



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**DESIGN REVIEW COMMITTEE
AGENDA**

**WEDNESDAY, MARCH 8, 2017
3:00 P.M.
ESQUIMALT COUNCIL CHAMBERS**

MEMBERS: Roger Wheelock (Chair) Wendy Kay
Ally Dewji Richard Iredale
Paul De Greeff Jill Singleton
Robert Schindelka

RESOURCE MEMBER: Cst. Franco Bruschetta [Non-Voting]

COUNCIL LIAISON: Councillor Beth Burton-Krahn
Councillor Olga Liberchuk

STAFF LIAISON: Bill Brown, Director of Development Services

SECRETARY: Pearl Barnard

- I. CALL TO ORDER**
- II. LATE ITEMS**
- III. ADOPTION OF AGENDA**
- IV. ADOPTION OF MINUTES – FEBRUARY 9, 2017 and FEBRUARY 16, 2017**
- V. STAFF REPORT**

DEVELOPMENT PERMIT APPLICATION

“West Bay Triangle”

**468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]
470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]
472 Head Street [Lot 4, Block H, Section 11, Esquimalt District, Plan 292]
515 Gore Street [Lot 1, Block H, Section 11, Esquimalt District, Plan 292]
509 Gore Street [Lot 2, Block H, Section 11, Esquimalt District, Plan 292]
922 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]
912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]**

PURPOSE OF APPLICATION:

The applicant is proposing to construct a the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units on a consolidation of nine subject properties to be regulated by Comprehensive Development District No. 98 [CD-98].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application is generally consistent with the design guidelines contained in Section 9.4 of the Township’s Official Community Plan

[attached]. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines [attached] as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit.

Evaluation of this application should focus on issues respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of buildings and other structures in relation to the relevant design guidelines.

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received March 2, 2017”, and the landscape plan prepared by Small and Rossell Landscape Architects, stamped “Received March 2, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received February 8, 2017” for the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, proposed to be located on the “West Bay Triangle” lands, be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

VI. STAFF LIAISON STATUS REPORT

VII. NEW BUSINESS

VIII. NEXT REGULAR MEETING

April 12, 2017

IX. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT
ADVISORY DESIGN REVIEW COMMITTEE MEETING MINUTES
HELD
FEBRUARY 9, 2017
ESQUIMALT COUNCIL CHAMBERS

MEMBERS PRESENT: Roger Wheelock (CHAIR) Richard Iredale
Wendy Kay Paul De Greeff
Jill Singleton Ally Dewji
Robert Schindelka Cst. Franco Bruschetta

STAFF LIAISON: Bill Brown, Director, Development Services

STAFF: Trevor Parkes, Senior Planner

COUNCIL LIAISON: Councillor Beth Burton-Krahn
Councillor Olga Liberchuk

TOWNSHIP'S SOLICITOR: Lui Carvello, Carvello Law Corporation

SECRETARY: Pearl Barnard

I. CALL TO ORDER

The Chair called the meeting to order at 2:00 p.m.

II. LATE ITEMS

No late items

III. ADOPTION OF AGENDA

Moved by Jill Singleton, seconded by Ally Dewji: That the agenda be adopted as distributed. **Carried Unanimously**

IV. ADOPTION OF MINUTES – January 11, 2017 Meeting

Bill Brown advised that on page 3 the statement “Mr. Brown clarified that LEED Gold 2009 is referred to in the Zoning Bylaw” is incorrect. To be removed.

Moved Wendy Kay, seconded by Jill Singleton: That the minutes of January 11, 2017 be adopted as amended. **Carried Unanimously.**

IV. STAFF REPORT

Bill Brown advised members that on page 5 of the Staff Report, under the heading “Alternatives” items 1 – 4 should read application for Development Permit not Rezoning.

(1) DEVELOPMENT PERMIT APPLICATION

**“REVIEW OF DESIGN REVISIONS FOR THE CORE AREA
WASTEWATER TREATMENT PLANT AT MCLOUGHLIN POINT”
337 Victoria View Road
Lot 1; Section 11; Plan EPP36468**

PURPOSE OF APPLICATION:

To ensure that the applicant's intentions are consistent with the Zoning Bylaw and the design guidelines for Development Permit Area No. 3 "Industrial".

Mr. Brown outlined that the applicant has provided revised drawings that respond to the comments and concerns expressed by the Committee members at the meeting held on January 11, 2017. Staff are asking DRC members to provide comments especially on the south elevation, as this is the first and last impression that most people will have of Esquimalt.

In attendance:

Harbour Resource Partnership

Jim Mann
Ernie Maschner
Jim Alders
Bev Windjack

Capital Regional District/Project Board

Ken Madill

Ernie Maschner presented the application. Mr. Maschner advised that they are back with revised drawings in response to the feedback and comments received from the Committee members at the January 11, 2017 meeting. He also outlined that as part of the Development Permit process they held three Open Houses, two were for Esquimalt and the other one was specifically for the DND residents.

Jim Mann thanked the Committee members for their input and comments from the last meeting. Mr. Mann then gave a PowerPoint presentation and outlined the proposed changes to the design. The building has been shifted away from the eastern shoreline and is now closer to Victoria View Road, the parking has been moved offsite and the roof top generators have been relocated at grade. The south elevation has been improved and the observation deck and retaining walls have been revised. The COR-TEN steel material has been replaced with stained concrete panels.

Bev Windjack gave a brief overview of the proposed changes to the landscape plan. Ms. Windjack outlined that the shifting of the building had allowed for considerable more planting on the site. She then gave a brief overview of the tree species, plant selection, and the rain garden features for the project.

