestrian access to storefronts and businesses with site connectivity to nearby amenities and services to help promote walking and the use of other active transportation modes.
8. Provide usable outdoor amenities such as seating, food gardens, mini-libraries, and play spaces in semi-public areas to enhance the experience of walking and recreating in the neighbourhood.

9. In residential neighbourhoods, provide space for larger trees and a second row of street trees as this will enhance the pedestrian experience by lowering wind velocity at street level, reducing excessive heating at ground level and absorbing vehicle and other urban noises.

24.5.2 Form and exterior design of buildings and structures.

Where it is feasible:

1. Orient larger roof surfaces to the south for potential use of solar panels or photo-voltaic roofing.
2. Use roof designs that reduce heat transfer into neighbouring buildings, helping reduce the local heat island effect and the need for cooling of buildings in warmer months.
3. Place more windows on the south side of buildings to increase solar gain, and fewer/ smaller windows on the north side to minimize heat loss.
4. Use roof over-hangs, fixed-fins or other solar shading devices on south and west facing windows to reduce peak summer heat gain while enabling sunlight penetration in winter months.
5. Install adjustable overhangs above windows that can help control the amount of sun exposure in warmer months thereby reducing need for cooling.
6. Provide building occupants with control of ventilation; i.e. windows that open.
7. Skylights are discouraged as they decrease insulating values and can interfere with solar panel installation.
8. Add rooftop patios and gardens, particularly food producing gardens, as they can contribute to local resilience, livability, and reduction in greenhouse gas production by reducing food transportation costs.
9. Install greenhouses for growing food on rooftops where neighbourhood privacy and light intrusion concerns are mitigated.
10. Avoid heavily tinted windows or reflective glass which will diminish the natural daylighting of interior spaces, thereby requiring increased energy requirements for interior lighting.
11. In exposed marine locations select durable materials that will withstand weather and sea spray, to ensure low maintenance costs and infrequent replacement needs.

24.5.3 Landscaping

Where it is feasible:

1. Develop a front yard landscape design that is natural and delightful so residents do not need to leave the neighbourhood to experience nature.
2. Choose open space and landscaping over dedicating space to the parking and manoeuvring of private motor vehicles.
3. Conserve native trees, shrubs and soils, thereby saving the cost of importing materials and preserving already sequestered carbon dioxide.
4. Use deciduous trees for landscaping along southern exposures, as they provide shade in the summer and allow more sunlight through in the winter.
5. Strategically place taller trees and vegetation on the south and west sides of buildings where there is more direct sun exposure.
6. Strategically place coniferous trees such that they can buffer winter winds.

7. As context and space allow, plant trees that will attain a greater mature size, for greater carbon storage; removal of healthy trees is discouraged as the loss of the ecosystem services provided by larger trees will take many years to recover.
8. Plant trees with a larger canopy cover along roadways and sidewalks, thereby providing shading of paved areas, lowering the heating of paved surfaces and reducing the wind velocities in these pedestrian areas.
9. Plant shorter and sturdier vegetation closer to buildings and other structures, and taller vegetation further away to avoid potential damage from strong winds blowing vegetation against buildings.
10. For commercial areas, strategically increase green space between buildings, allowing room for landscaped pathways to improve the pedestrian experience, promote walking, and provide for improved light penetration on to sidewalks.
11. For parking areas and along boulevard/ sidewalk edges; plant trees to provide shade, store carbon and reduce the heat island effect.

24.5.4 Machinery, equipment and systems external to buildings and other structures.

Where it is feasible:

1. For external lighting:
 - Choose efficient low-energy and long life technologies;
 - Design lighting to reinforce and compliment existing street lighting;
 - Use motion-sensitive or solar-powered lights whenever possible;
 - Layer lighting for varying outdoor needs; and
 - Provide lighting systems that are easily controlled by building occupants.
2. Use heat pumps, solar panels, green (living) roofing or an innovative system to improve a building's energy performance.
3. Use durable, vandalism and graffiti resistant materials where neighbourhood surveillance may be limited.
4. Design for on-site heat recovery and re-use of water.
5. In commercial and industrial areas: design bicycle parking facilities to be inviting for cyclists. Locate bike racks near the main building entrance, with adequate lighting and weather protection.
6. In commercial areas, provide fast charge electric vehicle charging stations near locations that have quick customer turnover, and ensure the station is easily accessible, well lit, and visible from the public street.
7. Provide car sharing facilities that are well lit, available for residents, and easily accessed from the public street.

24.5.5 Special Features

Where it is feasible:

1. Select building materials that have been shown to have a high level of durability for the use intended.

2. Use wood for construction as a means to sequester carbon dioxide - North American grown and sustainably harvested wood is preferable for building construction.
3. Select local and regionally manufactured building products whenever possible to reduce transportation energy costs.
4. Reuse of existing buildings and building materials is encouraged.
5. Choose materials that have a high likelihood of reuse or recycling at end of life.

25 DPA NO. 8: WATER CONSERVATION



25.1 Area

Land within the municipal boundaries of the Corporation of the Township of Esquimalt.

25.2 Designation

Development Permit Area No. 8 is designated for:

- Section 488(1)(i) – water conservation.

25.3 Justification

Guidelines that conserve water also reduce energy use from treating and distributing potable water and treating wastewater, and help communities prepare for expected water shortages from climate change.

Urban areas have high water demands. Landscaping uses a considerable quantity of potable water. Stormwater can be either a burden on municipal infrastructure and local shorelines or a resource used within the community to lessen water demand for landscaping.

The guidelines in this section are intended to implement the Township’s sustainability objectives to develop a green economy and reduce the overall risks and impacts of climate change through:

- Reduced per capita water consumption in new developments;
- Better use of existing water system infrastructure and reduced need for system capacity expansion; create a positive impact on the natural environment and hydrological systems;
- Innovation in the use of stormwater to reduce landscaping water requirements; and
- Reduced impact on the stormwater management system from the over use of potable water for landscaping.
- Wise use of potable and stormwater to reduce energy consumption and costs associated with the treating and distribution of potable water;
- By making the best use of existing infrastructure, the need for system capacity expansion and extension can be reduced;

- Reduced potable water consumption which leads to reduced energy consumption associated with the treating of wastewater;
- The best use of existing infrastructure so that the need for system capacity expansion and extension can be reduced;
- Use of stormwater for landscaping to assist in the conservation of local water reserves; and
- Rain gardens, retention ponds, and bioswales that can provide value as an urban design element and provide a source of delight in a passive recreation environment, and enhanced wildlife habitat and biodiversity.

25.4 Exemptions

The following do not require a development permit:

1. Changes to landscaping that does not decrease the permeability of a property
2. A minor alteration/ addition to the exterior of a building. For the purpose of this section, “minor” is defined as a change which does not do any of the following:
 - Increase the lot coverage by the lessor of 5% of the parcel or 50 m² (based on the site coverage of all buildings and structures);
 - Increase any bylaw non-conformities; or
 - Comprise an addition of more than 50 m² of gross floor area.
3. Installation of temporary tent/carport structures to be used for less than one year.

25.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain situations.

25.5.1 Building and Landscape Design

Where it is feasible:

1. Reduce the burden on built stormwater infrastructure by designing on-site retention systems to retain the first three centimetres (1.25”) of stormwater on site, per precipitation event.
2. Provide space for absorbent landscaping, including significantly sized trees on the site and by not allowing underground parking structures to extend beyond building walls.
3. Incorporate rainwater collection systems into roof design; consider using living roofs and walls as part of a rainwater collection system.
4. Incorporate rain gardens into landscaping and direct rainwater towards vegetated areas.
5. Intersperse paved surfaces with drought resistant vegetation that will provide shade on those surfaces and design the paved surfaces to drain into the vegetation.
6. Design landscaping with more planted and pervious surfaces than solid surfaces.
7. Direct stormwater towards adjacent public spaces, with rain gardens/ bioswales located on public property where it would benefit both the new development and the municipality and where it is deemed appropriate by municipal staff.

25.5.2 Landscaping - Select Plantings for Site and Local Conditions

Where it is feasible:

1. Retain existing native trees vegetation, and soil on site.
2. Plant species native to the Coastal Douglas-fir biogeoclimatic zone, as they are most suited to our climate and require little additional irrigation once established.
3. Consider shade, sunlight, heat, wind-exposure and sea spray, as well as water needs in the selection and placement of plant species.
4. Group plants with similar water needs into hydro-zones.

25.5.3 Landscaping – Retaining Stormwater on Site (absorbent landscaping)

Where it is feasible:

1. Preserve and restore treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Use pervious landscaping materials to enhance stormwater infiltration; permeable paving is preferable for surface parking areas.
3. Avoid disturbing, compacting and removing areas of natural soil, as these are naturally absorbent areas.
4. Locate civil servicing lines along driveways and other paved areas, to lessen the disturbance of natural soils and loss of their natural absorption qualities.
5. Use good quality top soil and compost for the finish grading of disturbed areas to contribute to the water holding capacity of newly landscaped areas.
6. Choose bark mulches or woodchips for walking paths for enhanced absorption.
7. Plant at densities that will ensure vegetated areas have 100% plant canopy coverage after two full growing seasons. Consider that understory native plants are adapted to local climates, absorb seasonal soil moisture and reduce compaction due to foot traffic.

25.5.4 Landscaping - Water Features and Irrigation Systems

Where it is feasible:

1. Use automated high efficiency irrigation systems where irrigation is required.
2. Incorporate stormwater retention features into irrigation system design.
3. Use recirculated water systems for water features such as pools and fountains.
4. Install plantings and irrigation systems to the Canadian Landscape Standard.

26 DPA NO. 9: ENGLISH INN



26.1 Area

Lands designated as English Inn Mixed-use are part of Development Permit Area No. 9 – English Inn.

26.2 Designations

Development Permit Area No. 9 is designated for the purpose of establishing objectives for:

- Section 488 (1)(d) – revitalization of an area in which a commercial use is permitted;
- Section 488 (1)(e) – form and character of intensive residential development;
- Section 488 (1)(h) – promoting energy conservation;
- Section 488 (1)(i) – promoting water conservation; and
- Section 488 (1)(j) – GHG emissions reduction.

26.3 Justification

These guidelines were developed to steward the design of development on the property known as the “English Inn” site at 429 Lampson Street in Esquimalt. The intent is to encourage new development to be sympathetic with, and a good neighbour to, both the existing heritage Samuel Maclure designed Manor House, known as Rosemead and the surrounding neighbourhood context, while providing opportunity for alternative massing solutions to accommodate market and building programmes. A key objective is a harmonious and sensitive development respectful of the Protected Property under Heritage Designation Bylaw 2807, including as described in the schedules thereto.

As a significant development project within the community this signature site has the potential to support Esquimalt’s target to reduce greenhouse gas emissions by 38% of 2007 levels by 2030; with the eventual goal of progressing towards carbon neutrality. Therefore, the guidelines support reductions in energy consumption in new buildings, and reduced long-term maintenance costs through the use of durable building materials.

Landscaping uses a considerable quantity of potable water. Stormwater can be either a burden on municipal infrastructure and local shorelines; or a resource used on the property to lessen water demand for landscaping. The intent is for the strategic retention of stormwater on this site.

The guidelines in this section are intended to support the Township’s sustainability objectives to develop a ‘green economy’ and reduce the overall risks and impacts of climate change.

26.4 Exemptions

The following do not require a Development Permit:

1. Construction of buildings or structures less than 10 m² in area.

26.5 Guidelines

These guidelines are not intended to slavishly replicate the mock Tudor vocabulary of the original house, but rather listen to its basic form, texture, proportions and composition of elements on site. The guidelines are descriptive and are intended to be feasible to execute. The guidelines incorporate features to encourage the promotion of energy and water conservation and the reduction of greenhouse gases.

26.5.1 Landscape and Significant Features

Where it is feasible:

1. Respect the qualities of the existing topography, natural rock outcrops and related significant trees (especially in the southeast corner).
2. Respect significant trees through appropriate building siting and design.
3. Landscape designs should reflect the character defining elements of the Manor House site and should use plant species suited to local climate and incorporate drought-tolerant, native species and other xeriscaping techniques that minimize the need for landscape irrigation.
4. Any use of the site should respect the existing landscape features. The hard landscaping of the Manor House site (including but not limited to the pavilion, fountain, stonework and retaining walls) represent the formal landscaped gardens characteristic of a home of this stature and era.
5. All building siting and design should respect the site lines from outdoor spaces. Landscaping at the rear of the Manor House site has been developed to form a courtyard for use by the buildings occupants and guests, and forms an integral part of the building context.
6. Use of materials should reflect the high quality already established on the site. The landscaped areas of the Manor House site, including the formal gardens, fountains, pavilions, hardscaping and courtyards are an important part of the character of the site and any proposed design should be sympathetic to these elements and this character.
7. The property has many unique and mature plants and trees and any proposal should endeavour to reuse and incorporate this material on the site to the extent possible.
8. Fences as a part of the landscape should be of high quality material and the use of chain link fences should be avoided.

26.5.2 Access and Parking

Where it is feasible:

1. Retain and simplify the existing driveway from Lampson Street to access the heritage property and lands beyond. Widen the north driveway judiciously around significant trees.
2. Maintain the domestic scale and character of the driveway on to Lampson Street including unobtrusive low level lighting; retain the existing stone gate posts.
3. Any surface parking, especially on the Manor House site, should be appropriately screened with landscaping and be designed not to detract from the character of the landscaping of the site. The use of permeable paving materials for parking areas is encouraged.
4. Incorporate appropriate stormwater management measures to ensure stormwater from the driveway infiltrates back into the ground to ensure no net runoff offsite.
5. Incorporate below grade parking, for the development site, to take advantage of the approximately one storey north/south cross fall across the site.
6. Avoid long open cut parking access ramps by accessing underground parking from the lower levels of the existing grade.
7. Appropriate bicycle and scooter storage should be provided in commercial and multiple-family buildings.
8. Commercial and multiple-family buildings should include provision for charging stations for electric vehicles where appropriate.

26.5.3 Energy and Water Conservation

Where it is feasible

1. Use green building standards and technology to reduce the environmental/ ecological footprint of development.
2. Use natural stormwater management techniques and measures to ensure that all stormwater is managed on the site with no net increase off site. It is a fundamental municipal requirement that all stormwater runoff be managed on site. This will substantially improve the existing condition.
3. Use of outdoor lighting on buildings or in the landscape should be designed to minimize light pollution and spillover on to neighbouring properties. All outdoor lighting should minimize wattage and be directed downward. Use of motion detectors and timers is encouraged.

26.5.4 Building Form and Character

Where it is feasible:

1. Break down building volumes into domestic sized increments.
2. Incorporate pitch roof language with dormers sympathetic to the heritage Maclure manor, reducing apparent building height and volume.
3. Consider relaxation of building setbacks where it can be shown that it is advantageous to building design and distribution of building mass and volume in relation to adjacent properties.
4. Respect significant trees through appropriate building siting and design.

26.5.5 Distribution of Building Volume

Where it is feasible:

1. Concentrate higher building volume towards the middle of the site and towards the easterly portions adjacent to the neighbouring DND property.
2. Keep building volumes lower towards the edges and composed as if made up of individual dwelling units, particularly towards the south. Massing towards the northern edges can typically accommodate another storey, since the English Inn site is a nominal level below the neighbours to the north.

