



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**DESIGN REVIEW COMMITTEE
AGENDA**

WEDNESDAY, SEPTEMBER 13, 2017

3:00 P.M.

ESQUIMALT COUNCIL CHAMBERS

MEMBERS: Roger Wheelock (Chair) Wendy Kay
Ally Dewji Graeme Verhulst
Bev Windjack Jill Singleton
Robert Schindelka

RESOURCE MEMBER: Cst. Franco Bruschetta [Non-Voting]

COUNCIL LIAISON: Councillor Beth Burton-Krahn
Councillor Olga Liberchuk

STAFF LIAISON: Bill Brown, Director of Development Services

SECRETARY: TBD

- I. CALL TO ORDER**
- II. ELECTION OF VICE CHAIR**
- III. LATE ITEMS**
- IV. ADOPTION OF AGENDA**
- V. ADOPTION OF MINUTES – JUNE 14, 2017**
- VI. STAFF REPORT**

DEVELOPMENT PERMIT

“West Bay Quay”

460 Head Street

[Lot 10, Block H, Section 11, Esquimalt District, Plan 292]

[Lot 9, Block H, Section 11, Esquimalt District, Plan 292]

PURPOSE OF APPLICATION:

The applicant is proposing to construct a the five storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units on a consolidation of two subject properties to be regulated by Comprehensive Development District No. 103 [CD-103].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application is generally consistent with the design guidelines contained in Section 9.4 of the Township’s Official Community Plan. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit.

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received July 21, 2017” and August 28, 2017, and the landscape plan prepared by LADR Landscape Architects, stamped “Received August 28, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received July 21, 2017” for the five storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units, proposed to be located on the “West Bay Quay” lands, **be forwarded to Council with a recommendation to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

VII. STAFF LIAISON STATUS REPORT

VIII. NEW BUSINESS

IX. NEXT REGULAR MEETING

October 11, 2017

X. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**ADVISORY DESIGN REVIEW COMMITTEE
MEETING MINUTES
HELD
JUNE 14, 2017
ESQUIMALT COUNCIL CHAMBERS**

MEMBERS PRESENT:	Paul De Greeff Roger Wheelock (Chair) Cst. Franco Bruschetta (left the meeting at 4:00pm)	Jill Singleton Robert Schindelka
REGRETS:	Ally Dewji, Richard Iredale and Wendy Kay	
STAFF LIAISON:	Bill Brown, Director, Development Services	
STAFF:	Karen Hay, Planner Trevor Parkes, Senior Planner Alex Tang, Planning Technician	
COUNCIL LIAISON:	Councillor Olga Liberchuk Councillor Beth Burton-Krahn	
SECRETARY:	Pearl Barnard	

There were 2 members of the public in attendance.

I. CALL TO ORDER

The Chair called the meeting to order at 3:26 p.m.

II. LATE ITEMS

Pertaining to Agenda Item V. STAFF REPORTS 1) Development Permit - 860 Esquimalt Road

- Two additional sign renderings

III. ADOPTION OF AGENDA

Moved by Jill Singleton, seconded by Robert Schindelka: That the agenda be adopted as circulated. **Carried Unanimously**

IV. ADOPTION OF MINUTES – March 8, 2017

Moved by Robert Scindelka, seconded by Jill Singleton: That the minutes of March 8, 2017 be adopted as distributed. **Carried Unanimously**

V. STAFF REPORT

**1) DEVELOPMENT PERMIT
860 Esquimalt Road
[PID 000-132-331 Lot 1 Section 11 Esquimalt District Plan 23904]**

Karen Hay, Planner outlined that the owner is proposing to renovate the building including substantial changes to the exterior of the building with new materials for the façade and new signage. Ms. Hay explained that the multiple proposed signs do not all meet the requirements of the Sign Regulation Bylaw; therefore, a Development Variance Permit will be required before a building permit can be issued.

Ms. Hay also advised that the legal description for 860 Esquimalt Road is incorrect and should read [PID 000-132-331 LOT 1 SECTION 11 ESQUIMALT DISTRICT PLAN 23904]

Jim Kerr, Architect, was in attendance.

Jim Kerr gave a PowerPoint presentation and an overview of the proposed new signage, materials, colours and lighting for the project. Mr. Kerr outlined that there will be some minor changes to the interior of the building, and not a lot of changes are proposed to the landscaping. He also gave a brief history of the property and explained that as part of the A & W Corporate reimaging program, all the A & W restaurants across Canada are being upgraded with new signage, cladding, materials and colours.

The Chair thanked the applicant for the presentation.

Committee Members had the following questions and comments:

- A member asked for clarification on the sign variances. Mr. Kerr advised that there are more signs than permitted. Ms. Hay gave an overview of some of the variances; the Sign Bylaw doesn't allow for more than three different signs on any one business, the "Burger Family" sign would be consider a wall sign, therefore, no other signage would be permitted on that portion of the building and the boomerang appears to be a canopy sign, which would be slightly larger and slightly higher than what is permitted. Ms. Hay advised that at this time not all the variances have been determined.
- A Member felt that further consideration should be given to complying with the Sign Bylaw. Suggestion was to scale back the width of the boomerang sign or the amount of sign boards on the frontage of the building.
- A member asked if the total height of the new pylon sign is the same as the existing pylon sign. Mr. Kerr advised he didn't know the exact height. The location and height of the existing sign is being surveyed.
- A member asked if the new design had been implemented anywhere in BC. Mr. Kerr advised that it has been implemented in Alberta, Saskatawan and the Interior. The redesign is relatively late coming to the Island compared to other parts of the Province and Western Canada.
- A member asked about canopies over the doorways. Mr. Kerr advised the side door would have a canopy.
- Will the outdoor tables have umbrellas? Mr. Kerr advised the tables still have the fittings, something to consider.
- A concern was raised that the outdoor tables are not being repainted or refurbished. Landscape plan also needs to address the landscape furnishings.
- The front area could be a nice place for people to congregate but it is not very welcoming and is lacking in soft landscaping.
- Concerns were raised that the site will be nearly 100% impervious surface and hard landscaping, a complete lack of appreciation of how important soft landscaping is in an urban setting. A stark building on a stark piece of pavement. Consider adding some soft landscaping areas where achievable.
- Consider more visual leads or a softer walk to the parking area to make the walk more accessible. Pedestrian circulation for the site needs to be addressed.
- A concern that the landscaping strip areas are being paved over.

RECOMMENDATION:

Moved by Paul De Greeff, seconded by Jill Singleton: That the Esquimalt Design Review Committee [DRC] provides the following comments and recommendation to Council and the Director of Development Services, on the exterior alterations and new signage proposed for 860 Esquimalt Road, as illustrated in the architectural drawings prepared by James Kerr, Architect, AIBC, stamped “Received May 29, 2017”, for the property at PID 000-132-331, Lot 1, Section 11, Esquimalt District, Plan 23904 [860 Esquimalt Road]: **Present the application again to the Design Review Committee and address the following:**

1. Improved pedestrian circulation on the site;
2. Provide a landscape plan that addresses the soft landscape areas where further plantings are technically achievable; and
3. Consider further respecting, at least in part, the Sign Bylaw.

The Motion Carried Unanimously.

2) REZONING APPLICATION

615 Fernhill Road

[PID 004-757-742 Lot B Section 11 Esquimalt District Plan 12446]

Bill Brown introduced Alex Tang, Planning Technician to the Committee. The Chair welcomed Alex.

Alex Tang outlined that the applicant is requesting to change the zoning from the current RD-1 [Two Family Residential] zoning to a Comprehensive Development Zone [CD] to facilitate a new four storey, residential building containing 10 residential units.

Mikael Wilkin, Owner/Applicant and Michael Moody, MJM Architect Inc. were in attendance.

Michael Moody gave a PowerPoint presentation and an overview of the site plan, building design, setback, elevations and material and colours for the project. Mr. Moody advised that the Garry Oak tree on the site would be removed.

The Chair thanked the applicant for their presentation.

Committee Members had the following questions and comments:

- Members felt the proposed development would; enhance the neighbourhood, is an appropriate use for the site, and is consistent with the Official Community Plan.
- A member complemented the applicant on their presentation package; it is very clear and has a lot of good graphics to show the vision.
- A member asked what amenities the proposal would provide to the neighbourhood. Mr. Wilkin feels this development will; improve that section of Fernhill Road, introduce new people to the area and also provide affordable housing which is much needed in the community.
- A concern was raised with the location of the screening between the subject property and the property to the south. Screening needs to be located on the subject property.
- Rendering looks pretty stark for the frontage. Consider additional trees on the road frontage.
- Concerns that one ornamental tree is not a sufficient replacement for the Garry Oak tree. Landscape plan needs to address this.

- Concerns with drainage issues on the permeable pavers. Consider using the permeable pavers to treat rainwater.
- Members had no objection to the reduction of parking.

RECOMMENDATION:

Moved by Paul De Greeff, seconded by Jill Singleton: That the Esquimalt Design Review Committee [DRC] recommends that the application for rezoning to permit a new four storey, residential building containing 10 residential units and incorporating siting, height and massing consistent with architectural plans provided by MJM Architect Inc., stamped "Received May 30, 2017" for the property at PID 004-757-742, Lot B Section 11 Esquimalt District Plan 12446 [615 Fernhill Road], **be forwarded to Council with a recommendation for approval with the following conditions:**

That the applicant:

1. Consider adjusting the site plan so that vegetative screening between the subject property and the property to the south can be located on the subject property;
2. Consider tree plantings in the front setback that more closely compensate for the loss of the Garry Oak; and
3. Examine the grading and drainage issues and examine the possibility of using permeable pavers to treat rainwater runoff.

The reason: The proposed development will enhance the neighbourhood.
The Motion Carried Unanimously.

VI. STAFF LIASON STATUS REPORT

1. The Rezoning application for 101 Island Highway: Public Hearing occurred on Monday, June 12th.
2. The Triangle Lands south 460/464 Head Street: Public Hearing is scheduled for Monday, June 19th.
3. The Esquimalt Town Square project 1235 Esquimalt Road is moving forward.
4. The English Inn project 429 Lampson Street is also moving forward.

VII. NEW BUSINESS

The Chair advised that it is Paul De Greeff's last meeting. The Chair and members thanked Paul and wished him good luck.

VIII. NEXT REGULAR MEETING

Wednesday, July 12, 2017

IX. ADJOURNMENT

On motion the meeting adjourned at 4:50 p.m.



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: September 13, 2017

STAFF REPORT

DATE: September 8, 2017

TO: Chair and Members of the Design Review Committee

FROM: Trevor Parkes, Senior Planner

SUBJECT: DEVELOPMENT PERMIT APPLICATION
“West Bay Quay”
460 Head Street [Lot 10, Block H, Section 11, Esquimalt District, Plan 292]
464 Head Street [Lot 9, Block H, Section 11, Esquimalt District, Plan 292]

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received July 21, 2017” and August 28, 2017, and the landscape plan prepared by LADR Landscape Architects, stamped “Received August 28, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received July 21, 2017” for the five storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units, proposed to be located on the “West Bay Quay” lands, be forwarded to Council with a recommendation to **either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application

The applicant is proposing to construct a the five storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units on a consolidation of two subject properties to be regulated by Comprehensive Development District No. 103 [CD-103].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application is generally consistent with the design guidelines contained in Section 9.4 of the Township’s Official Community Plan [attached]. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines [attached] as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and

landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit.

Evaluation of this application should focus on issues respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of buildings and other structures in relation to the relevant design guidelines.

Context

Applicant/Owner: Tenfold Projects Inc., Inc. No. BC1040336 [David Price]

Architect: Hillel Architecture Inc. [Peter Hardcastle]

Property Size: Metric: 1207 m² Imperial: 13,000 ft²

Existing Land Use: Vacant Single Family Residential and vacant Commercial land [parking lot].

Surrounding Land Uses:

North: 2-6 Storey Commercial/ Residential Mixed Use
South: DND Federal Land [Work Point]
West: Single Family and Two Family Residential
East: Marine Commercial

Existing Zoning: RS-1 [Single Family Residential] – Lot 9
C-7 [West Bay Commercial] – Lot 10

Proposed Zoning: CD-103 [Comprehensive Development District No. 103 – Pending Adoption]

Existing OCP Designation: Commercial Mixed-Use [No change required]

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received by the submission deadline:

Community Safety Services: Staff have completed a preliminary evaluation of the proposed new building and have no concerns at this time. Construction must satisfy BC Building Code and Esquimalt's Building Code Bylaw, 2002, No. 2538 requirements. Detailed review of plans will occur at time of Building Permit application.

Fire Services: Fire Services staff have completed a preliminary evaluation of the proposed new building and offer no objections at this time. Staff note that it would be desirable to see overhead utilities relocated underground. Detailed additional comments will be provided as part of the consideration of the Building Permit Application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed commercial mixed use development including commercial space and 12 residential units located on two properties within the West Bay neighbourhood. The applicant is responsible for retaining the services of a qualified professional for the design, construction and supervision of all Works and Services required to

service the proposed development as indicated in Esquimalt's Subdivision and Development Control Bylaw No. 2175. A capacity study should verify all main sizes, material, etc., and to determine if the Sewer and Drain mains are of adequate size and condition to handle the increased flows introduced as a result of the development. Existing municipal infrastructure may need to be relocated or SRWs may need to be secured. Adjacent properties across Head Street are to remain fully serviced during construction. Additional review comments will be provided when detailed civil engineering drawings are submitted.

Review of the design proposed public plaza has been completed by the Director of Engineering and Public Works who has agreed to receive all concrete surfacing and public realm furniture as detailed in the landscape plan stamped "Received August 28, 2017". Installation and maintenance of boulevard plantings as well as the associated irrigation system, with the exception of the 7 boulevard trees, will be the responsibility of the Developer/ Strata Council. Roles and responsibilities will be defined through the issuance of a Boulevard Alteration Permit to be processed through staff should Council approve the proposed private and public realm planting and hard-scape design.

Parks Services: Parks staff have completed a review of the proposed on-site and off-site landscaping and have no concerns with the proposed private realm planting plan. Staff note that Parks Services does not maintain street boulevards to a municipal park standard therefore the enhanced planting scheme proposed for the public plaza will be the responsibility of the Developer/ Strata Council to install and maintain. The Director of Parks and Recreation Services has agreed to assume responsibility for the 7 proposed boulevard trees as part of the municipal street tree inventory.

ISSUES:

Zoning

CD-103 zoning was specifically tailored to accommodate this proposal. The proposed design is consistent with the form and character presented supporting the rezoning application and the proposed building height, massing, density, siting and parking requirements satisfy all of the CD-103 zone regulations outlined in Amendment Bylaw No. 2896 [attached] which is pending adoption by Council.

Density, Lot Coverage, Setbacks, Height and Parking:

The following chart details the setbacks, lot coverage and floor area ratio and parking requirements of this proposal.

	Comprehensive Development Zone No. 103	
Floor Area Ratio	1.32	Zoning Bylaw, 1992, No. 2050 does not contain a zone that could accommodate this commercial mixed- use proposal.
Lot Coverage		
<ul style="list-style-type: none"> • 1st Storey • 4th Storey • 5th Storey 	<ul style="list-style-type: none"> 46% 35% 19% 	
Setbacks		Staff present this summary table as the basis for a site
<ul style="list-style-type: none"> • Front [SE] • Rear [NW] • Exterior Side [NE] • Exterior Side [SW] 	<ul style="list-style-type: none"> 2.7 m plus 1.5 m [Canopy] 1.2 m plus 0.3 m [Canopy] 2.4 m plus 1.5 m [Canopy] 2.6 m plus 1.1 m [Canopy] 	

Building Height	18 m (5 Storeys)	specific zone written to accommodate this proposal should it be forwarded in the development review process.
Off Street Parking	Parking Bylaw No. 2011 spaces required for all uses = 37 Total spaces proposed = 34	
Useable Open Space	60 sq.m. [4 th Storey Terrace]	
Commercial Floor Area	Not less than 270 square metres	

Official Community Plan

The West Bay Quay lands are located within a predominantly low rise neighbourhood consisting of a mix of commercial properties, single and two family residential homes, a marine community, and military uses including training facilities and housing. The exception to the established one to three storey built environment is the DND "Accommodations" building located at Work Point to the south of the subject properties. This building is approximately 7 storeys [22 metres] in height and commands a substantial presence in the area due to the combination of height and massing of the building.

The West Bay Quay would integrate into the existing and future urban fabric by acknowledging the height and mass of the DND Accommodations building while serving as the more contemporary sibling to the approved, but not yet build, West Bay Triangle development, thereby completing this section of the West Bay Village.

Policy direction contained in the Esquimalt Official Community Plan was reviewed by staff and presented to Council as part of the consideration of the rezoning application. Official Community Plan, Section 9.4.5, Guidelines for Owners of Land within the Development Permit Area No. 2 – Commercial is specifically relevant to consideration of this Development Permit Application [attached].

OCP Section 9.4.5(a) supports windows facing the street and doors opening onto the street. This is consistent with the proposed design.

OCP Section 9.4.5(b) supports ornamental lighting highlighting the building and lighting pedestrian area while avoiding the casting of glare or direct light onto adjacent sites. The drawing package does not specifically detail the lighting plan however the proposed design could accommodate glass and lighting consistent with this policy.

OCP Section 9.4.5(c) supports building design that limits the casting of shadows on public space. At five storeys in height, the proposed building casts shadows that affect the subject lands, the southern portion of the West Bay Triangle development, Head Street and the proposed public plaza. Notably, these shadows do not rest in any one area for prolonged periods. During the summer months the shadow impacts are expected to be significantly reduced from those detailed on Page A1.4 of the drawing package.

OCP Section 9.4.5(d) supports canopies covering pedestrian walkways. The proposed design includes generous canopies over the sidewalk on Head Street to create a covered transitional space between the indoor uses and the outdoor public realm. Furthermore the outdoor patio of the proposed restaurant commercial space benefits from cover provided by the decks of the units on the second storey.

OCP Section 9.4.5(e) supports screened or underground parking be provided. If surface parking is proposed it should be screened with landscaping. The proposed design addresses this

guideline as parking is located underground and the 10 surface parking spaces accessed off Lyall Street are screened from Head Street by the building and by substantial plantings along Lyall Street.

OCP Section 9.4.5(f) supports the inclusion of Crime Prevention through Environmental Design principles in any project. The proposed design is well lit with good connectivity to the public street and resident overlook onto private and public areas.

OCP Section 9.4.5(g) supports decreased setbacks where appropriate. This proposal maintains appropriate setbacks in order to create ample space in front of businesses for walking and the display of goods.

OCP Section 9.4.5(h) supports screening of garbage and recycling facilities. The proposal has these facilities contained within the building, accessed from the parking courtyard.

OCP Section 9.4.5(i) supports the retention of trees and habitat wherever possible. This proposal is a complete revitalization of the subject lands including a significant excavation for underground parking facilities. Accordingly the proposal has not included the retention of any trees. Staff note that not less than 7 boulevard trees and an additional 4 private realm trees are proposed to be added to the site as part of the execution of the landscaping for this site, significantly more than present today.

West Bay Neighbourhood Design Guidelines

The West Bay Quay development proposal has been tailored to address the West Bay Neighbourhood Design Guidelines resulting in an application that is fundamentally consistent with these development directives. Development Services staff completed a review of the proposed design and note the following apparent inconsistencies to the West Bay Neighbourhood Design Guidelines:

Page 10, Bullet 4 – "Avoid locating off-street surface parking adjacent to active public streets and open spaces.....". The proposed design partially achieves this policy when considered from Head Street and the southern public plaza, however, surface parking is provided adjacent to Lyall Street. Staff note that careful thought has been focused on ensuring that appropriate plantings have been proposed to screen this parking area without undermining the desirable function of easily accessible commercial visitor parking to the site.

Page 17, Bullet 2 – "Locating off-street surface parking in front of buildings,.....immediately adjacent to the public sidewalk or open spaces.....is strongly discouraged and should be avoided.". Once again, the proposed design partially achieves this policy when considered from Head Street and the southern public plaza, however, surface parking is provided with screening adjacent to Lyall Street.

Page 27 – The detailed design of the public realm incorporates prefabricated steel and timber furnishing in lieu of more substantial curvilinear concrete seating. This is consistent with maintenance and repair concerns received from Township staff.

ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**

2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**

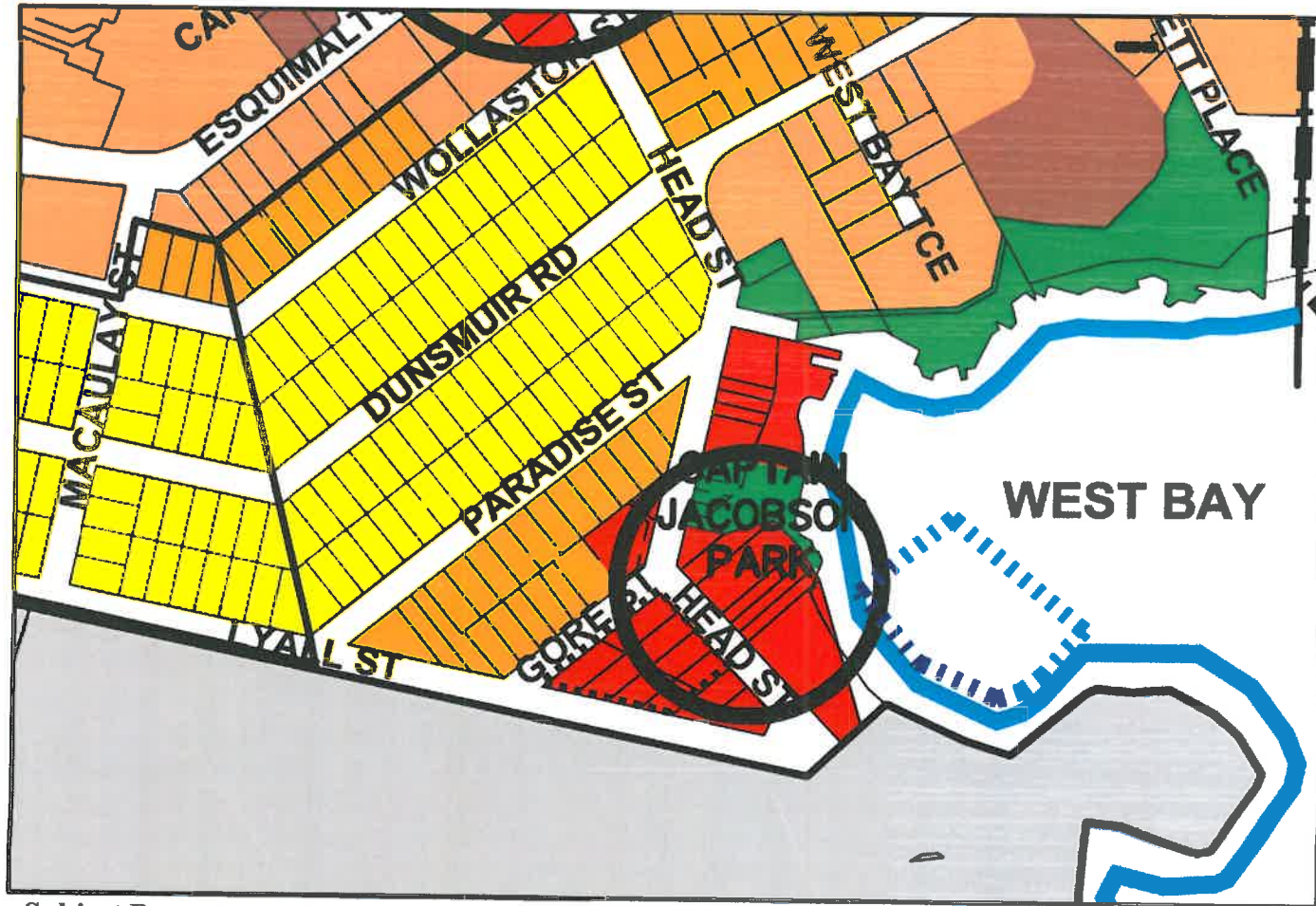
460 Head Street – ‘Marina View’



Subject Property Boundary:



Extract from Official Community Plan Bylaw No. 2646 Schedule 'A'
Land Use Designations



Subject Property:

LEGEND	
	Single and Two-Unit Residential
	Townhouse Residential
	Multi-Unit, Low-Rise Residential (2 to 4 storeys)
	Multi-Unit, High-Rise Residential (5 to 6 storeys)
	Floating Homes Residential
	Parks and Open Space
	Trails existing and proposed
	Transportation Corridor
	Industrial
	Commercial Mixed-Use
	Commercial Node
	Institutional
	DND Federal Land
	Water Area
	Municipal Boundary

2.3 Commercial—Mixed Land Use

Commercial activity in Esquimalt is clustered in four main groupings:

- Esquimalt Village;
- Esquimalt Road/Head Street;
- Craigflower Road/Tillicum Road; and
- West Bay Harbour.

2.3.1 General Commercial - Mixed Use Objectives

- a) To create a diversified commercial and employment sector that provides a wide range of goods and services to residents of Esquimalt.
- b) To foster the creation of an identifiable and vibrant Esquimalt Village that successfully integrates commercial, public and residential activity.
- c) To encourage growth through revitalization and redevelopment of commercial areas.
- d) To encourage a mix of ground-level commercial and upper-level(s) residential.

2.3.2 General Commercial - Mixed Use Policies

- a) The Township encourages a mix of commercial and multi-unit residential developments in all commercial-mixed use areas denoted on "Schedule A". These will have commercial uses on the ground floor and residential uses above.
- b) All commercial-mixed use areas are designated Development Permit Areas, as shown on "Schedule C" in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.
- c) The Township encourages public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.
- d) The Township will develop signage guidelines for each commercial area, as part of design guidelines for these areas. The Township, in partnership with interested members of the community, will take the initiative to design and install entrance signage at key street locations, including entrances to Esquimalt.
- e) The Township encourages the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas. The Township is amenable to using density bonusing, or providing variances to zoning or parking regulations for redevelopment proposals.
- f) To encourage the use of bicycles, provision should be made in new commercial buildings for bicycle parking for employees and visitors. Secure bicycle parking for employees should be provided in the ratio of one (1) parking space per ten (10) full-time employees with a minimum of one (1) space for each new building. In all new commercial buildings, six (6) bicycle parking spaces should be available for the use of temporary visitors.
- g) End of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

- h) Where all of the following criteria are met in a commercial building, Council may reduce the off-street parking requirement through the Development Permit:
 - i) Two (2) or more secure bicycle storage spaces are provided;
 - ii) Shower and change rooms are provided;
 - iii) Six (6) visitor bicycle parking spaces are provided; and
 - iv) The building is located within 200 metres of a regional bus route.
- i) Lands outside the Commercial–Mixed Use designation on “Schedule A” will not be considered for commercial zoning unless the following criteria are met:
 - i) The project is needed to serve tourists or local residents and cannot be appropriately located within established commercial areas;
 - ii) The density and scale of the project is sensitive to the prevailing character of surrounding lands;
 - iii) The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
 - iv) The project’s parking requirements can be satisfied on-site and will not unduly affect neighbouring residences; and
 - v) The proponent demonstrates that the neighbourhood has been consulted and residents have had an opportunity to express their concerns.