The Chair thanked the applicants for their presentation.

Committee Members had the following questions and comments:

- Members applauded the applicant on what they had done and the many changes that had been made to the design. It is a very complex building.
- A member asked for more information on the polycarbonate panels. Mr. Mann briefly described the product and advised it is often used in recreational facilities where good lighting with no glare is desired.
- Members commented that they preferred the original material on the tertiary treatment building. It related nicely to the tsunami wall and looked like a land form.
- A member asked if there was going to be additional mechanical units on the roof and if so, would they be visible from the water level. Mr. Mann advised there will be other mechanical units on the roof but, they will not be visible from the water level.

- Members commented that they love the green roof as a stormwater retention and ecosystem. However, it seems like there is a bit of a divorce in design between the landscaped roof and the industrial look of the operation & maintenance buildings. To connect the design between the green roof and the non green roofs a member suggested that instead of a vegetated roof that they have some kind of rock treatment that reflects the shoreline.
- A member inquired about the maintenance of the green roof and expressed concerns that if it wasn't maintained it could eventually look ratty from above.
- Great that there is potential for a future walkway along the shoreline.

RECOMMENDATION:

Moved by Richard Iredale seconded by Jill Singleton: The Esquimalt Design Review Committee recommends that the Development Permit Application for the Core Area Waste Water Treatment Plant **return to the Design Review Committee with revised drawings. The Motion Carried Unanimously**

(2) REZONING APPLICATION

“Marinaview”

460 Head Street [Lot 10, Block H, Section 11, Esquimalt District, Plan 292]

464 Head Street [Lot 9, Block H, Section 11, Esquimalt District, Plan 292]

PURPOSE OF APPLICATION:

Trevor Parkes, Senior Planner gave a brief overview of the project. Mr. Parkes outlined that the applicant is requesting a change in zoning from the current mix of RS-1 [Single Family] and C-7 [West Bay Commercial] zoning to a Comprehensive Development zone [CD] to facilitate the consolidation of the subject properties and authorize a new five storey, commercial/ residential mixed use building containing ground floor commercial space and 12 residential units.

Staff are asking the DRC members to provide comments on the proposed height, density and massing, proposed unit sizes, siting, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites; and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan and the West Bay Neighbourhood Design Guidelines. Staff would also like feedback relating to the porte cochere that is proposed off of Head Street and whether or not the DRC members feel that it is an appropriate form for a commercial face to the village.

Mark Lindholm, owner / applicant; Peter Hardcastle and Karen Hillel, Hillel Architecture; Carole Rossell, Landscape Architect; and David Price and Max Tomaszewski, Consultants were in attendance.

Peter Hardcastle presented the application. Mr. Hardcastle outlined that the proposed development is a companion project to the West Bay Triangle Development. He then outlined the history of the project and gave an overview of the site plan, building design and materials and parking for the project.

The Chair thanked the applicant for the presentation.

Committee members had the following questions and comments:

- Wonderful presentation, very lovely design, fabulous job. Can now see how the relationship between the Marinaview and the West Bay Triangle projects will work.
- Nice job of developing the pocket park at the east end of the site.

- Concerns were raised about the porte cochere. Is it worth the loss of retail frontage? Members felt additional commercial space would be better.
- The public gathering space is a great concept, great place for people to come and socialize. A member expressed concerns that if people congregate it could potentially cause noise complaints.

Cst. Franco Bruschetta excused himself from the meeting at 4:01 p.m.

RECOMMENDATION:

Moved by Paul De Greeff, seconded by Wendy Kay: That the Esquimalt Design Review Committee recommends that the application for rezoning to facilitate consolidation of two properties located between Head Street and Lyall Street to permit a new five storey, commercial/ residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units, and incorporating siting, height and massing consistent with architectural plans prepared by Hillel Architecture, stamped "Received January 18, 2017" **be forwarded to Council with a recommendation for approval with the condition that the applicant consider the removal of the porte-cochere as the proposal generally conforms to the Design Guidelines and the removal of the porte-cochere will allow provisions for additional commercial space. The Motion Carried Unanimously**

(3) DEVELOPMENT PERMIT APPLICATION

"West Bay Triangle"

468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]

470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]

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920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]

918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]

912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]

PURPOSE OF APPLICATION:

Trevor Parkes, Senior Planner gave a brief overview of the project. Mr. Parkes outlined that the applicant is proposing to construct a six storey, commercial and residential mixed use building containing ground floor commercial space and 73 residential units on a consolidation of nine properties, to be regulated by Comprehensive Development zone No. 98 [CD-98].

Staff are asking the DRC members to comment on issues respecting the character of the development, including landscaping, the siting, form, exterior design and finish of the buildings and other structures.

Mark Lindholm, owner / applicant; Peter Hardcastle and Karen Hillel, Hillel Architecture; Carole Rossell, Landscape Architect; and David Price and Max Tomaszewski, Consultants were in attendance.

Peter Hardcastle gave a PowerPoint presentation outlining the site plan, and a brief overview of the building design, massing, colours, materials and parking for the site.

Carole Rossell, Landscape Architect gave an overview of the proposed landscape plan for the project. Ms. Rossell outlined the plant selection and tree species for the site.

The Chair thanked the applicants for their presentation.