26.5.6 Basic Building Volume and Roof Forms

Where it is feasible:

1. Employ basic building elements not much more than twice the bulk of the Manor House proper to create an overall composition whereby the whole reads as an assemblage of these parts.
2. Compose building elements to shape and define spaces between and within; not to exist as objects in space.
3. Employ a language of roof pitch typically to reflect that of the Manor House; to be inhabited within, not simply sit on top of habitable space.
4. Figuratively, pull the roof forms down around the occupied spaces.
5. Utilize dormers – pitched or single slope – to provide light and views from habitable space within the roof.
6. Utilize stepped down gables, or single pitch runoffs to further break down scale and create more intimate relationships with the ground. These elements can be used in succession.
7. Roof overhangs and window placement should be coordinated to provide cooling and shade during summer and solar access for passive heating in the winter.
8. Roof surfaces should be designed to accommodate solar energy collection devices. Skylights are discouraged, as a benefit of natural daylight penetration is not sufficient from an energy perspective, to outweigh their heat loss due to low insulation value.

26.5.7 Building Orientation and Access to Sunlight

Where it is feasible:

1. Buildings should be located, oriented and designed to facilitate the retention of passive solar heat (e.g. south facing windows), reduce heat loss and support natural ventilation.
2. Reduce energy consumption of electric lighting by maximizing opportunities for the distribution of natural daylight into a building's interior spaces (excluding the use of skylights).
3. Avoid the use of heavily tinted or reflective glazing that reduces solar heat gain but also reduces the penetration of light.
4. Placement and retention of deciduous trees is encouraged such that these trees provide summer-season shading, and winter-season solar access.
5. While respecting the importance of the existing character of the site's landscape character design of on-site landscaping should minimize shading impacts and the potential for solar thermal or photo-voltaic systems on the site and surrounding properties.

26.5.8 Windows – Types and Proportions

Where it is feasible:

1. Employ bay windows, bracketed in upper storeys, or stepped out on lower storeys to form decks off upper stories, to break down scale of end walls.
2. Employ basic window element having a vertical proportion – 1:1.4 – 1:2.2.
3. Vary size from floor to ceiling to vary small openings for secondary spaces.
4. Increase amount of transparency by stringing multiple units or by employing basic units at regular intervals.
5. Create horizontal strip glazing condition by exploring recurrent smaller units.
6. Break down scale and texture where appropriate with divided light muntins or zinc cam in double glazed units.
7. Large single well-proportioned sheets can be employed in conjunction with divided lites to capture views.

26.5.9 Renewable and Alternative Energy

Where it is feasible:

1. Support on-site renewable energy systems and technologies such as solar hot water, solar photo-voltaic, micro wind turbines and heat pumps.
2. Encourage on-site resource recovery through technologies where possible such as heat exchangers on ventilation and domestic water supply.

26.5.10 Materials Management

Where it is feasible:

1. Recycling infrastructure and facilities especially for organics is encouraged.
2. Building materials which are durable for the use intended should be sourced locally or regionally to reduce transportation requirements whenever possible and economic.
3. Reuse existing building and landscape materials on site where practical and economic.
4. Encourage construction waste diversion planning as part of the development process, including the identification of designated areas for the collection of recyclable materials.

27 DPA NO. 10: ESQUIMALT TOWN SQUARE



27.1 Area

Lands legally described as Lot 1, Section 11, Suburban Lot 40, Esquimalt District, Plan EPP32782 (PID: 029-349-311) are designated as Development Permit Area No. 10 - Esquimalt Town Square.

27.2 Designations

Development Permit Area No. 10 is designated for the purpose of:

- Section 488(1)(a) – protection of the natural environment, its ecosystems and biological diversity;
- Section 488(1)(d) – revitalization of an area in which a commercial use is permitted;
- Section 488(1)(e) - establishment of objectives for the form and character of intensive residential development;
- Section 488(1)(f) – establishment of objectives for the form and character of commercial and multi-family residential development;
- Section 488(1)(h) – establishment of objectives to promote energy conservation;
- Section 919.1(1)(i) – establishment of objectives to promote water conservation; and
- Section 919.1(1)(j) – establishment of objectives to promote the reduction of greenhouse gas emissions.

27.3 Justification

These guidelines were developed to steward the design of a mixed use development known as Esquimalt Town Square. The development needs to promote the revitalization of the Township's core area while being sympathetic to the surrounding land uses and built form and offering exemplary site and building design that incorporates the principles of urban ecology. In order to conserve energy and resources, the development should incorporate features that contribute to reduced consumption of water and energy.

Incorporation of design elements that promote walkability will help the Township meet its greenhouse gas (GHG) reduction targets. Enhancement of the relationship between people and the built

environment is inextricably linked to both the form and character of the development but also the intensities of the residential development. The level of human comfort on the site is also enhanced through the use of extensive landscaping and design that assists ecological functioning.

27.4 Exemptions

The following do not require a Development Permit:

1. Alterations to landscaping as long as the form and character of the landscaping remains the same; and
2. Installation of public art.

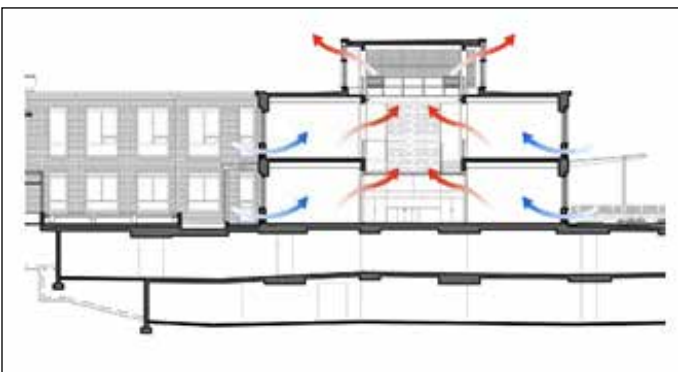
27.5 Guidelines



1. Buildings should be designed to capture passive solar energy.



2. Sun shading should be considered to control heat gain in the summer.



3. Design for on-site heat recovery, and plan for future, long-term neighbourhood heat-resource opportunities.



4. Manage rainwater on the site including reduction of burden on stormwater sewer infrastructure and cleaning for down-stream use.



5. Pollution management protocols (including sedimentation and oil-removal), as well as recycling programs should be followed during demolition, excavation and construction of the project.



6. Prioritize use of high quality, regionally sourced products & materials.



7. Where appropriate, make building uses visible from the outside.



8. Avoid mirrored or tinted glazing at the street level.



9. Locate clearly identified and weather-protected entrances to be accessible from public streets and squares.



10. Integrate pedestrian-oriented signage with frontages and facades.



11. Way-finding and commercial signage, lighting and weather protection (canopies, etc) should be part of the architectural design.



12. Orient upper-storey windows and balconies to overlook adjoining streets and public space.



13. Use architectural emphasis to define street corners.



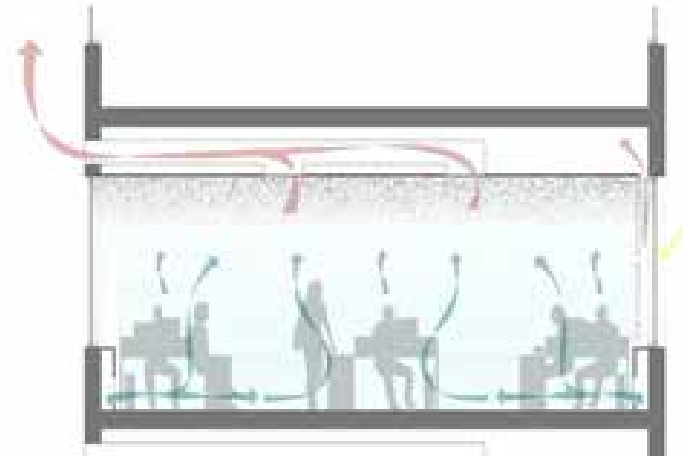
14. Locate on-street parking, where possible, in front of shops.



15. Maximize glazing at grade along commercial streets. Provide sight-lines from inside buildings to allow for casual surveillance of open public spaces, streets and sidewalks.



16. Avoid excessively long blank walls adjacent to public streets.



17. Follow green building practices in energy and water conservation, greenhouse gas production, indoor air quality and material durability in all aspects of building and site design.



18. Provide for building occupants to overlook public streets, parks, walkways and spaces, considering security and privacy of residents.



19. Provide for slightly raised entrances to ground floor residences along with private yards that are accessible from the fronting street or lane to encourage community interaction.



20. Use of indigenous plant species is encouraged.



21. All buildings should be bird friendly.



22. All exterior lighting should avoid excessive stray light pollution and should meet International Dark-Sky standards.



23. Wherever possible, outdoor storage and parking areas should be screened from view.

28 DPA NO. 11: WEST BAY



28.1 Area

All lands outlined and indicated as “West Bay” (Schedule “H”) are part of DPA No. 11.

28.2 Designations

Development Permit Area No. 11 is designated for the purpose of establishing objectives for:

- Section 488(1)(a) – protection of the natural environment, its ecosystems and biological diversity;
- Section 488(1)(b) – protection of development from hazardous conditions;
- Section 488(1)(d) – revitalization of an area in which a commercial use is permitted;
- Section 488(1)(e) - establishment of objectives for the form and character of intensive residential development;
- Section 488(1)(f) – establishment of objectives for the form and character of commercial and multi-family residential development;
- Section 488(1)(h) – establishment of objectives to promote energy conservation;
- Section 919.1(1)(i) – establishment of objectives to promote water conservation; and
- Section 919.1(1)(j) – establishment of objectives to promote the reduction of greenhouse gas emissions.

28.3 Justification

The West Bay Neighbourhood is a neighbourhood with unique characteristics that is under increasing development pressure. In order to retain the form and character of the neighbourhood while at the same time allowing redevelopment to occur, a set of robust design guidelines is required. With increased multi-family development comes increased densification in the neighbourhood. This makes it imperative to have guidelines that address such issues as the relationship of the building to the street, privacy, and neighbourliness. In addition, because the West Bay Neighbourhood includes a sheltered marine environment it is home to a variety of both marine and land based animal and plant species. Therefore, it is important to protect the neighbourhoods natural environment, ecosystems, and

biological diversity. Also, its coastal location means that it is susceptible to tsunamis, therefore, guidelines related to protecting people and development from the potential hazards related to tsunamis are important. Finally, redevelopment offers opportunities to conserve energy and water and reduce greenhouse gas emissions through the use of design techniques that help to create green healthy buildings and open spaces.

28.4 Exemptions

All exemptions in underlying development permit areas apply.

28.5 Guidelines

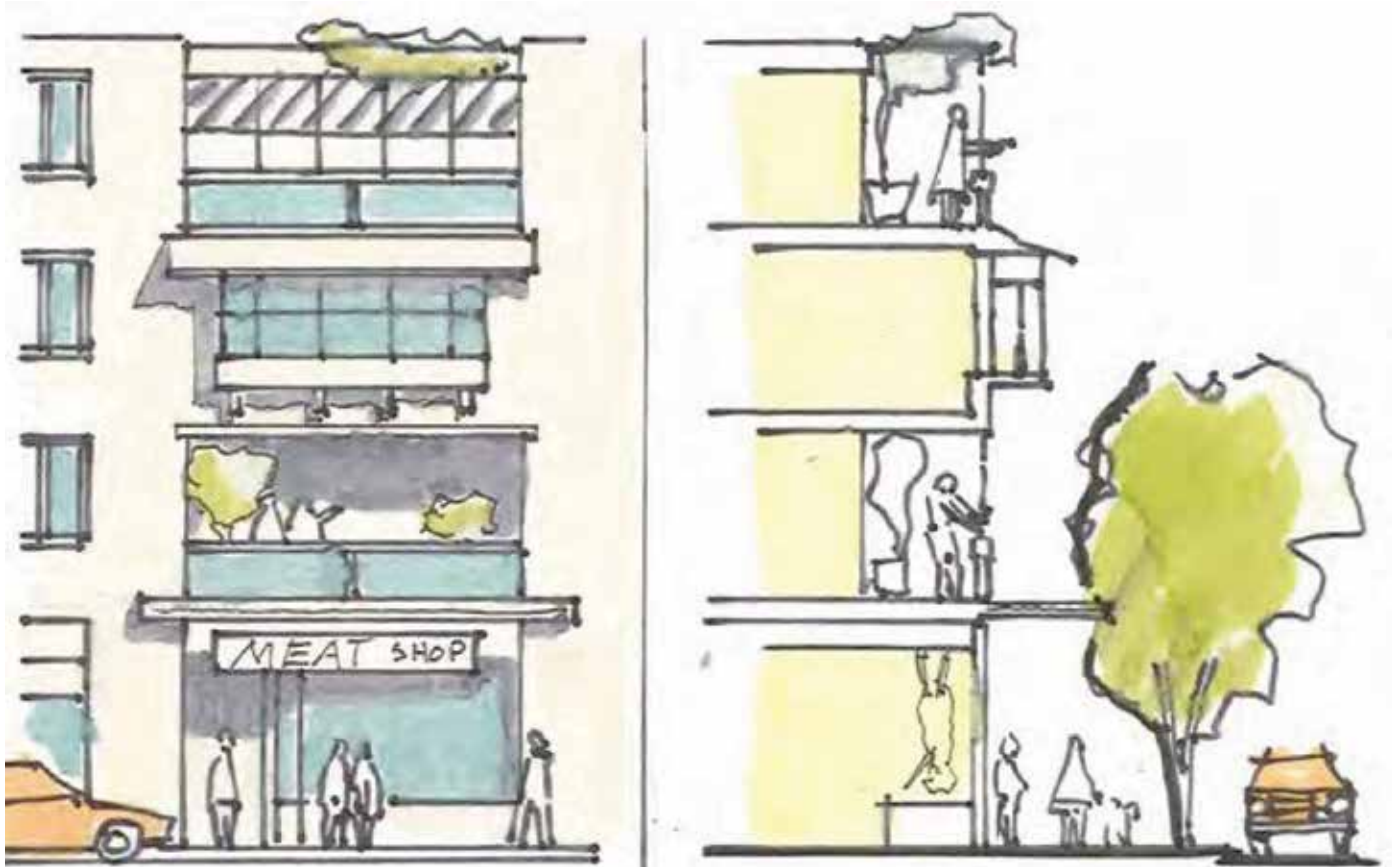
Commercial and Mixed-Use Buildings

1. Locate publicly oriented active uses at grade and at or near the sidewalk edge.
2. Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades.
3. A signage and lighting program for any commercial development should be designed as a totality with signs, lighting, and weather protection architecturally integrated from the outset.