[Amendment Bylaw [No. 6], 2010, No. 2730 - Adopted May 25, 2010]

- j) In mixed commercial and multi-unit residential developments, buildings up to 12 storeys in height and with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.
- K) Development proposals with heights and/or densities greater than those set out in Section 2.3.2 (j) may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor space where new commercial buildings provide affordable, accessible, or special needs housing units or amenities for the benefit of the community.

9.4 Development Permit Area No. 2 – Commercial

9.4.1 Scope

All lands designated Commercial on Schedule “C” are part of DPA No. 2.

9.4.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, commercial.

9.4.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

Where new development is to occur within Esquimalt’s commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials, and by their orientation to the street rather than to a parking lot or internal square.

The goals for Development Permit Area No. 2 are:

- a) to enhance the aesthetic image of Esquimalt’s commercial district, particularly those areas that are considered community focal points, such as the Village, the Head Street/Esquimalt Road intersection and major entrance points to the municipality;
- b) to revitalize existing commercial areas by encouraging a variety of businesses;
- c) to encourage growth in the tax base through diversified commercial development and redevelopment of existing commercial areas; and
- d) to encourage integrated residential/institutional/commercial uses in commercial areas.

9.4.4 Requirements of Owners of Land within the Development Permit Area

- a) Owners of land within Development Permit Area No. 2 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:
 - i) subdivide lands; or
 - ii) construct or alter a building or structure;without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.
- b) Exemptions:

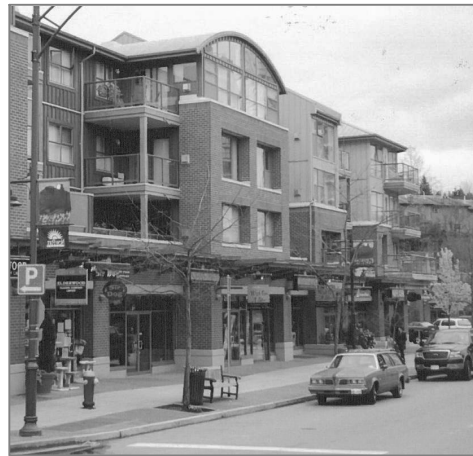
The following do not require a development permit:

 - i) construction of buildings or structures less than 10 square metres in area;

- ii) minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted by the municipal tree protection bylaw; and
- vi) replacement or changing of existing signs, provided the sign area is not to be increased.

9.4.5 Guidelines for Owners of Land within the Development Permit Area

- a) Commercial building facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
- b) Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, commercial lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
- c) Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- d) Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
- e) Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- f) The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
- g) Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
- h) Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

BYLAW NO. 2896

A Bylaw to amend Bylaw No. 2050, cited as the
“Zoning Bylaw, 1992, No. 2050”

THE MUNICIPAL COUNCIL OF THE CORPORATION OF THE TOWNSHIP OF
ESQUIMALT, in open meeting assembled, enacts as follows:

1. This bylaw may be cited as the “*ZONING BYLAW, 1992, NO. 2050, AMENDMENT BYLAW NO. 2896*”.
2. That Bylaw No. 2050, cited as the “Zoning Bylaw, 1992, No. 2050” be amended as follows:
 - (1) by adding the following words and figures in Section 31, Zone Designations, in the appropriate alpha-numeric sequence:

“Comprehensive Development No. 103 (460 Head Street) CD No. 103”

- (2) by adding the following text as Section 67.90 (or as other appropriately numbered subsection within Section 67):

67.90 COMPREHENSIVE DEVELOPMENT DISTRICT NO. 103
[CD NO. 103]

In that Zone designated as CD No. 103 [Comprehensive Development District No. 103] no Building or Structure or part thereof shall be erected, constructed, placed, maintained or used and no land shall be used except in accordance with and subject to the regulations contained in or incorporated by reference into this Section.

(1) **Permitted Uses**

Only Mixed Commercial / Residential use is permitted and the following Uses and no others shall be permitted as part of that development:

- a) Dwelling – Multiple Family
- b) Home Occupation
- c) Business and Professional Office
- d) Personal Service Establishment
- e) Retail Store
- f) Restaurant
- g) Liquor Store
- h) Convenience Store
- i) Group Children’s Day Care Centre
- j) Boat Rental and Passenger Charter

(2) **Floor Area Ratio**

(a) **Residential Uses**

The Floor Area Ratio shall not exceed 1.0.

(b) **Combined Mixed Use**

The combined Floor Area Ratio for all uses shall not exceed 1.32.

(3) **Unit Size**

Dwelling Units shall not be less than 60 square metres.

(4) **Number of Dwelling Units**

Not more than twelve (12) Dwelling Units shall be located in this Zone.

(5) **Size and Location of Commercial Space**

(a) The minimum Floor Area dedicated to Commercial Uses shall not be less than 270 square metres located on the First Storey.

(b) Commercial Uses are not permitted on any Storey located above the First Storey.

(6) **Parcel Size**

The minimum Parcel Size of fee simple Parcels created by subdivision shall be 1200 square metres.

(7) **Building Height**

No Principal Building shall exceed a Height of 18 metres.

(8) **Lot Coverage**

(a) Principal Buildings and Structures combined shall not cover more than 46% of the Area of the Parcel.

(b) Principal Buildings shall not cover more than 35% of the Area of the Parcel on the fourth storey.

(c) Principal Buildings shall not cover more than 19% of the Area of the Parcel on the fifth storey.

(9) **Siting Requirements (As illustrated on Figure 1.)**

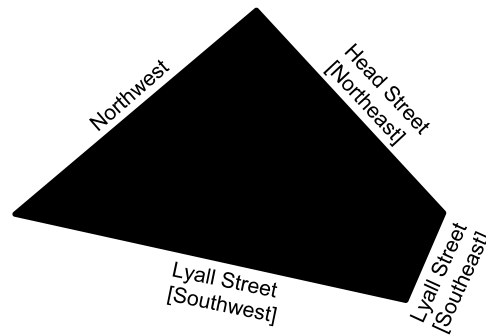


Figure 1.

(a) Principal Buildings:

- (i) No Principal Building shall be located within 2.4 metres of the northeastern Lot Line.
- (ii) No part of a Principal Building located above the fourth storey shall be located within 4.7 metres of the northeastern Lot Line.
- (iii) No Principal Building shall be located within 2.7 metres of the southeastern Lot Line.
- (iv) No part of a Principal Building located above the fourth storey shall be located within 5.0 metres of the southeastern Lot Line.
- (v) No Principal Building shall be located within 2.6 metres of the southwestern Lot Line.
- (vi) No Principal Building shall be located within 1.2 metres of the northwestern Lot Line.

(b) Accessory Buildings:

No Accessory Buildings shall be permitted.

(10) **Siting Exceptions**

Within the CD-103 zone, the minimum distance to a Lot Line may be reduced by not more than the following distances to accommodate balconies and exterior canopies, attached to and forming part of a Principal Building:

- (a) Northeastern Lot Line: 1.5 metres
- (b) Southeastern Lot Line: 1.5 metres
- (c) Southwestern Lot Line: 1.1 metres
- (d) Northwestern Lot Line: 0.3 metres

(11) **Useable Open Space [Rooftop Patio]**

- (a) Useable Open Space, in the form of an unenclosed rooftop patio accessible to all dwelling units, and having an area not less than 60 square metres, shall be provided on the northwestern most part of the building.
- (b) Notwithstanding Section 15(4)(e)(ii) Guards for rooftop patios are not required to step back a minimum of 1.5 metres from the building edge.

(12) **Fencing**

Subject to Part 4, Section 22, no fence shall exceed a Height of 1.2 metres.

(13) **Off-Street Parking**

- (a) Off-street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011(as amended).
- (b) Notwithstanding Section 13(a) Parking shall be provided for Commercial Use at a ratio 1 space per 25 square metres of gross floor area with a minimum provision of 18 parking spaces.

- (3) by changing the zoning designation of PID 009-174-851, Lot 10, Block H, Section 11, Esquimalt District, Plan 292 [460 Head Street], shown cross-hatched on Schedule 'A', from West Bay Commercial [C-7] Zone to CD No. 103 [Comprehensive Development District No. 103].
- (4) by changing the zoning designation of PID 009-174-869, Lot 9, Block H, Section 11, Esquimalt District, Plan 292 [464 Head Street], also shown cross-hatched on Schedule 'A', from Single Family Residential [RS-1] Zone to CD No. 103 [Comprehensive Development District No. 103].
- (5) by changing Schedule 'A' Zoning Map, attached to and forming part of "Zoning Bylaw, 1992, No. 2050" to show the changes in zoning classification effected by this bylaw.

READ a first time by the Municipal Council on the 23rd day of May, 2017.

READ a second time by the Municipal Council on the 23rd day of May, 2017.

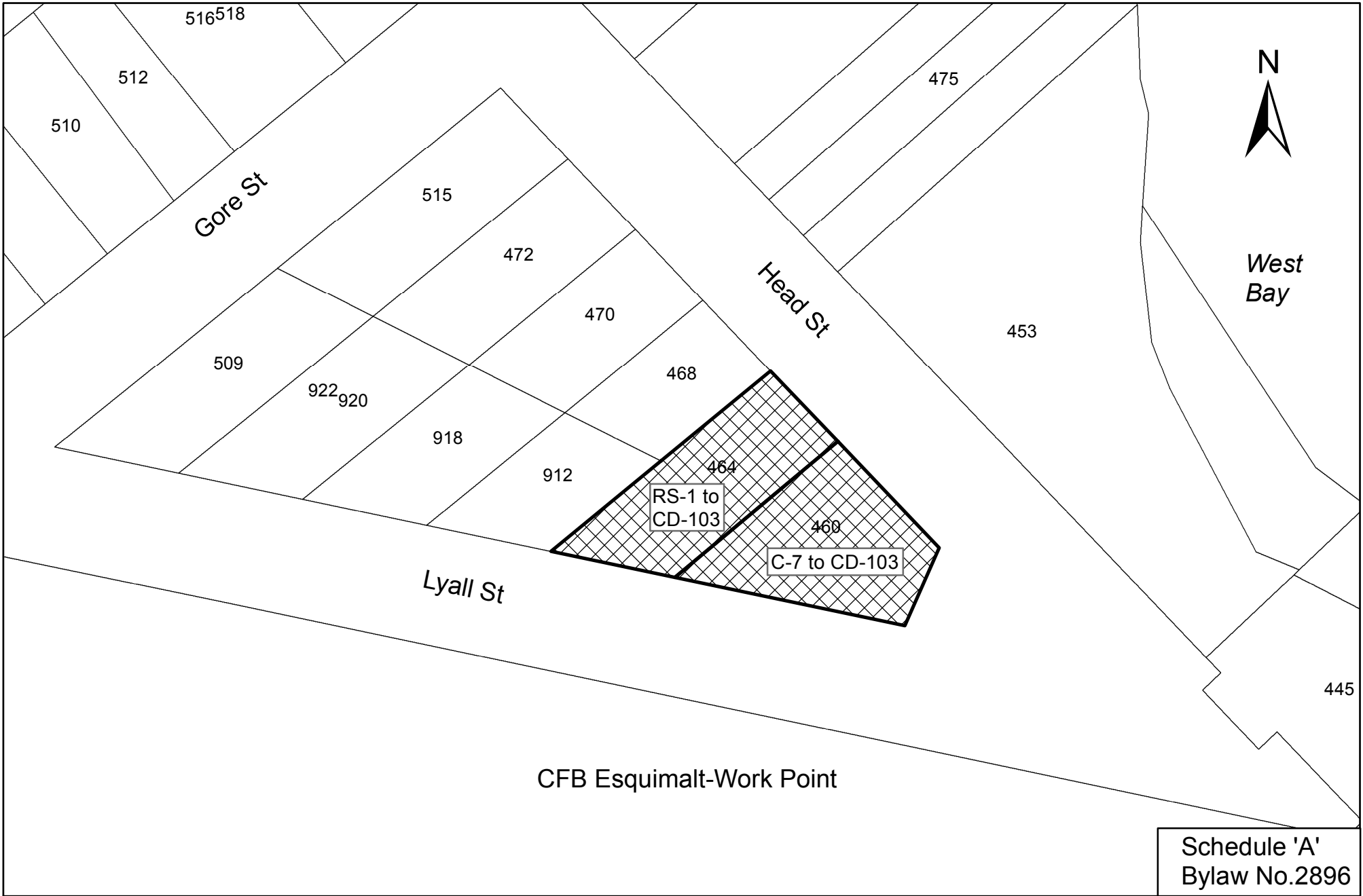
A Public Hearing was held pursuant to Sections 464, 465, 466, and 468 of the *Local Government Act* on the 19th day of June, 2017.

READ a third time by the Municipal Council on the 19th day of June, 2017.

ADOPTED by the Municipal Council on the ____ day of _____, 2017.

BARBARA DESJARDINS
MAYOR

ANJA NURVO
CORPORATE OFFICER



West Bay

CFB Esquimalt-Work Point

Schedule 'A'
Bylaw No.2896



West Bay Neighbourhood Design Guidelines



We would like to thank the following members of the West Bay Design Guidelines Stakeholders Group whose local insight and involvement in the process was of great benefit to the development of these design guidelines.

West Bay Design Guidelines Stakeholders Group:

Brian Emmett
Kris Samuels
Lieutenant-Colonel Ulpiano Honorio
Carolyn Gisborne
Christina Clarke
Laurie Hurst
Jeff Miller
Janice Rose
Scott Hartman
Liz Dill
Jim Witter
Peter Hardcastle
Mark Lindholm
Sally Reid
Katrina Dwulit
Julie Flatt
Carole Witter

Submitted: 10 September, 2015.

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1. Purpose & Overview

Purpose and Overview

These design guidelines were developed through a collaborative, community-based process to ensure new development enhances rather than detracts from West Bay's unique and rich character and identity. These design guidelines focus on the interface between and integration of public and private lands towards the preservation and enhancement of the identity, social vitality and overall liveability that characterizes the West Bay neighbourhood. This includes integration with federal Department of National Defense (DND) lands, integration of local ecosystems, and connection to the harbourfront and the range of recreational and other activities associated with the working harbour.

This document includes:

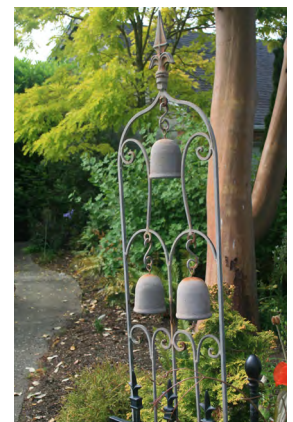
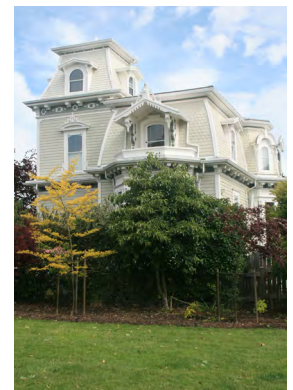
- An inventory and analysis of neighbourhood design characteristics;
- A vision statement and set of design principles as the rationale for the design guidelines;
- Development Permit Area design guidelines for Form and Character;
- A set of general design guidelines and recommendations for public realm design and improvements; and,
- Specific guidance on site planning and design for development opportunity sites along Head Street south adjacent to the harbourfront and including the "Triangle Lands", are also included.

The Spirit of West Bay

During the West Bay Design Guidelines Stakeholder Workshop, we asked: "What words capture the spirit of West Bay?" The following is a summary of responses:



Neighbourhood Characteristics: Photo Essay



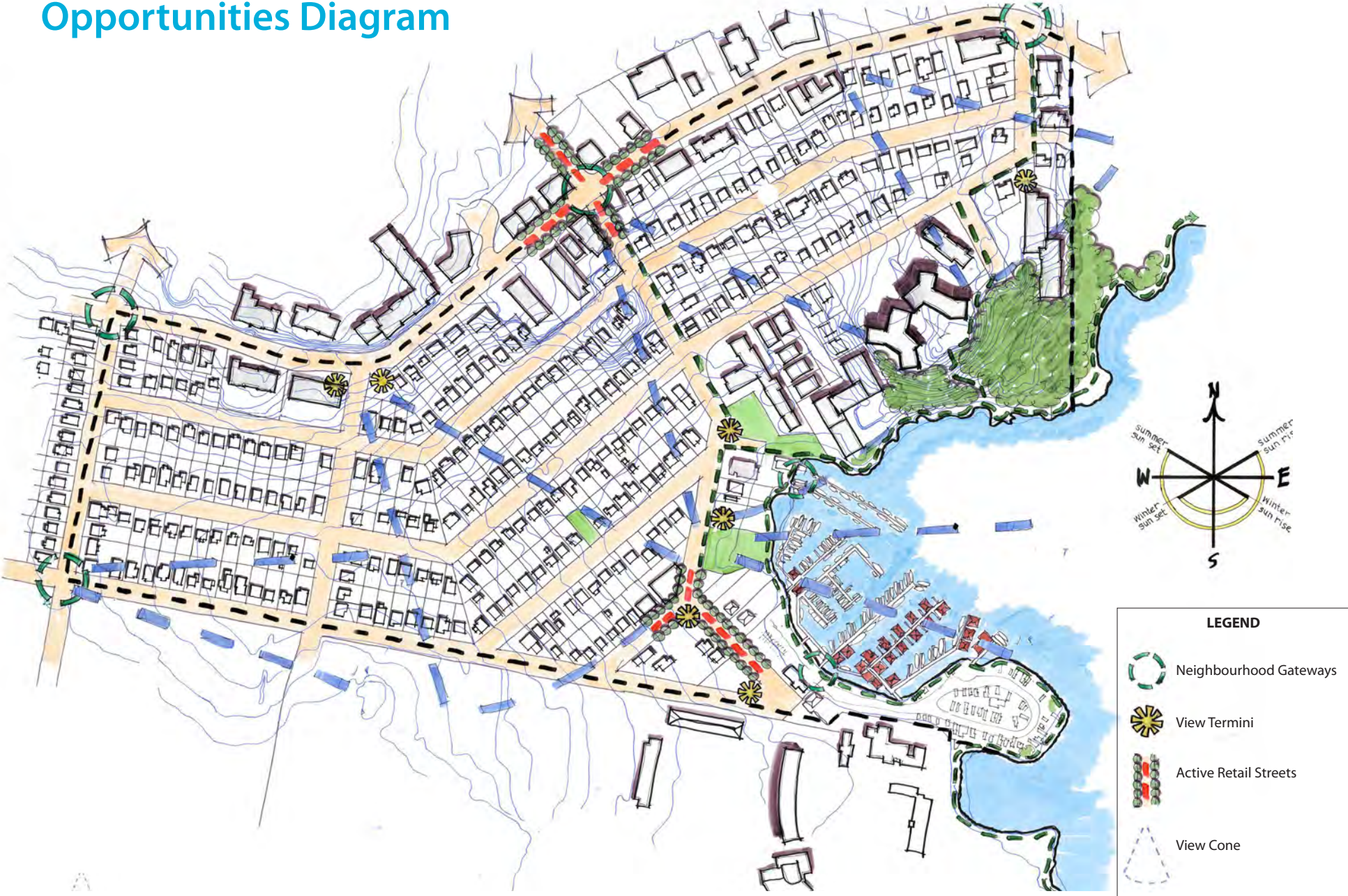
West Bay Ortho Photo and Study Area



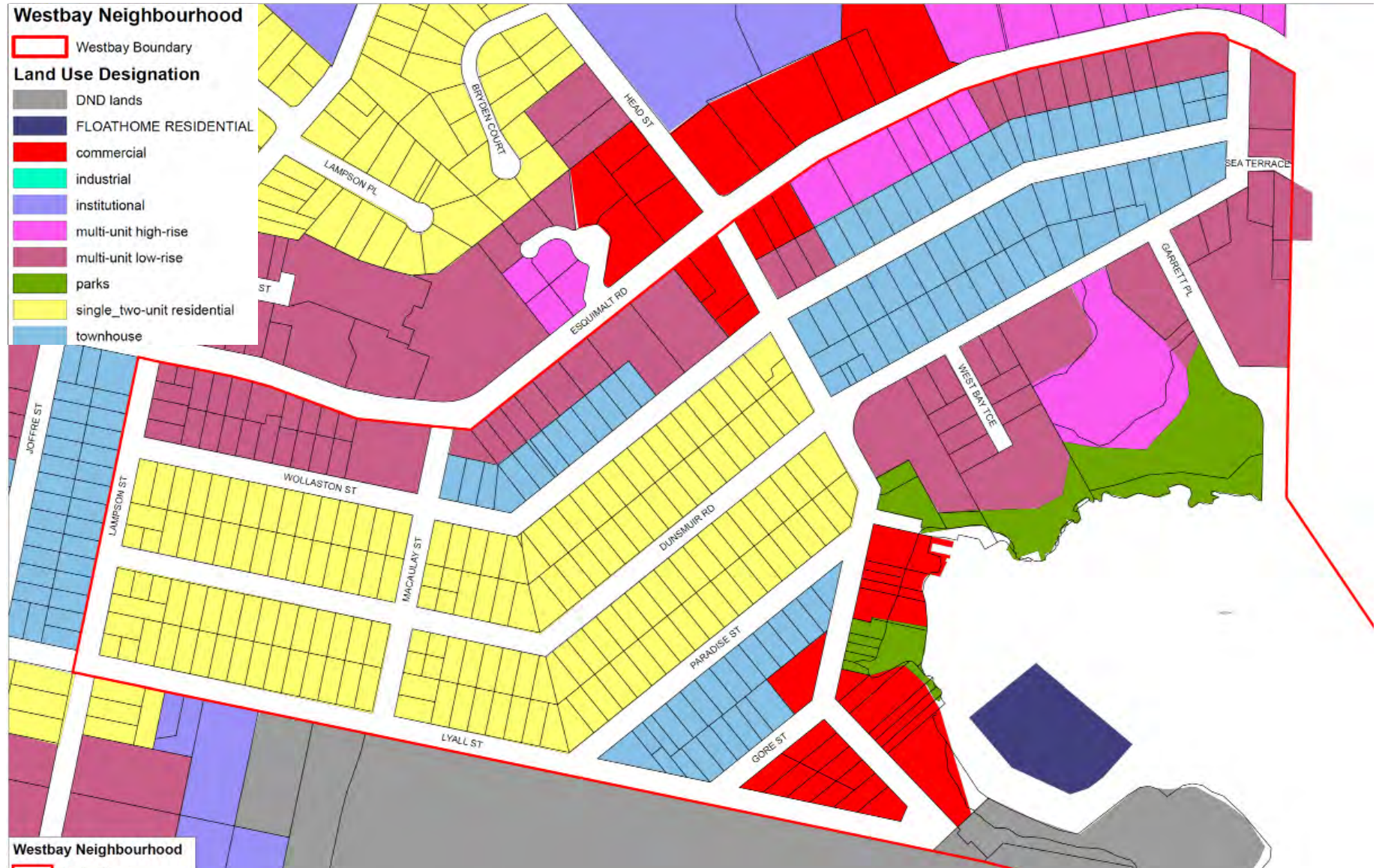
Neighbourhood Characteristics: Topography and Building Massing (Existing)



Neighbourhood Characteristics: Opportunities Diagram



West Bay Neighbourhood - OCP Land Use



2. Design Vision & Principles

Vision

West Bay is a thriving, attractive and walkable harbourfront neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbourfront recreation, and sunny open spaces with strong connections to the harbour, make this a truly liveable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.



Photo courtesy: Josh McCulloch, PictureBC

Design Principles

The following design directions for the public realm were identified through the workshop to help maintain and enhance the unique identity of West Bay:

- **Strong connection to the harbourfront:** Maintain and enhance visual and physical connections to the harbour and harbourfront trail network.
- **A walkable neighbourhood:** Publicly accessible buildings present a friendly face to streets and open spaces to make them active, attractive and inviting to pedestrians. Make connections within and to adjacent neighbourhoods to make walking easy.
- **Human scale:** Use architectural features, details, and site design elements that are of human proportion and clearly oriented for public, pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it on foot.
- **Marine ecology:** Enhance, restore and celebrate the unique marine and upland ecology. Layer and integrate mobility, recreation, stormwater, habitat and ecological systems in the context of the working harbour.
- **Harbourfront community & identity:** Ensure building and open space design that supports and enhances the unique sense of community and identity, including its architectural and marine heritage. More broadly, this includes the rich natural and human history of the area and associated expression of local culture, festivals, and gathering at and near the harbour front and water's edge.
- **Neighbourliness:** Ensure new development responds positively to the existing context by ensuring a sensitive transition in scale, by minimizing view and shadow impacts, and by responding to the positive design characteristics, if only subtly, of adjacent development.

3. Design Guidelines

Overview

Urban design is the comprehensive and cohesive combination of buildings, streets, open spaces and the natural environment and has, as its objective, the creation of memorable public spaces.

The essence of good urbanism is determined by the relationship between:

- the built and natural environments, and
- the public and private realm - the areas between buildings and public open spaces - at street level.

Buildings, streets, and other public open spaces scaled for human comfort and use are essential to the creation of a functional, aesthetically rich and vibrant neighbourhood. Building fronts and their orientation to streets and public open spaces are therefore the strongest determinants of the character and quality of neighbourhoods.

The design elements and approaches on the following pages provide a starting point and preliminary framework in maintaining and enhancing what is special, unique and wonderful about West Bay. They also provide a foundation to ensure new development and public realm improvements contribute to a vibrant, safe and accessible neighbourhood environment that encourages walking and street life while creating a unique social and physical environment that celebrates the beautiful natural setting of West Bay.



Photo: Joaquin Karakas



Photo: Joaquin Karakas

A Friendly Face to the Street

INTENT

- To define residential and commercial streets with active and attractive building fronts that have a positive orientation to streets and other public open spaces.
- To ensure buildings are sited and designed to be welcoming, and to encourage street vitality, visual interest, and safety.

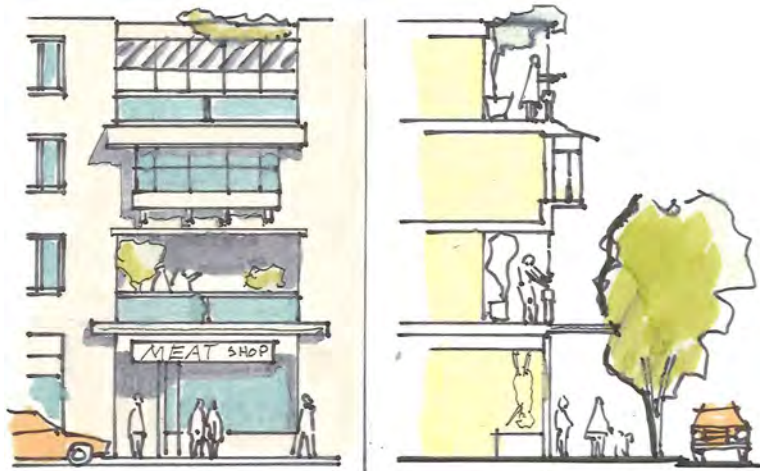
GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

- Locate publicly oriented active uses at grade and at or near the sidewalk edge.
- Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;
- A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.

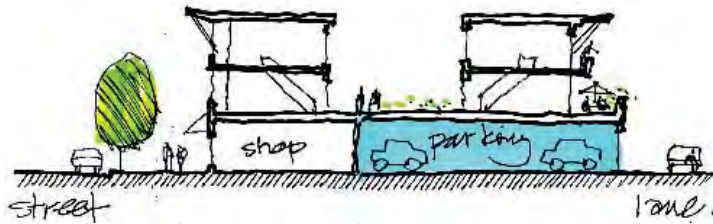


Active uses oriented to public streets and openspaces creates vibrant and attractive neighbourhoods (above, below and below left)





Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.

- Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.
- On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Locate on-street parking at the curb in front of shops.
- Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
- Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



Photo: Joaquin Karakas

On corner sites, develop street-facing façades for both streets. .

- Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.
- Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
- Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above and right)



Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

Photo: Dan Burden



Incorporate plantings and other landscape details such as banners.



Screen



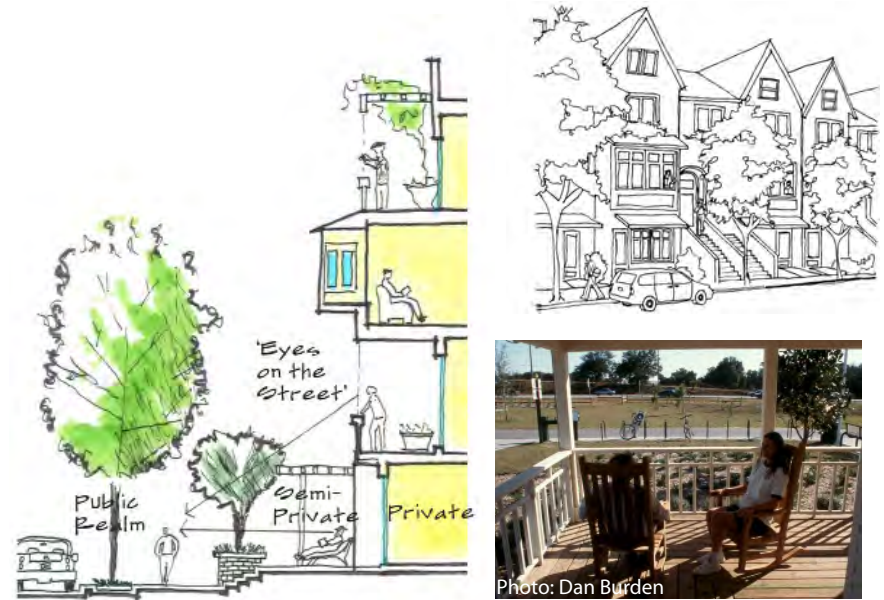
When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.

- Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:
 - » Install a vertical trellis in front of the wall with climbing vines or other plant material
 - » Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
 - » Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
 - » Employ quality materials of different textures and colours to make the wall more interesting visually
 - » Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
 - » Incorporate walls into a patio or sidewalk café space
 - » Terrace (step down) retaining walls



GUIDELINES: RESIDENTIAL BUILDINGS

- Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
- incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
- Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
- Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
- Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.



Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



Photo: Dan Burden



Photo: Joaquin Karakas



Photo: Joaquin Karakas



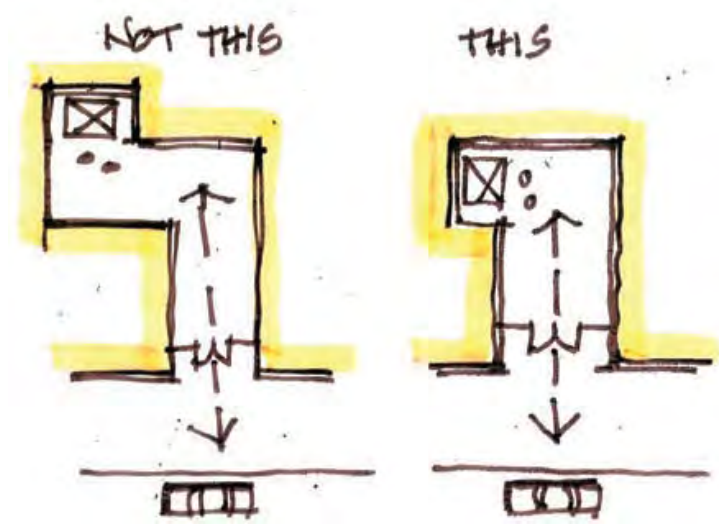
Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street. .



Photo: Joaquin Karakas

Consider using a landscape transition zone between entryways and public sidewalks.

- Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
- A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
- Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Direct sight lines into elevator lobbies are safer

VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
- New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.
- Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour
- Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's
- Mark/celebrate corners and street-end views through building and open space design.
- Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

- New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. "Flood Hazard Area Land Use Management Guidelines" (2004) (http://www.env.gov.bc.ca/wsd/public_safety/flood/pdfs_word/guidelines-2011.pdf).



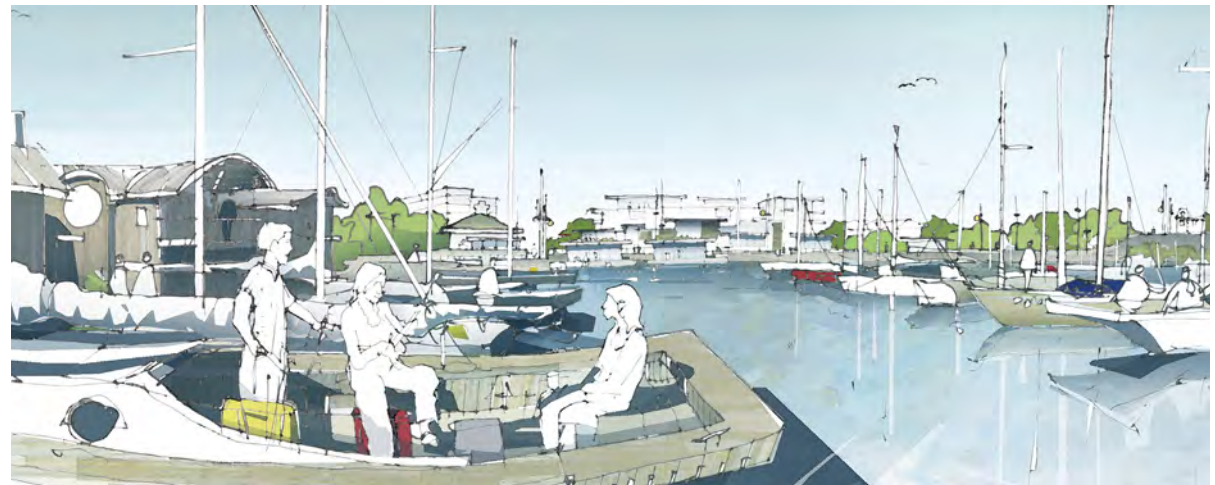
Photo: Bill Brown

Maintain street-end views to the harbour-front



Photo: Franc D'Ambrosio Architecture and Urbanism

Create new public connections to harbourfront uses and activities.

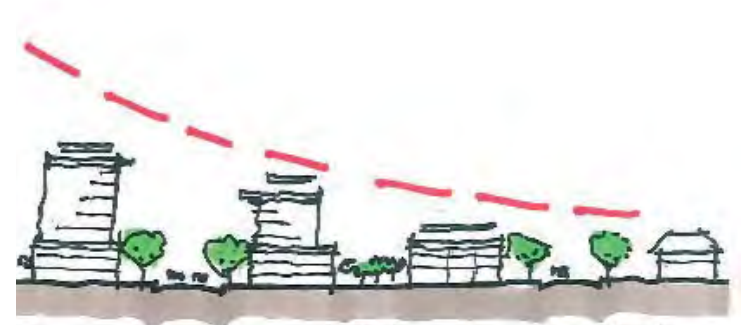


Neighbourliness

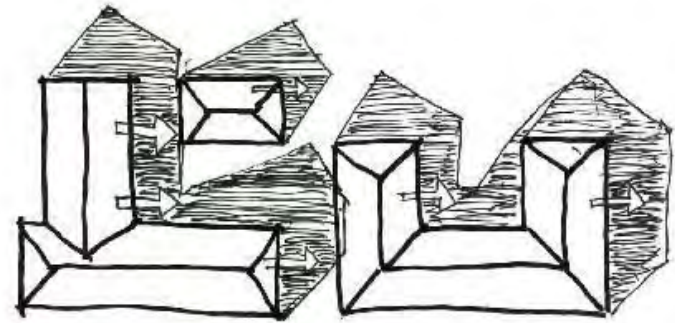
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

GUIDELINES

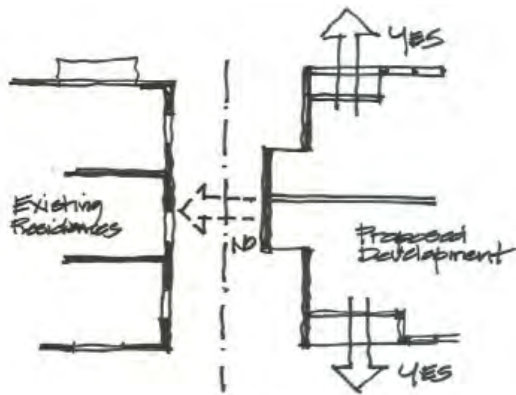
- New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.
- Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
- In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.
- Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
- Locate development to minimize view impacts on existing and planned future development.
- Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.
- Sun shade diagrams for the Winter Solstice will be required only when significant impacts are expected such as shading solar panels
 - » Winter Solstice: 9 a.m., 12 noon, 3 p.m.



Transition in scale to existing context



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and buildings

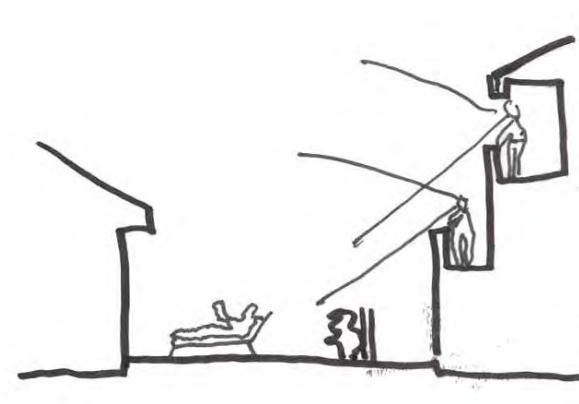


Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.



This building steps down to the slope to reduce the visual impact from it's massing

- Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
- Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
- Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
- Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 - » Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 - » Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 - » Stagger windows to not align with adjacent, facing windows.
 - » Primary windows into habitable spaces should not face interior side-yards



Minimize overlook into adjacent private yards

Architectural Concept: Achieving a Human Scale

OVERVIEW AND INTENT

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

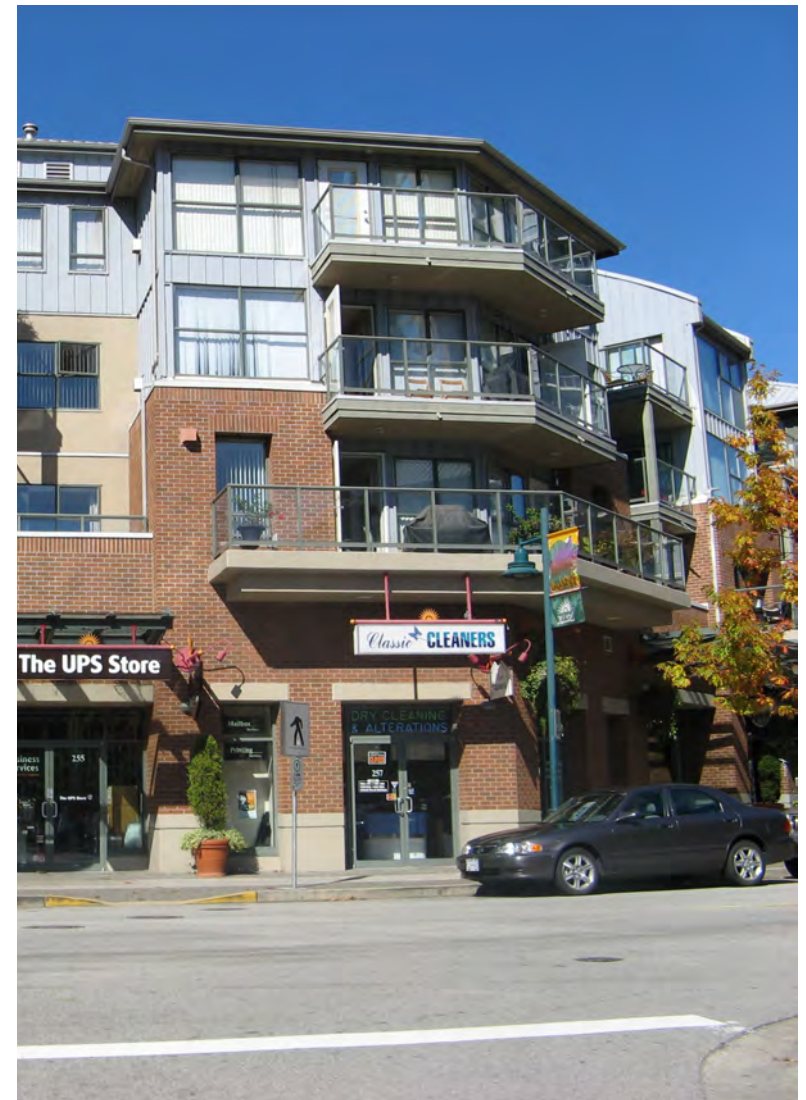
Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in a modern composition.

GENERAL GUIDELINES

- The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- Design buildings to express their internal function and use.
- Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.
- Examples of architectural features include:
 - » Building height, massing, articulation and modulation
 - » Bay windows and balconies
 - » Corner features accent, such as turrets or cupolas
 - » Decorative rooflines and cornices
 - » Building entries
 - » Canopies and overhangs
- Examples of architectural details include:
 - » Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
 - » Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
 - » Articulation of columns and pilasters
 - » Ornament or integrated artwork
 - » Integrated architectural lighting
 - » Detailed grilles and railings
 - » Substantial trim details and moldings
 - » Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians



Photo: Franc D'Ambrosio Architecture and Urbanism

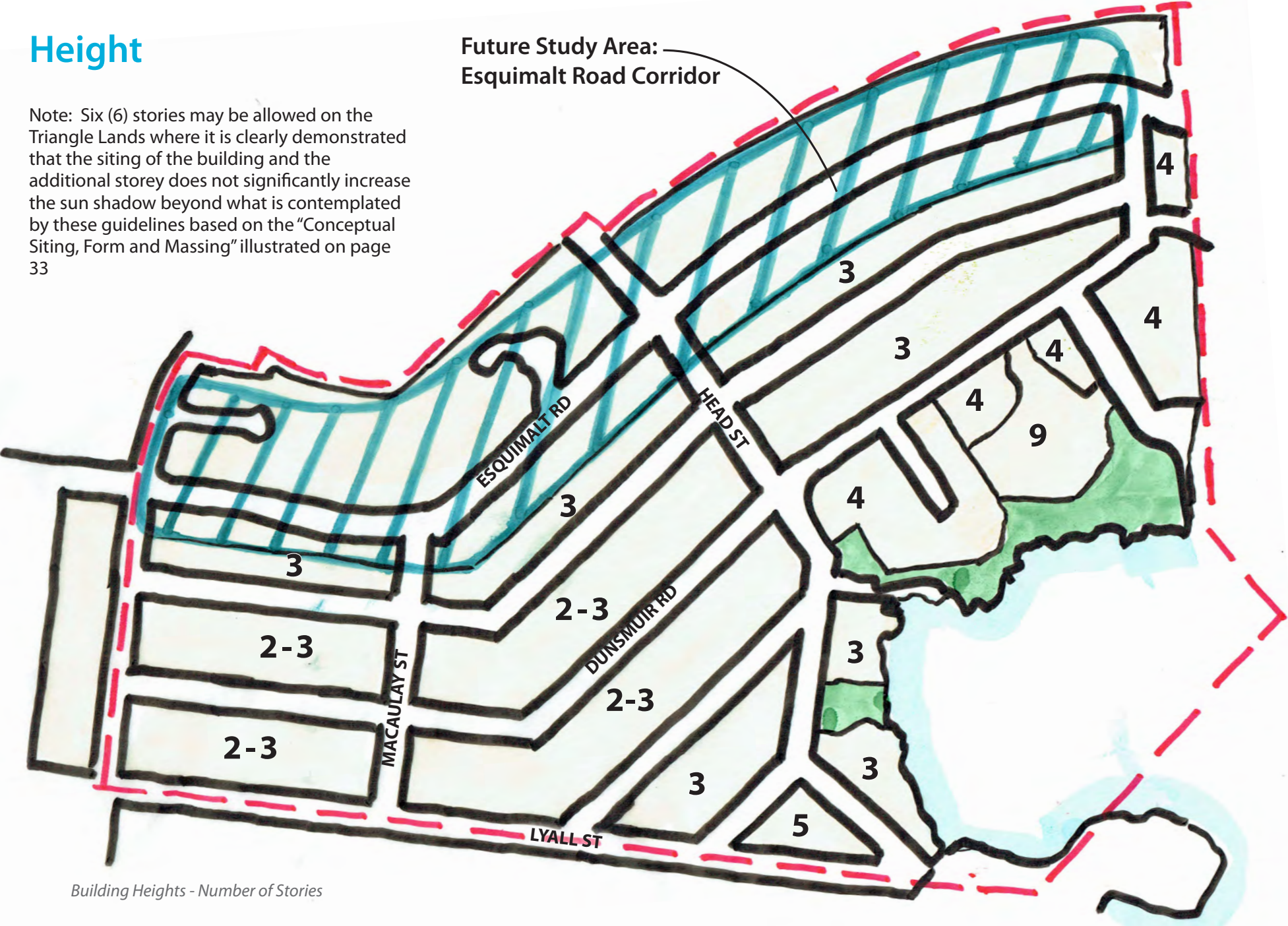
Design entrances to distinguish between individual commercial and residential.

- Locate and design entrances to create building identity and to distinguish between individual commercial and/or residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
- Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
- Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
- Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - » Are vertically proportioned and approximately the size and proportion of a traditional window
 - » Include substantial trim or molding
 - » Are separated from adjacent windows by a vertical element
 - » Are made up of small panes of glass
 - » Are separated with moldings or jambs but grouped together to form larger areas of glazing
- The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
- In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
- Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.

Height

Note: Six (6) stories may be allowed on the Triangle Lands where it is clearly demonstrated that the siting of the building and the additional storey does not significantly increase the sun shadow beyond what is contemplated by these guidelines based on the "Conceptual Siting, Form and Massing" illustrated on page 33

Future Study Area:
Esquimalt Road Corridor



Building Heights - Number of Stories

Green Healthy Buildings & Open Spaces

- Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.
- Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
- Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.

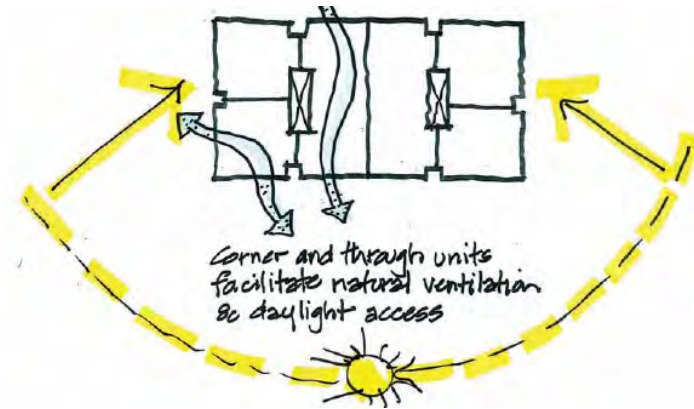
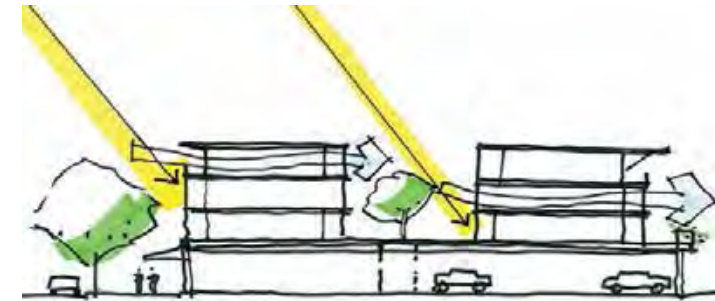
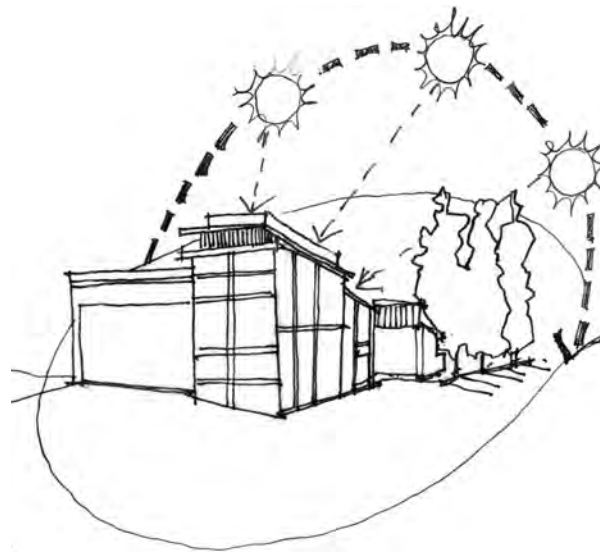
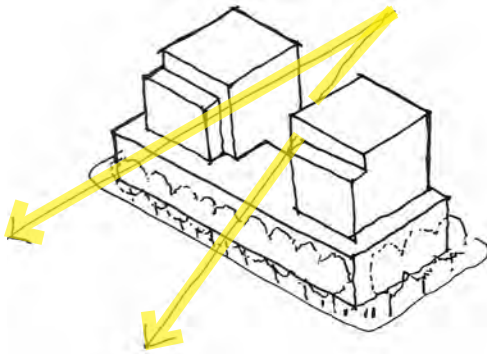


Photo: Joaquin Karakas

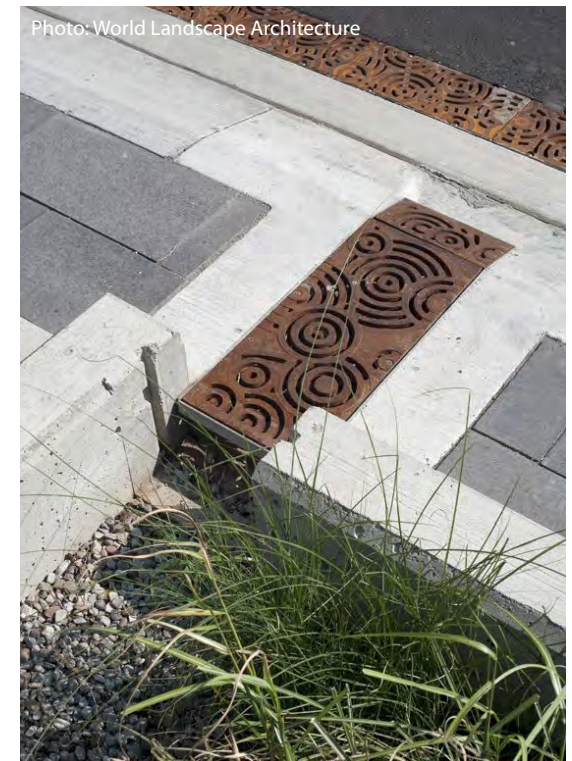
Incorporate common areas as defining elements of projects.



A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.



- New buildings should not block significant views or solar access to adjacent buildings and open spaces.
- Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
- Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
- Retention and infiltration best management practices for rainwater should be used as appropriate.
- Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.



Use best management practices for rainwater where appropriate.

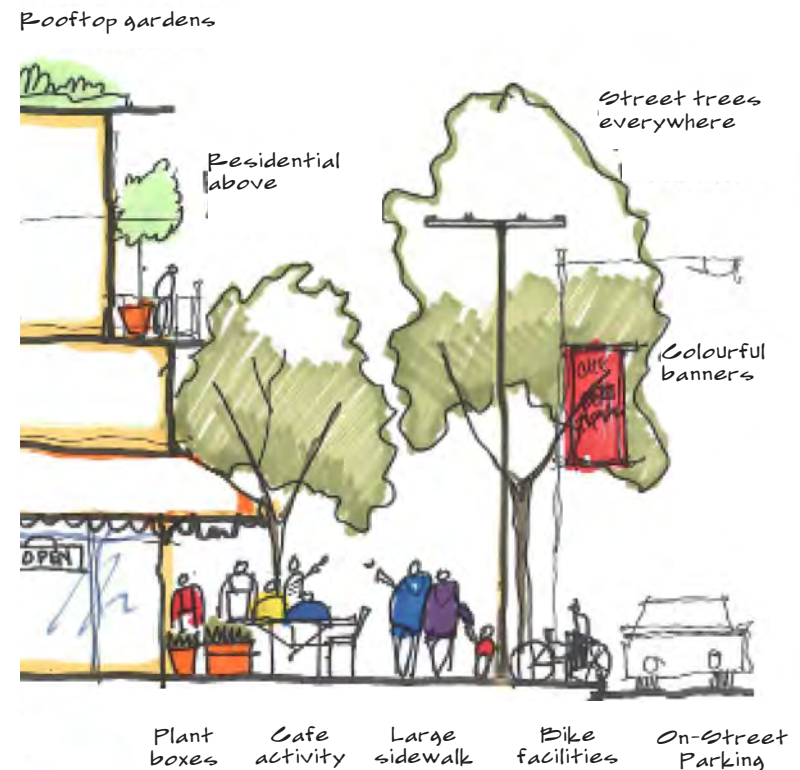
Public Realm Design: Pedestrian Comfort & Safety

INTENT

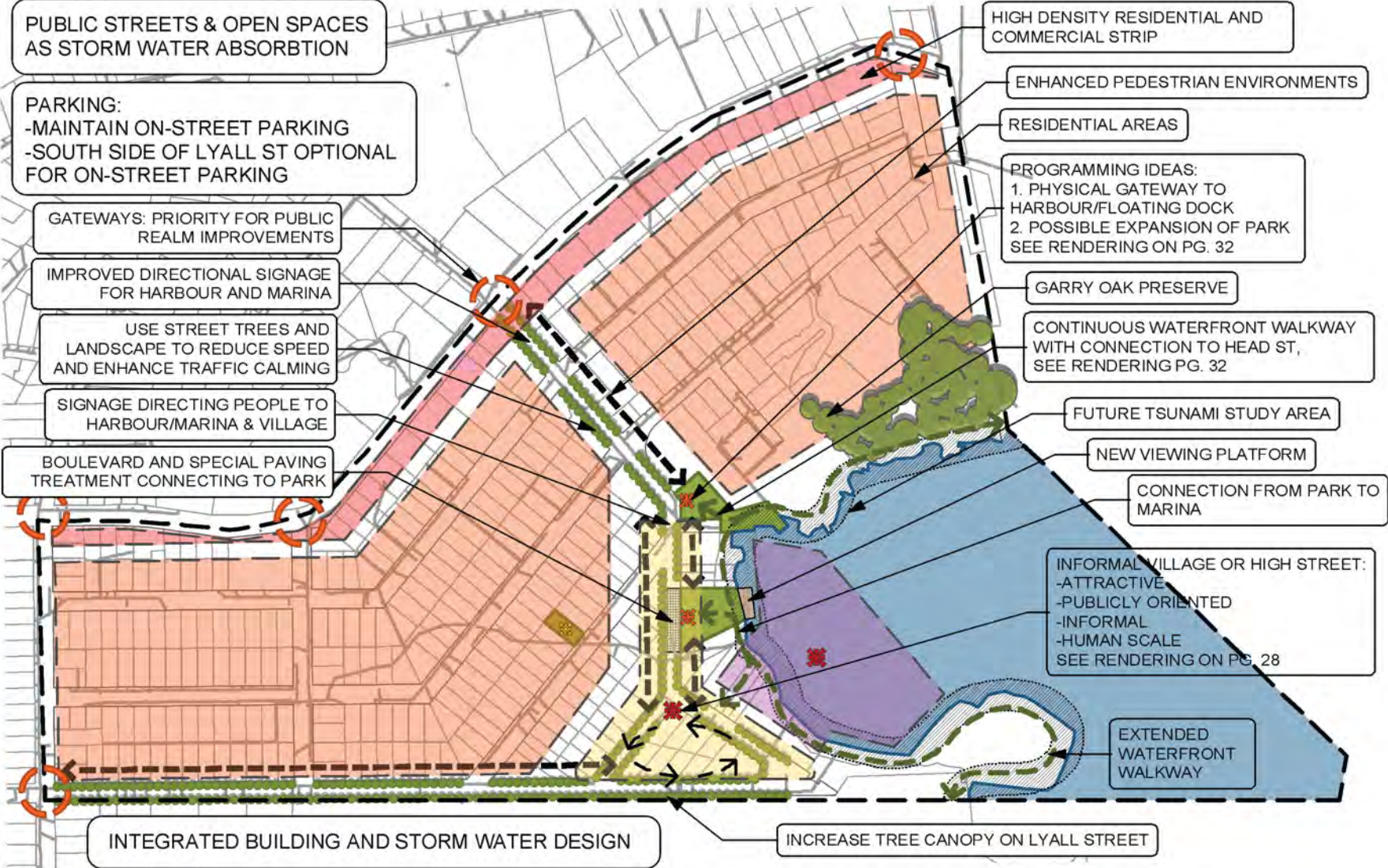
To ensure that the design of streets and open spaces creates visual interest, comfort, and safety for pedestrians and contributes to a unique local identity and sense of place for West Bay.