Committee members had the following questions and comments:

- Beautiful design, the detailing is lovely, beautiful job on the building facades and the materials chosen.
- Concerns were expressed that street trees are missing on the landscape plan.
- Members felt the triangle portal down to the parking garage is a lost opportunity. Consider enclosing the parking garage and adding enhanced green space.
- A member commented that the parking area off Gore Street and Lyall Street would make a wonderful park, children's play area or community space.
- A concern was raised about the interface between the components of the public and private realm on Head Street. A member suggested that the applicant revisit the separation between the two.
- A member asked how the garage and recycling would be contained. Mr. Harcastle advised it would be contained within the building off the internal parking courtyard.
- Surface Parking was discussed. Members expressed concerns that the West Bay Design Guidelines do not support surface parking. Can it be removed?
- A member commented that there is a lost opportunity in exploring the dynamics of the triangle site in the building. Would have been very interesting to see some of those triangular elements brought into the building itself, something to add a little more identity to the building. Could help to create more of a destination as well as a landmark for the community.

Wendy Kay excused herself from the meeting at 5:24 p.m.

- Members had further discussion on the West Bay Design Guidelines regarding the surface parking. A member commented that they thought the parking was adequately screened but felt the applicant could emphasize the screening a bit more; so that it blends in.

Jill Singleton excused herself from the meeting at 5:30 p.m.

RECOMMENDATION:

Moved by Richard Iredale, seconded by Paul De Greeff: That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped "Received January 20, 2017", and the landscape plan prepared by Small and Rossell Landscape Architects, stamped "Received January 20, 2017", and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped "Received February 3, 2017" for the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, proposed to be located on the "West Bay Triangle" lands, **be returned to the Design Review Committee with an amended design that address the following:**

1. consideration be given to eliminate the surface parking;
2. the landscape plan be amended to include additional street trees; and
3. consideration be given to enclosing the triangle portal down to the parking garage to allow for enhanced green space within the triangle area.

The Motion Carried (Ally Dewji opposed)

The reason: The proposal is inconsistent with the West Bay Neighbourhood Design Guidelines.

**DEVELOPMENT PERMIT APPLICATION – (Re-discussed)
“REVIEW OF DESIGN REVISIONS FOR THE CORE AREA
WASTEWATER TREATMENT PLANT AT MCLOUGHLIN POINT”
337 Victoria View Road
Lot 1; Section 11; Plan EPP36468**

Mr. Brown clarified that as per the BYLAW 2792 Section 25, the Design Review Committee must make a decision within 30 days of receiving the application; therefore, a recommendation to Council must be made at the February 9, 2017 meeting.

Moved by Richard Iredale, seconded by Paul De Greeff: The Esquimalt Design Review Committee recommends that the Development Permit Application for the Core Area Waste Water Treatment Plant **be forwarded to Council with a recommendation of approval subject to the following revisions:**

1. **Use board form concrete on the façade of the tertiary treatment portion of the building so that it blends into the rocky coast environment better;**
2. **explore architectural elements such as shape to help harmonize the operations and maintenance part of the building with the plant part of the building; and**
3. **explore the option of adding green roof over the plant portion of the building. The Motion Carried (1 opposed Roger Wheelock)**

VI. STAFF LIASON STATUS REPORT

No status report was given

VII. NEW BUSINESS

VIII. NEXT REGULAR MEETING

Thursday, February 16, 2017

IX. ADJOURNMENT

On motion the meeting adjourned at 5:40 p.m.

VICE CHAIR, DESIGN REVIEW COMMITTEE
THIS 8th DAY OF MARCH 2017

ANJA NURVO,
CORPORATE OFFICER



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**ADVISORY DESIGN REVIEW COMMITTEE
SPECIAL MEETING MINUTES**

HELD

FEBRUARY 16, 2017

ESQUIMALT COUNCIL CHAMBERS

MEMBERS PRESENT: Roger Wheelock (CHAIR) Richard Iredale
Paul De Greeff Jill Singleton
Robert Schindelka

REGRETS: Wendy Kay, Ally Dewji and Cst. Franco Bruschetta

STAFF LIAISON: Bill Brown, Director, Development Services

TOWNSHIP SOLICITOR: Lui Carvello, Carvello Law Corporation

SECRETARY: Pearl Barnard

I. CALL TO ORDER

The Chair called the meeting to order at 3:05 p.m.

II. LATE ITEMS

No late items

III. ADOPTION OF AGENDA

Moved by Jill Singleton, seconded by Robert Schindelka: That the agenda be adopted as distributed. **Carried Unanimously**

IV. STAFF REPORT

DEVELOPMENT PERMIT APPLICATION

**“REVIEW OF DESIGN REVISIONS FOR THE CORE AREA
WASTEWATER TREATMENT PLANT AT MCLOUGHLIN POINT”**

337 Victoria View Road

Lot 1; Section 11; Plan EPP36468

PURPOSE OF APPLICATION:

The purpose of this application is to ensure that the applicant's intentions are consistent with the Zoning Bylaw and the design guidelines for Development Permit Area No. 3 “Industrial” including the May 2013 Revised Design Guidelines for the McLoughlin Point Wastewater Treatment Plant that specifically apply to this project.

Bill Brown asked the DRC members to comment specifically on the south elevation and the harmonization of the two parts of the buildings.

In attendance:

Harbour Resource Partnership

Jim Mann

Jim Alders

Bev Windjack

Capital Regional District Project Board
Ken Madill
Sharon Singh
Elizabeth Scott

Elizabeth Scott presented the application.

Jim Mann gave a PowerPoint presentation and outlined the proposed changes to the design. Mr. Mann explained that revisions had been made to the parapet height and the treatment of the tertiary tank. The operations and maintenance building has been revised, the staircase enclosure is now a lighter colour metal cladding and the level of planting and berming in the retaining walls has been enhanced. Mr. Mann addressed the green roof concerns, he explained that there are a variety of vents and things coming up in various places on the roof and from an operational stand point it was not practical to add more green roofs.