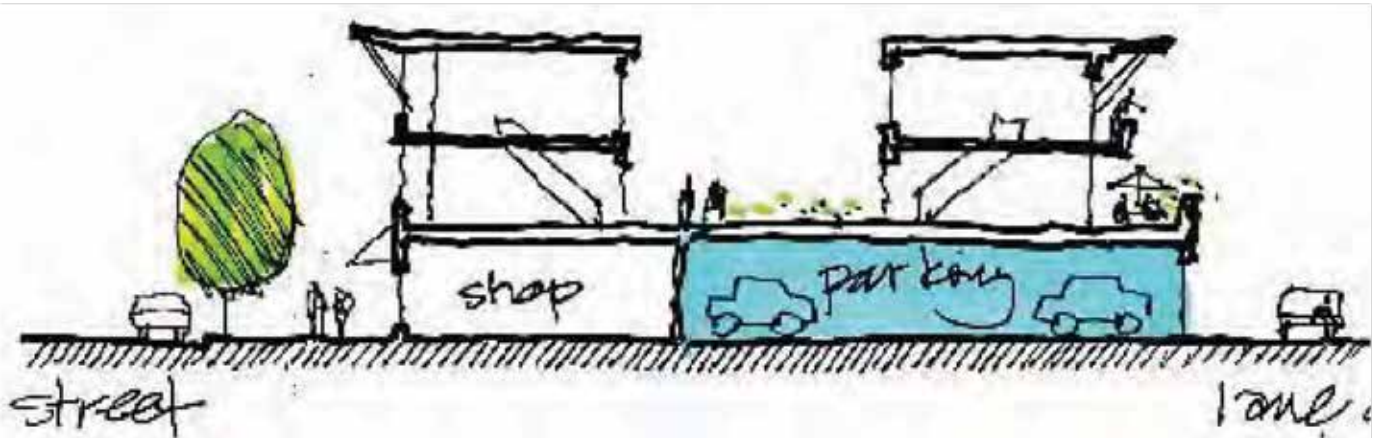


Active uses oriented to public streets and open spaces creates vibrant and attractive neighbourhoods (above).

4. Provide pedestrian access to storefronts and businesses from the adjacent public street and orient upper-storey windows and balconies to overlook adjoining public open spaces.
5. On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
6. Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
7. Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.
8. Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.



Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.



On corner sites, develop street-facing façades for both streets.

Photo: Joaquin Karakas

9. Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
10. Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape.



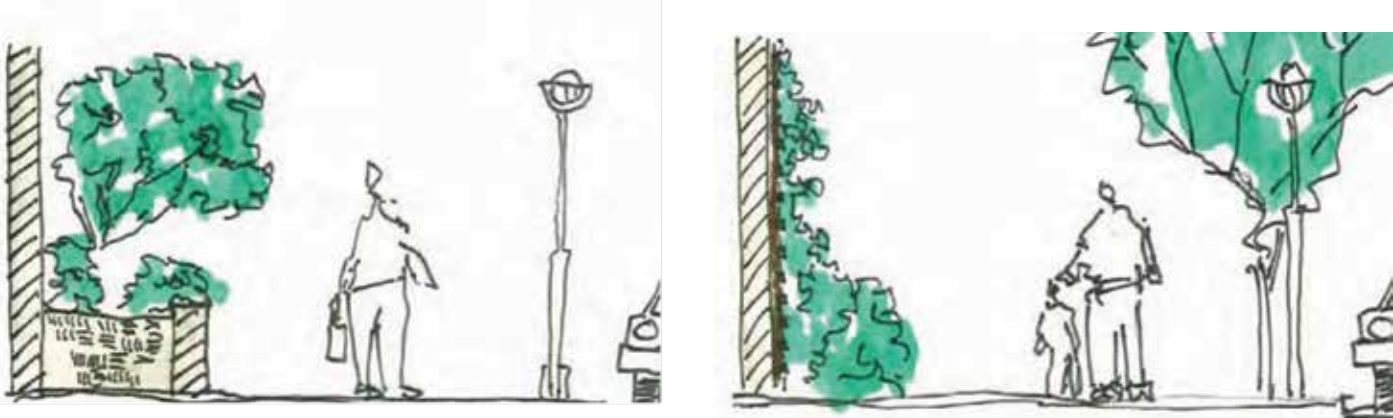
Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

11. Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:

- Install a vertical trellis in front of the wall with climbing vines or other plant material.
- Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening.
- Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface.
- Employ quality materials of different textures and colours to make the wall more interesting visually.
- Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest.
- Incorporate walls into a patio or sidewalk café space.
- Terrace (step down) retaining walls.



Incorporate plantings and other landscape details such as banners.



When unavoidable, blank walls should be screened with landscaping or through incorporation of a patio cafe or special materials to make it visually more interesting.



Residential Buildings

1. Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways and communal spaces, while ensuring the security and privacy of residents.
2. Incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
3. Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
4. Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
5. Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.



Incorporate a front patio or stoop to create street activity and association among neighbours.

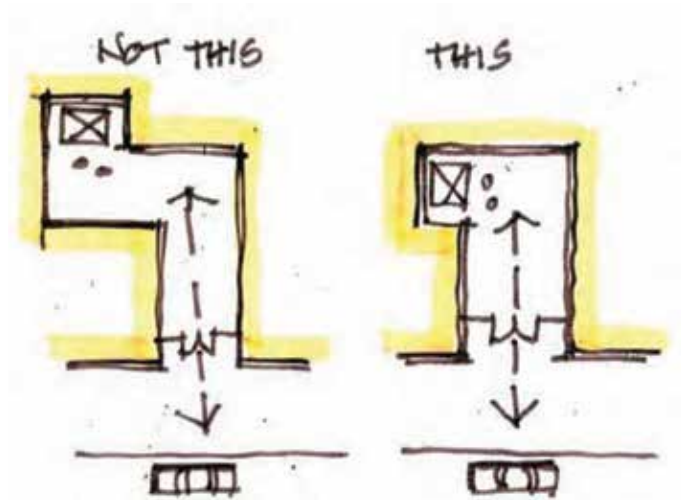
6. Locate off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public open spaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
7. A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
8. Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple accesspoints to enhance building access and connectivity with adjacent open spaces.



Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street.



Consider using landscape transition zone between entryways and public sidewalks.



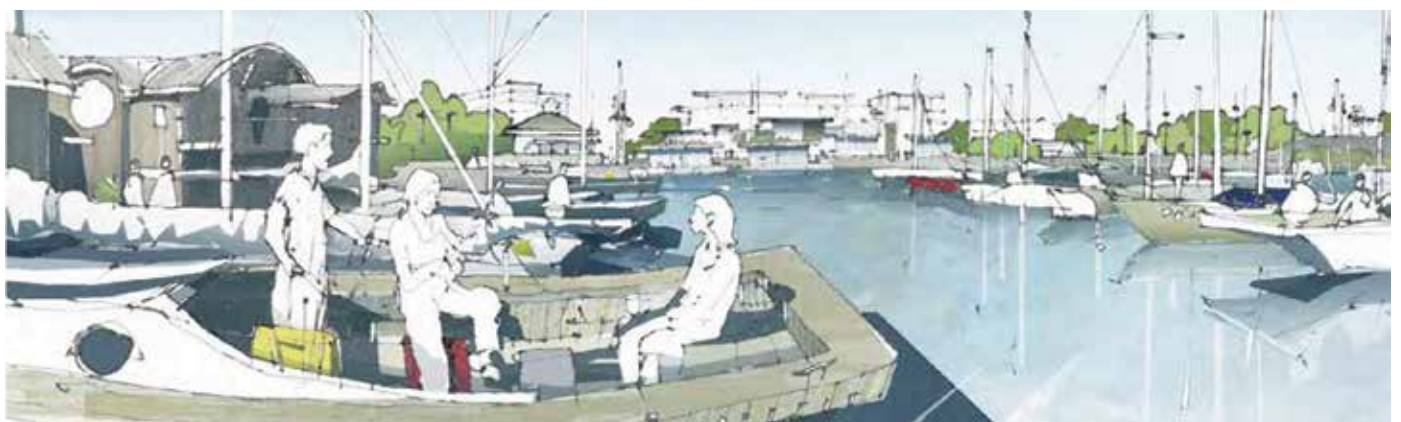
Direct sight lines into elevator lobbies are safer.

Visual & Physical Connections to the Harbour

1. Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
2. New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art and the harbour.
3. Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour.
4. Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina.
5. Mark/celebrate corners and street-end views through building and open space design.
6. Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.
7. New development adjacent or near to the harbourfront should respond to relevant sections of the the provincial "Flood Hazard Area Land Use Management Guidelines."



Maintain street-end views to the harbour-front



Create new public connections to harbour front uses and activities.

Neighbourliness

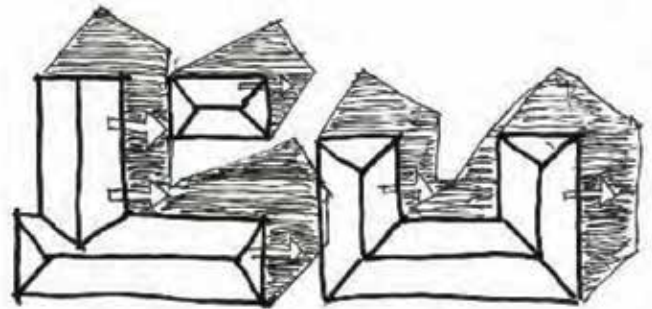
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale and use of adjacent buildings.

1. New projects should provide a sensitive transition to nearby, less intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk and scale between the anticipated development potential of adjacent zones.
2. Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
3. In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.

4. Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
5. Locate development to minimize view impacts on existing and planned future development.
6. Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.

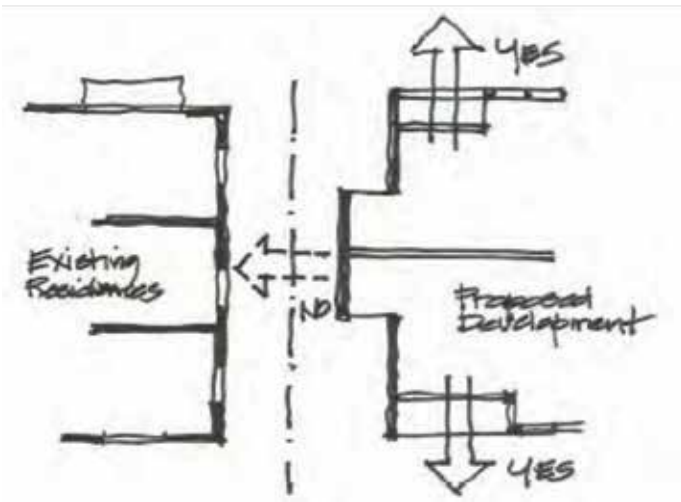


Transition in scale to existing context.



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and building.

7. Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
8. Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active open spaces is strongly discouraged and should be avoided.
9. Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
10. Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 1. Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 2. Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 3. Stagger windows to not align with adjacent, facing windows.
 4. Primary windows into habitable spaces should not face interior side-yards.



This building steps down to the slope to reduce the visual impact from its massing.

Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.



Minimize overlook into adjacent private yards.

Architectural Concept: Achieving a Human Scale

Overview and Intent

These are general guidelines for architecture and are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements and materials allow people to feel comfortable using and approaching it.

Building Articulation

Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys and façade elements such as window treatments, building entries and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in modern composition.

General Guidelines

1. The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
2. Design buildings to express their internal function and use.
3. Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians. Examples of architectural features include:
 1. Building height, massing, articulation and modulation;
 2. Bay windows and balconies;
 3. Corner features accent, such as turrets or cupolas;
 4. Decorative rooflines and cornices;
 5. Building entries; or
 6. Canopies and overhangs.

Examples of architectural details include:

1. Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.);
2. Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors);
3. Articulation of columns and pilasters;
4. Ornament or integrated artwork;
5. Integrated architectural lighting;
6. Detailed grilles and railings;
7. Substantial trim details and moldings; or
8. Trellises and arbors.



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians.

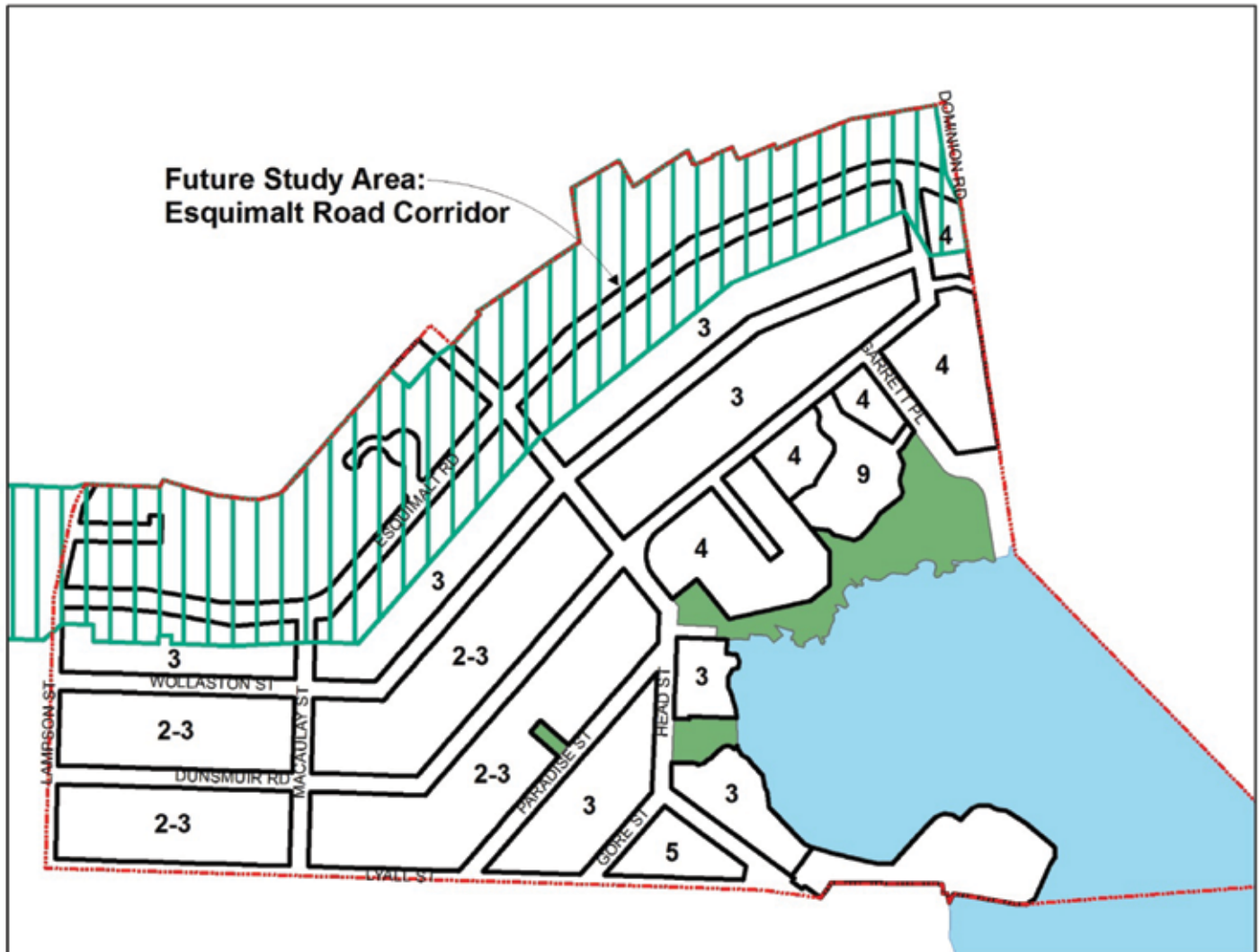
4. Locate and design entrances to create building identity and to distinguish between individual commercial and residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
5. Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
6. Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
7. Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 1. Are vertically proportioned and approximately the size and proportion of a traditional window;
 2. Include substantial trim or molding;
 3. Are separated from adjacent windows by a vertical element;
 4. Are made up of small panes of glass; or
 5. Are separated with moldings or jambs but grouped together to form larger areas of glazing.
8. The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
9. In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
10. Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.