GUIDELINES

- Provide a continuous planting of street trees along both sides of neighbourhood streets, with priority for the Head Street South Village High Street and Esquimalt Road as indicated in the Recommended Public Realm Improvements map on page 25.
- Provide zebra- or ladder-painted crosswalks, or crosswalks made of special paving materials, at all key pedestrian crossings to increase driver awareness.
- Incorporate corner bulges into streetscape design in key pedestrian areas to enhance pedestrian crossings and provide space for landscaping, seating, rain gardens and public art.
- Provide adequate public streetscape amenities including benches, planters, garbage receptacles, bike racks, and distinctive bus shelters in areas with high pedestrian activity.
- Café tables are permitted and encouraged on public sidewalks in retail areas
- Pedestrian-oriented lighting should be provided with priority for high-activity pedestrian areas such as along Head Street and Esquimalt Road.
- Small plazas and squares should be incorporated in prominent locations with good solar gain, such as at Lyall and Head Street.
- Public art should be incorporated into parks, plazas and other key activity areas, especially at gateways and entry points, to enhance the sense of identity and entry into the neighbourhood.



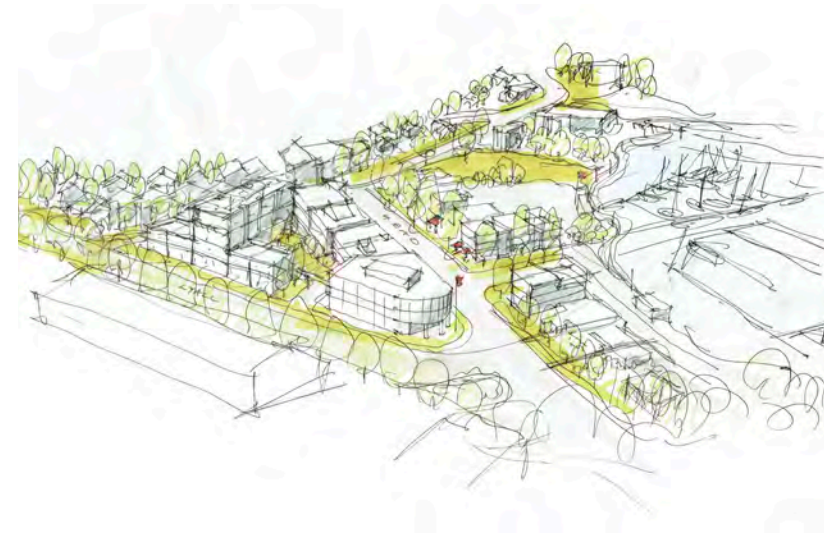
Recommended Public Realm Improvements



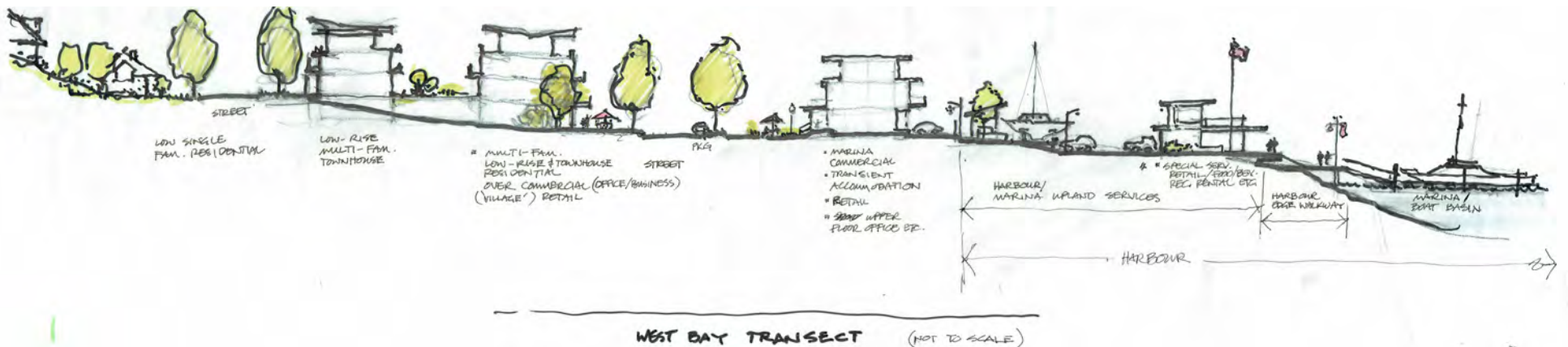
4. Site Specific Guidelines: Harbourfront Urban Village High Street

An important objective in the development of the design guidelines to address local concerns about density, height and massing, shadowing and view impacts, as well as the interface between federal lands and local ecosystems, specifically, in the harbourfront urban village area centred on Head Street South between Lyall and Paradise Streets. To this end, these guidelines were developed with input and feedback from neighbourhood stakeholders. This included an integrated design workshop (charrette) where design concepts and approaches for both public and private realms were developed. The sketch examples on this page were produced during this charrette.

This section is intended to provide site planning and design guidance and recommendations for this part of the West Bay Neighbourhood, based on stakeholder input. It is important to note that the concepts and illustrations on the following pages do not represent a development proposal, but rather a conceptual implementation of the preceding West Bay Neighbourhood Design Vision, Principles and Guidelines.



Sketches from the West Bay Design Workshop (above and below)



URBAN VILLAGE HIGH STREET LOOKING NORTH-WEST AT CORNER OF HEAD AND LYALL STREET

Maximum 5 storey building height



Urban Village high street defined by low scale buildings with active fronts and lush street tree canopy

Harbourfront retail, food and beverage uses

New public connection to harbourfront and Hidden Harbour Marina

Landmark corner plaza located at south gateway to urban village with views to harbourfront

URBAN VILLAGE HIGH STREET (HEAD STREET SOUTH) LOOKING SOUTH

Street trees, pedestrian oriented lighting and banners line either side of the street.

Continuous planting of street trees along south-west side of Head Street

Maximum 3 storey street wall along Head Street south

A rhythm of pedestrian oriented shop fronts and entryways line the street with sidewalk cafes and other uses spilling out onto the sidewalk

Landscaped bulb-outs and parking bays on north-east side of Head Street



Marked (zebra striped) pedestrian crossings at Gore and Lyall Streets

LOOKING NORTH TO HARBOUR AND URBAN VILLAGE HIGH STREET

*Publicly oriented active uses
activate the harbourfront*



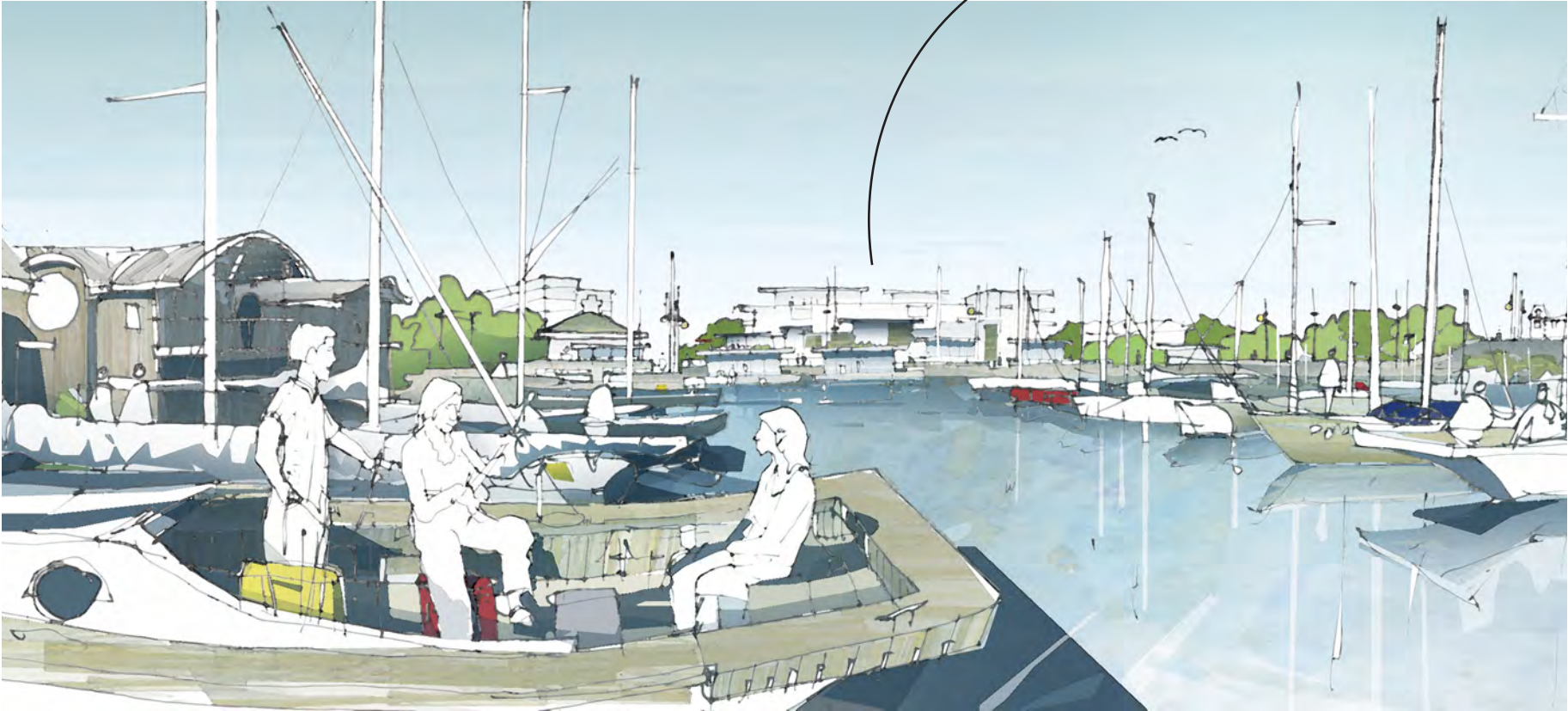
*Parking areas along the
harbourfront represent great
redevelopment opportunities where
appropriate*

*Continuous walking connecting
Songhees to West Bay Urban
village/marina and beyond*

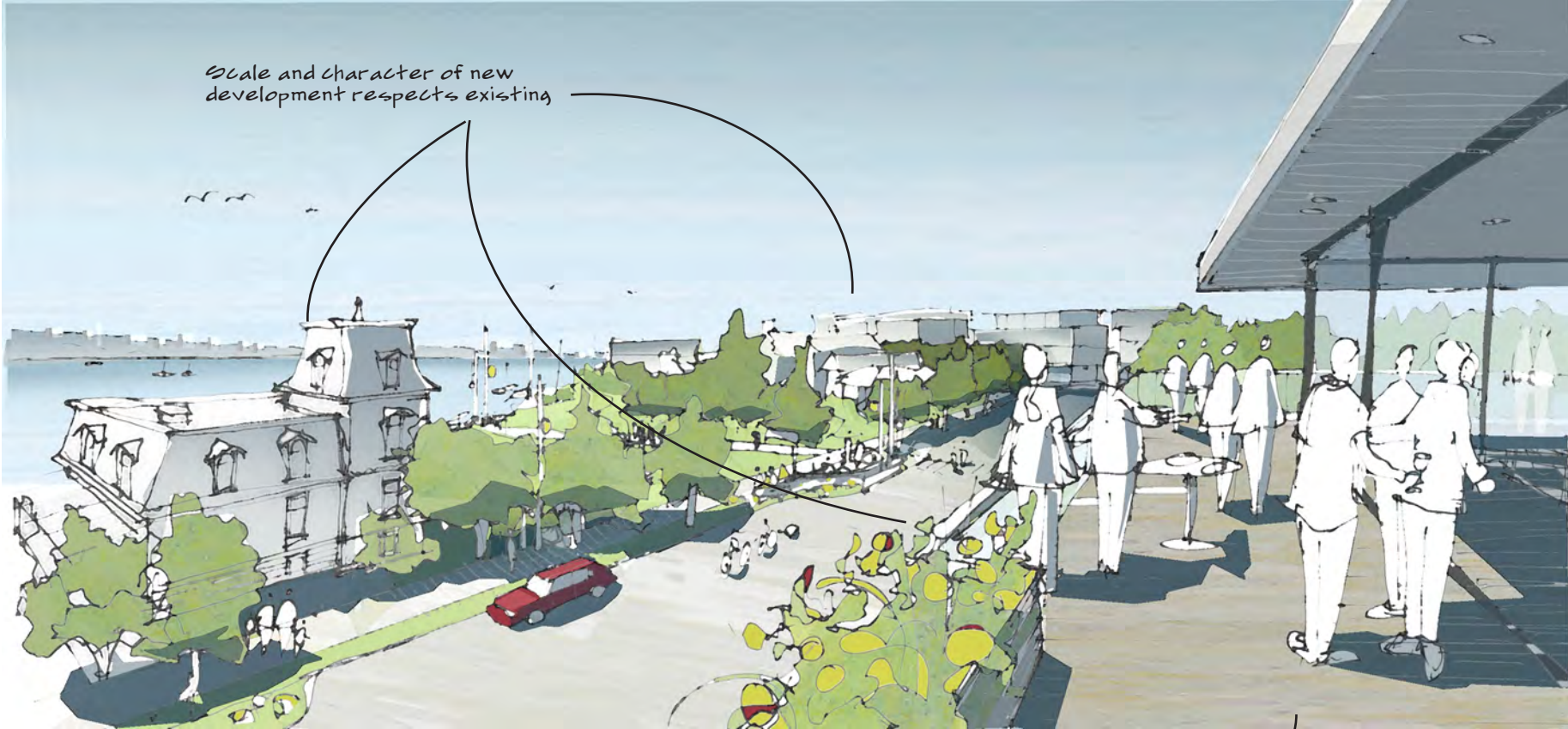
VIEW TO TOWARDS URBAN VILLAGE HIGH STREET FROM THE HARBOUR LOOKING WEST

Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity.

New development scaled to be compatible with, and not overwhelm, harbourfront character and identity. Also exhibits the transition massing from existing 7 storey on DND land.



VIEW FROM FUTURE BUILDING LOOKING SOUTH-EAST TO HARBOUR ACROSS URBAN VILLAGE HIGH STREET AND CAPTAIN JACOBSON PARK



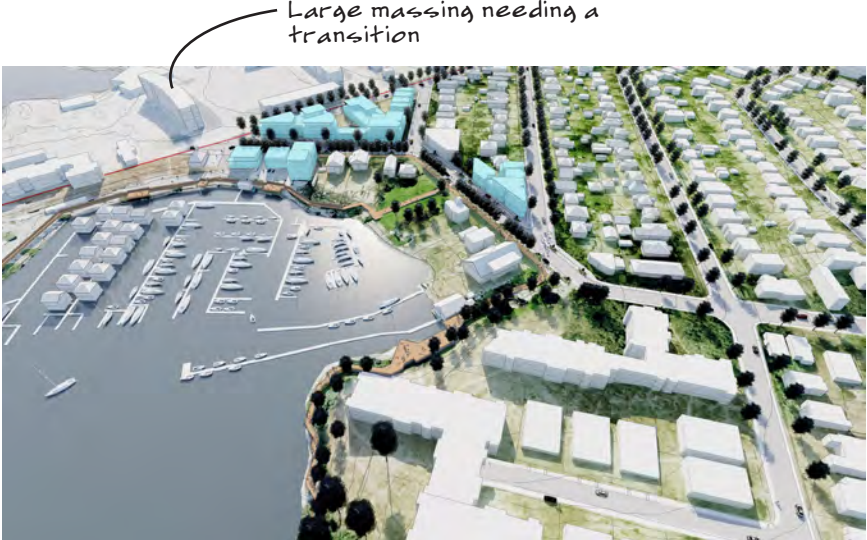
Scale and character of new development respects existing

Upper storeys step back to let sun onto urban village high street

HEAD STREET LOOKING SOUTH TOWARDS GORE AND LYALL

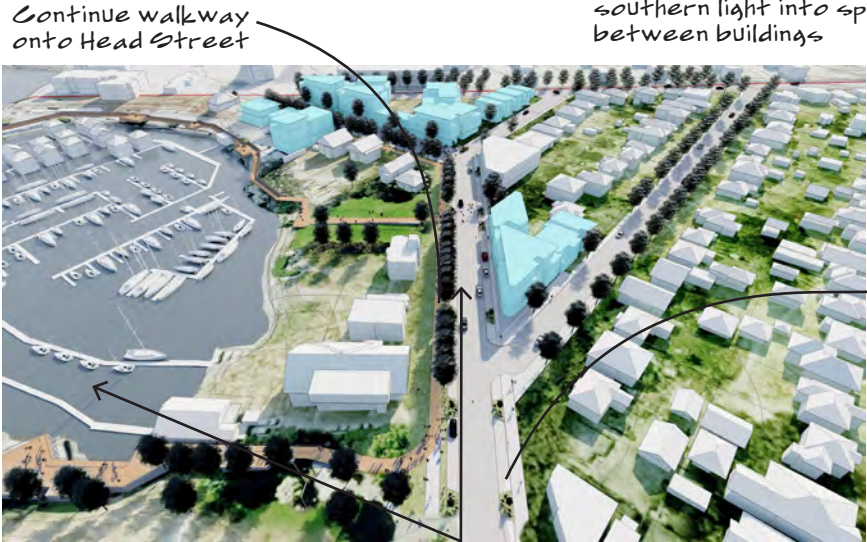


URBAN VILLAGE HIGH STREET: CONCEPTUAL SITING, FORM AND MASSING



Large massing needing a transition

- Legend**
- Conceptual massing
 - Continuous Boardwalk/Walkway



Continue walkway onto Head Street

Masses oriented to let southern light into spaces between buildings



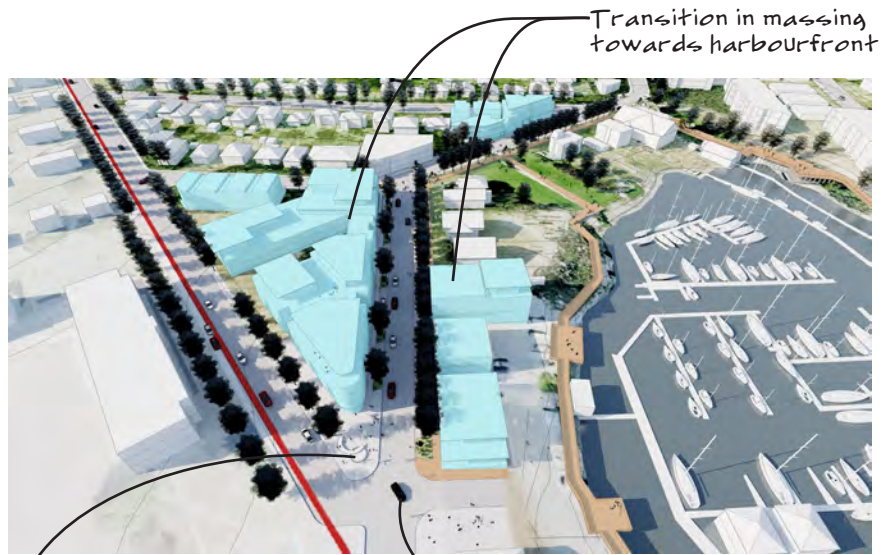
Continuous walkway

Public realm enhancements opportunity areas

New intersection

Improved view corridor

View on page 32.



Transition in massing towards harbourfront

Landmark corner plaza located at South gateway to urban village, with views to harbourfront. See sketch on page 27.

New intersection



View on page 28

Preserve and highlight heritage building

View termini



Streetscape Guidelines

OVERVIEW

The design, construction and maintenance of streetscapes in the West Bay Neighbourhood should serve to reinforce its unique identity by incorporating:

1. Strong connection to the harbourfront;
2. A walkable neighbourhood;
3. Human scale;
4. Harbourfront ecology;
5. Harbourfront community & identity, and
6. Neighbourliness.

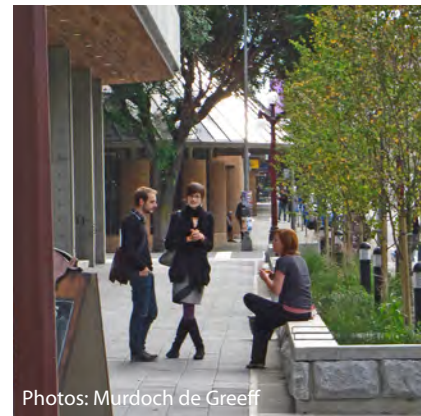
To illustrate the integration of these principles four typical streetscape sections and plans are presented below. These include Head Street North, Head Street South, Lyall Street West, and Lyall Street East. The sections focus on Public Realm improvements within two distinct areas of the West Bay Neighbourhood; West Bay Urban Village Gateways and West Bay Urban Village.

WEST BAY URBAN VILLAGE GATEWAY

West Bay Urban Village is accessible by Lyall Street and Head Street. Both streets currently have 18m wide Right of Ways (R.O.W) and accommodate two way traffic in standard 4.25m wide lanes. Objectives for the Lyall Street and Head Street Gateways include: improving the pedestrian environment, maintaining on-street parking, improved wayfinding, increased tree canopy, and integrated stormwater management. The Lyall Street West section shows a typical treatment through an existing residential lot on Lyall across from the Department of Defense (DND) lands. The Head Street North section shows a typical treatment near the intersection of Head and Esquimalt.

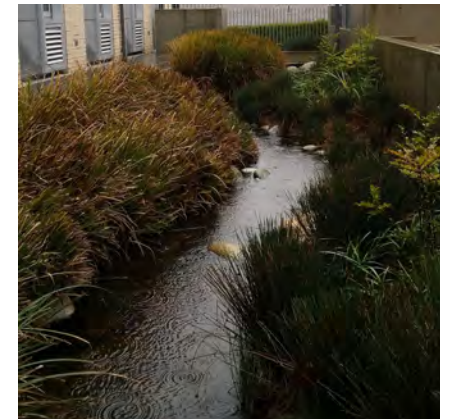
WEST BAY URBAN VILLAGE

West Bay Urban Village is located on the harbour front adjacent to the Marina. The area is identified for potential future development with a primary focus on the 'Triangle Property'. Some of the objectives for the Urban Village include: increased density, creation of places to gather, an inviting and active public realm, and reduced speed/traffic calming. Both the Lyall Street East section and Head Street South section show typical treatments at the triangle property and suggest a 'Skinny Streets' approach with reduce travel lane widths (3.0m)



Photos: Murdoch de Greeff

Integrated Storm Water Management, The Atrium building, Victoria BC



Integrated Storm Water Management, Reliable Controls building, Victoria BC

Integrated Storm Water Management

Integrate rain gardens, swales, tree planting, and other suitable BMP's to capture, slow, and treat storm water before it enters the harbour.



Photos: Murdoch de Greeff

Storm Water Plaza,
Cecelia Road, Victoria BC



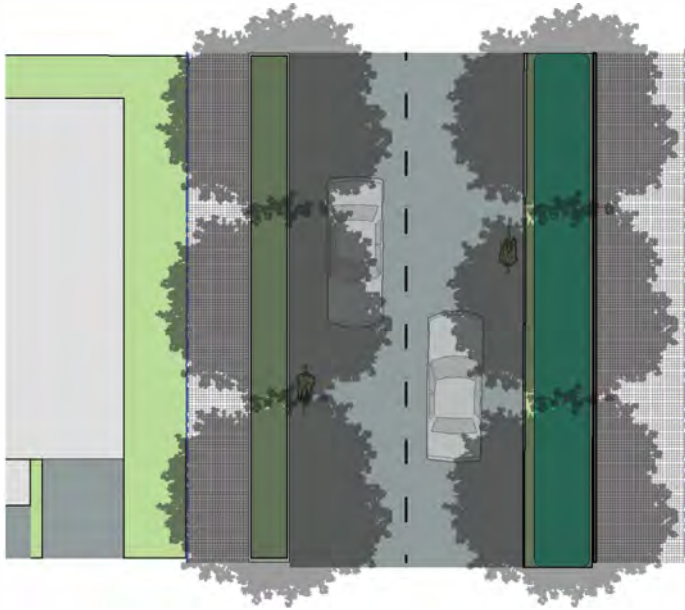
Photo: Paul de Greeff

Storm Water Streetscape,
Portland, Oregon



Photo: Murdoch de Greeff

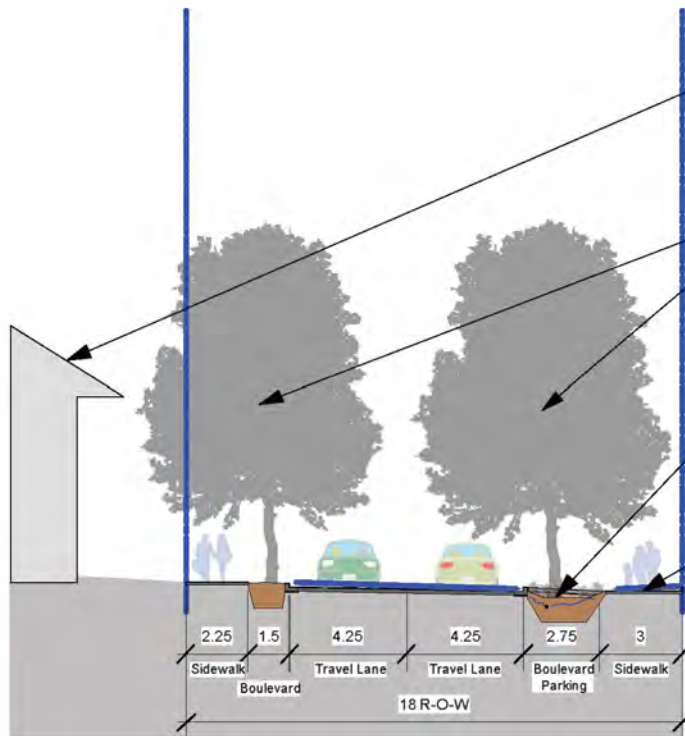
Storm Water Traffic Bulge,
Trent St., Victoria BC



Head Street North

Head street North, near the intersection of Head and Esquimalt Road, is intended to act as a Gateway to the Village, Harbour, and Marina. The intent on this section of Head is to increase the appeal of the pedestrian environment, employ clear wayfinding strategies to direct traffic towards the Village and/or Harbour/Marina.