Mr. Mann thanked the Committee members for their input and comments.

The Chair thanked the applicants for their presentation.

Committee Members had the following questions and comments:

- Members thanked Mr. Mann for the great presentation and for coming back. Well done, the revisions are consistent with the comments and concerns raised by the Committee members.
- A member indicated that the design had come a long way but still remains an industrial plant on waterfront. Another member commented that they were now proud to be part of this project.
- A member commented that he thought there are too many different materials being used for the project.
- It was suggested that the applicant:
 - Explore the use of board form concrete or dark brick to help marry the tertiary building to the landscape.
 - Consider more articulation of the retaining wall in front of the tertiary plant.
 - With the deeper soil in front of the tertiary building, consider adding some larger trees there.
- A member commented it was a wonderful design, and that the project will have relatively little visual impact for Esquimalt but a lot for the rest of Victoria. The project didn't respect the natural shoreline or the gateway location from the beginning, it was a relatively impossible task given the small size of the site.
- A member commented that they couldn't see an opportunity for a walkway or public access along the Department of National Defence shoreline.

RECOMMENDATION:

Moved by Richard Iredale, seconded by Robert Schindelka: That the Esquimalt Design Review Committee recommends that the development permit application for the Core Area Waste Water Treatment Plant be forwarded to Council with a recommendation **for approval with the following conditions:**

That the applicant:

- 1. Consider more articulation of the retaining wall systems; and**
- 2. explore the use of board form concrete or dark brick to help marry the tertiary building to the landscape.**

The Motion Carried (1 opposed Roger Wheelock)

VI. STAFF LIASON STATUS REPORT

No status report was given

VII. NEW BUSINESS

VIII. NEXT REGULAR MEETING

Wednesday, March 8, 2017

IX. ADJOURNMENT

On motion the meeting adjourned at 3:50 p.m.

VICE CHAIR, DESIGN REVIEW COMMITTEE

THIS 8th DAY OF MARCH 2017

ANJA NURVO,
CORPORATE OFFICER

DRAFT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: March 8, 2017

STAFF REPORT

DATE: March 3, 2017

TO: Chair and Members of the Design Review Committee

FROM: Trevor Parkes, Senior Planner

SUBJECT: DEVELOPMENT PERMIT APPLICATION
“West Bay Triangle”
468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]
470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]
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RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received March 2, 2017”, and the landscape plan prepared by Small and Rossell Landscape Architects, stamped “Received March 2, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received February 8, 2017” for the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, proposed to be located on the “West Bay Triangle” lands, be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application

The applicant is proposing to construct a the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units on a consolidation of nine subject properties to be regulated by Comprehensive Development District No. 98 [CD-98].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application is generally consistent with the design guidelines contained in Section 9.4 of the Township's Official Community Plan [attached]. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines [attached] as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit.

Evaluation of this application should focus on issues respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of buildings and other structures in relation to the relevant design guidelines.

Context

Applicant/Owner: Triangle Estates Ltd. [Mark Lindholm]

Architect: Hillel Architecture Inc. [Peter Hardcastle]

Property Size: Metric: 4080 m² Imperial: 43,917 ft²

Existing Land Use: Single Family and Two Family residences and vacant Commercial land

Surrounding Land Uses:

North: Neighbourhood Commercial/ West Bay Commercial
South: DND Federal Land [Work Point]
West: Single Family and Two Family Residential
East: Marine Commercial

Existing Zoning: RS-1 [Single Family Residential] – 5 Lots
RD-2 [Two Family Small Lot Residential] – 1 Lot
C-7 [West Bay Commercial] – 2 Lots

Proposed Zoning: CD-98 [Comprehensive Development District No. 98 – Pending Adoption]

Existing OCP Designation: Commercial Mixed-Use [No change required]

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received by the submission deadline:

Building Inspection: Construction must satisfy BC Building Code and Esquimalt's Building Code Bylaw, 2002, No. 2538 requirements. Detailed review of plans will occur when at time of Building Permit application.

Fire Services: Fire Services staff have completed a preliminary evaluation of the proposed new building and offer no objections at this time. Detailed additional comments will be provided as part of the consideration of the Building Permit Application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed commercial mixed use development including commercial space and 73 residential units located on 9 properties within West Bay. The applicant is responsible for retaining the services of a qualified professional for the design, construction and supervision of all Works and Services required to service the proposed development as indicated in Esquimalt's Subdivision and Development Control Bylaw No. 2175. A capacity study should verify all main sizes, material, etc., and to determine if the Sewer and Drain mains are of adequate size and condition to handle the increased flows introduced as a result of the development. Existing municipal infrastructure may need to be relocated or SRWs may need to be secured. Existing properties are to remain fully serviced during construction. Additional review comments will be provided when detailed engineering drawings are submitted.

Parks Services: Parks staff have completed a review of the proposed on-site and off-site landscaping and have no concerns with the planting plan. Tree protection for the Plum trees and the Garry Oak tree scheduled for retention should be put in place prior to any demolition or construction activity beginning on the site. Tree Removal Permits will be required for the removal of protected trees and municipal boulevard trees.

Comments from the Design Review Committee [DRC]

This application was originally considered at the regular meeting of DRC held on February 9, 2017. Members comments were generally supportive of this proposal with members stating the design was beautiful, with lovely detailing of the building facades and excellent choices for cladding materials. A member noted the provision of surface parking is not consistent with the West Bay Neighbourhood Design Guidelines and that it would be desirable to see that space used as a park. Members indicated that consideration should be given to enclosing the central triangular portal above the parking courtyard and using it as green space. Members were united in their desire to see street trees added to the site plan and landscape plan.