Design entrances to distinguish between individual commercial and residential.

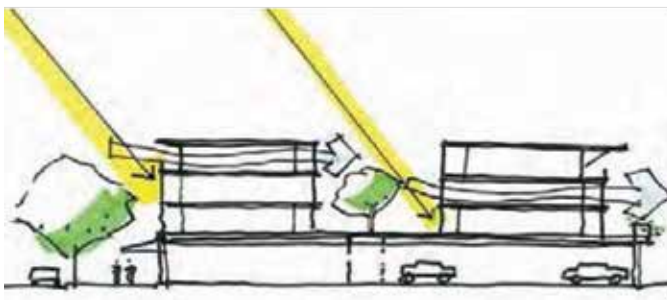
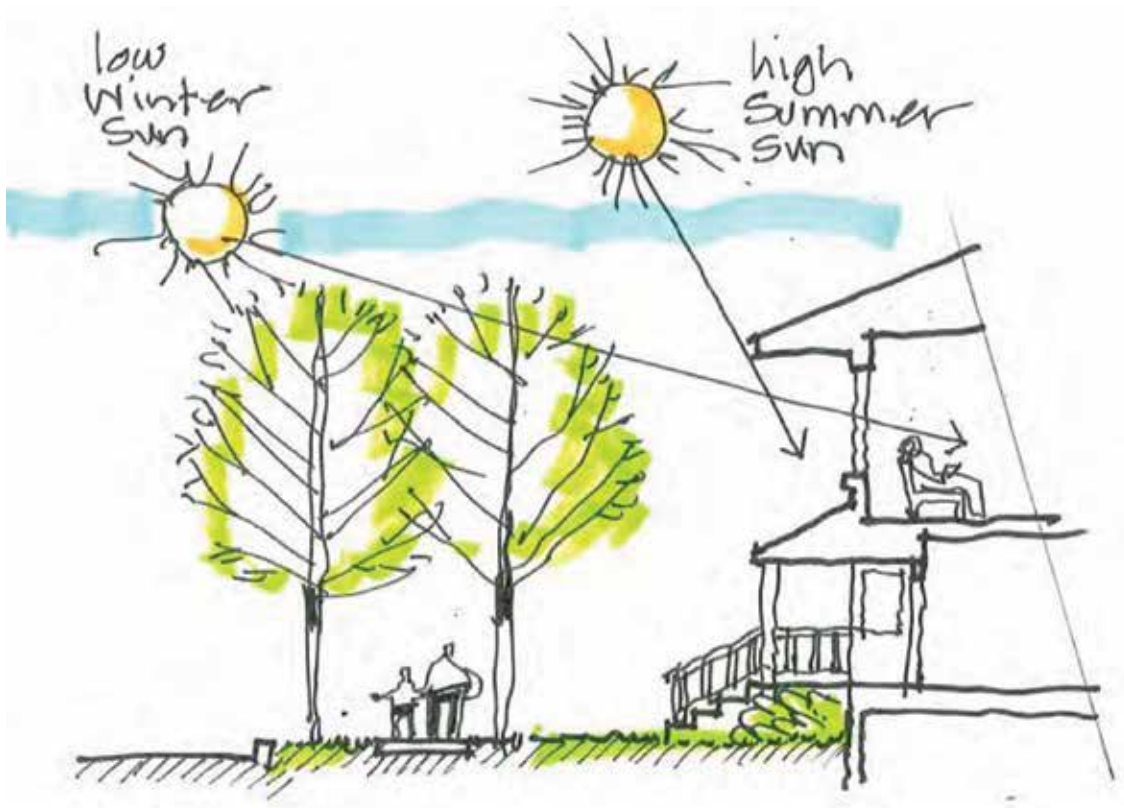
Height

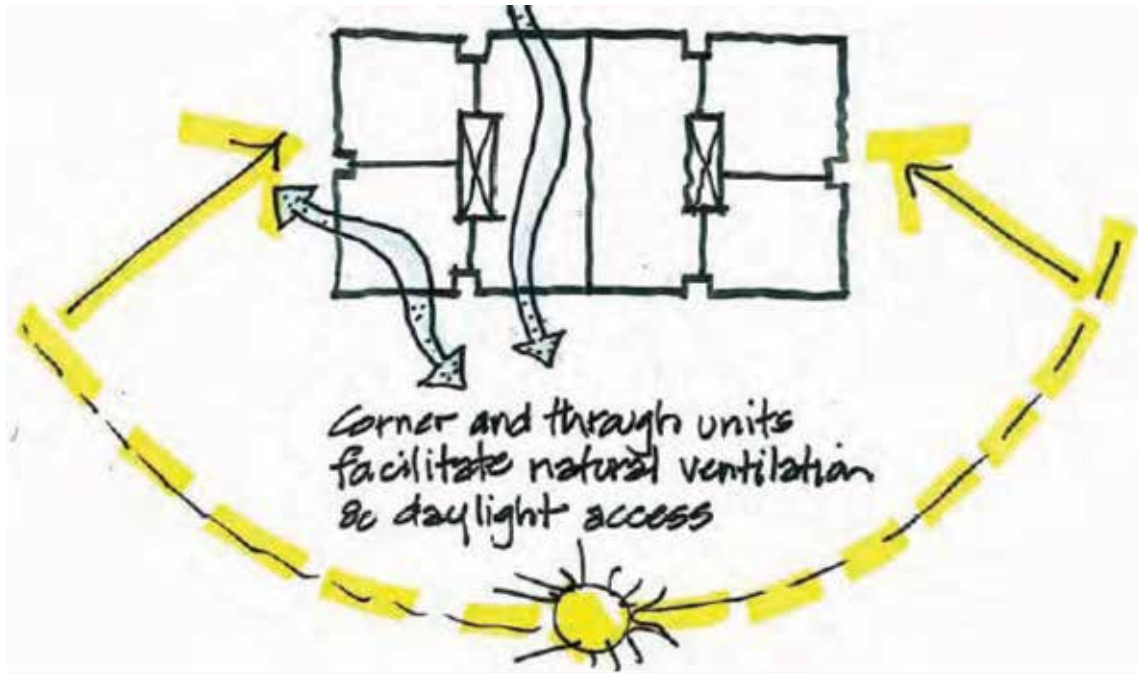
Note that the heights shown are storeys and are guidelines. They are not height regulations.



Green Healthy Buildings and Open Spaces

1. Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing livability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces and being responsive to the existing ecosystems and natural context.
2. Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
3. Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.



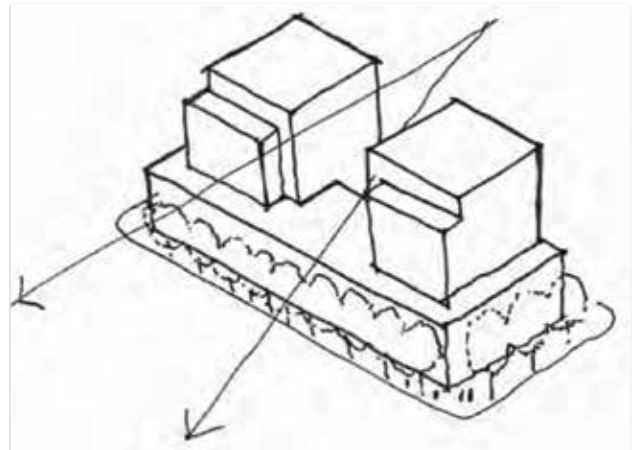


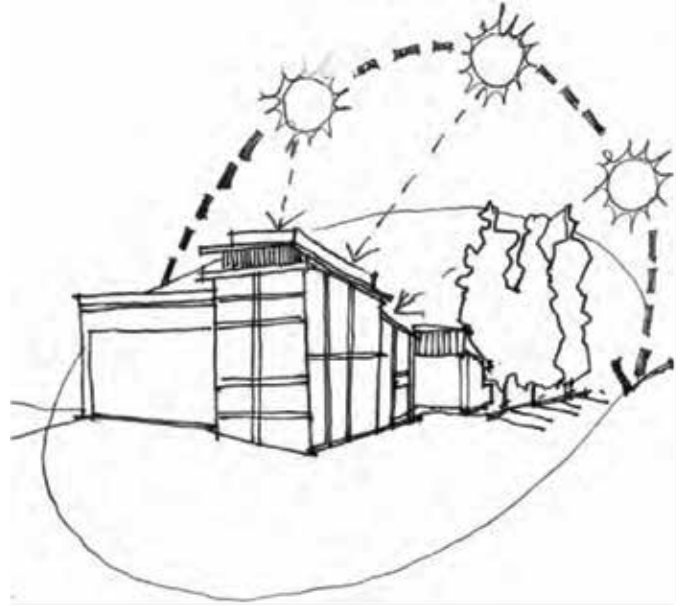
Incorporate common areas as defining elements of projects.

4. New buildings should not block significant views or solar access to adjacent buildings and open spaces.
5. Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
6. Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
7. Retention and infiltration best management practices for rainwater should be used as appropriate.
8. Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.



A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.





Use best management practices for rainwater where appropriate.



Urban village High Street looking north-west at corner of Head and Lyall Street.



Urban village High Steet (Head Street South) looking south.



Looking north to harbour and urban village High Street



View towards urban village High Street from the harbour looking west.



View from future building looking south-east to harbour across urban village High Street and Captain Jacobson Park.



Head Street looking south towards Gore and Lyall.



Urban village High Street: conceptual siting, form and massing.



Urban village High Street: conceptual siting, form and massing.



Urban village High Street: conceptual siting, form and massing.

29 DPA NO 12: MCCLOUGHLIN POINT



29.1 Area

Development Permit Area No. 12 applies only to Lot 1, Section 11, Plan EPP36468. PID 030-006-813.

29.2 Designation

Development Permit Area No. 12 is designated for the purpose of establishing objectives for:

- Section 488(1)(a) – natural environment, its ecosystems and biological diversity;
- Section 488(1)(b) – protection of development from hazardous conditions;
- Section 488(1)(d) – form and character, revitalization of area in which commercial use permitted;
- Section 488(1)(f) – form and character of industrial development;
- Section 488(1)(h) – energy conservation;
- Section 488(1)(i) – water conservation; and
- Section 488(1)(j) – GHG emissions reduction.

29.3 Justification

Site of a former oil storage facility, McLoughlin Point is an oceanfront site and a prominent entrance to Victoria Harbour. An objective is redevelopment of mixed uses, including revitalization when commercial uses are permitted, of impeccable design and cohesive operation, respecting adjacent DND lands as well as the broader communities. Public access from land and the water is encouraged, primarily for educational purposes. Water access recognizes the historic use of the land. Public access by land may be limited by DND.

As an oceanfront site, there are nearby natural habitat considerations worthy of protection and preservation, as well as enhancement of the subject property itself given the history of heavy industrial use. The subject property is susceptible to high winds, and high tides, including risk of tsunami and therefore development on the site must occur in a manner to protect it from these natural hazards, while respecting the need to do so in a manner that does not increase risks for other properties or harm the natural environment. The report “Modelling of Potential Tsunami Inundation Limits and

Run-up” for the Capital Region was completed by the Capital Regional District’s Local Government Emergency Program Advisory Commission; it identified risks for McLoughlin Point that the CRD has indicated it has been considering to date in its proposal for this site.

The CRD has also prepared and submitted a number of professional reports which identify special conditions at McLoughlin Point, including for a sewage treatment plant, and further justify designation to achieve objective both the CRD’s and Township’s objectives. These include but are not limited to the reports entitled:

“Core Area Wastewater Treatment Program McLoughlin Point Zoning Application, Township of Esquimalt McLoughlin Point Risk Assessment,” dated December 18, 2012 prepared by Stantec Consulting Ltd;

“Archaeological Review of the Proposed Capital Regional District McLoughlin Point Wastewater Treatment Facility,” dated January 2013 prepared by Tera Environmental Consultants;

“Capital Regional District Core Area Wastewater Treatment Program McLoughlin Point Wastewater Treatment Facility Traffic Considerations,” dated January 23, 2013 prepared by Bunt & Associates;

“Terrestrial Environmental Effects of the Proposed Capital Regional District McLoughlin Point Wastewater Treatment Facility,” dated January 2013 prepared by Tera Environmental Consultants; and

“Community Impact and Mitigation Report,” dated January 2013 and prepared by CitySpaces Consulting Ltd.

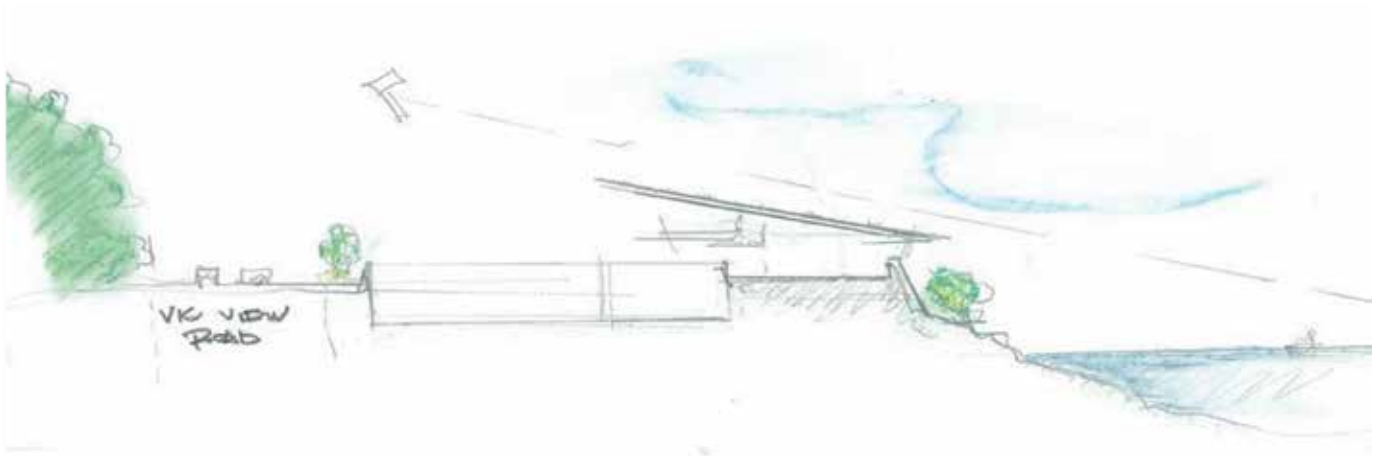
Prominent regional facilities should be models of energy and water efficiency and reduction of greenhouse gas emissions, and such is an objective of this development permit area. This is supported and/or required under the goals established pursuant to the 2007 Capital Region Community Energy Planning process.

29.4 Exemptions

There are no additional exemptions for McLoughlin Point.