Strategies to achieve this include: Increased sidewalk width to encourage walking, a double row of trees to accentuate Head as a linear corridor to the harbour/village, source controls such as onstreet rain gardens to treat stormwater, and similar building scales to reinforce the street end/harbour as a focal point. Development fronting onto the east side of this section of Head street should maintain physical and visual permeability to the waterfront by integrating paseos and achieving a minimal building separation of 6 metres



Neighbourliness-

Transition building heights from shoreline to upland areas to minimize shadow and view impacts.

Human Scale-

Utilize a double row of trees to accentuate the linearity of Head Street as a Gateway and the Village as a destination at the terminus.

Upland Ecology-

Install onstreet rain gardens to capture, treat and slow street run off before it enters the harbour downstream.

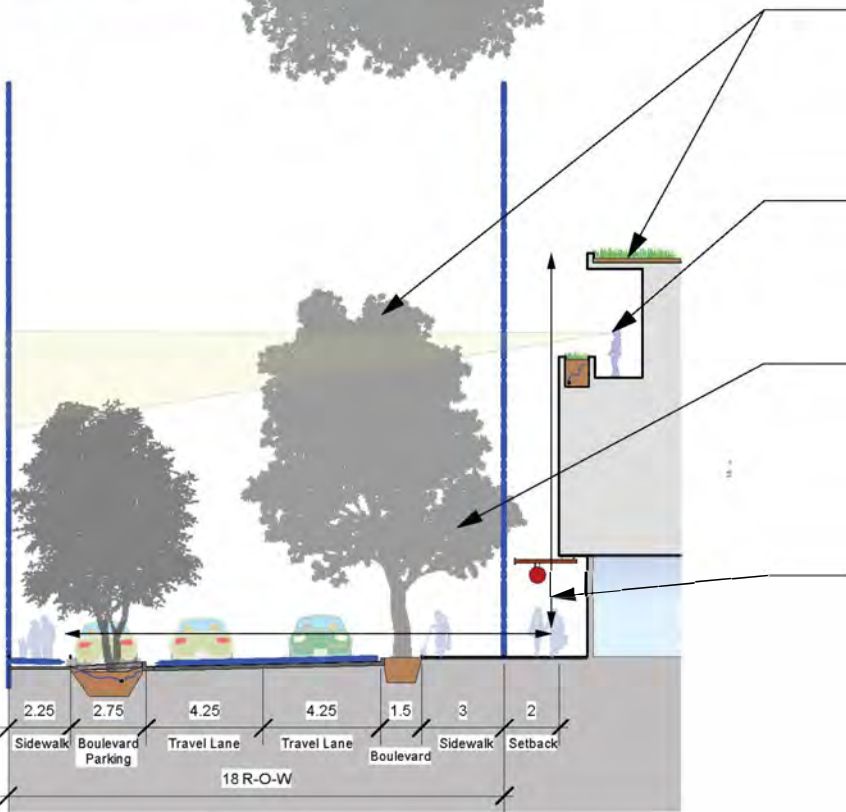
Walkable Neighbourhood-

Utilise vegetated boulevard strips to provide separation from vehicular traffic and increase sidewalk widths on gateway streets to promote wayfinding while contributing to attractive and inviting pedestrian environments.

Head Street South

Head street South, is intended to act as the communities Village or High Street. The intent on this section of Head is to provide public open spaces that contribute to a sense of being on the waterfront while supporting the commercial viability of the area. The *West Bay Village* public space should include but is not limited to waterfront access, space for gathering and festivals, and an attractive pedestrian oriented environment.

Strategies to achieve this include: *Distance to Building Height Ratios* of between 1:1 and 3:1, use of green building techniques to preserve the health and visual aesthetic of the shoreline, and transitions in scale to perserve upslope and adjacent views.



Waterfront Community and Identity-

Green Roofs, street trees and building heights support each other to reinforce the identity of West Bay as a Green Waterfront Community. Designs acknowledge views both to and from the inner harbour.

Strong Connection to the Waterfront/Neighbourliness-

Buildings step back and transition in scale to maximize potential for harbour views while minimizing shadow impacts on adjacent properties.

Waterfront Ecology-

Tree plantings intercept rainwater, reduce storm water flows through evapotranspiration, and provide valuable bird habitat contributing significantly to the shoreline ecology.

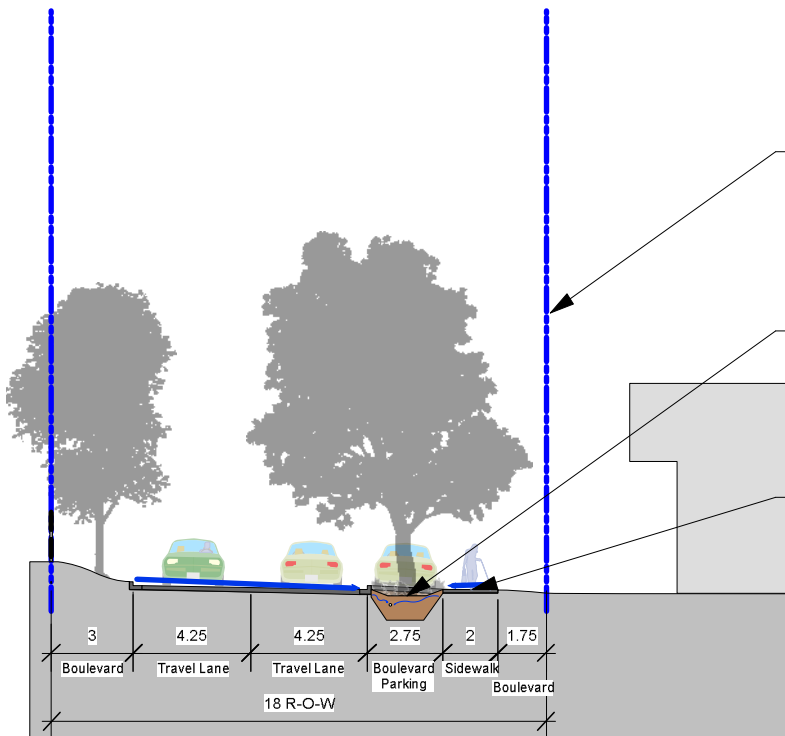
Human Scale-

Buildings should be between 1:1 and 3:1 *Distance to Building Height Ratio* to create intimate and inviting public open spaces.



Lyall Street West- Existing Residential

West Lyall Street acts as an important connection from the harbour to the surrounding residences, Esquimalt village, and the Esquimalt Rec. Center. Future developments along this corridor should focus on improving the pedestrian environment to increase the walkability of the neighbourhood. Strategies to achieve this include: Tree planting to reduce the visual scale of the street and provide shade, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour downstream, and separated boulevards to increase the attractiveness of the pedestrian environment.



Human Scale-

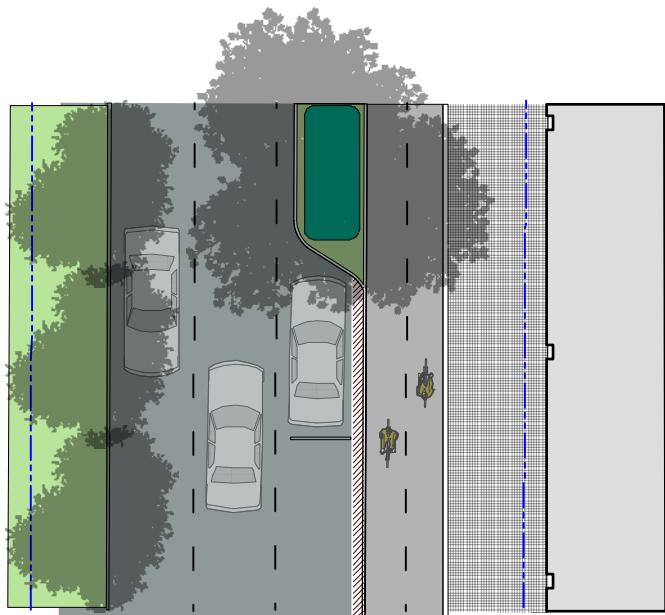
Tree planting can reduce the visual scale of the street while offering summer shade for the street and sidewalk.

Waterfront Ecology-

Integrated rain gardens to treat stormwater and provide habitat for birds and pollinators.

A Walkable Neighbourhood-

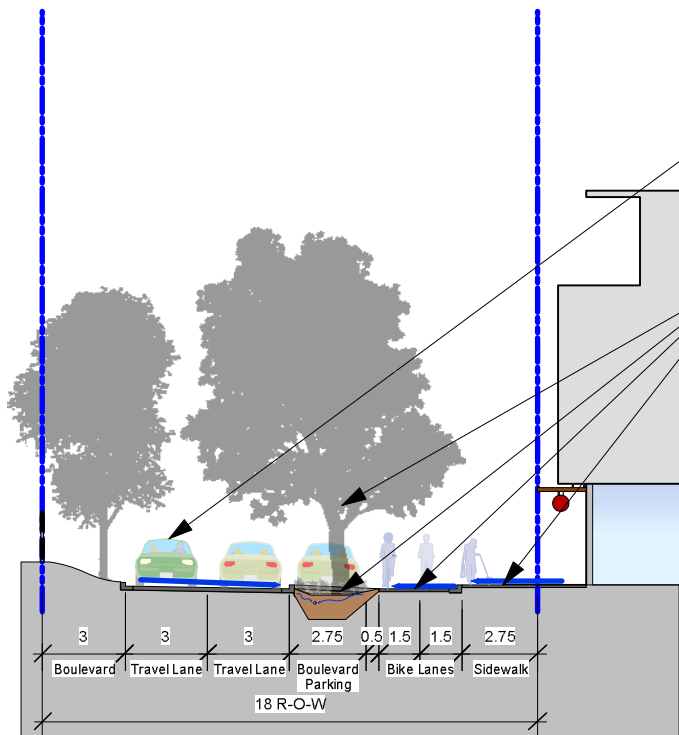
Use separated boulevards to remove sidewalks from vehicular traffic and increase the attractiveness of the pedestrian environment.



Lyall Street East- West Bay Village

East Lyall Street at the harbour interface is meant to act as part of the *West Bay Village*. The intent here is to contribute to the sense of a waterfront village while providing an attractive pedestrian environment with places for festivals and gatherings. Located adjacent to the DND lands Lyall street, in general, should focus on pedestrian improvements on the North side which is more heavily used by the public.

Strategies to achieve this include: Tree planting to reduce the visual scale of the street, vehicular travel lane reductions to provide space for bike lanes and encourage alternative modes of transportation, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour, and seperated boulevards to increase the attractiveness of the pedestrian environment.



Walkable Neighbourhood

Skinny Streets help to maximize pedestrian oriented space and allow for increased sidewalk widths as well as seperated bike lanes.

Waterfront Ecology-

Significant tree planting, green infrastructure, bike lanes, access corridors to waterfront recreation, clear wayfinding, and attractive pedestrian spaces should be layered and integrated to create a vibrant community, ecologically healthy shoreline, and healthy upland ecology.

3b

Lyall Street- West Bay Village
Scale: 1:200



21 July 2017

Design Review Committee Chair
c/o Esquimalt Planning Department
Esquimalt City Hall

RE: **Marinaview now known as Westbay Quay**
460 & 464 Head Street, Esquimalt, BC
Development Permit Submission



101 1851 Oak Bay Avenue
Victoria BC V8R - 1C3

phone: 250 . 592 . 9198
fax: 250 . 592 . 9178

Members of Design Review Committee, c/o Trevor Parkes, Senior Planner,

Please find enclosed a Development Permit Submission for phase 2 of the Westbay Triangle lands, known here under its new name as Westbay Quay. A proposal for a comprehensive redevelopment of the following two single family home lots being amalgamated into one land holding:

*Civic address of record for Development permit application: 460 & 464 Head Street consisting of:
PID 009-174-851 Lot 10, Block H, Section 11, Esquimalt District, Plan 292 [460 Head Street];
PID 009-174-869, Lot 9, Block H, Section 11, Esquimalt District, Plan 292 [464 Head Street];*

Hillel Architecture was retained in August of 2014 to chart a new course for the Development of the Westbay Triangle. Hillel Architecture engaged in a comprehensive community consultation process, involving neighbouring residents, Esquimalt Planning and Development staff, Esquimalt Engineering staff, the Esquimalt Chamber of Commerce, and numerous one on one interviews which defined an outcome for the Westbay Triangle as a whole. From this body of work, and within that consultation process, a rezoning and development proposal was illustrated for one portion of the land holdings known as the Westbay Triangle, and one portion developed for the remaining half, formerly known as Marinaview, and now issued in its final public name as Westbay Quay.

The Westbay Triangle went on to receive its DP approval and its construction documents are now underway, for construction coincident with Westbay Quay as one continuous build out.

Hillel Architecture presented Marinaview to the Design Advisory Panel Thursday February 9th 2017, and earned approval to proceed to council for a request of a Public Hearing.

Hillel Architecture presented Marinaview to the Esquimalt Advisory Planning Commission, February 21st, 2017. The APC unanimously approved the project proceeding to council for a request of a public hearing.

Hillel Architecture hosted a neighbourhood consultation meeting with the residents surrounding Marinaview March 8th, 2017, from which no voice of objection came forward, and letters of support were received.

Marinaview Public Hearing was held May 19th, 2017, and proceeded without a voice of objection. City Council voted unanimously to approved the RZ application.

Through its own consultation with Neighbours, Esquimalt Staff, and later with the members of APC, DRC, the proposal evolved subtly, each time improving, and each time not waivering from its original form as developed with, and supported by those earlier participants: the neighbours.

Westbay Quay is different

The Marinaview design objectives differ from the Westbay Triangle. The Westbay Triangle establishes a dynamic village shopping district, respects the location of, and architectural style of, the neighbouring heritage homes and businesses. The architectural style, its materiality and detailing, respects not only the local heritage buildings but also the practical hard working nature of buildings serving our marine industries along the coastal edge. The Triangle also at this street level has a series of design instruments which define the perceived boundaries of this village.

The Triangle's deep front yard setbacks are designed to provide a leisurely outdoor shopping area where one can be surrounded by, and walk through a display space enfused with the products and services of those retailers. This outdoor village concludes at one end facing Gore, and parking entry (interrupting pedestrians) and protecting the quiet residential nature of Gore Street.

In the direction of Westbay Quay, the wide front yard setback concludes with planters which feed pedestrians into a narrower pedestrian walkway. Westbay Quay steps forward subtly indicating the end of the dynamic village, a change in setting to open ocean and marina views. This concludes at a sun filled public outdoor social space.

Westbay Quay has a different focus.

Westbay Quay has a different focus. Westbay Quay has a different perspective. Quite literally Westbay Quay, has a differing view. Where the Triangle focuses its attention internal to the village scene, and benefitting all local businesses, Westbay Quay's sightlines are outward, far in nature, and wide open. Out from under the canopy of mature trees and substantive canopies of the Triangle, one step removed from the heritage setting of the village, towards the more modern forms of the Westbay Marina, the Marina's contemporary float homes, and the modern lines of power and sail boats. Here in the open, in a location where orientation towards the sun is all day long, is both an area of substantive public seating, and a commercial tenant's outdoor seating surrounding this location reserved for a full service restaurantur.

This context is clean and bright, open, its sightlines far reaching and its social spaces bathed in sun all day long, and conducive to longer leisurely stays. Westbay Quay is a design extension of the Westbay Triangle, conceived together, and has several design ques joining the two buildings together. Yet Westbay Quay is the younger sibling. Its materials brighter and more contemporary. Its lines and form decidedly more modern. Where its sighting offers panoramic views the building

responds and offers up panoramic vistas for its patrons or residents alike.

Since its original presentation to the DRC the materials have escalated in value and perception. The exterior of Westbay Quay, voluntarily developed as non-combustible, is enclosed with composite metal building panels and anodized aluminum glazing systems. The composition is anchored at the street level with a textured composite metal panel band surrounding the lowest level of residential decks to afford them privacy and to define the village level of shops and businesses. Here at street level real stone veneer ties the composition to the Triangle and is a matching stone. However the heavy timber framing, heavy timber brackets with hot dipped galvanized components of that larger more industrial sibling are designed in Westbay Quay as a more refined palette of materials and surfaces. Here the brackets are matched in size and scale and of similar components but executed in smooth structural metal in form, with aluminum components. Exterior siding in the village setting with a textured bandsawn face with oil finishes are escalated to a more refined surface of Aluminum plank siding in a wood tone manufactured by Longboard and colour matched to finishes at the Triangle. Where one was heavy duty, or industrial, of character filled to match its heritage building setting, Westbay Quay pulls the total composition to a more contemporary feel. Signage, light fixtures, building hardware, and building detailing all demonstrate a refinement, a higher level of finish. Where the Triangle had some products of brushed finishes, those same products at Westbay Quay will be polished. Where the Triangle's building detailing is suitable in scale to its larger form, Westbay Quay will have its flashings, reveals, and alignment of edges purposefully scaled smaller, tighter, and as aligned as waterproofing and materials permit.

Enclosures

3 sets 24x36 full size submission drawings 1 set 11x17 record set of submission drawings Certificates of Title, of less than 30 days. Application form Green building check list UBS digital record of same + video flyover of building massing in neighbourhood setting, with neighbouring buildings

Project Data Summary

Proposal Data	Current Zoning
Permitted uses of C-7 respected	Proposal conforms to permitted and prohibited uses
Site Area 1207.7m ²	
Lot Coverage 45.2% (all roofs, canopies, balconies)	No ground floor maximum specified, second floor and higher deemed not to exceed 80%
Floor Space Ratio 1 : 1.31	No FSR stated
Building Footprint 456.2 m ²	No ground floor maximum specified, second floor and higher deemed not to exceed 80% or ±966.16 m ²
12 ocean view residences	
3 ground floor commercial tenancies	
Building ht. 5 stories / 17.7m	13 m by current zoning (±4 stories), 5 stories permitted by WBCDG

<p>Building setback front yard</p> <ul style="list-style-type: none"> • Head Street: 2.4m bldg. face / 1m balcony • Head at Lyall St: 2.8m bldg face / 1.2m Balcony • Lyall Street: 2.8m bldg. face / 1.8 balcony • (note: balcony overhangs do not exceed permitted roof overhangs) 	<p>0m by current zone, 2m implied by WBCDG</p> <p>0m, no mention in WBCDG</p> <p>0m, no mention in WBCDG</p>
<p>Building setback exterior side yard</p>	<p>0m, no mention in WBCDG</p>
<p>Building setback rear yard to residential uses</p> <ul style="list-style-type: none"> • over 21m shown, unit to unit along Head Street • respects 12m no build setback along Lyall Street 	<p>3m by current zoning 3m by current zoning</p> <p>3m by current zoning 3m by current zoning</p>
<p>Independent entry provided</p>	<p>In mixed use buildings, residential suites are to be serviced by an independent entry</p>

Westbay Quay is a five storey mixed use, voluntarily non-combustible building hosting 12 ocean view residences and 3-4 ground floor commercial tenancies, one of which is specifically designed to attract a full service restaurateur. The project originally meet the Schedule C Parking Bylaw without seeking a variance, and now with an increase in commercial space seeks a variance for 3 stalls, and has a design solution of three new street side stalls effectively meeting the need in another way.

18 calculated commercial parking stalls are provided, 10 above grade, and 8 are demonstrated below grade for employee & leaseholder designated use. These commercial stalls are not available for residential use, or available for sale to others. Serving the needs of the employees & leaseholders below grade also ensures that surface parking is: *always available for, and used by patrons in the high turnover manner the Bylaw had intended* - without a series of employee vehicles that can become more permanent daily parked vehicles occupying prime spaces. 16 spaces are provided as required for residential use.

Street side parking

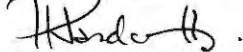
Combined, The Westbay triangle and Westbay Quay contribute 23 street side public parking spaces This is a significant number of high turnover public stalls, recessed safely from traffic flow, benefitting the new and existing businesses of the Westbay and Westbay marina area. In addition it is important to note that the Westbay Village area is well served by bus routes, community linking pedestrian walkways and ocean side trails – The Songhees Walkway - leading to the downtown core. It must also be remembered that here, in Westbay, a large portion of the neighbourhood resides off shore. Their parking has already been provided. To this village setting they will gladly walk.

Summary

Most importantly, the design of both Westbay Quay enclosed here, and the formerly submitted Westbay Triangle grew out of the same consultation process. The design solution evolved conversations and reviews from multiple contributions. We hope that in the enclosed DP application package, and in each of the coming presentations, that everyone involved, from neighbours, Esquimalt staff, local businesses, Mayor and Council members, and members of the DRC can see a piece of their personal contributions in the now fully realized composition of the Westbay Triangle.

Regards

Peter Hardcastle



Hillel Architecture Inc.



WESTBAY QUAY, 460 & 464 HEAD STREET, WESTBAY



Hillel
architecture
inc.
101 1891 Oak Bay Avenue
Victoria BC V8R 1K3
phone 250.592.9998
fax 250.592.9978



CIVIC ADDRESS

CIVIC ADDRESS
460 & 464 Head Street, Victoria, BC

PROJECT DESCRIPTION

A 5 storey mixed use proposal combining a main public floor with marine view restaurant with outdoor social spaces, combined with 4 floors of residential ocean view units above.

All commercial tenants add to Westbay Village scene.
All residences are ocean view suites.

PROJECT DATA

PROPOSED PROJECT

ZONING

460 Head Street - Current Zone is C-7
464 Head Street - Current Zone is RS-1

Combined properties are designed in conformance with Westbay Community Development Guidelines produced by Esquimalt, August 2015.

DENSITY

Site Area (m2) 1,207.7 m2 (13,000 ft2)
Lot Coverage % 45.2% - 545.9 m2 (building + overhangs as footprint)
Floor Space Ratio 1.31 : 1
Building Footprint 456.2 m2 (4,910 ft2) / 545.9 c/w decks and misc overhangs
Total Floor Area

Esquimalt definition for floor space ratio calculations
Ground Floor: 372.5 m2 (4,010 ft2)
2nd Floor: 366 m2 (3,940 ft2)
3rd Floor: 366 m2 (3,940 ft2)
4th Floor: 301 m2 (3,240 ft2)
5th Floor: 176 m2 (1,895 ft2)
Total Floor Area: 1,581.5 m2 (17,025 ft2)

Building 1 Ground Floor breakdown:
CRU 101 / 102 / 103 combined Group D, Group E 152 m2 (1,636 ft2)
CRU 104: Group A 126 m2 (1,360 ft2)

Maximum Height 17.7m ± above average grade 6.05m (23.75m geodetic)
Maximum Storeys 5 storeys

SITE SETBACKS

Head Street -2.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 38
Drawings demonstrate 2.4m to building face
Junction at Head & Lyall Streets -illustrated similar to 2.0m page 38 of development guidelines
Drawings demonstrate 2.8m to building face
Lyall Street -1.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 40
Drawings demonstrate 2.8m to building face
Interior Side Yard -1.2m demonstrated to internal sideyard
No build Zone Minimum 12m, no build zone to neighbouring property at West Bay 1
Drawings demonstrate 12.4m setback to building face

RESIDENTIAL UNITS

No. of Residential Units **12 total residential units plus 4 commercial units**

Ground Floor: 4 commercial units
2nd Floor: 4 residential units
3rd Floor: 4 residential units
4th Floor: 3 residential units
5th Floor: 1 residential unit
16 total

2nd & 3rd Floor Residential Units
1 One Bedroom Unit
3 Two Bedroom Units

4th Floor Residential Units
3 Two Bedroom Units

5th Floor Residential Unit
1 Two Bedroom plus den Unit

LEGAL ADDRESS

PID 009-174-869, Lot 9, Block H, Section 11, Esquimalt District, Plan 292 (464 Head Street)
PID 009-174-851, Lot 10, Block H, Section 11, Esquimalt District, Plan 292 (460 Head Street)

PARKING

Parking stalls on site

Parking Bylaw Standards referenced:
Cafe @ 14m2 or 5 seats per stall
Grocer @ 35m2
Retail @ 25m2
Office @ 30m2

CRU 101 / 102 / 103 combined @ 152 m2 / 25 6 stalls (Group D or E)
CRU 104 @ 126 m2 / 14 9 stalls, or:
1 stall / 5 seats x 68 14 stalls (inside + outside seating)
Total commercial: 20 parking stalls

Total Residential 12 x 1.3 each: 15.6 stalls required
18 stalls provided
Residential Guest Parking (25% assigned as visitors - 3 stalls)
Total Project provided: **38 stalls req'd / 34 provided on site**
(24 underground stalls / 10 surface stalls)

Note to Parking totals: 19 former parallel parking stalls along street increased to 23 - these are not included in calculations above

Commercial Loading Stall

Not required / less than 700 m2 commercial space

parking stalls off site

5 parallel street parking stalls provided on municipal property and not counted

BIKE PARKING

Bicycle parking Class I (enclosed in bldg)
Commercial bicycle parking Class II (outdoor racks)

18 provided (1.5 per residential unit) As per: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1.i, page 16

Guest bicycle parking: Residential Class II (outdoor racks)

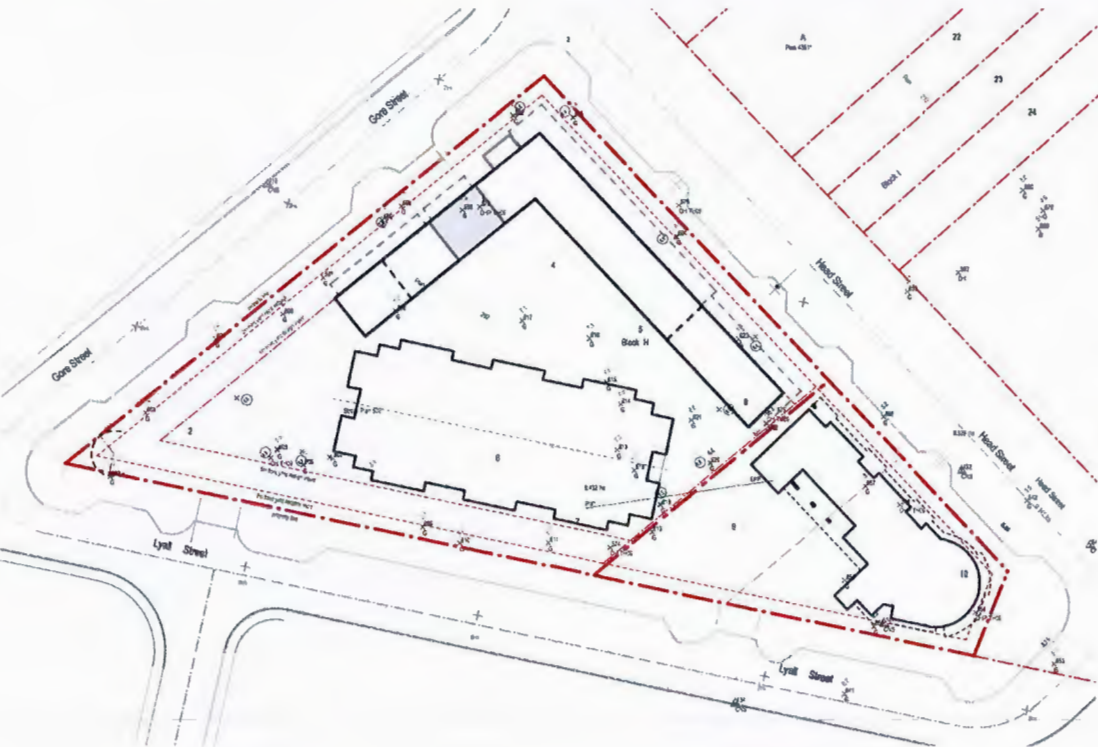
6 total provided for commercial staff and patrons
6 guest bikes as per: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1.i, page 16