The DRC recommended that the application be returned to the Design Review Committee with an amended design that addresses the following:

1. consideration be given to eliminate the surface parking;
2. the landscape plan be amended to include additional street trees; and
3. consideration be given to enclosing the triangle portal down to the parking garage to allow for enhanced green space within the triangle area.

The Reason: The proposal is inconsistent with the West Bay Neighbourhood Design Guidelines.

In response to this approved motion the applicant has provided a revised set of drawings, stamped "Received March 2, 2017" [attached] which include a refined site plan and landscape plan detailing notable changes to the public realm treatment on all three frontages of the site. The treatment of public realm of Head Street has been enhanced by the inclusion of street trees and an unimpeded 2.0 metre wide public sidewalk while Gore Street also benefitted from the addition of street trees and an expanded boulevard on the southwest end of the frontage adjacent to the transit stop. Particular attention was paid to the Lyall Street frontage where the sidewalk was repositioned interior to a substantial boulevard containing street trees and additional screening was added to better shield the surface parking area. This change was a result of collaboration between the architect and Esquimalt staff that revealed additional area was available in the public realm to achieve a treatment more consistent with that contained in the West Bay Neighbourhood Design Guidelines.

The applicant's architect supplemented this revised submission with a letter detailing the

specific changes as they relate to the issues identified by the DRC at the February 2017 meeting [attached].

ISSUES:

Zoning

CD-98 zoning was specifically tailored to accommodate this proposal. The proposed design is consistent with the form and character presented supporting the rezoning application and the proposed building height, massing, density, siting and parking requirements satisfy all of the CD-98 zone regulations outlined in Amendment Bylaw No. 2873 [attached] which is pending adoption by Council.

Density, Lot Coverage, Setbacks, Height and Parking:

The following chart details the setbacks, lot coverage and floor area ratio and parking requirements of this proposal.

	Comprehensive Development Zone	
Floor Area Ratio	1.40	Zoning Bylaw, 1992, No. 2050 does not contain a zone that could accommodate this commercial mixed-use proposal. Staff present this summary table as the basis for a site specific zone written to accommodate this proposal should it be forwarded in the development review process.
Lot Coverage		
• 1st Storey	55%	
• 2nd Storey	63%	
• Above 2nd Storey	23%	
Setbacks		
• Front	5.8 m plus 3.0 m [Canopy]	
• Rear	6.7 m plus 1.8 m [Canopy]	
• Interior Side	0.9 m plus 0.9 m [Canopy]	
• Exterior Side	3.2 m plus 0.5 m [Canopy]	
Building Height	7.6m [Head Street and Gore Street] 22 m (6 Storeys) [Lyllall Street]	
Off Street Parking	Total spaces required for all uses = 114 Total proposed = 119	
Useable Open Space	15% [Including 2 nd Storey Terrace]	
Commercial Floor Area	Not less than 395 square metres	

Official Community Plan

The West Bay Triangle lands are located within a predominantly low rise neighbourhood consisting of a mix of commercial properties, single and two family residential homes, a marine community, and military uses including training facilities and housing. The exception to the established one to three storey built environment is the DND "Accommodations" building located at Work Point to the south of the subject properties. This building is approximately 7 storeys [22 metres] in height and commands a substantial presence in the area due to the combination of height and massing of the building.

The West Bay Triangle would integrate into the existing urban fabric by complimenting the DND Accommodations building while presenting a 2 storey built form sympathetic to the existing residential and commercial buildings along Gore Street and Head Street.

The current Land Use Designation of properties adjacent to the West Bay Triangle site include

Commercial Mixed-use for lands to the east, adjacent to the water, Park and Open Space for Captain Jacobson Park and Townhouse Residential for properties on the north side of Gore Street and the south side of Paradise Street. Based on these land use designations, it is not unreasonable to expect some of these properties adjacent to the proposed West Bay Triangle building to redevelop to a modestly higher density than is present today.

Policy direction contained in the Esquimalt Official Community Plan was reviewed by staff and presented to Council as part of the consideration of the rezoning application. Official Community Plan, Section 9.4.5, Guidelines for Owners of Land within the Development Permit Area No. 2 – Commercial is specifically relevant to consideration of this Development Permit Application [attached].

OCP Section 9.4.5(a) supports windows facing the street and doors opening onto the street. This is consistent with the proposed design.

OCP Section 9.4.5(b) supports ornamental lighting highlighting the building and lighting pedestrian area while avoiding the casting of glare or direct light onto adjacent sites. The drawing package does not specifically detail the lighting plan however the proposed design could accommodate glass and lighting consistent with this policy.

OCP Section 9.4.5(c) supports building design that limits the casting of shadows on public space. At six storeys in height, the proposed building casts shadows that affect the subject lands, Gore Street and Head Street, however, these shadows do not rest in any one area for prolonged periods. During the summer months the shadow impacts are significantly reduced as they are almost entirely contained on the subject lands as detailed in the drawing package.

OCP Section 9.4.5(d) supports canopies covering pedestrian walkways. The proposed design includes generous 3 metre deep canopies over the sidewalk on Head Street to create a covered transitional space between the indoor uses and the outdoor public realm.

OCP Section 9.4.5(e) supports screened or underground parking be provided. If surface parking is proposed it should be screened with landscaping. The proposed design addresses this guideline as all parking is screened within the building with the exception of 12 spaces near the intersection of Lyall Street and Gore Street which are wrapped in landscape screening.

OCP Section 9.4.5(f) supports the inclusion of Crime Prevention through Environmental Design principles in any project. The proposed design is well lit with good connectivity to the public street and resident overlook onto internal courtyard areas.