29.5 Guidelines

1. Consider the establishment of an 8.0 m buffer from the high watermark.
2. Consider the establishment of a 4.0 m heavily landscaped buffer within the 8.0 m buffer to hide the building(s) on the site.
3. Consider stepping buildings back on the site with the lowest buildings located closest to the shore.
4. Consider the establishment of a seawall using as its design precedent, the convention centre in Seattle.
5. Consider the establishment of an historical interpretation program.
6. Consider the establishment of public access to the 8.0 m buffer area via a public dock.
7. Consider the incorporation of water features as public art within the design of the building.
8. Consider design and construction in a manner that mitigates environmental and human health impacts (in particular those related to odour and noise), and contributes to the visual quality and scenic beauty of the harbour entrance.
9. Any proposed buildings or structures must incorporate the findings of the “Modelling of Potential Tsunami Inundation Limits and Run-up” for the Capital Region that has been completed by the Capital Regional District’s Local Government Emergency Program Advisory Commission.



10. The treatment system will support environmental, social and economic sustainability, and be considered part of CRD climate action initiatives.
11. Wastewater should be treated as a resource and, wherever possible and practical, provide opportunities for resource recovery and reuse.
12. The McLoughlin Point facility should meet, or exceed the CRD's and the Township of Esquimalt's policies on sustainability and building excellence.
13. Acknowledge and plan for major tsunami events, climate warming effects, and post-disaster resiliency.
14. Incorporate durable, long-lasting, and timeless materials and design strategies.
15. Respect view impacts from all sides, and from above.
16. Incorporate designs that, while respecting the site, ensure the highest standards of materials and workmanship, and are aesthetically pleasing.
17. Incorporate public art into the design.
18. Where feasible, design for on-site heat recovery, and plan for future, long-term, neighbourhood, heat-resource opportunities.
19. Incorporate a green roof system into the Operations and Controls building and other buildings, where appropriate.
20. While much of the site is impervious rocky shoreline, where possible, introduce methods to clean and reduce stormwater runoff, incorporate rain gardens, and consider practical ways to re-use water.
21. Restrict impact on the shoreline, except for those areas where wastewater lines enter or exit the treatment plant.
22. Building forms should respect the site.
23. Wall elements, relating to tsunami and associated catastrophic event protection, such as stepped walls that incorporate angled features, projections, wall terraces, and textures, should reflect the character of the rocky shoreline.

24. The design must demonstrate how the buildings and structures will fit into the site, responding to the shoreline in the forefront, and the evergreen treeline and rocky knoll backdrop.
25. Building heights should vary, but not exceed 15 m, from the finished grade.
26. Design aesthetics should be optimized with the use of appropriate, high quality materials.
27. Exterior building materials, including exterior details, must be selected to withstand intense weather and sea conditions, and must be of a high standard to ensure low maintenance.
28. Doors, overhead doors, and other closures (including hatches, grilles, and louvres) should be durable, thermally resistant, and suitably finished for the marine environment.
29. Windows should have high performance glazing, and be capable of providing natural ventilation, where appropriate.
30. Roof areas must consider views from above.
31. Clarifiers and aerated filters must be covered to meet noise and odour principles.

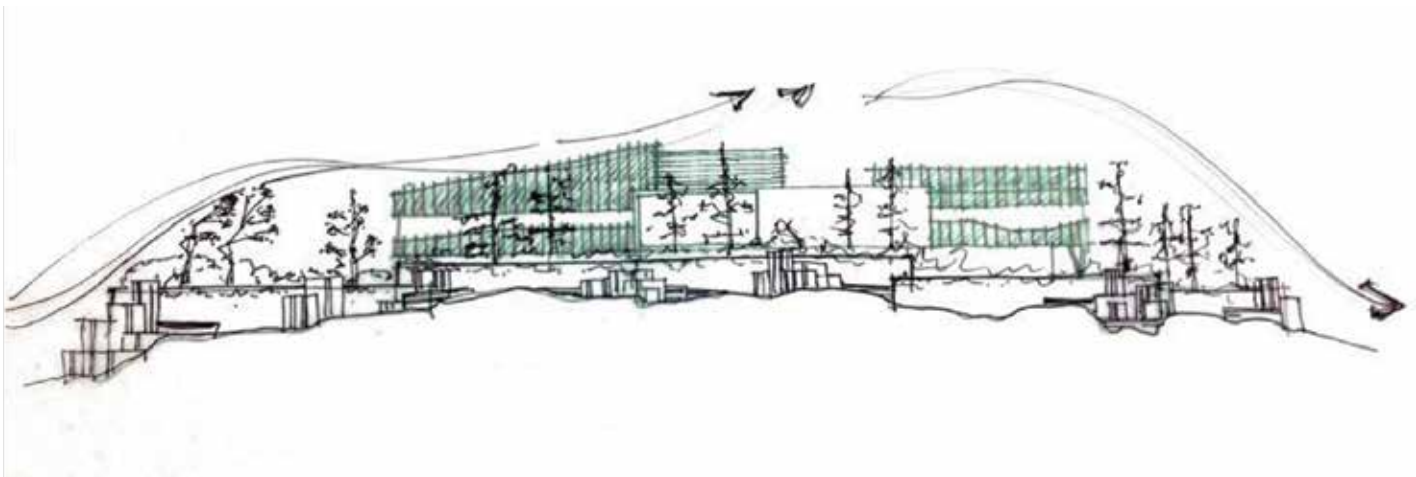


Lighting

1. Light fixtures should provide no more than the minimum lighting needed for their intended purposes, and not exceed levels recommended by the Illuminating Engineering Society for North America Recommended Practice Manual: Lighting for Exterior Environments.
2. Light fixture shields should be specified to reduce impacts on other properties, and when seen from the designated viewpoints.
3. All lighting should be directed downward, and not into the night sky.
4. Energy efficient fixtures should be specified, with consistent colour for all lighting.

Landscape Elements

1. The design concept is based on site conditions, views from the harbour, and a windswept rocky shoreline. With this in mind, landscape elements should include:
 - Use of plant species that are designated hardy to harsh, and for sea spray environments - situate plants such that the force of the wind shapes their future forms;
 - A retaining wall system designed to reflect the rugged and rough-textured surface of boulders and exposed-rock shorelines; and
 - Outdoor storage and parking areas screened through the use of berms, fences, landscaping and/or solid noise-absorbing barriers.



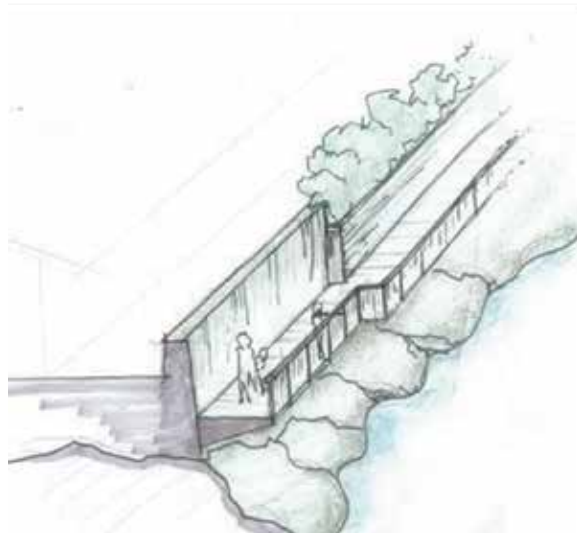
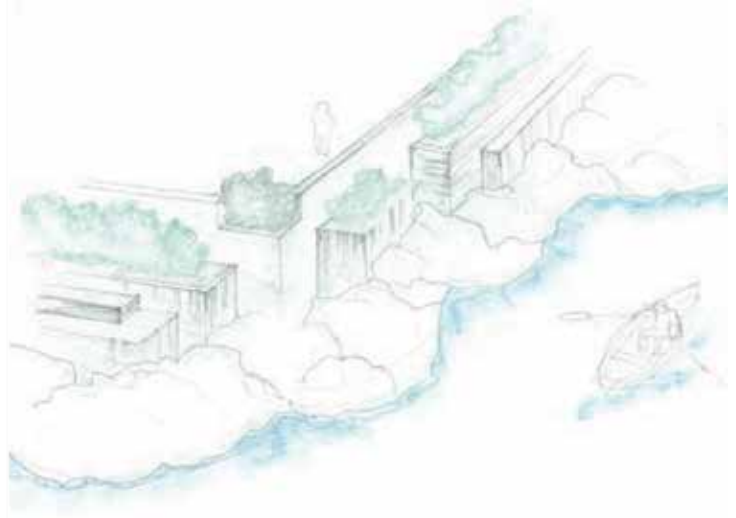
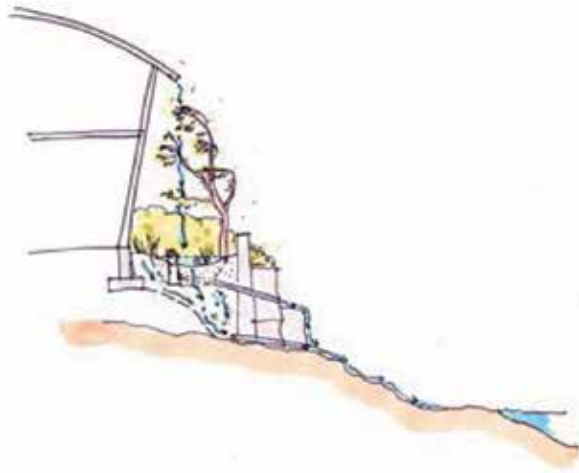
Guidelines for Seawalls and Walls

The retaining wall system should be designed to reflect the rugged and textured surface of the exposed-rock shorelines. To reduce view impacts for neighbouring communities and water/air traffic, the mass of the wall (combined height and width) will be broken up visually with features such as board form relief, wall projections, vertical elements, and wall protrusions. It should feature both rounded, smooth and angular surfaces to reflect the natural shoreline.

Walls are divided into two types: 1) primary walls, which are prominent perimeter retaining walls, and feature walls within the plant; and 2) secondary walls, which serve as infill between the primary walls.



1. Walls must not protrude beyond the high watermark (HWM 1.804m geodetic).
2. The site must be protected by a continuous tsunami protection wall that has a top elevation of not less than 6.5 m above the high watermark.
3. The appearance of wall heights greater than 4.0 m must be minimized by placing step walls in the tsunami protection wall.
4. Planted stepped walls should be a minimum depth of 1.0 m horizontally to allow for landscape elements to be introduced. Where this is not possible, shallower multiple steps may be used.
5. All surfaces of the primary perimeter retaining walls must be finished with random board-formed recesses or other suitable architectural treatment. Vertical recesses should be spaced randomly. A smooth finish should be considered for secondary walls.
6. The design should plan for development of a pedestrian pathway along the waterfront side of the site.

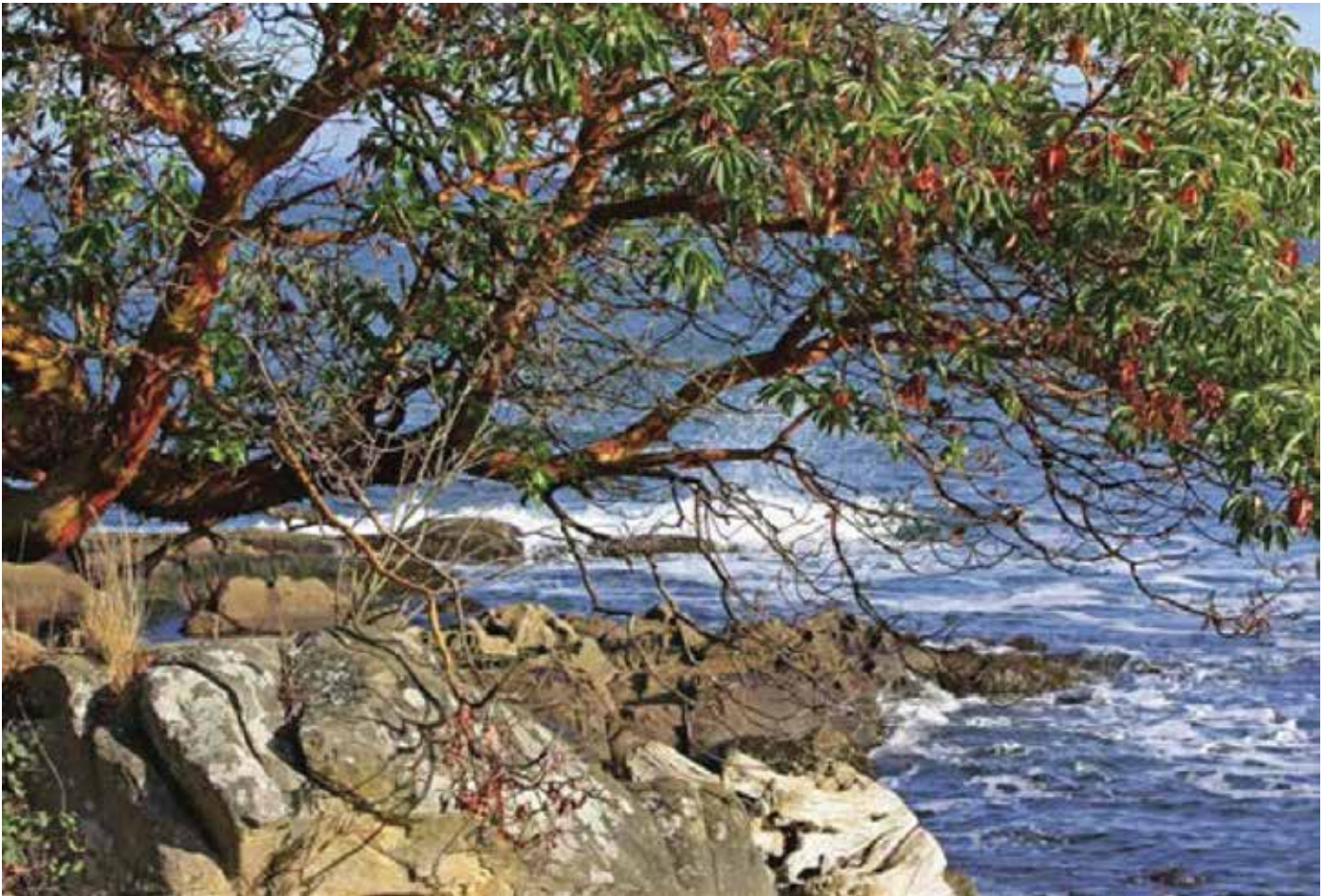


Guidelines for Planting - General

1. Distribution of plants will be limited due to sea spray and wind exposure, particularly on the south side of the site
2. Planting will exclude lawns.
3. Mature plant heights must be at least 60 cm tall for all planted areas to shade undesirable weed species.
4. Planting densities must ensure that vegetated areas will have 100% plant coverage after two full growing seasons.
5. Planted areas will be irrigated with a high efficiency irrigation system.
6. Plants should be drought tolerant and require minimal water after the two-year establishment period.
7. Green roofs must be installed fully established to minimize wind erosion and maintenance.
8. All planting will be to BCNLA/BCSLA Landscape Standards.