BUILDING STORAGE

Residential Storage

12 residential storage lockers provided (min 1 per residential unit)

AVERAGE GRADE POINTS



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NO.	DATE	DESCRIPTION	APPROVED BY
1	21 July 2017	Development Permit	
2	14 March 2017	Revised Application	
3	18 January 2017	Receiving Application	
4			
5			
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7			
8			
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10			

WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

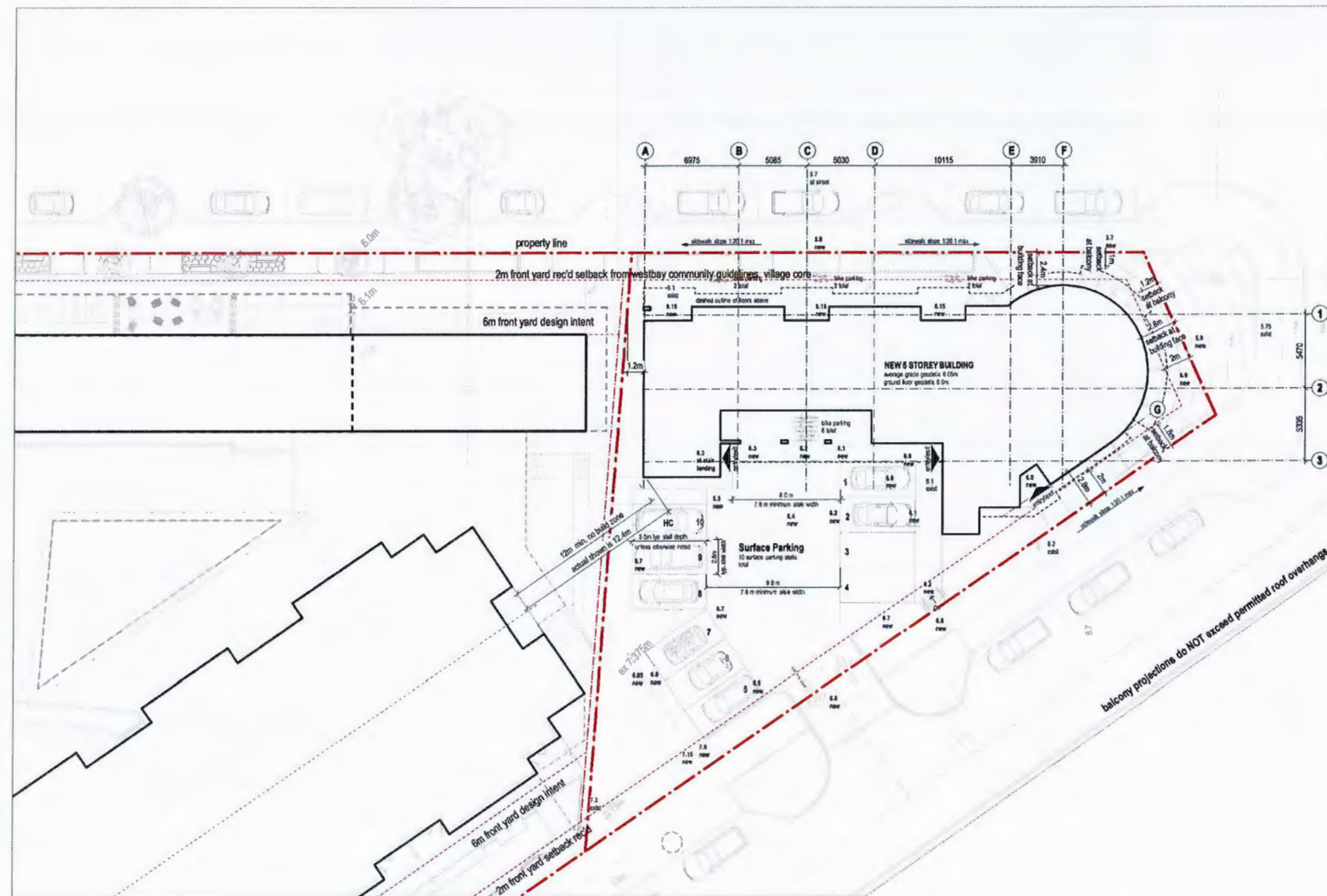
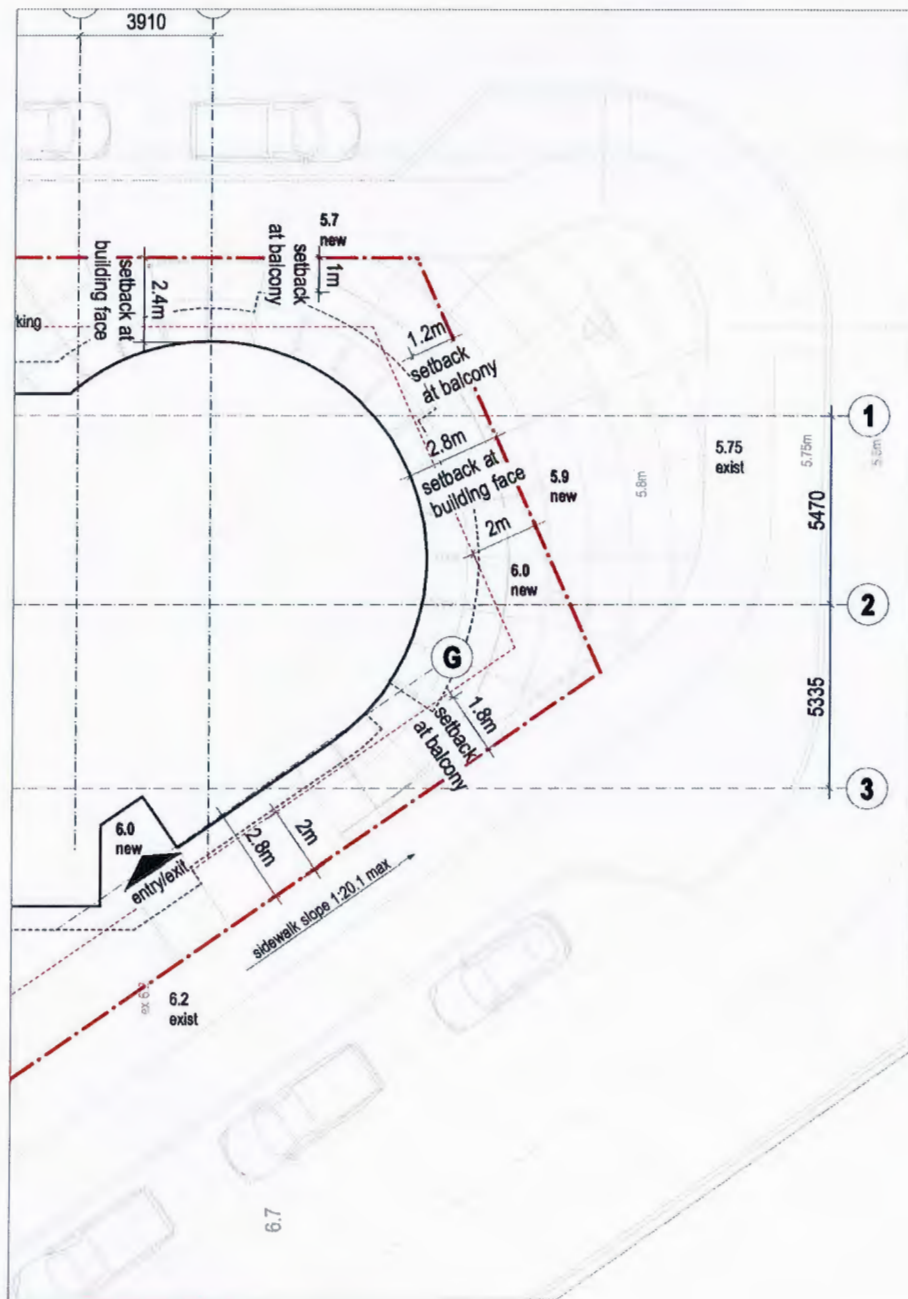


WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

Project: WESTBAY QUAY
Drawing title: Existing Site & Project Data

Scale: 1:1000

Revision No. 10
Drawing No. A1.1



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no.	date	description	approved by
1	31 July 2017	Development Permit	
2	1st March 2017	Public consultation	
3	18 January 2017	Receiving Application	
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5			
6			
7			

WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

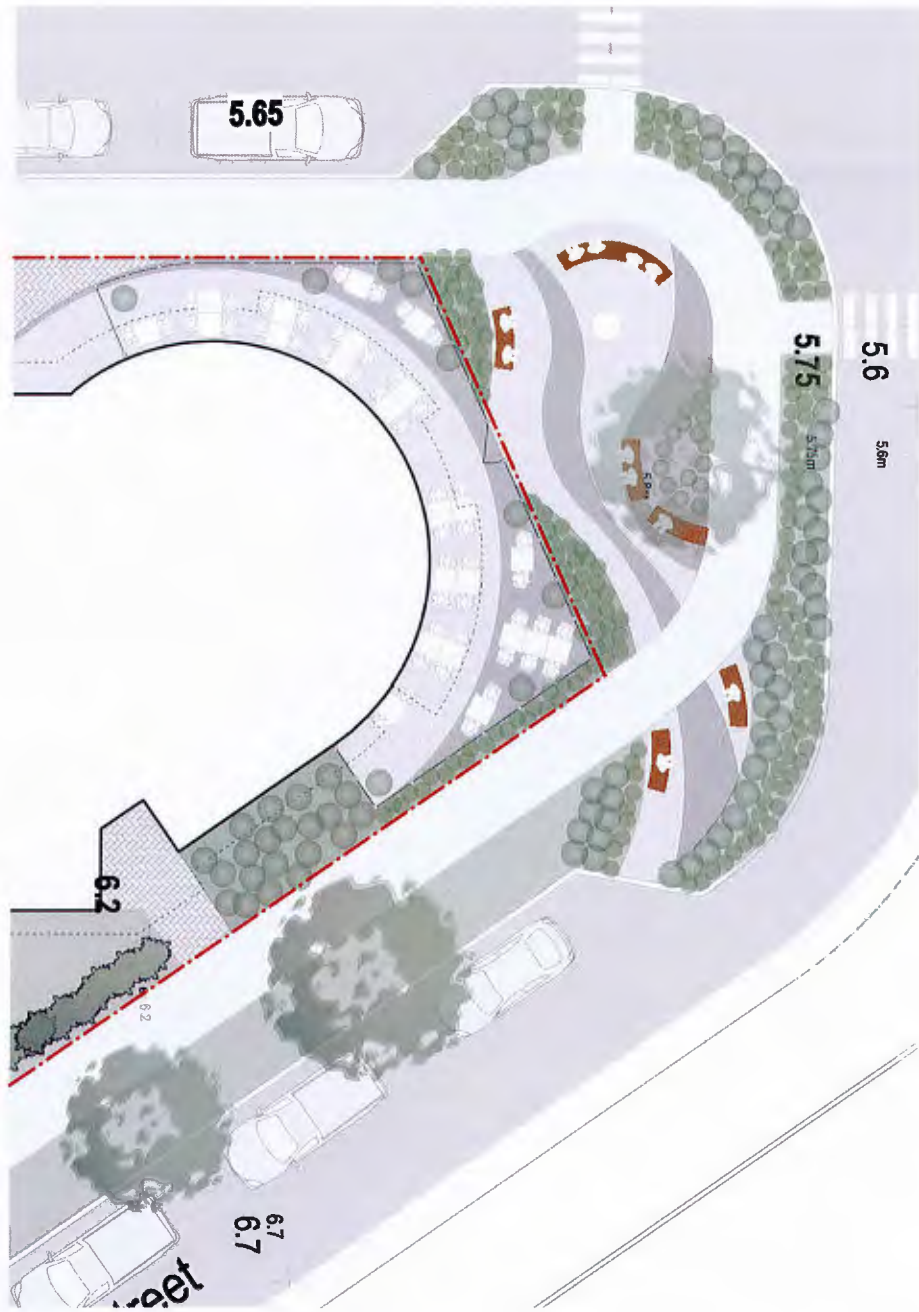
Hillel
architecture

401 85th Oak Bay Avenue
Victoria BC V8K 1C3
phone 250-552-3998
fax 250-552-9778

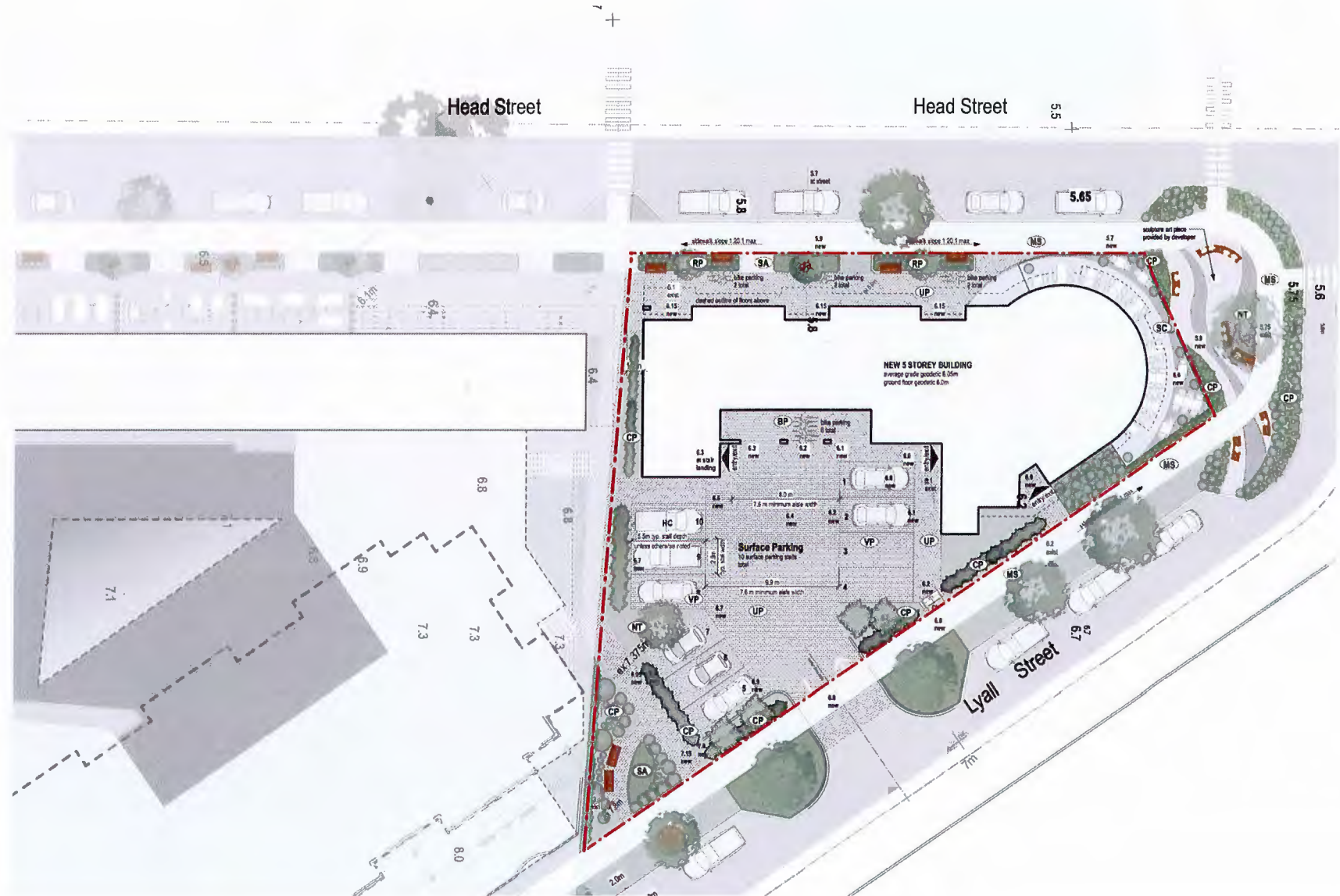
project
WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title
Site Development

revision no. drawing no.
A1.2



2 Detailed Site Plan
A1.3 metric scale: 1 : 100



1 Site Finishes
A1.3 metric scale: 1 : 200

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SITE FINISHES LEGEND

- Use of Finishes typical of all elevations
- CP common planting area - see landscape plan for details
 - SA seeded or sodded grass area - see landscape plan for details
 - NT approximate location of new tree planting area - see landscape plan for details
 - RP raised concrete planter - see landscape plan for details
 - UP unit paver area - pathways and patios
 - APS new asphalt surface
 - MS municipal sidewalk - poured concrete finish
 - DA drive aisle - coloured poured concrete finish to match unit pavers
 - SC glazed screening of outdoor patio for commercial unit 104
 - RW natural stone veneer on concrete retaining wall with a concrete cap
 - CW concrete retaining wall with a concrete cap
 - BP six bike class 2 visitors bike parking / lockable
 - VP asphalt surface vehicle parking stalls
 - PB precast concrete parking blocks, typical all parking stalls
 - FL parking / site lighting

H

date	description	revision
28 August 2017	Issued with Landscaping Plan	
21 July 2017	Development Permit	
14 March 2017	revised re-submission	
16 January 2017	Recording Application	

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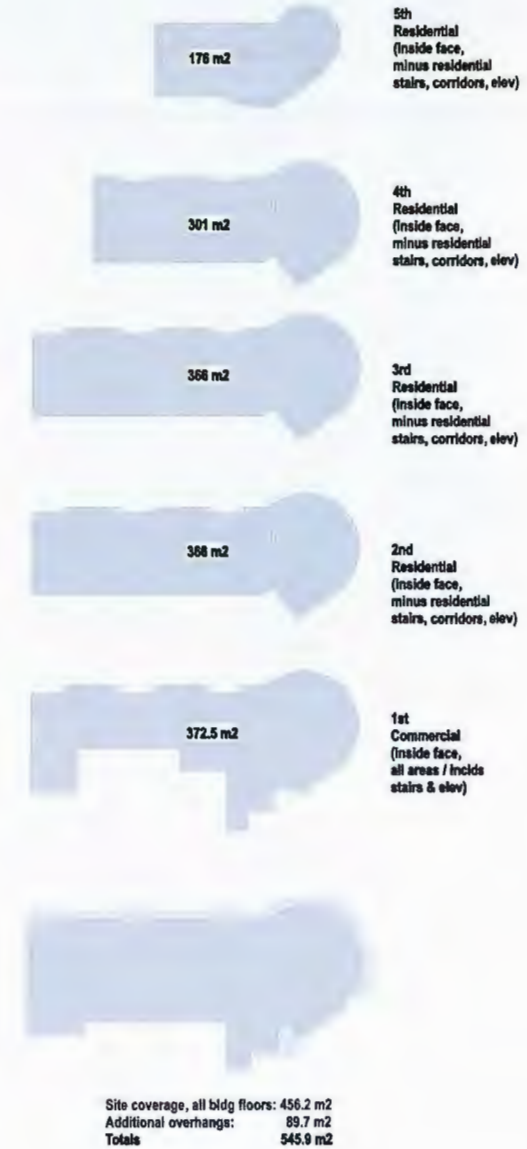
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



Project: **WESTBAY QUAY**
460 & 464 HEAD STREET, ESQUIMALT BC

Drawing title: **Site Finishes**

revision no: **A1.3**



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DATE	21 July 2017	DESCRIPTION	Development Permit
DATE	16 March 2017	DESCRIPTION	initial re-submission
DATE	16 January 2017	DESCRIPTION	Resizing Application
DATE		DESCRIPTION	
DATE		DESCRIPTION	
DATE		DESCRIPTION	

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460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



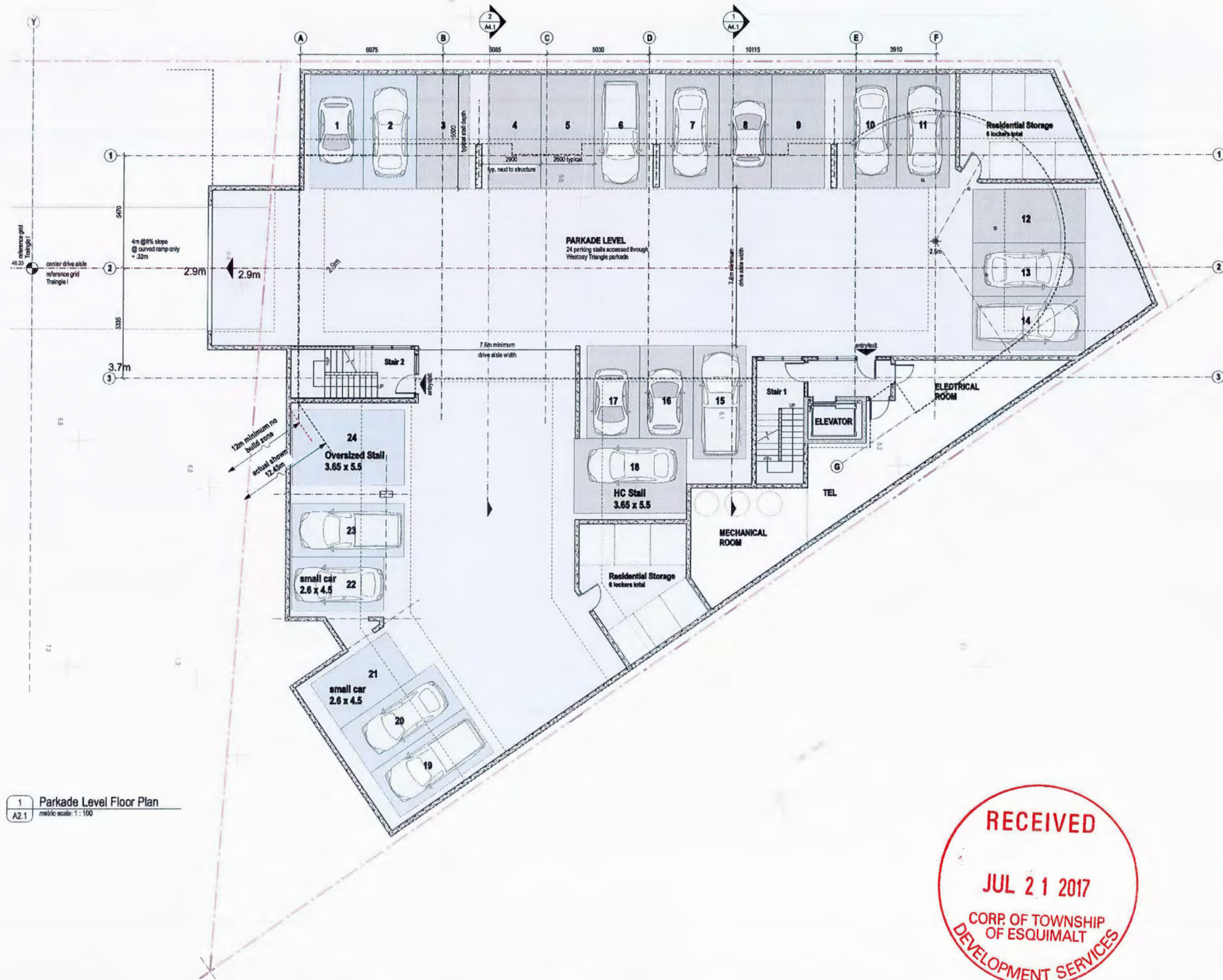
301 18th Cdn. Bay Avenue
Victoria, BC V8K 1C3
phone: 250-522-3708
fax: 250-522-3709



project
WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title
Shadow Studies

drawing no.
A1.4



1 Parkade Level Floor Plan
metric scale: 1:100
A2.1

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no.	date	description
1	21 July 2017	Development Permit
2	14 March 2017	revised by architect
3	10 January 2017	Planning Application
author		checked by
drawn by		date
scale	sheet no.	total no.
	18.28	

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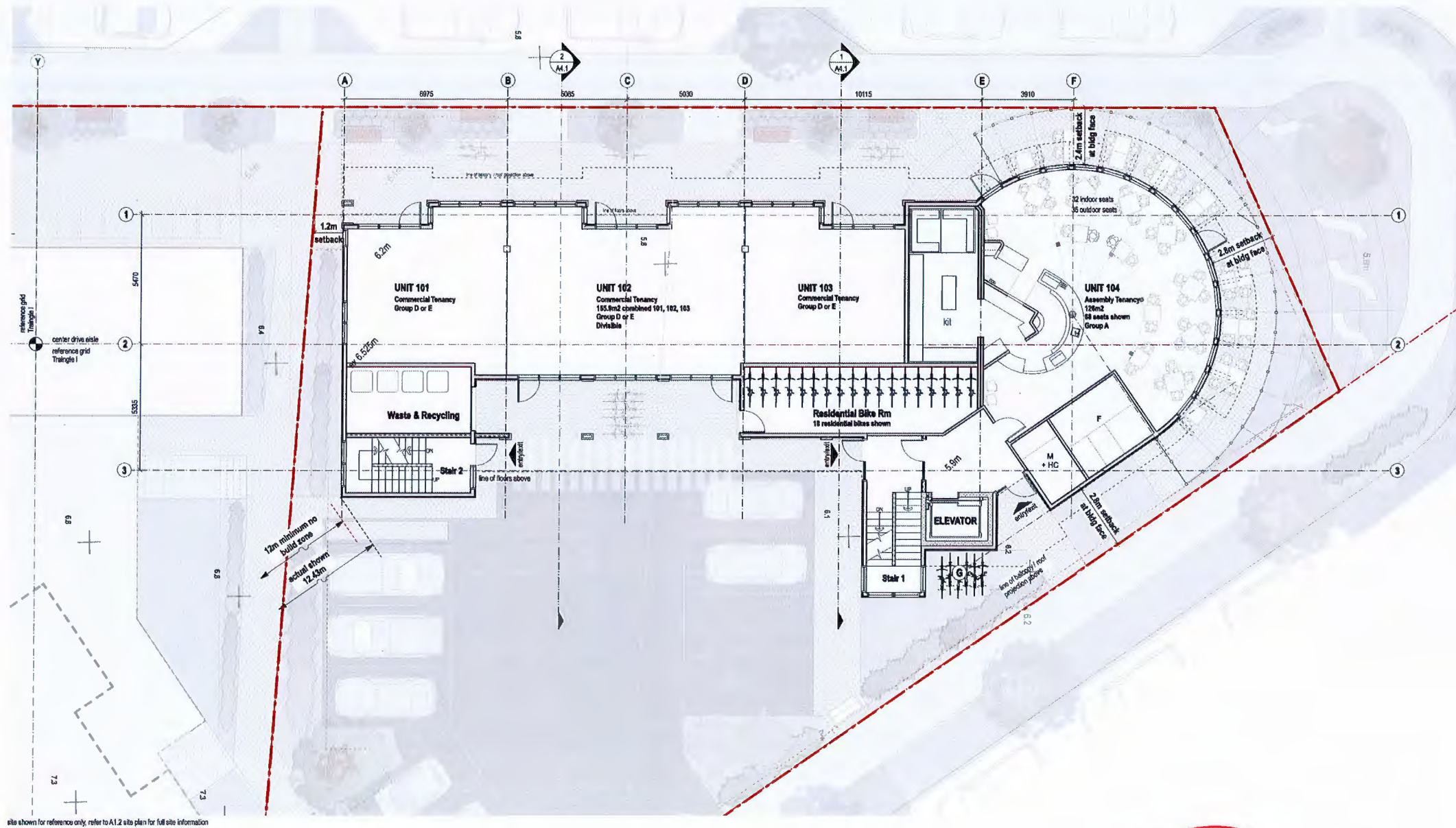
Hillel
architecture
INC.