OCP Section 9.4.5(g) supports decreased setbacks where appropriate. This proposal maintains generous setbacks in order to create ample space in front of businesses for walking and the display of goods.

OCP Section 9.4.5(h) supports screening of garbage and recycling facilities. The proposal has these facilities contained within the building, accessed from the interior parking courtyard.

OCP Section 9.4.5(i) supports the retention of trees and habitat wherever possible. This proposal is a complete revitalization of the subject lands including a significant excavation for underground parking facilities. Accordingly the proposal has included the retention of only two trees on or abutting the site, both mature and both located outside the excavation area. It is noteworthy that the proposed landscaping and public realm plantings will increase the number of trees on the lands.

West Bay Neighbourhood Design Guidelines

The West Bay Triangle development proposal has been tailored to address the West Bay Neighbourhood Design Guidelines resulting in an application that is, for the most part, consistent with these policies with the notable exception of orienting the tallest portion of the building east to west as opposed to the recommend orientation of north to south. The applicant has provided a detailed shadow analysis that indicates the proposed design is superior in mitigating shadowing than any alternatives. As shadow casting was identified as a major concern for local residents during the public consultation process of the West Bay Neighbourhood Design Guidelines, the applicant has chosen to proceed with the design as presented.

Development Services staff completed a review of the proposed design and note the following apparent inconsistencies to the West Bay Neighbourhood Design Guidelines:

Page 10, Bullet 4 – "Avoid locating off-street surface parking adjacent to active public streets and open spaces.....". The proposed design is consistent with this policy with the exception of the surface parking provided adjacent to Lyall Street near the Gore Street intersection. Staff note this parking area provides desirable visitor parking that has been approved by Council and secured via covenant as part of the rezoning of these lands. Staff also note that, in response to the DRC comments, substantial additional screening has been added to mitigate the impact of this feature consistent with current OCP policy.

Page 17, Bullet 2 – "Locating off-street surface parking in front of buildings,...immediately adjacent to the public sidewalk or open spaces.....is strongly discouraged and should be avoided.". Once again, the proposed design achieves this policy when considered from Head Street and Gore Street; however, surface parking is provided adjacent to Lyall Street. Staff note that while a surface parking area is inconsistent with this policy, the parking area provides an intuitive destination for both commercial and residential visitors to the site and this aspect of the design has been approved by Council as part of the rezoning application.

Page 38 – The public sidewalk has been amended to achieve 2.0 metres of unimpeded width across the Head Street frontage and boulevard trees now co-exist with on-street parking spaces adjacent to the sidewalk. While sidewalk width is still less than the recommended 3.0 metres, staff acknowledge that the West Bay Triangle development incorporates a substantially larger setback than proposed in the guidelines and accommodates a separated, private realm, sidewalk 4.0 metres in width. Changes to the streetscape treatment along all three street frontages that revise tree placement from the back of sidewalk in the private realm to boulevard trees placed between the vehicular and pedestrian realms result in the proposed streetscape being generally consistent with that envisioned in the guidelines

Page 40 – Streetscape design envisioned for Lyall Street East is inconsistent with the established guidelines. Staff note the proposed design does not accommodate the conceptual two way separated bike lane and acknowledge that inclusion of this design feature in the public realm is premature at this time as further consultation with the community on this concept is required. The streetscape plan for Lyall Street has been revised to be consistent with that detailed for Lyall Street West including a boulevard complete with street trees that separates on-street parking from a more generous sidewalk.

Public Notification

As this is a Development Permit application without any requested variances, public notification


is not required.

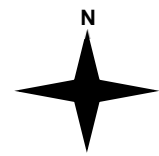
ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**