Guidelines for Planting Along Seawalls

1. Distribution of plants will be limited due to sea spray and wind exposure.
2. Trees must be situated more than 10 m from the south facing wall, as this will be a high wind velocity area.
3. The following species are considered appropriate for use along the waterfront:
 - *Pinus contorta* var. *Contorta* (Shore Pine);
 - *Arbutus menzesii* (Pacific Madrone);
 - *Rosa nutkana* (Nootka Rose);
 - *Symphoricarpus albus* (Snowberry);
 - *Arbutus unedo* (Strawberry Tree);
 - *Myrica californica* (Sweet Gale);
 - *Lonicera pileata* (Privet Honeysuckle); and
 - *Mahonia aquifolium* (Oregon Grape)

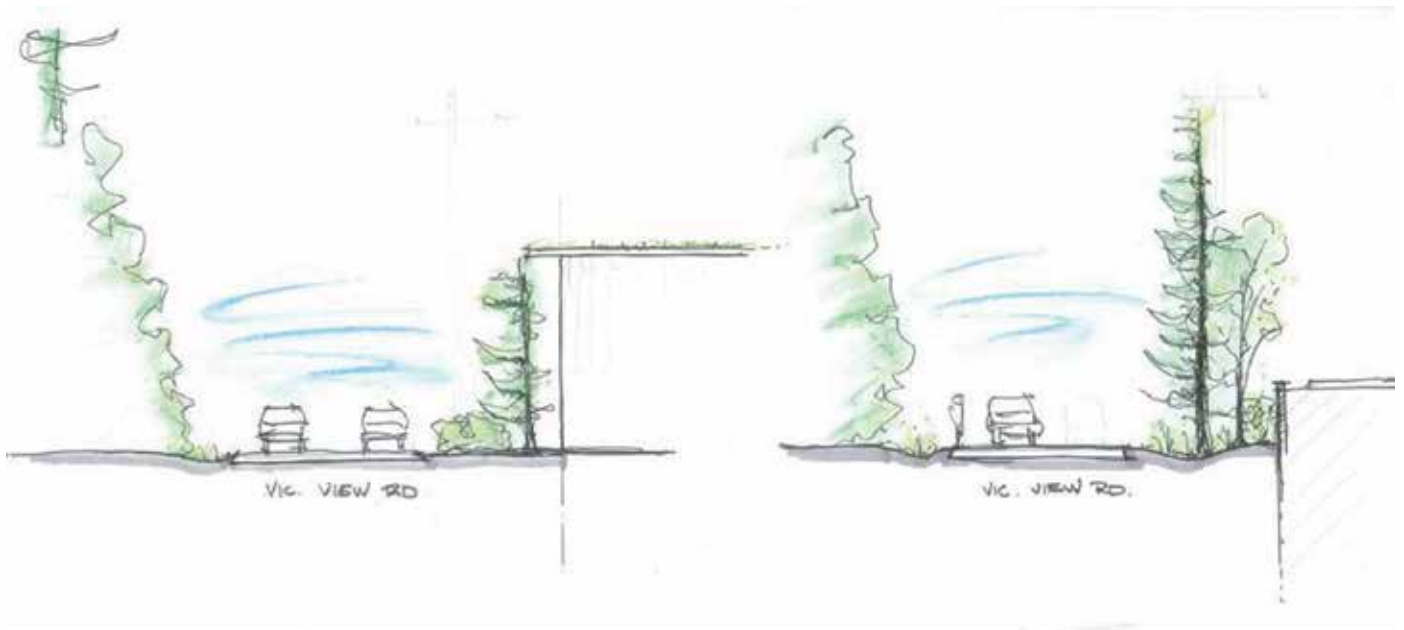


Guidelines for Planting Adjacent to Building Entrances

Planting around the building entrances can be more design driven, and specific hard and soft landscaping should complement the building architecture.

Guidelines for Screening on Victoria View Road

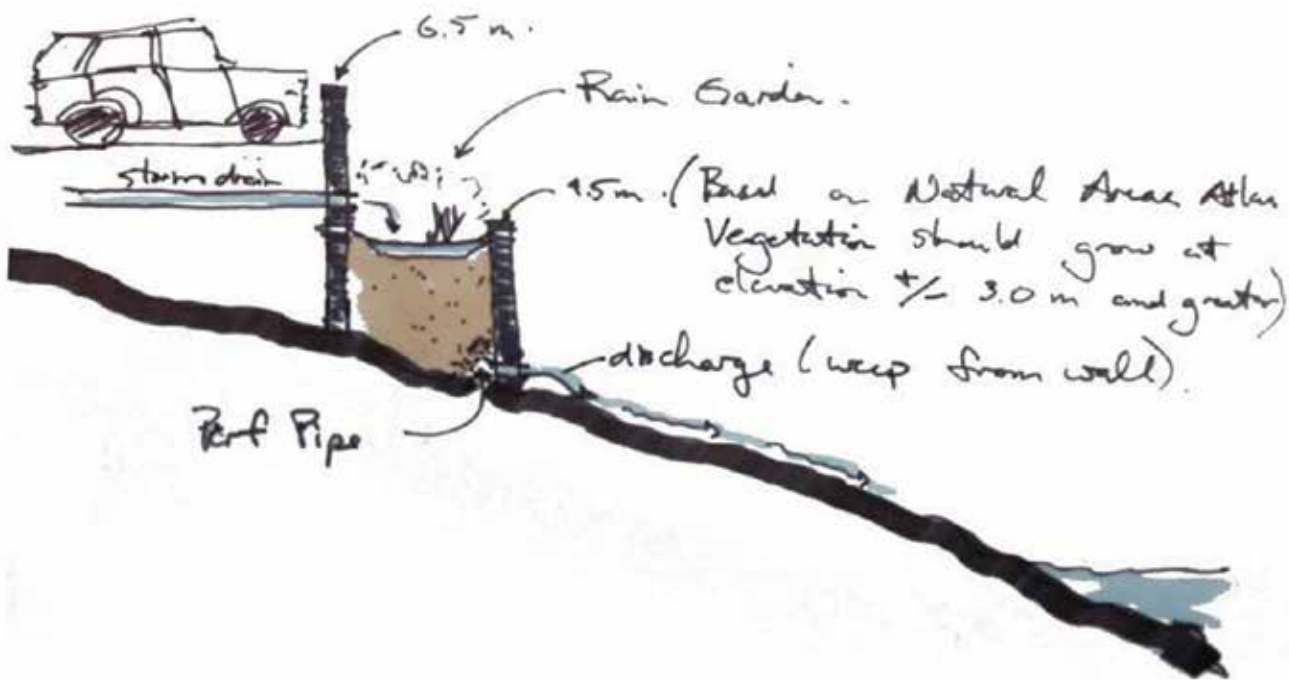
1. To break up the mass of concrete walls, introduce screening (mostly of coniferous tree plantings) along the road frontage and adjacent property lines. The CRD should work with the Department of National Defence to allow for landscaping along the road frontage adjacent to the site. Cluster trees to provide clear 8 m wide gaps to allow for future maintenance access (from a crane).
2. A continuous shrub border will be required at the base of the wall to screen the lower retaining wall and reduce the risk of vandalism. Shrubs in this area are to be native species only, with the exception of those adjacent to the two entrances, where lower evergreen screening is desirable.
3. In situations with larger retaining walls, vines can be considered, but must be supported by cable systems.
4. The following species are considered appropriate for use in screening applications:
 - *Pseudotsuga menziesii* (Douglas Fir);
 - *Rosa nutkana* (Nootka Rose);
 - *Symphoricarpus albus* (Snowberry); and
 - *Parthenocissus tricuspidata* (Boston Ivy).



Stormwater Management

The following stormwater management measures should be considered for the site:

1. Stormwater from the internal roadways and parking areas will be treated to remove 80% of TSS from a 6-month rain event prior to discharge.
2. Treatment of roadway and parking run-off can come in the form of:
 - A combination of rain gardens and bioswales adjacent to the parking and roadways, complete with raised overflow basins, and under drains connected to the storm drain system;
 - Aqua-pave permeable paving, complete with an under drain system in discrete areas where direction of run-off to a bioswale is not feasible; or
 - A combination of these.
3. A conventional storm drain will be installed with an outfall to the ocean. All drainage from the site will eventually be discharged through this pipe.
4. The buildings will connect directly to the piped storm drain system. Building drainage will bypass the treatment system. However, a rain garden, stormceptor, or similar end-of-pipe treatment device could be installed if treatment of roof drainage is required.



Parking and Services

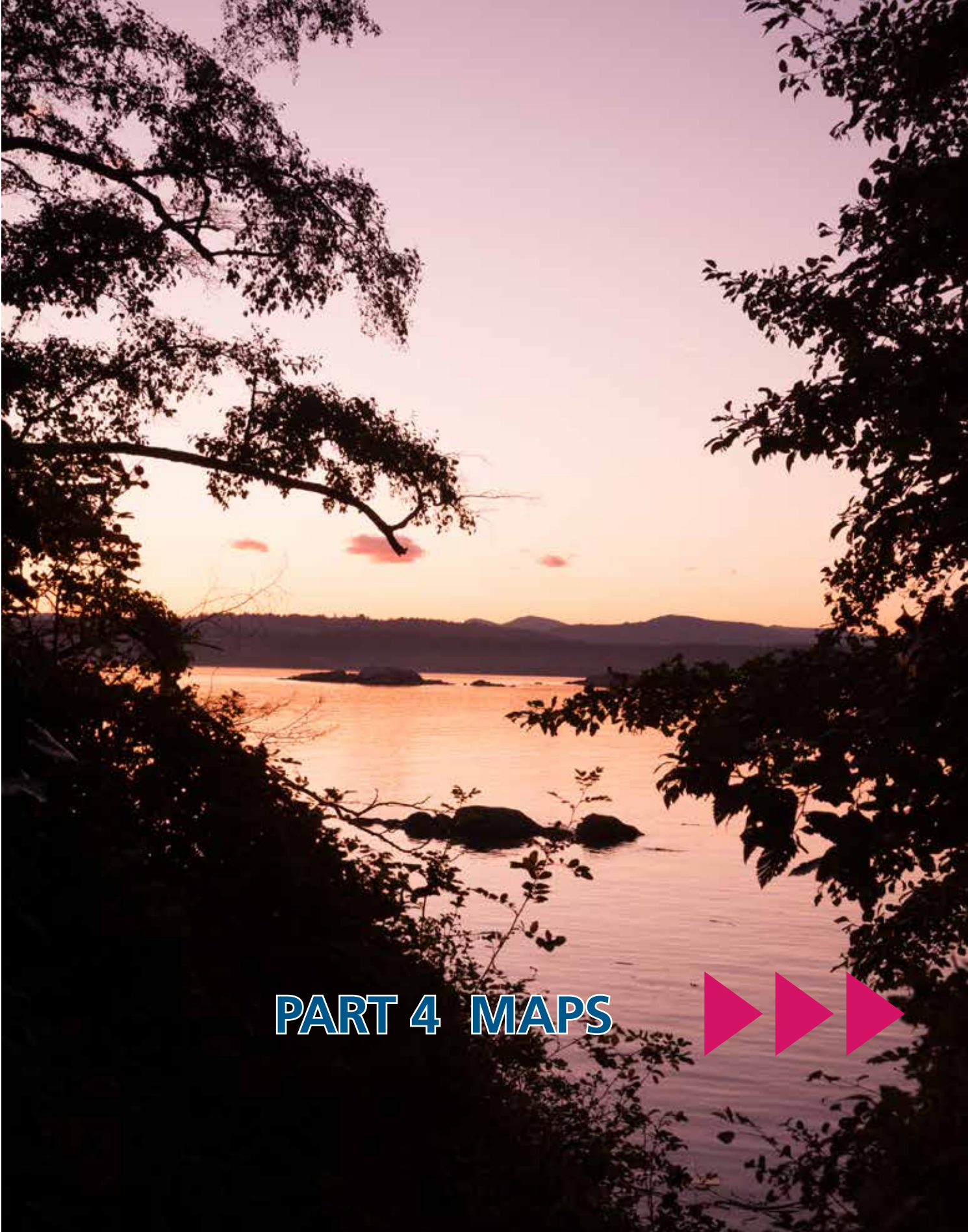
Parking for visitors, plant and system operation staff, and CRD maintenance vehicles should be suitably screened through the use of berms, fences, landscaping and/or solid noise-absorbing barriers to minimize visual impact.

Signage

Limit signage to directional and identification as required for wayfinding.

Public Art and Education

1. Public art shall be provided. The CRD and Township of Esquimalt will work together to confirm the process and requirements.
2. Plans should include capacity for organized, educational site visits to learn about the functioning of the treatment system, the regional liquid waste management program, resource recovery, etc.



PART 4 MAPS



OFFICIAL COMMUNITY PLAN: MAPS



Schedule A

Present Land Use Designations

Schedule B

Proposed Land Use Designations

Schedule C

Roads Network

Schedule D

Sewer Network

Schedule E

Water Network

Schedule F

Public Parks and Facilities

Schedule G

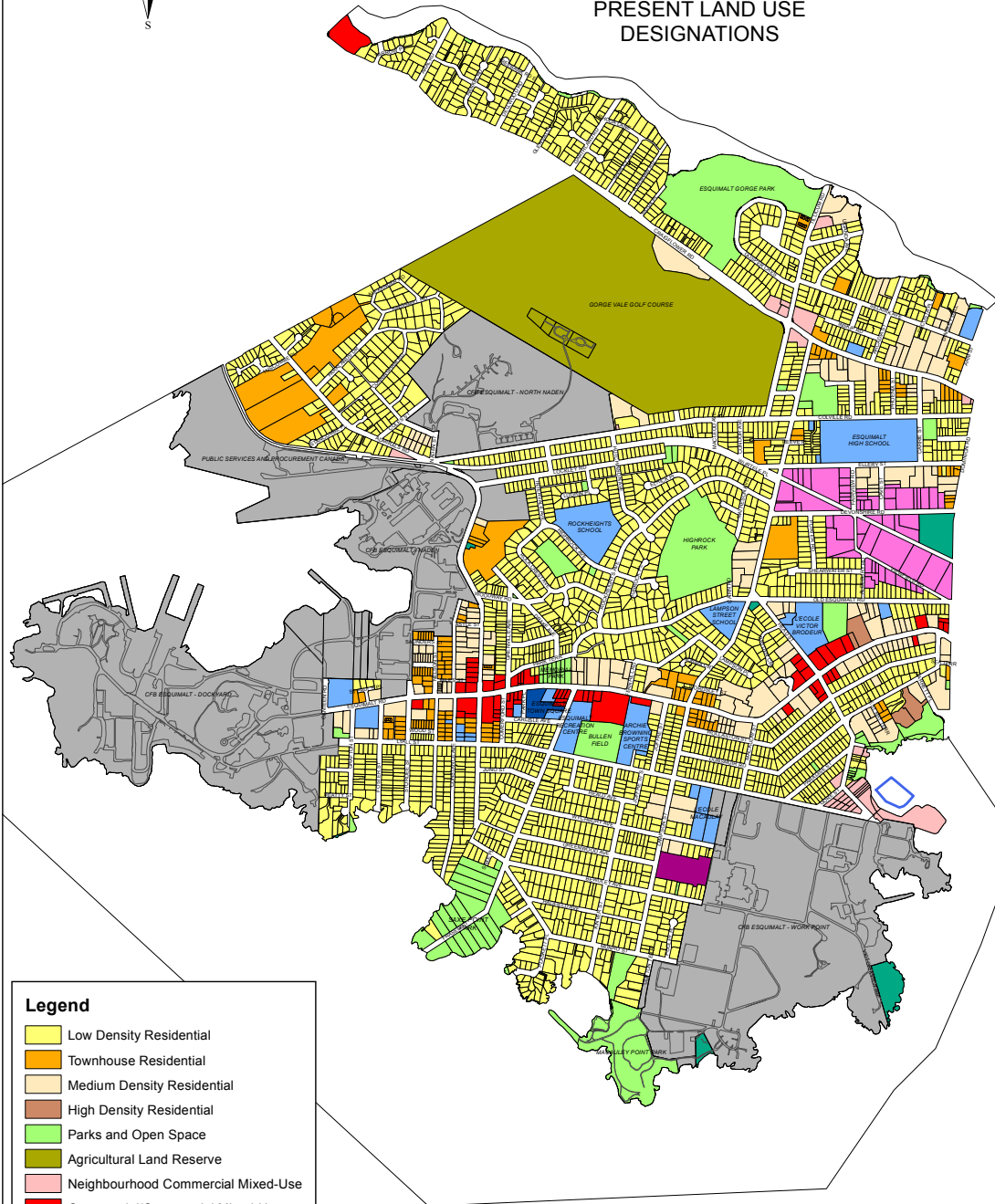
Public View Corridors

Schedule H

Development Permit Areas

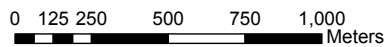
OFFICIAL COMMUNITY PLAN

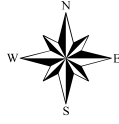
SCHEDULE A
PRESENT LAND USE
DESIGNATIONS