101 1891 Oak Bay Avenue
Victoria BC V8R 4K3
phone 250.592.7994
fax 250.592.7978

project
WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title
Parkade Level Floor Plan

sheet no. **A2.1**



1 Ground Floor Plan
 A2.2 metric scale: 1:100

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no.	date	description	development permit
1	21 July 2017	development permit	Development Permit
2	14 March 2017	development permit	Access to Underpass
3	16 January 2017	development permit	Resolving Application
4		development permit	
5		development permit	
6	08 October	development permit	10.23

WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION

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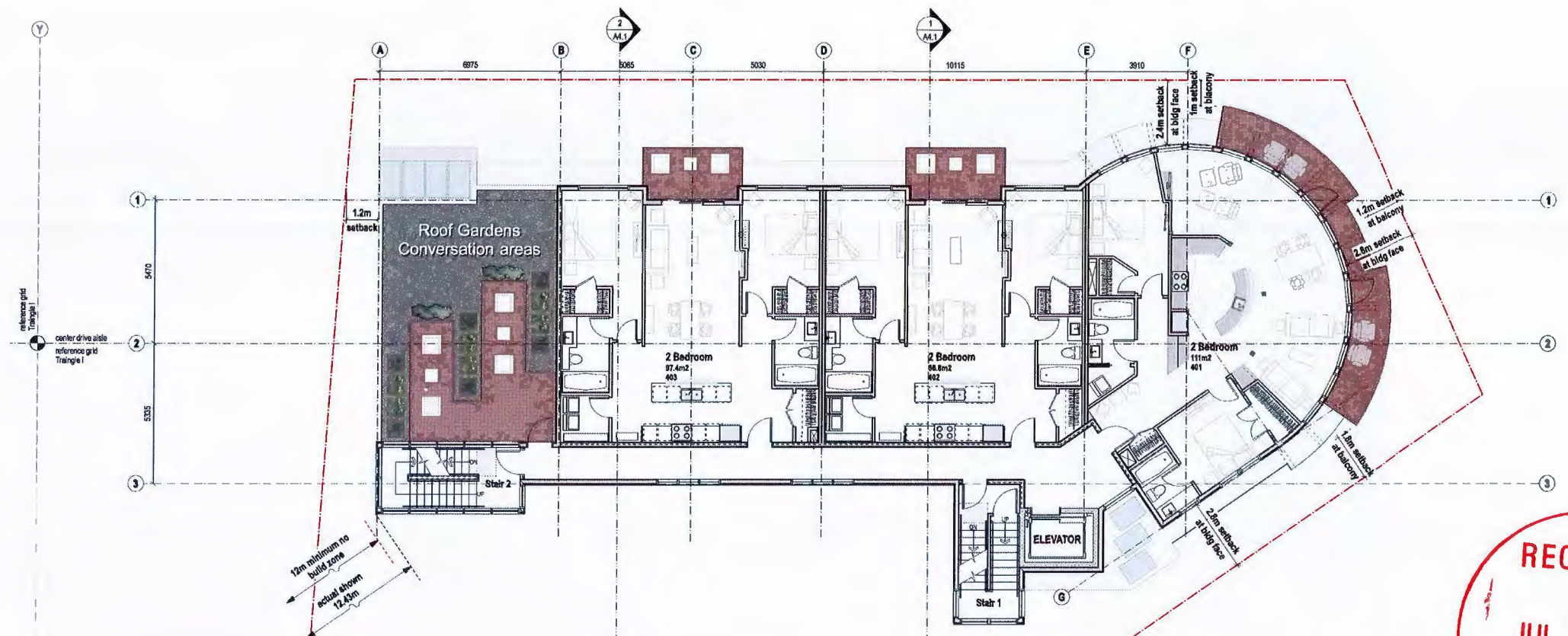
Hillel
 architecture

101 1854 Oak, Dairy Avenue
 Victoria BC V8R 1C5
 phone: 250-592-9988
 fax: 250-592-9978

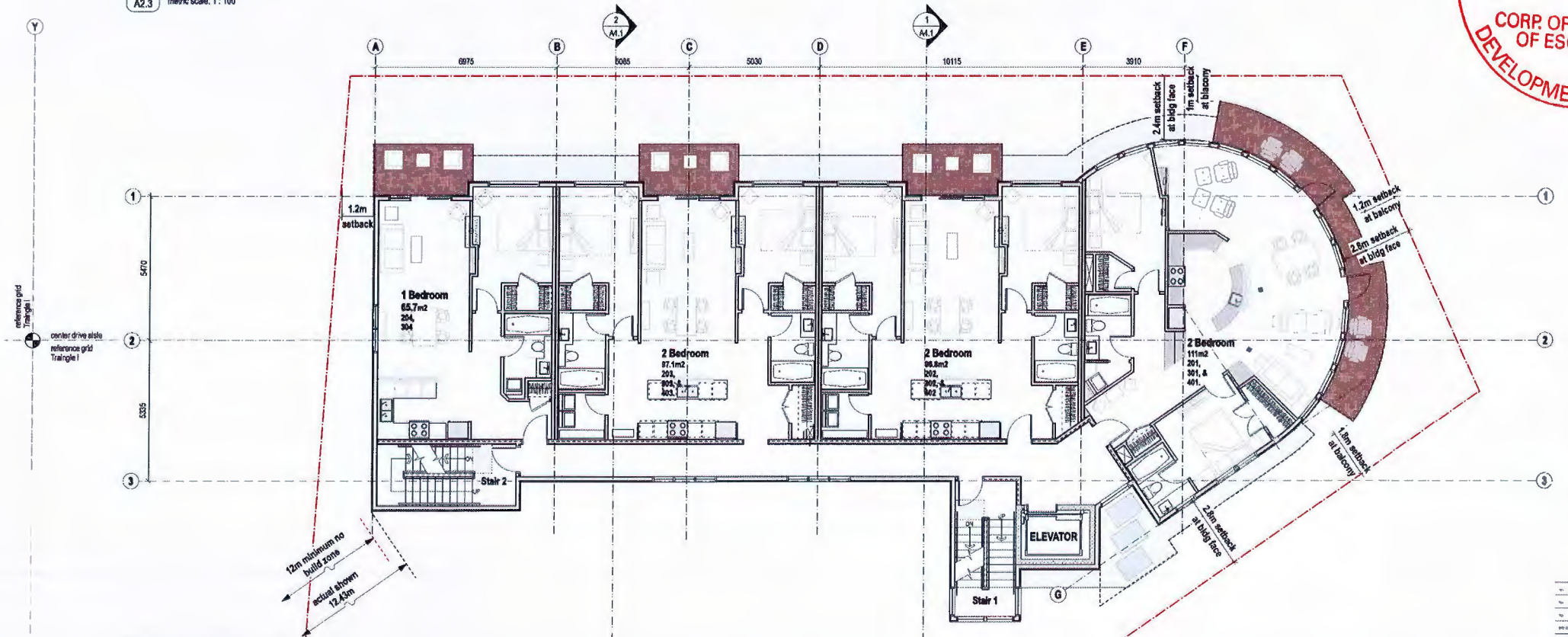
project:
WESTBAY QUAY
 460 & 464 HEAD STREET, ESQUIMALT BC

drawing title:
Ground Floor Level Floor Plan

drawing no:
A2.2



1 4th Floor Plan
A2.3
metric scale: 1:100



2 2nd & 3rd Floors Plan
A2.3
metric scale: 1:100



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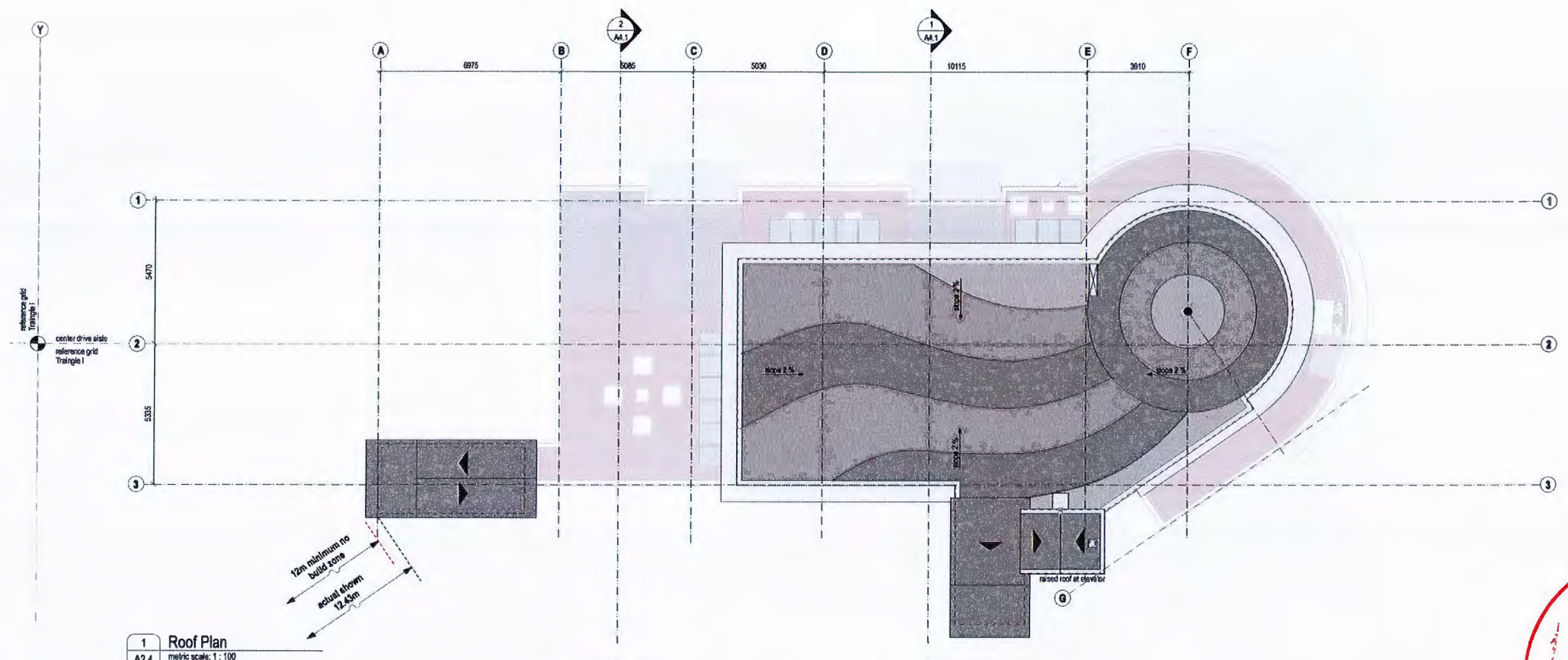
rev	21 July 2017	description	Development Permit
rev	18 March 2017	description	revised re-submission
rev	16 January 2017	description	Planning Application
drawn		by	
check		by	
date	16/10/16	project number	15 33

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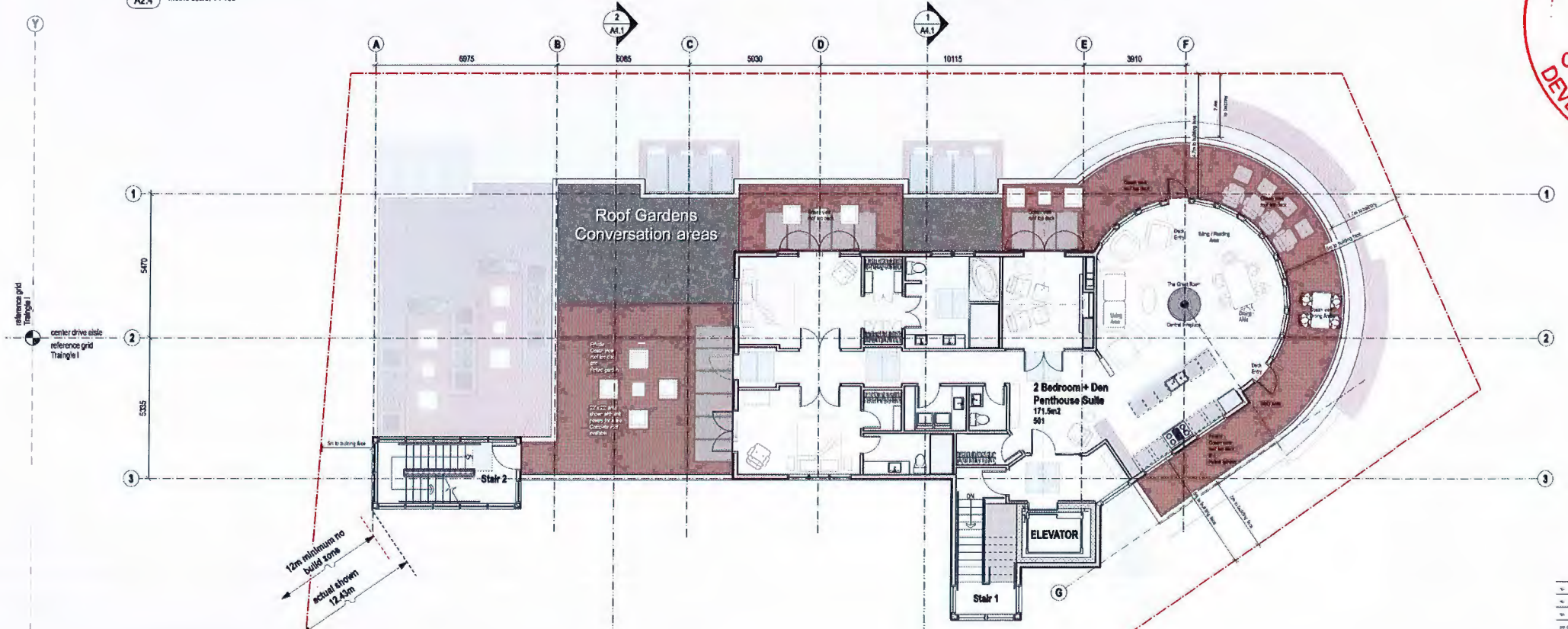
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460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT





1 Roof Plan
A2.4 metric scale: 1:100



2 5th Floor Plan
A2.4 metric scale: 1:100



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NO.	DATE	DESCRIPTION	BY
1	21 July 2017	Development Permit	
2	14 March 2017	revised re-submission	
3	16 January 2017	Hearing Application	
4			
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460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

Hillel
architecture

101 187th Street, Des Moines
Victoria BC V8R 1K3
phone: 250-592-9198
fax: 250-592-9178

PROJECT:
WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

DRAWING TITLE:
5th Floor Plan & Roof Plan

PROJECT NO. _____ DRAWING NO. **A2.4**



1 East Exterior Elevation - Head Street View
A3.1 metric scale: 1:100



2 East Exterior Elevation - Head Street View
A3.1 metric scale: 1:200

Exterior Finishes Legend
List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray parapet flashing flashing
- 02 Cementitious wood fascia board, smooth face - arch spec colour
Item Q2a: bright white
Item Q2b: custom colour shown, see foam green.
- 03 Aluminum plank fascia board.
Colour to match to the Westbay Triangle building finishes.
- 04 Non combustible T&G white cementitious board soffit finish.
- 05 Structural roof support brackets. Prefabricated metal components reflect a design extension of those executed in the Westbay Triangle.

Exterior Finishes & Components

- 10 Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown
Item 10a: oxford white
Item 10b: custom colour shown, see foam green.
- 11 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 12 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray. Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 13 Dark gray elastomeric paint finish to exposed concrete components
- 14 Aluminum entry doors, custom colour see foam green, glazed storefront entry doors in anodized alum storefront framing system.
- 15 Clear anodized aluminum glazed storefront window / door system
- 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
- 17 Graphite gray painted hollow core steel door and frame.
- 18 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 19 Upstand balcony wall: Textured Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown. Colour matched flashing cap to assembly.

Exterior Features

- 20 Continuous cementitious accent trim hand, painted
Item 20a: oxford white
Item 20b: custom colour shown, see foam green
- 21 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 22 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray. Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 23 Exterior light fixture
- 24 Exterior building signage, building wide uniform approach to signage to be submitted independent to DP under typical sign permit processes, issued by town.
- 25 Anodized aluminum framed stilet unit

no.	date	description	development phase
1	21 July 2017	approved	Development Permit
2	14 March 2017	approved	re-submission
3	16 January 2017	approved	Planning Application
4			
5			
6	05/10/2012	approved	18.83

WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

101 1891 Oak, Dairy Avenue
Victoria BC V8R 1K3
phone 250.592.9998
fax 250.592.9978

project:
WESTBAY QUAY
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title:
Exterior Elevation - Head Street View

drawing no. **A3.1**



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1 West Exterior Elevation - Lyall Street View
 A3.2 metric scale: 1:100



2 West Exterior Elevation - Lyall Street View
 A3.2 metric scale: 1:200

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray parapet flashing flashing
- 02 Cementitious wood fascia board, smooth face - arch spec colour Item 02a: bright white Item 02b: custom colour shown, sea foam green
- 03 Aluminum plank fascia board. Colour to matched to the Westbay Triangle building finishes.
- 04 Non combustible T&G white cementitious board soffit finish
- 05 Structural roof support brackets. Prefabricated metal components reflect a design extension of those executed in the Westbay Triangle.

Exterior Finishes & Components

- 10 Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown Item 10a: colorid white Item 10b: custom colour shown, sea foam green
- 11 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 12 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 13 Dark gray elastomeric paint finish to exposed concrete components
- 14 Aluminum entry doors, custom colour sea foam green, glazed storefront entry doors in anodized alum storefront framing system.
- 15 Clear anodized aluminum glazed storefront window / door system
- 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
- 17 Graphite gray painted hollow core steel door and frame.
- 18 Side mounted raftering system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 19 Upstand balcony wall: Textured Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown. Colour matched flashing cap to assembly.

Exterior Features

- 20 Continuous cementitious accent trim band, painted Item 20a: oxford white Item 20b: custom colour shown, sea foam green
- 21 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 22 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 23 Exterior light fixture
- 24 Exterior building signage, building wide uniform approach to signage to be submitted independent to DP under typical sign permit processes, tenant by tenant.
- 25 Anodized aluminum framed skylight unit

H

No.	Date	By	For	Development Permit
1	21 July 2017	HL	HL	Development Permit
2	14 March 2017	HL	HL	revised re-submission
3	10 January 2017	HL	HL	Revised Application
4				
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WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

101 1851 Oval, Esquimalt
Victoria, BC V8K 1K3
phone 250.592.9988
fax 250.592.9978

project: **WESTBAY QUAY**
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title: **Exterior Elevation - Lyall Street View**

drawing no: **A3.2**



1 South Exterior Elevation - Facing Marina View Point
A3.3 metric scale: 1:100

2 North Exterior Elevation - Facing Triangle 1
A3.3 metric scale: 1:100



3 South Exterior Elevation - Facing Marina View Point
A3.3 metric scale: 1:200



4 North Exterior Elevation - Facing Triangle 1
A3.3 metric scale: 1:200

Exterior Finishes Legend
List of Finishes typical of all elevations

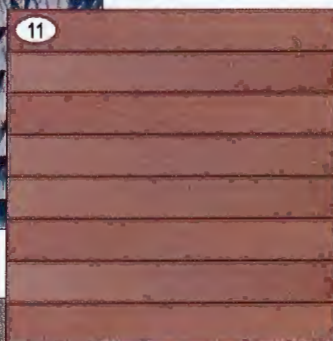
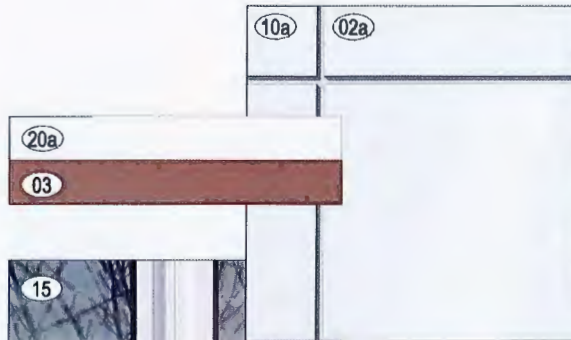
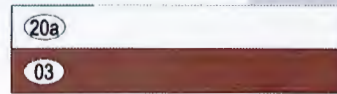
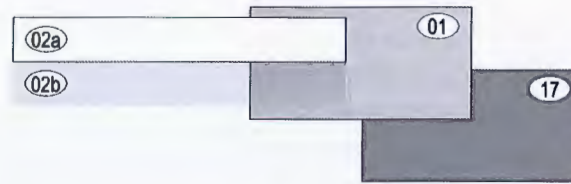
- Roofing**
- 01 Pre-finished metallic gray parapet flashing
 - 02 Cementitious wood laple board, smooth face - arch spec colour
 - 03 Aluminum plank basic board
 - 04 Non combustible T&G white cementitious board soffit finish
 - 05 Structural roof support brackets. Prefabricated metal component to reflect a design extension of those executed in the Westbay Triangle.
- Exterior Finishes & Components**
- 10 Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown
 - 11 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
 - 12 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
 - 13 Dark gray elastomeric paint finish to exposed concrete components
 - 14 Aluminum entry doors, custom colour sea foam green, glazed storefront entry doors in anodized alum storefront framing system.
 - 15 Clear anodized aluminum glazed storefront window / door system
 - 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
 - 17 Graphite gray painted hollow core steel door and frames
 - 18 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
 - 19 Upward balcony wall: Tucked Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown. Colour matched flashing cap to assembly.
- Exterior Features**
- 20 Continuous cementitious accent trim band, painted
 - 21 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
 - 22 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
 - 23 Exterior light fixture
 - 24 Exterior building signage, building wide uniform approach to signage to be submitted independent to DP under typical sign permit processes, tenant by tenant.
 - 25 Anodized aluminum framed skylight unit

No.	Date	By	For	Comments
1	21 July 2017	Author	Development Permit	
2	14 March 2017	Author	Initial Submission	
3	18 January 2017	Author	Reasoning Application	
		Design		
		Check		
	18 July 2017	Author		18.33

WESTBAY QUAY
DEVELOPMENT PERMIT APPLICATION
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

Hillel architecture
431 19th C.A. Bay Avenue
Victoria, B.C. V8R 1K5
phone 250-592-7994
fax 250-592-7978

Project		WESTBAY QUAY 460 & 464 HEAD STREET, ESQUIMALT BC	
Drawing No.		Exterior Elevations - North & South	
Author	Checked	Scale	Sheet No.
			A3.3



Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray perspet flashing
- 02a Cementitious wood fascia board, smooth face - arch spec colour
Item 02a: bright white
Item 02b: custom colour shown, sea foam green.
- 03 Aluminum plank fascia board.
Colour to match to the Westbay Triangle building finishes.
- 04 Non combustible T&G white cementitious board soffit finish.
- 05 Structural roof support brackets. Prefabricated metal components reflect a design extension of those executed in the Westbay Triangle.

Exterior Finishes & Components

- 06 Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown
Item 10a: color white
Item 10b: custom colour shown, sea foam green.
- 07 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 08 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 09 Dark gray elastomeric paint finish to exposed concrete components
- 10 Aluminum entry doors, custom colour sea foam green, glazed storefront entry doors in anodized alum storefront framing system.
- 11 Clear anodized aluminum glazed storefront window / door system
- 12 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
- 13 Graphite gray painted hollow core steel door and frame.
- 14 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 15 Upstand balcony walk: Textured Metal based Rainscreen Aluminum Composite Wall System complete with integrated metal reveals where shown. Colour matched flashing cap to assembly.