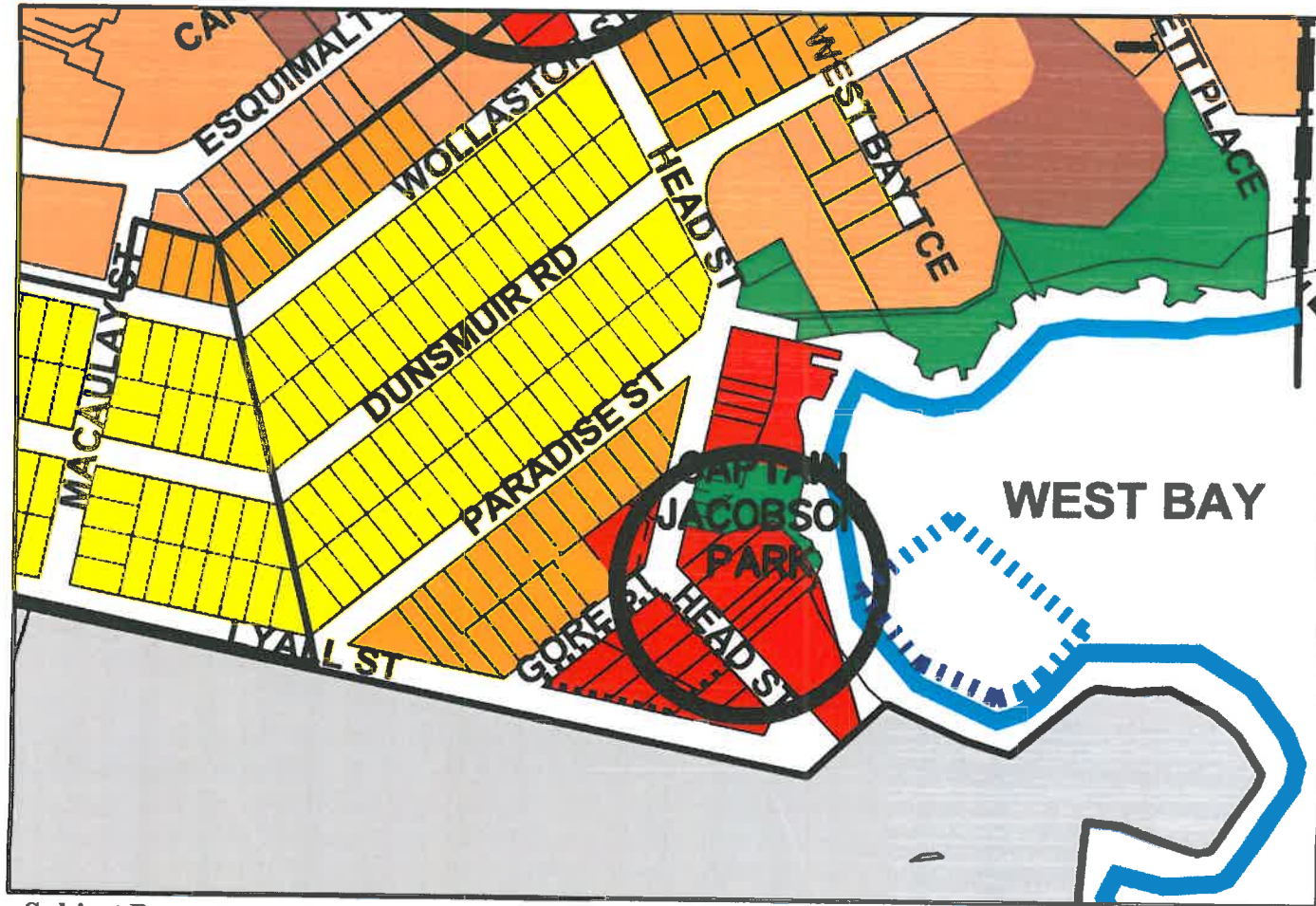
468 Head Street – ‘West Bay Triangle’



Subject Property Boundary: 



Extract from Official Community Plan Bylaw No. 2646 Schedule 'A'
Land Use Designations



Subject Property:

LEGEND	
	Single and Two-Unit Residential
	Townhouse Residential
	Multi-Unit, Low-Rise Residential (2 to 4 storeys)
	Multi-Unit, High-Rise Residential (5+ storeys)
	Parks and Open Space
	Trails existing and proposed
	DND Federal Land
	Water Area
	Commercial Node
	Municipal Boundary
	Commercial Mixed-Use
	Industrial
	Institutional
	Transportation Corridor

2.3 Commercial—Mixed Land Use

Commercial activity in Esquimalt is clustered in four main groupings:

- Esquimalt Village;
- Esquimalt Road/Head Street;
- Craigflower Road/Tillicum Road; and
- West Bay Harbour.

2.3.1 General Commercial - Mixed Use Objectives

- a) To create a diversified commercial and employment sector that provides a wide range of goods and services to residents of Esquimalt.
- b) To foster the creation of an identifiable and vibrant Esquimalt Village that successfully integrates commercial, public and residential activity.
- c) To encourage growth through revitalization and redevelopment of commercial areas.
- d) To encourage a mix of ground-level commercial and upper-level(s) residential.

2.3.2 General Commercial - Mixed Use Policies

- a) The Township encourages a mix of commercial and multi-unit residential developments in all commercial-mixed use areas denoted on "Schedule A". These will have commercial uses on the ground floor and residential uses above.
- b) All commercial-mixed use areas are designated Development Permit Areas, as shown on "Schedule C" in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.
- c) The Township encourages public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.
- d) The Township will develop signage guidelines for each commercial area, as part of design guidelines for these areas. The Township, in partnership with interested members of the community, will take the initiative to design and install entrance signage at key street locations, including entrances to Esquimalt.
- e) The Township encourages the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas. The Township is amenable to using density bonusing, or providing variances to zoning or parking regulations for redevelopment proposals.
- f) To encourage the use of bicycles, provision should be made in new commercial buildings for bicycle parking for employees and visitors. Secure bicycle parking for employees should be provided in the ratio of one (1) parking space per ten (10) full-time employees with a minimum of one (1) space for each new building. In all new commercial buildings, six (6) bicycle parking spaces should be available for the use of temporary visitors.
- g) End of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

- h) Where all of the following criteria are met in a commercial building, Council may reduce the off-street parking requirement through the Development Permit:
 - i) Two (2) or more secure bicycle storage spaces are provided;
 - ii) Shower and change rooms are provided;
 - iii) Six (6) visitor bicycle parking spaces are provided; and
 - iv) The building is located within 200 metres of a regional bus route.
- i) Lands outside the Commercial-Mixed Use designation on "Schedule A" will not be considered for commercial zoning unless the following criteria are met:
 - i) The project is needed to serve tourists or local residents and cannot be appropriately located within established commercial areas;
 - ii) The density and scale of the project is sensitive to the prevailing character of surrounding lands;
 - iii) The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
 - iv) The project's parking requirements can be satisfied on-site and will not unduly affect neighbouring residences; and
 - v) The proponent demonstrates that the neighbourhood has been consulted and residents have had an opportunity to express their concerns.

[Amendment Bylaw [No. 6], 2010, No. 2730 - Adopted May 25, 2010]

- j) In mixed commercial and multi-unit residential developments, buildings up to 12 storeys in height and with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.
- k) Development proposals with heights and/or densities greater than those set out in Section 2.3.2 (j) may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor space where new commercial buildings provide affordable, accessible, or special needs housing units or amenities for the benefit of the community.