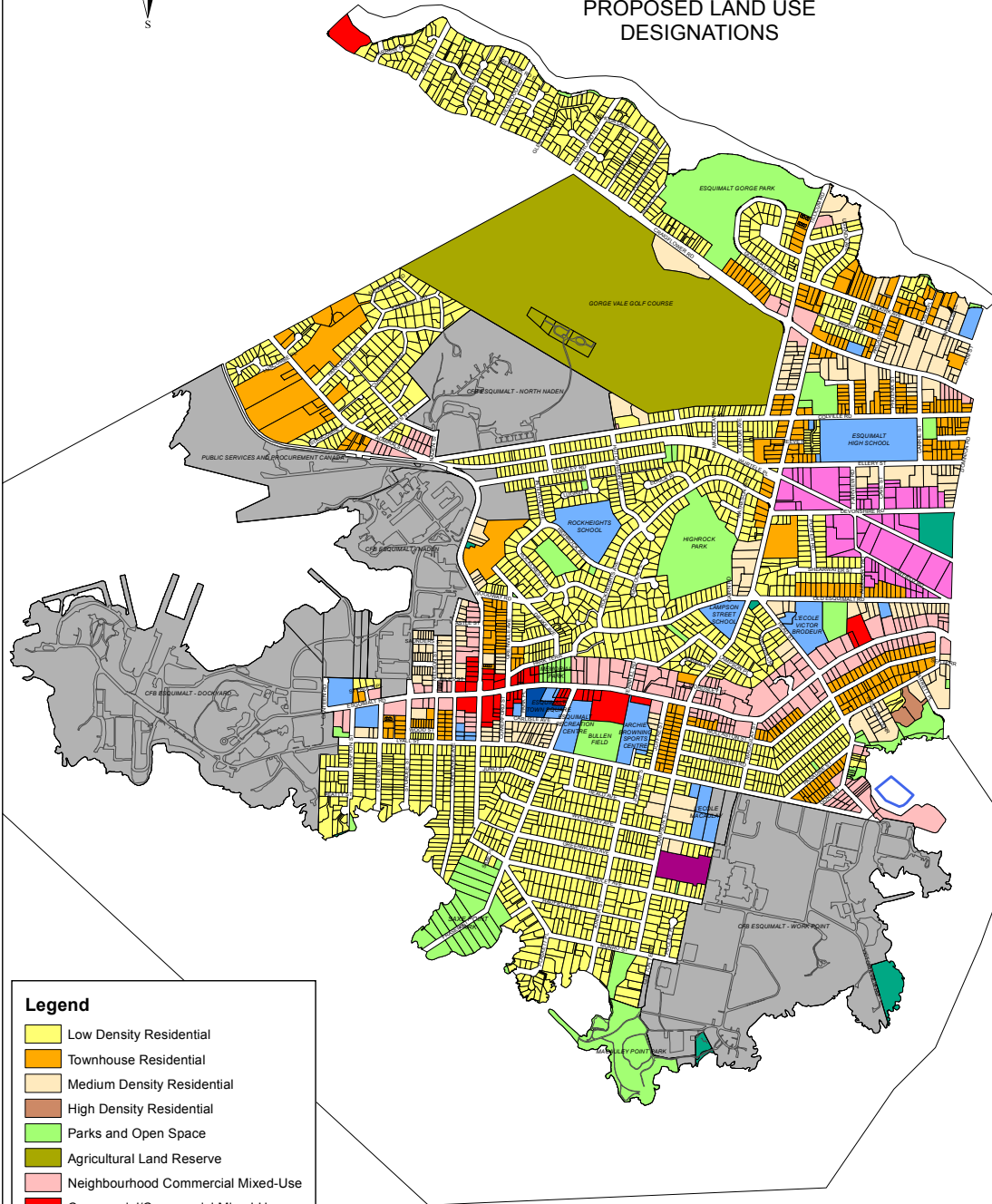
Legend

- Low Density Residential
- Townhouse Residential
- Medium Density Residential
- High Density Residential
- Parks and Open Space
- Agricultural Land Reserve
- Neighbourhood Commercial Mixed-Use
- Commercial/Commercial Mixed-Use
- Industrial
- Public Utilities
- Institutional
- Federal Land
- English Inn Mixed-Use
- Esquimalt Town Square
- Floathome Residential



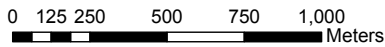


OFFICIAL COMMUNITY PLAN
SCHEDULE B
 PROPOSED LAND USE DESIGNATIONS

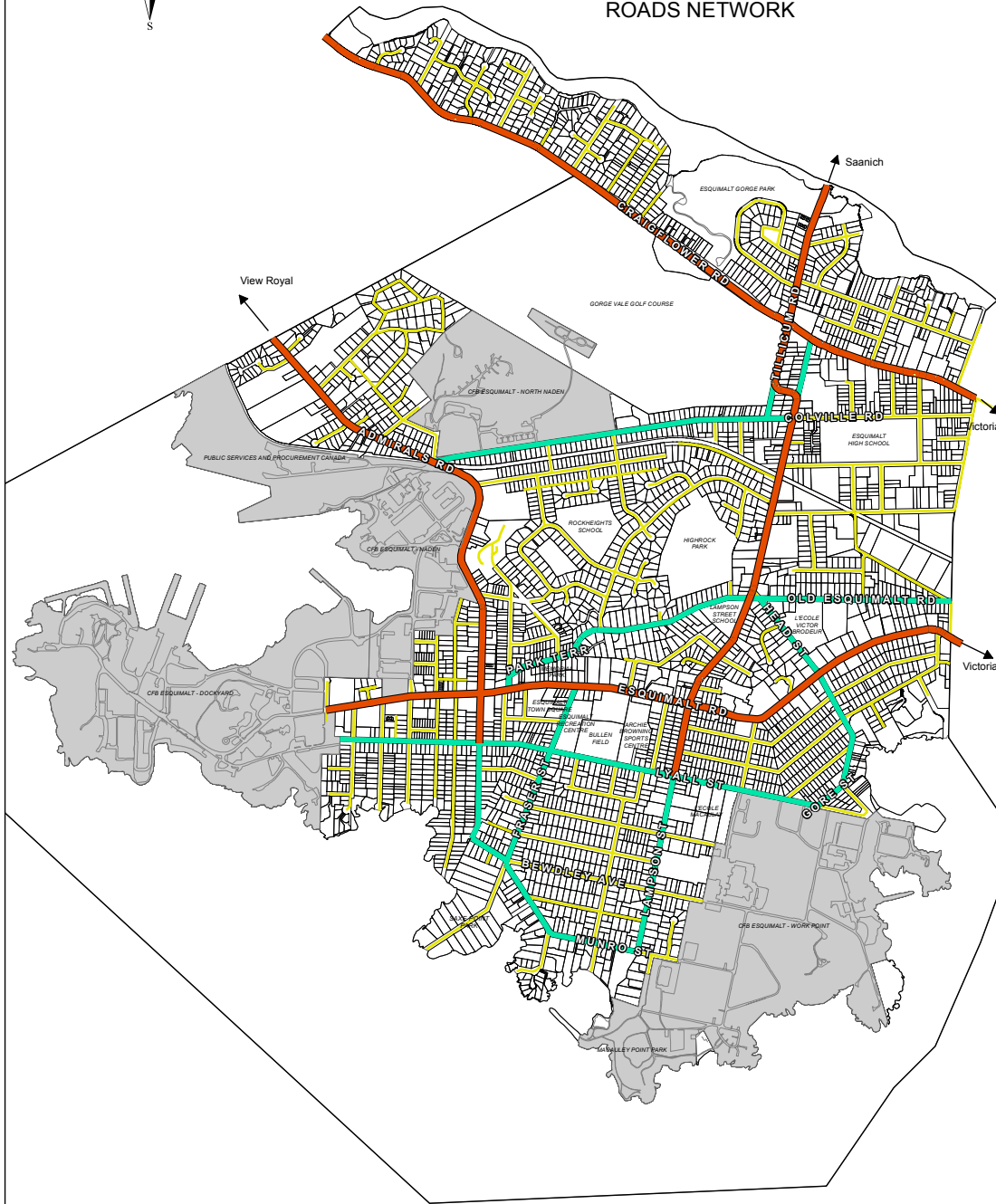
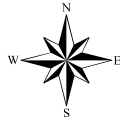


Legend

	Low Density Residential
	Townhouse Residential
	Medium Density Residential
	High Density Residential
	Parks and Open Space
	Agricultural Land Reserve
	Neighbourhood Commercial Mixed-Use
	Commercial/Commercial Mixed-Use
	Business
	Public Utilities
	Institutional
	Federal Land
	English Inn Mixed-Use
	Esquimalt Town Square
	Floathome Residential

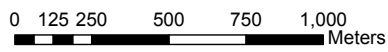


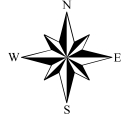
OFFICIAL COMMUNITY PLAN
SCHEDULE C
 ROADS NETWORK



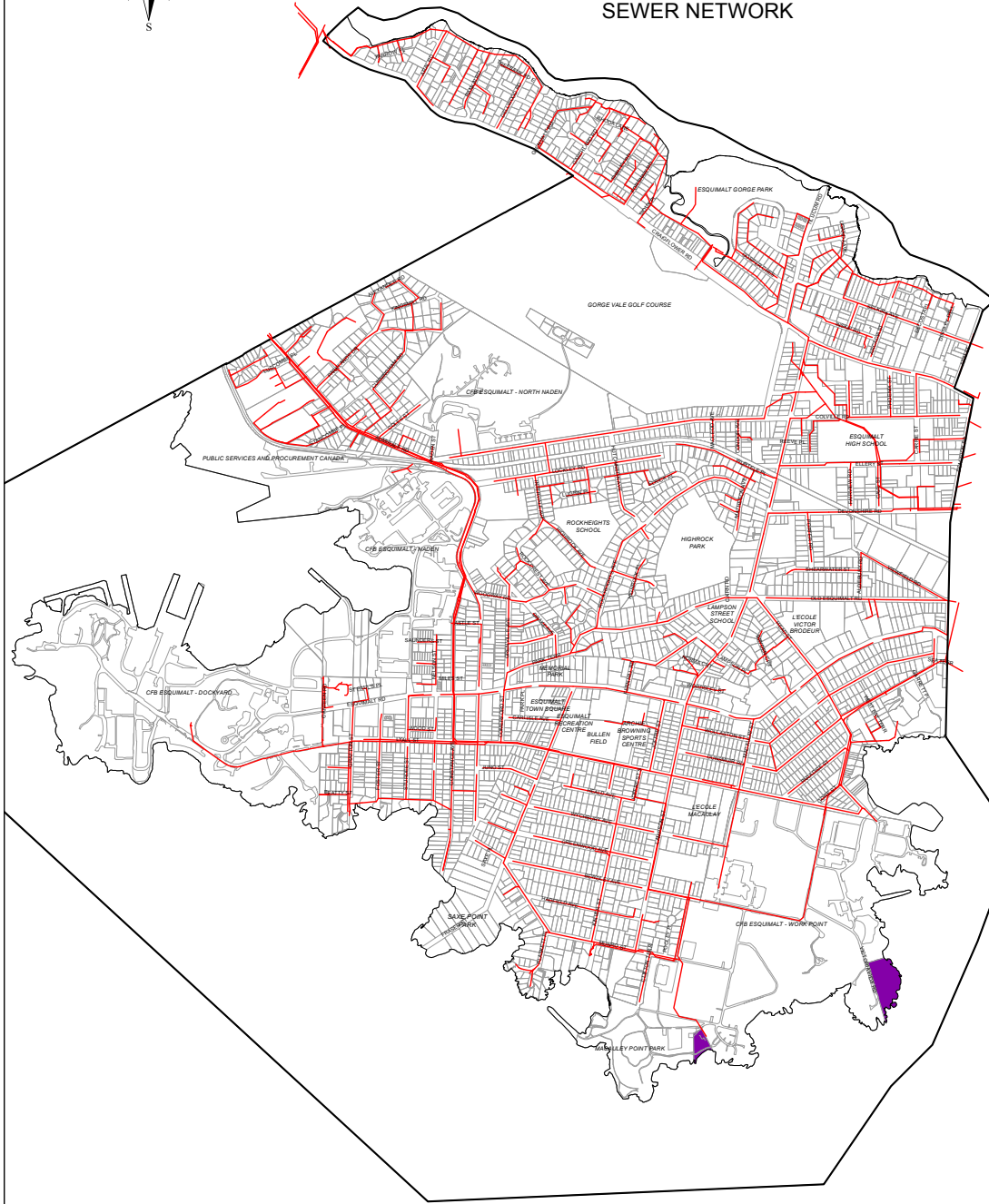
Legend

- Major Roads
- Residential Collector Roads
- Roads (Built)
- Township of Esquimalt Boundary