Exterior Features

- 16 Continuous cementitious accent trim band, painted Item 20a: color white
Item 20b: custom colour shown, sea foam green
- 17 Aluminum plank 1x4 T & G siding coloured matched to cedar siding finishes executed in the Westbay Triangle.
- 18 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray Stone veneer product colour matched & joint treatment matched to the Westbay Triangle.
- 19 Exterior light fixture
- 20 Exterior building signage, building wide uniform approach to signage to be submitted independent to DP under typical sign permit processes, tenant by tenant.
- 21 Anodized aluminum framed skylight unit

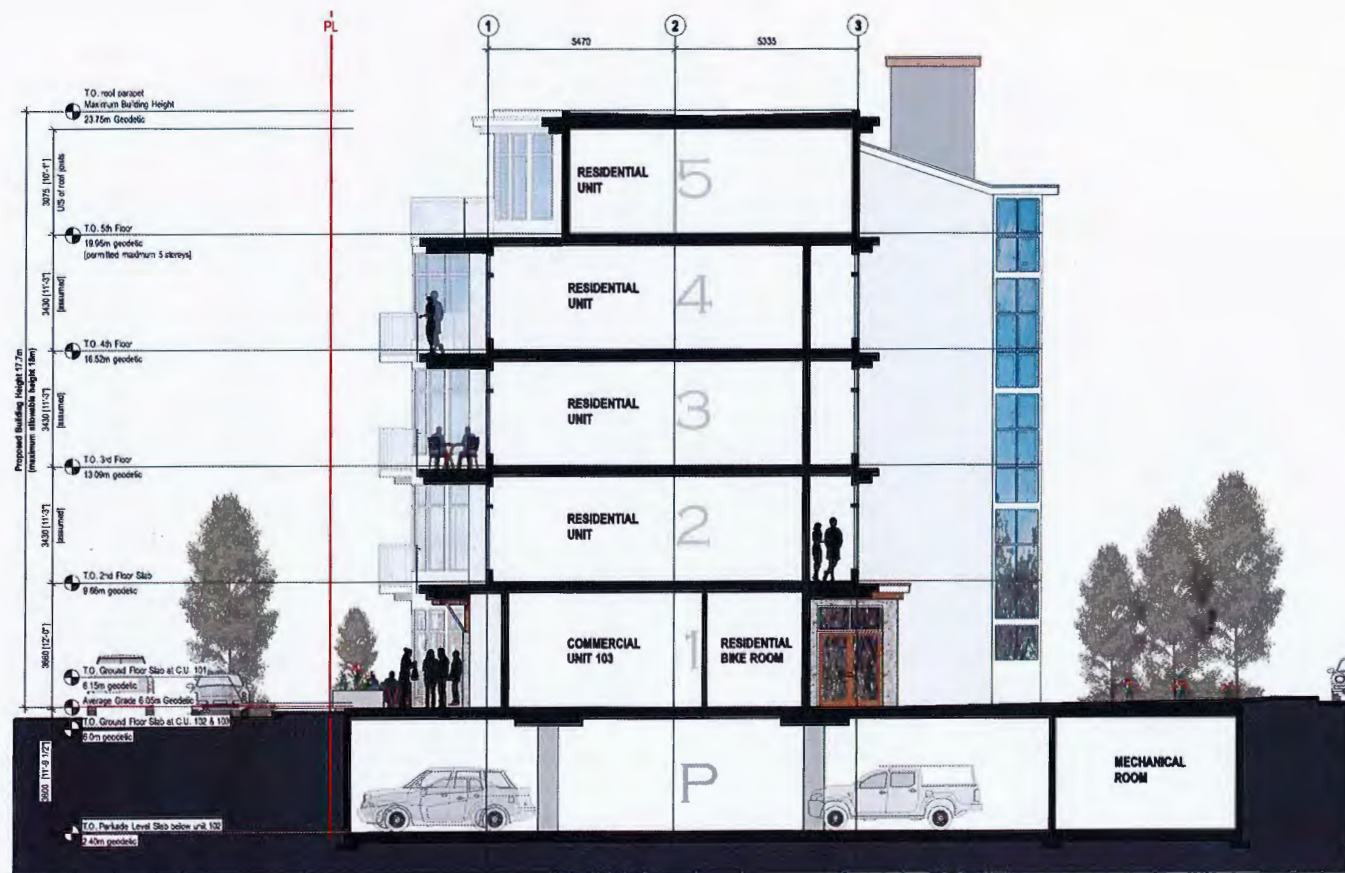


Rev	Date	By	Check	Description
1	21 July 2017	mm	mm	Development Permit
2	14 March 2017	mm	mm	interior re-submission
3	10 January 2017	mm	mm	Reopening Application
4		mm	mm	
5		mm	mm	

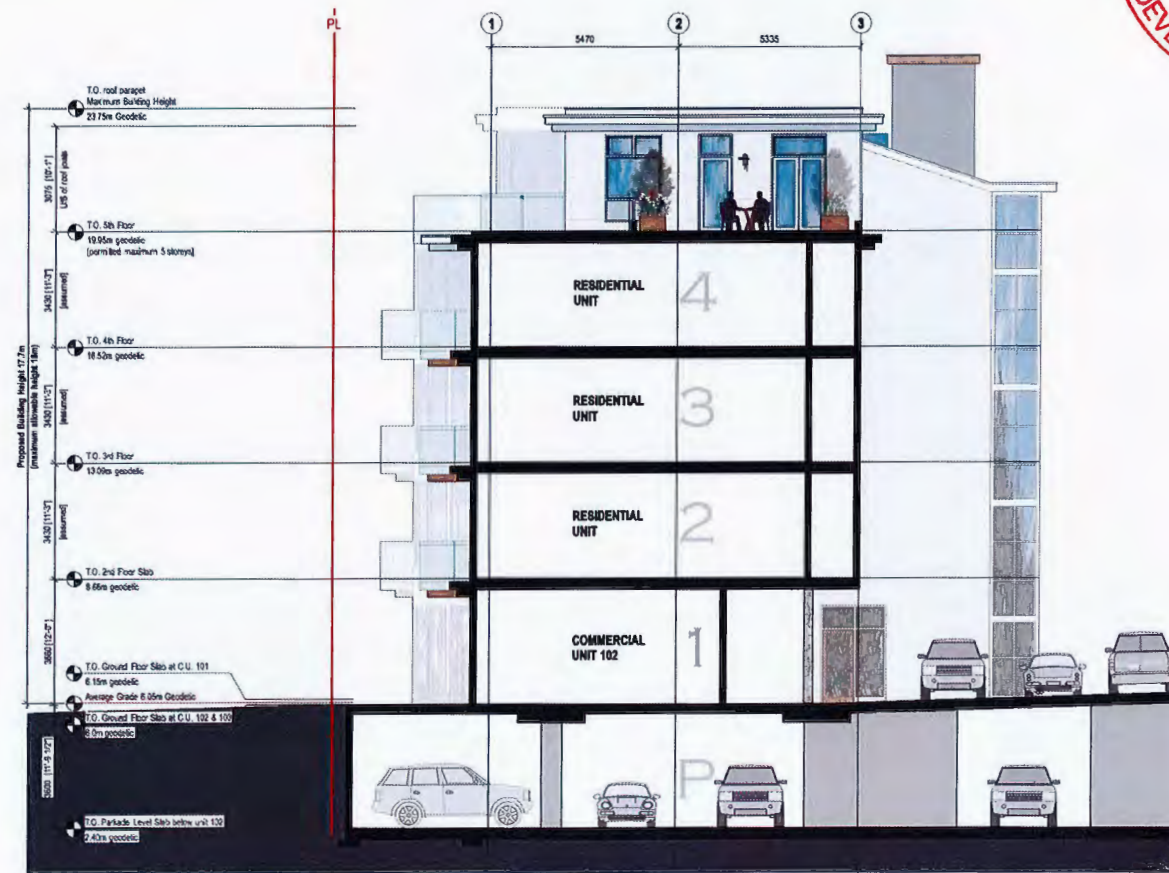
WESTBAY QUAY
DEVELOPMENT PERMIT APPLICATION
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



<p>project: WESTBAY QUAY 460 & 464 HEAD STREET, ESQUIMALT BC</p> <p>drawing title: Exterior Materials & Colour Board</p>	
<p>101 10th C&A, Bay Avenue Victoria BC V8R-1K3 phone 250-592-7998 fax 250-592-5978</p>	<p>revision no. A3.4</p>



1 Building Section at CRU 102
metric scale: 1:100



2 Building Section at Porte Cochère
metric scale: 1:100

Handwritten signature

NO.	REV.	DATE	DESCRIPTION	BY	CHKD.
1		21 July 2017	Development Permit		
2		14 March 2017	revised re-submission		
3		18 January 2017	Planning Application		

WESTBAY QUAY

DEVELOPMENT PERMIT APPLICATION
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



project		WESTBAY QUAY 460 & 464 HEAD STREET, ESQUIMALT BC	
drawing title		Building Sections	
drawing no.	revision no.	drawing no.	revision no.
		A4.1	



Ground Level Landscape Plan

1:200

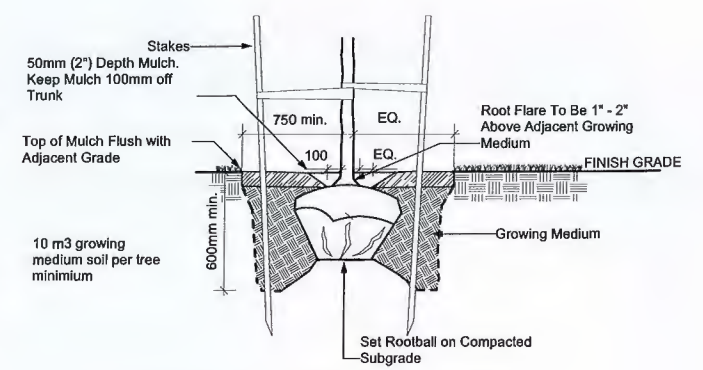


Ground Level Character Images

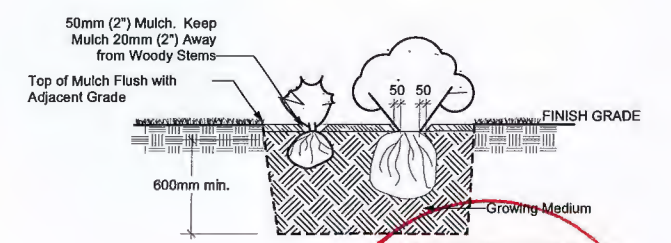
Recommended Nursery Stock

Trees	Botanical Name	Common Name	Size	
Total: 7	Acer rubrum 'October Glory'	October Glory Red Maple	6 cm cal.	
	Fagus sylvatica 'Riversii'	River's Purple European Beech	6cm cal.	
	Ginkgo biloba	Ginkgo	6cm cal.	
	Parrotia persica	Persian Ironwood	#8 cm cal., multi-stem	
Medium Shrubs	Botanical Name	Common Name	Size	
Total: 78	Cistus x hybridus	White Rockrose	#5 pot	
	Gaultheria shallon	Salal	#5 pot	
	Leycesteria formosa 'Golden Lanterns'	Golden honeysuckle	#5 pot	
	Nandina domestica 'Gulf Stream'	Gulf Stream Heavenly Bamboo	#5 pot	
	Taxus baccata	English Yew	#5 pot, spiral topiary	
Small Shrubs	Botanical Name	Common Name	Size	
Total: 179	Erica x darleyensis 'Kramer's Rota'	Kramer's Rota Heather	#1 pot	
	Lavandula stoechas 'Otto Quast'	Spanish Lavender	#5 pot	
Groundcovers	Botanical Name	Common Name	Size	
Total: 136	Fragaria chiloensis	Coastal Strawberry	#1 pot	
	Lonicera pileata	Box-leaf honeysuckle	#5 pot	
	Rubus calycinoides	Creeping Bramble	#5 pot	
Perennials, Annuals and Ferns	Botanical Name	Common Name	Size	
Total: 934	Calamagrostis x acutiflora 'Karl Foerster'	Foerster's Feather Reed Grass	#1 pot	
	Carex oshimensis 'Evergold'	Evergold Sedge	#1 pot	
	Festuca ovina glauca 'Elijah Blue'	Elijah Blue Fescue	#1 pot	
	Geranium macrorrhizum	Cranesbill	#1 pot	
	Helictotrichon sempervirens	Blue Oat Grass	#1 pot	
	Hemerocallis x 'Stella de Oro'	Stella De Oro Dwarf Daylily	#1 pot	
	Miscanthus sinensis 'Gracillimus'	Maiden Grass	#5 pot	
	Nassella tenuissima (Stipa tenuissima)	Mexican Feather Grass	#1 pot	
	Ophiopogon japonicus 'Nanus'	Dwarf Mondo Grass	#1 pot	
	Green Roof	Botanical Name	Common Name	Size
	Total: 147	Blechnum spicant	Dear fern	#1 pot
		Buxus microphylla var. koreana 'Esaes' P.P.	Wedding Ring® Boxwood	#5 pot
		Carex oshimensis 'Evergold'	Evergold Sedge	#1 pot
Dryopteris erythrosora		Autumn Fern	#1 pot	
Ophiopogon japonicus 'Nanus'		Dwarf Mondo Grass	#1 pot	
Oxalis oregana		Wood Sorrel	#1 pot	
Polystichum munifolium		Sword Fern	#5 pot	
Prunus lusitanica		Portugal Laurel	#15 pot, tree form	
Rhododendron 'Gumpo White'		White Gumpo Azalea	#5 pot	
Taxus baccata		English Yew	#5 pot, spiral topiary	

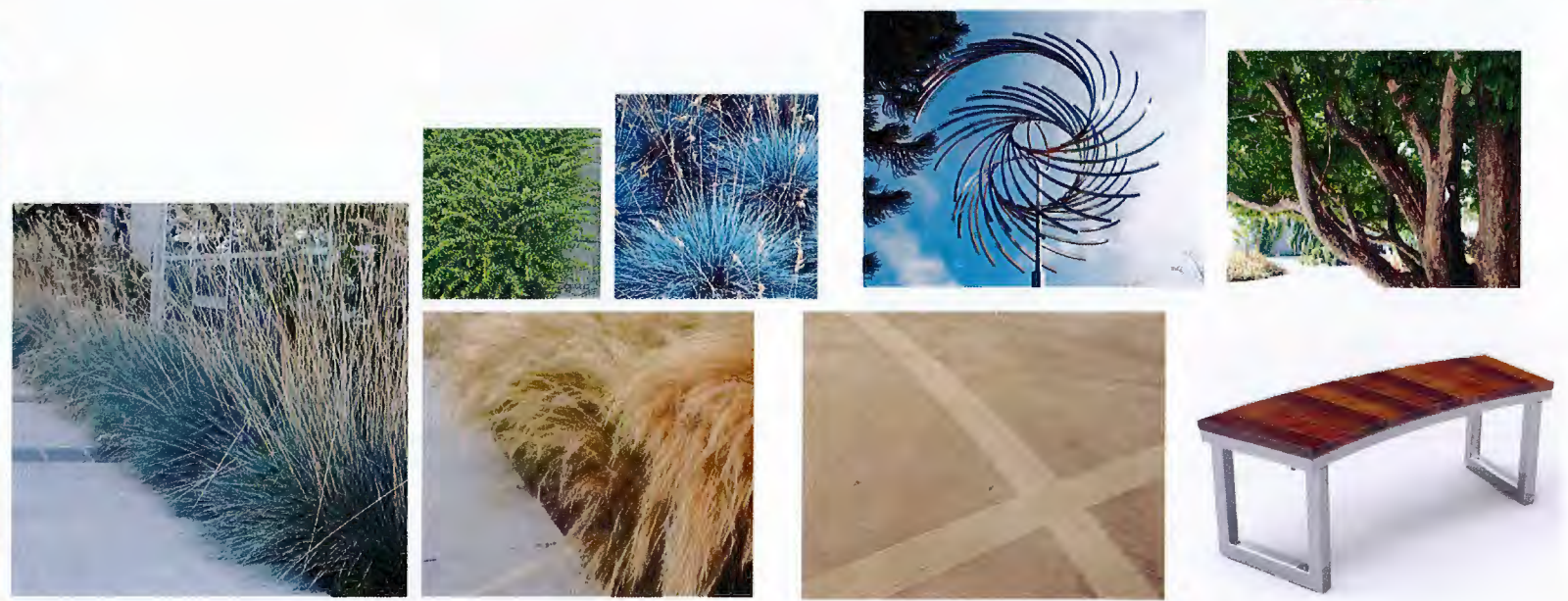
- Notes:
1. All work to be completed to current BC SLA Landscape Standards
 2. All soft landscape to be irrigated with an automatic irrigation system



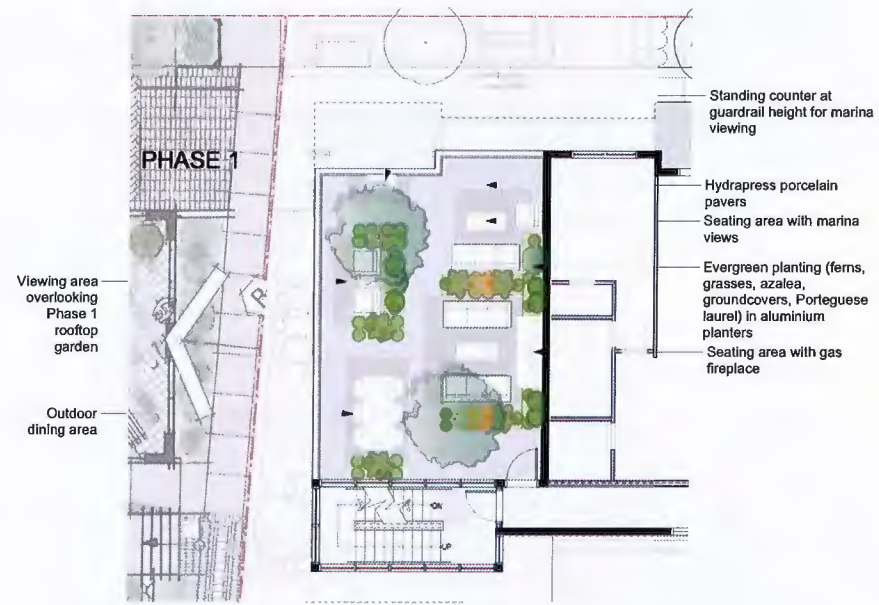
Tree Planting at Grade - Typical Detail 1:25



Shrub Planting at Grade - Typical Detail 1:25

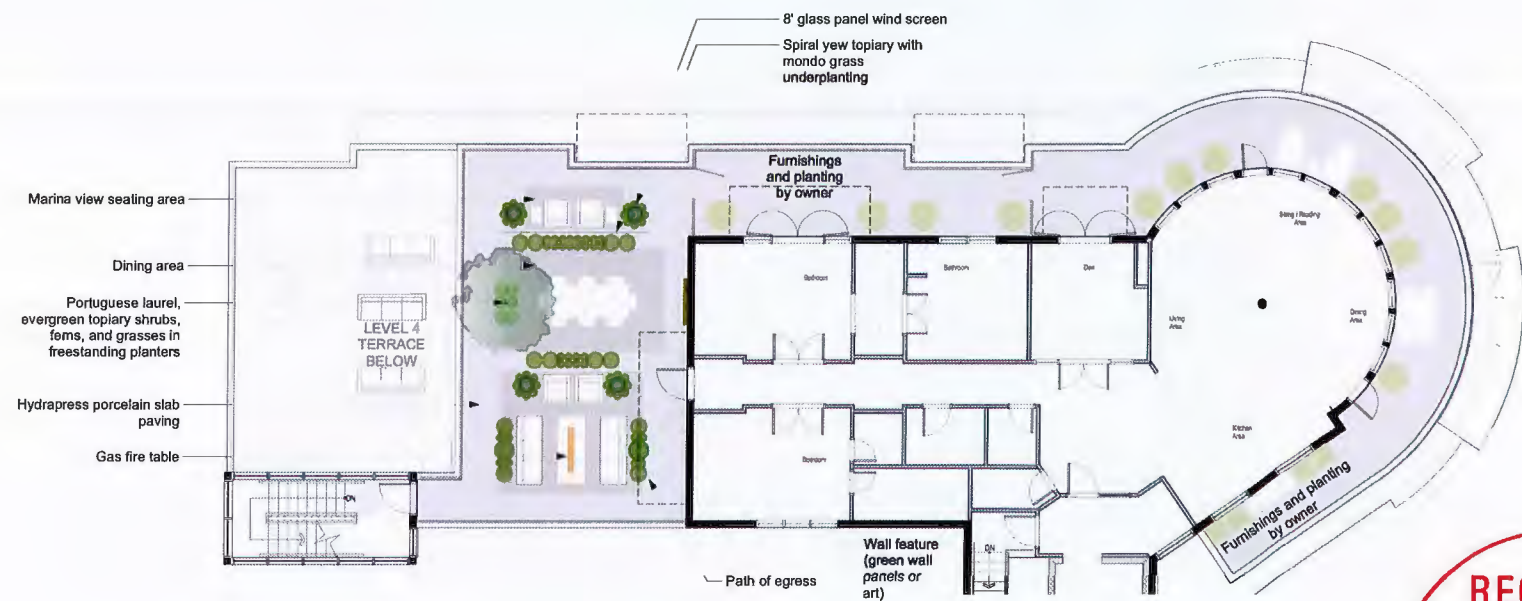


Westbay Quay | Landscape Concept



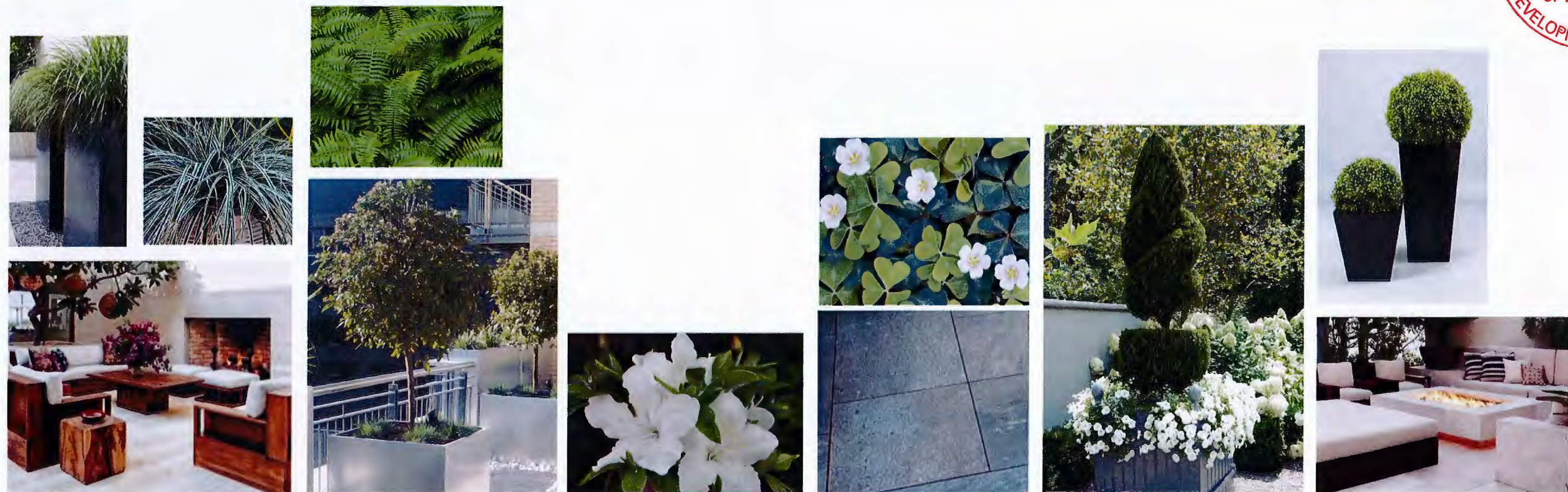
4th Floor Amenity Terrace

1:100



5th Floor Penthouse Terrace

1:100

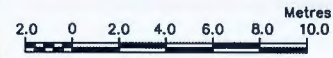


Roof Terrace Character Images

Westbay Quay | Landscape Concept

Plan Showing Proposed Building Location Situated on Proposed Development of Lots 9 and 10, Block H, Section 11, Esquimalt District, Plan 292;

Township of Esquimalt



The intended plot size of this plan is 559 mm in width by 432 mm in height (C size) when plotted at a scale of 1:200

Legend:

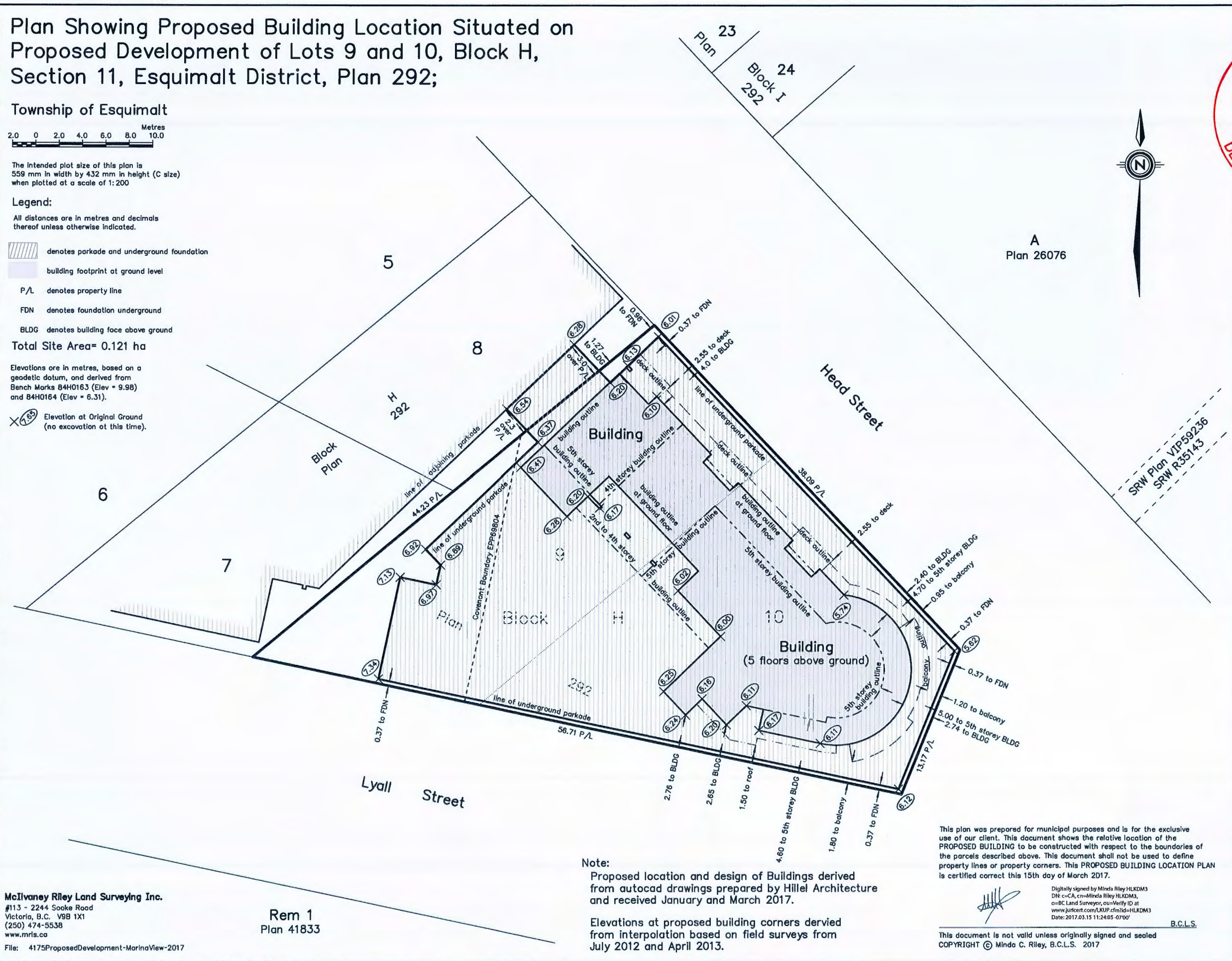
All distances are in metres and decimals thereof unless otherwise indicated.

- denotes parkade and underground foundation
- building footprint at ground level
- P/L denotes property line
- FDN denotes foundation underground
- BLDG denotes building face above ground

Total Site Area= 0.121 ha

Elevations are in metres, based on a geodetic datum, and derived from Bench Marks 84H0163 (Elev = 9.98) and 84H0164 (Elev = 6.31).

Elevation at Original Ground (no excavation at this time).



A
Plan 26076

SRW Plan VIP59236
SRW R35143

McIlvaney Riley Land Surveying Inc.
#113 - 2244 Sooke Road
Victoria, B.C. V9B 1X1
(250) 474-5538
www.mrls.ca
File: 4175ProposedDevelopment-MarinaView-2017

Rem 1
Plan 41833

Note:
Proposed location and design of Buildings derived from autocad drawings prepared by Hillel Architecture and received January and March 2017.

Elevations at proposed building corners derived from interpolation based on field surveys from July 2012 and April 2013.

This plan was prepared for municipal purposes and is for the exclusive use of our client. This document shows the relative location of the PROPOSED BUILDING to be constructed with respect to the boundaries of the parcels described above. This document shall not be used to define property lines or property corners. This PROPOSED BUILDING LOCATION PLAN is certified correct this 15th day of March 2017.

Digitally signed by Minda Riley HLKDM3
DN: c=CA, cn=Minda Riley HLKDM3,
o=BC Land Surveyor, ou=Verify ID at
www.juricent.com/LKUP.cfm?id=HLKDM3
Date: 2017.03.15 11:24:05 -0700

B.C.L.S.

This document is not valid unless originally signed and sealed
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