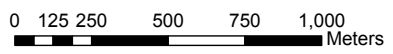


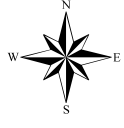
OFFICIAL COMMUNITY PLAN
SCHEDULE D
SEWER NETWORK



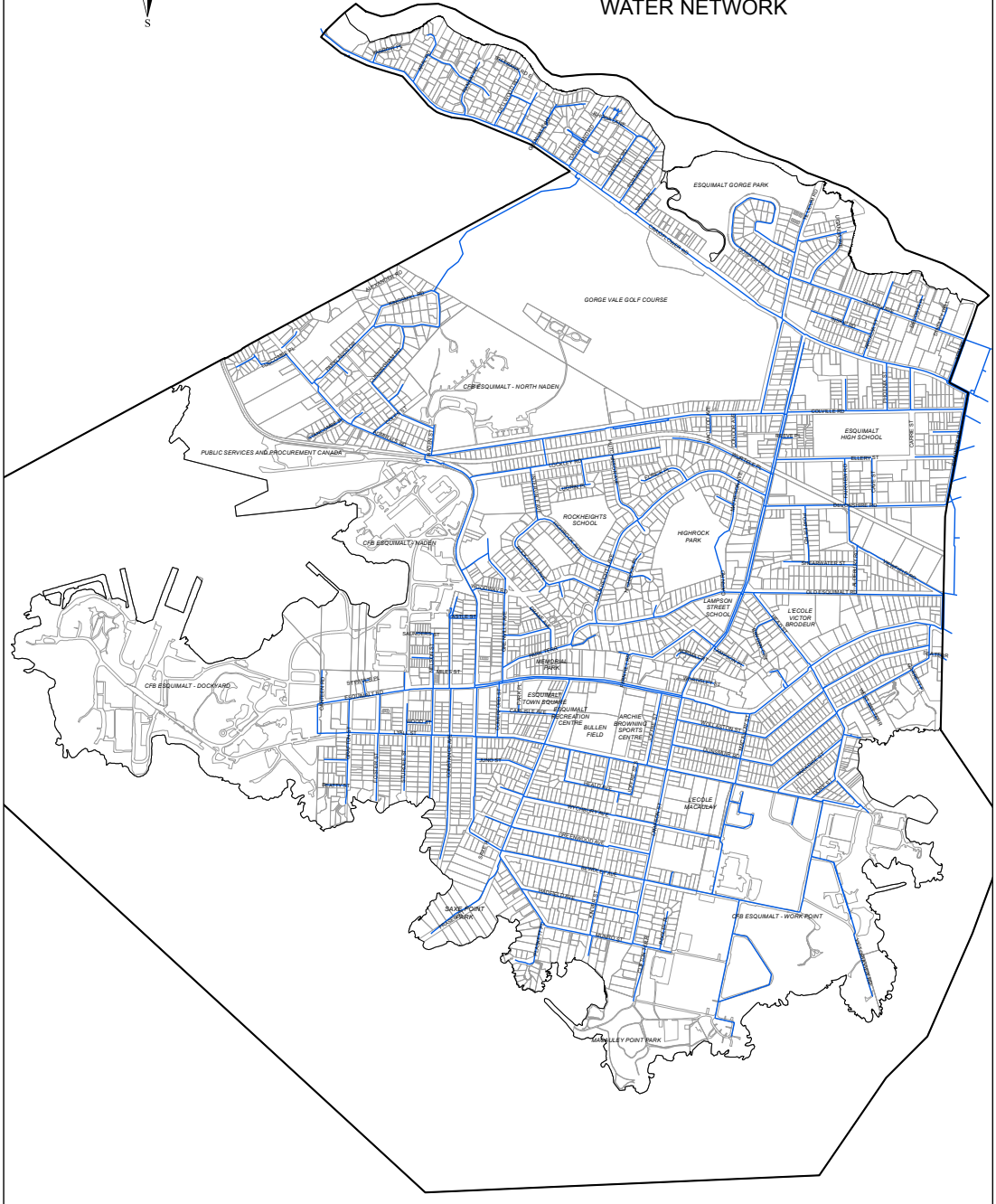
Legend

- Township of Esquimalt Boundary
- Sewer Mains
- Sewage or Waste Treatment Facility





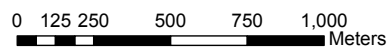
OFFICIAL COMMUNITY PLAN
SCHEDULE E
 WATER NETWORK

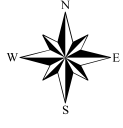


Legend

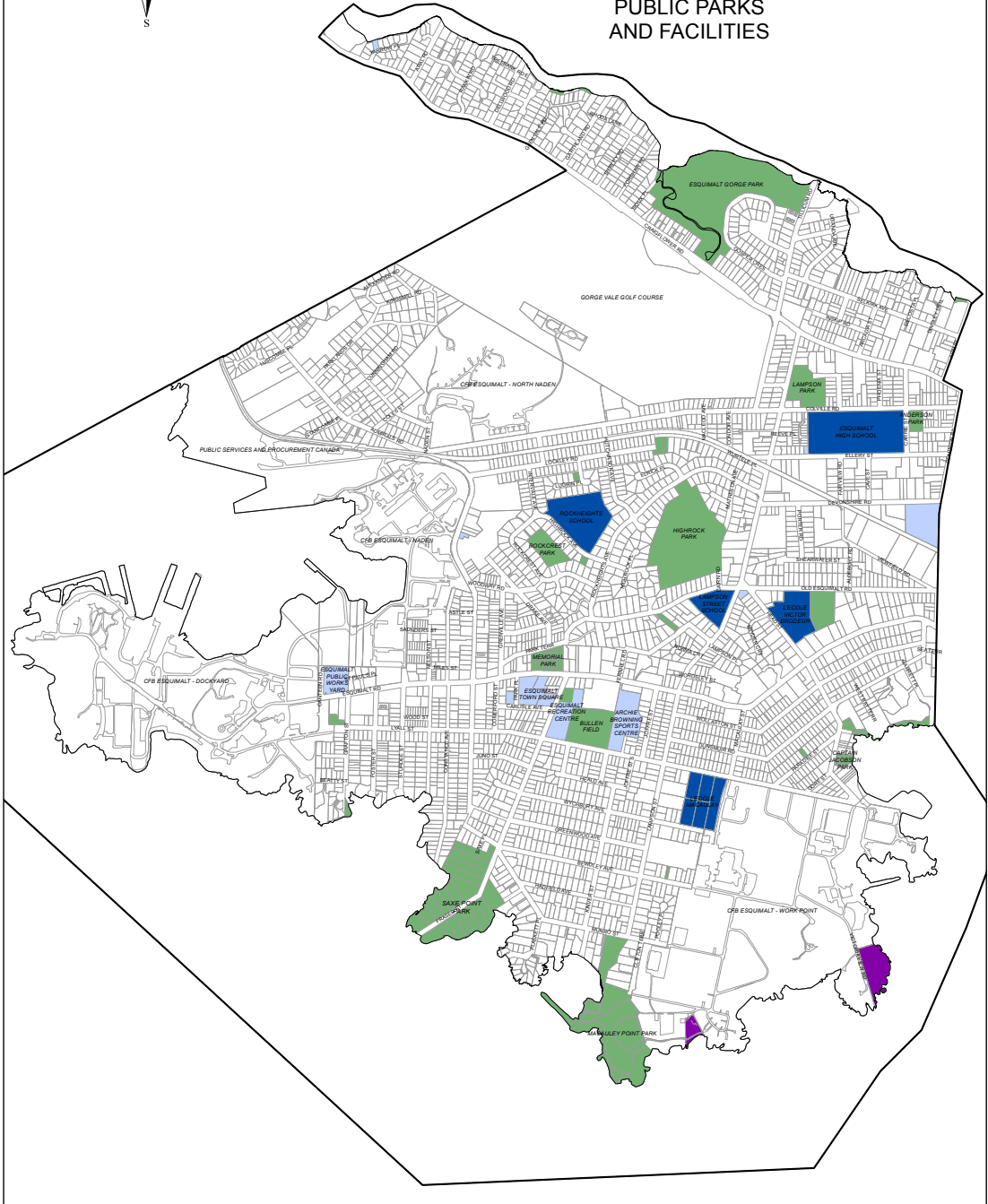
- Township of Esquimalt Boundary
- Water Mains

No new major water systems are proposed by the Township of Esquimalt. The City of Victoria provides and operates the water services in the Township of Esquimalt.



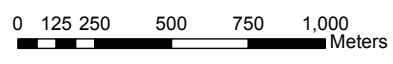


OFFICIAL COMMUNITY PLAN
SCHEDULE F
PUBLIC PARKS
AND FACILITIES



Legend

- Township of Esquimalt Boundary
- Parks
- Schools
- Public Facilities
- Waste Treatment or Disposal Sites



OFFICIAL COMMUNITY PLAN

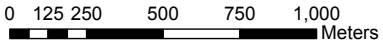
SCHEDULE G

PUBLIC VIEW CORRIDORS



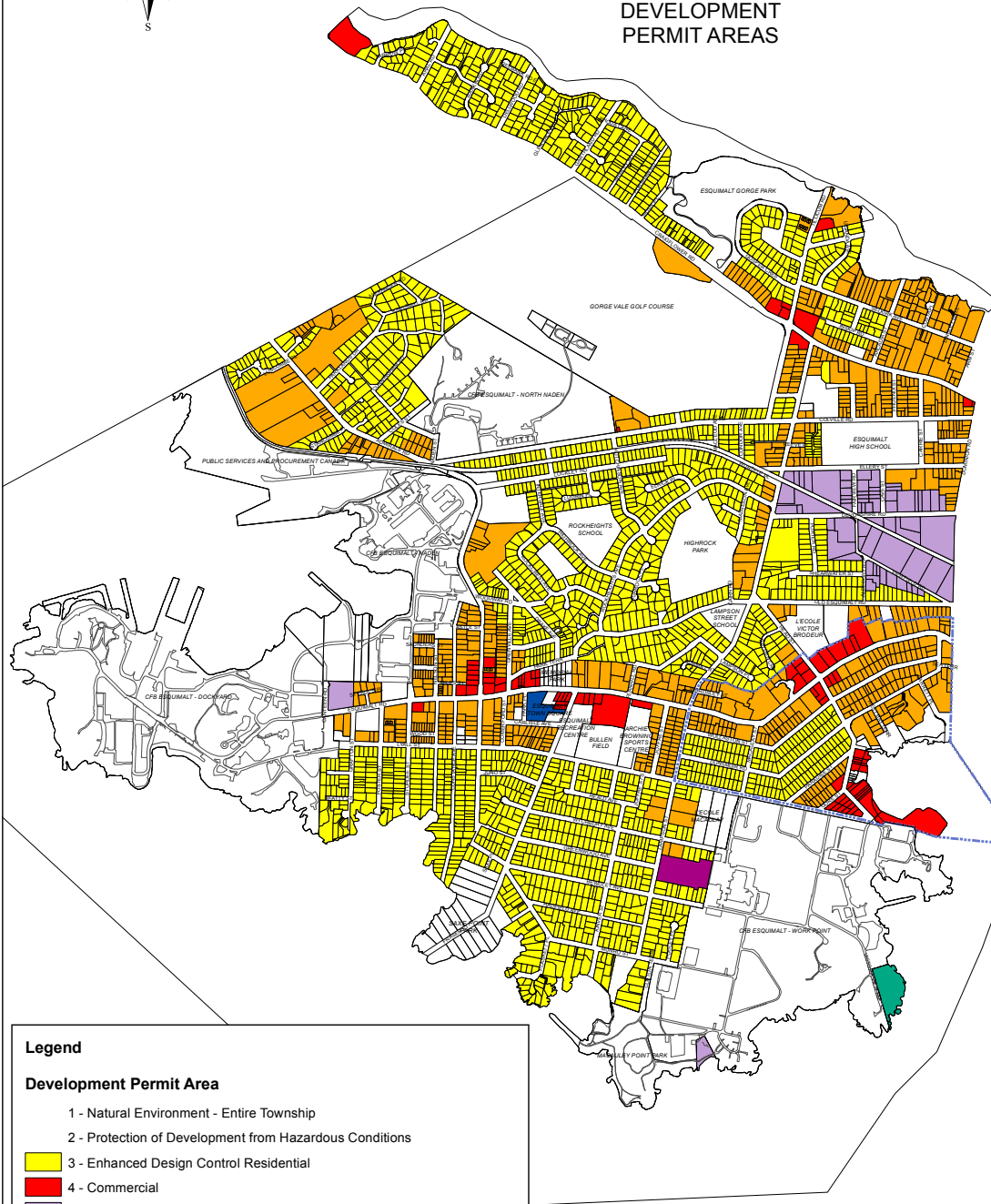
Legend

- Township of Esquimalt Boundary
- View Cones





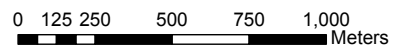
OFFICIAL COMMUNITY PLAN
SCHEDULE H
 DEVELOPMENT
 PERMIT AREAS



Legend

Development Permit Area

- 1 - Natural Environment - Entire Township
- 2 - Protection of Development from Hazardous Conditions
- 3 - Enhanced Design Control Residential
- 4 - Commercial
- 5 - Industrial
- 6 - Multi-Family Residential
- 7 - Energy Conservation and Greenhouse Gas Reduction - Entire Township
- 8 - Water Conservation - Entire Township
- 9 - English Inn
- 10 - Esquimalt Town Square
- 11 - West Bay
- 12 - McLoughlin Point





PART 5 APPENDICES



Appendix 1

Township of Esquimalt Heritage Values



Township of Esquimalt Heritage Values

Derived from Heritage Values Open House held October 18, 2014

Introduction

On completing its review of the 'Township of Esquimalt Heritage Values Report -November 2014', the Heritage Advisory Committee (HAC) has identified the following heritage values for the Township of Esquimalt. The heritage values outlined below follow from the five themes established by Heritage B.C.

Peopling the Land

Esquimalt's heritage lies in the physical qualities that have made it a natural place for occupation for centuries. Its sheltered deep water harbour, moderate climate, and bountiful land and sea have allowed the Township to develop into a place of prosperous business and employment, while maintaining a sense of community in spite of being part of a larger metropolitan area.

Developing Economies

Economically, Esquimalt has played an important role as a regional centre of industry and employment. The strength of the community's economic heritage lies in its favourable connection between the land and water, its physical location between two harbours, its connection to the Esquimalt and Nanaimo Railway and its proximity to the larger metropolitan area; qualities that have supported and sustained Esquimalt's economic evolution over time. Esquimalt is home to two graving docks, one being the oldest the other being the largest on the Pacific Coast. Shipbuilding, manufacturing, commerce, industry, and tourism are the foundation of Esquimalt's sustainability.

Governance and Institutional Life

Esquimalt's institutional and governmental identity is unique. It involves relationships with the Esquimalt and Songhees First Nations and multiple levels of Government. These relationships have influenced how the community has developed physically, economically, and socially since the mid-nineteenth century.

Esquimalt has deep institutional roots as a military base and the role of the military in shaping the community is intrinsic to its heritage – Esquimalt has identified itself as a military community since the formal recognition of the Royal Navy Dockyard in 1865 and the establishment of Work Point Barracks as the army base in 1887. Esquimalt is Canada's primary defence establishment on Canada's West Coast.

Building Social and Community Life

Socially, the "small town" heritage of the community has endured since its earliest days, and is supported by traditional activities such as festivals, parades, and sporting events, and by gathering places such as parks, public facilities, schools and places of worship, pubs, social and sporting clubs. Esquimalt residents value the fact that their community fosters a strong sense of connection to one's neighbours and to nature.

Expressing Intellectual and Cultural Life

Esquimalt is an excellent place to live, work and play and this is reflected in its built heritage and cultural landscapes. Its residential, commercial, and industrial buildings reflect the fact that Esquimalt has appealed to and continues to attract a diverse population. The importance of Esquimalt's natural heritage is evident in the orientation of its neighbourhoods, and the number of areas dedicated as park.

Township of
ESQUIMALT

The logo for the Township of Esquimalt features the words "Township of" in a smaller, blue, sans-serif font above the word "ESQUIMALT" in a large, dark blue, serif font. A stylized blue wave graphic is positioned below the word "ESQUIMALT", starting under the 'E' and ending under the 'T'.