9.4 Development Permit Area No. 2 – Commercial

9.4.1 Scope

All lands designated Commercial on Schedule “C” are part of DPA No. 2.

9.4.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, commercial.

9.4.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

Where new development is to occur within Esquimalt’s commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials, and by their orientation to the street rather than to a parking lot or internal square.

The goals for Development Permit Area No. 2 are:

- a) to enhance the aesthetic image of Esquimalt’s commercial district, particularly those areas that are considered community focal points, such as the Village, the Head Street/Esquimalt Road intersection and major entrance points to the municipality;
- b) to revitalize existing commercial areas by encouraging a variety of businesses;
- c) to encourage growth in the tax base through diversified commercial development and redevelopment of existing commercial areas; and
- d) to encourage integrated residential/institutional/commercial uses in commercial areas.

9.4.4 Requirements of Owners of Land within the Development Permit Area

- a) Owners of land within Development Permit Area No. 2 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:
 - i) subdivide lands; or
 - ii) construct or alter a building or structure;without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.
- b) Exemptions:

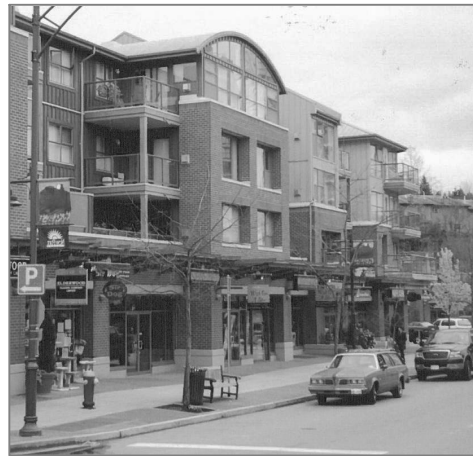
The following do not require a development permit:

 - i) construction of buildings or structures less than 10 square metres in area;

- ii) minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted by the municipal tree protection bylaw; and
- vi) replacement or changing of existing signs, provided the sign area is not to be increased.

9.4.5 Guidelines for Owners of Land within the Development Permit Area

- a) Commercial building facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
- b) Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, commercial lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
- c) Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- d) Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
- e) Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- f) The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
- g) Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
- h) Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

BYLAW NO. 2873

A Bylaw to amend Bylaw No. 2050, cited as the
“Zoning Bylaw, 1992, No. 2050”

THE MUNICIPAL COUNCIL OF THE CORPORATION OF THE TOWNSHIP OF
ESQUIMALT, in open meeting assembled, enacts as follows:

1. This bylaw may be cited as the “*ZONING BYLAW, 1992, NO. 2050, AMENDMENT BYLAW NO. 2873*”.
2. That Bylaw No. 2050, cited as the “Zoning Bylaw, 1992, No. 2050” be amended as follows:

- (1) by adding the following words and figures in Part 31, Zone Designations, in the appropriate alpha-numeric sequence:

“Comprehensive Development No. 98 (468 Head Street) CD No. 98”

- (2) by adding the following words and figures in Section 2 - Definitions of PART 1 - Interpretation:

“**Boat Rental and Passenger Charter**” means the use of land or a building or structure for the booking, registration, outfitting, and commercial transactions for boat rental, marine tours, diving expeditions, kayaking, sailing, fishing and other boat excursions.

- (3) by adding the following text as Section 67.85 (or as other appropriately numbered subsection within Section 67):

67.85 COMPREHENSIVE DEVELOPMENT DISTRICT NO. 98 [CD NO. 98]

In that Zone designated as CD No. 98 [Comprehensive Development District No. 98] no Building or Structure or part thereof shall be erected, constructed, placed, maintained or used and no land shall be used except in accordance with and subject to the regulations contained in or incorporated by reference into this Part.

- (1) **Permitted Uses**

Only Mixed Commercial / Residential use is permitted and the following Uses and no others shall be permitted as part of that development:

- a) Dwelling – Multiple Family
- b) Home Occupation
- c) Business and Professional Office
- d) Personal Service Establishment

- e) Retail Store
- f) Restaurant
- g) Liquor Store
- h) Convenience Store
- i) Group Children's Day Care Centre
- j) Boat Rental and Passenger Charter

(2) **Floor Area Ratio**

(a) **Residential Uses**

The Floor Area Ratio shall not exceed 1.30.

(b) **Combined Mixed Use**

The combined Floor Area Ratio for all uses shall not exceed 1.40.

(3) **Unit Size**

Dwelling Units shall not be less than 45 square metres.

(4) **Number of Dwelling Units**

(a) Not less than sixty five (65) Dwelling Units shall be located in this Zone.

(b) Not more than seventy three (73) Dwelling Units shall be located in this Zone.

(5) **Size and Location of Commercial Space**

(a) The minimum Floor Area dedicated to Commercial Uses shall not be less than 395 square metres located on the First Storey.

(b) The maximum Floor Area dedicated to Commercial Uses shall not be greater than 425 square metres.

(c) Commercial Uses are not permitted on any Storey located above the First Storey.

(d) Commercial Uses are not permitted within 30 metres of the southwestern Lot Line.

(6) **Parcel Size**

The minimum Parcel Size of fee simple Parcels created by subdivision shall be 4075 square metres.

(7) **Building Height**

(a) No Principal Building shall exceed a Height of 23 metres.

(b) No Accessory Building shall exceed a Height of 3.3 metres.

(8) **Lot Coverage**

(a) Principal Buildings, Accessory Buildings and Structures combined shall not cover more than 64% of the Area of the Parcel.

(b) Notwithstanding Section 7(a), Principal Buildings shall not cover more than 56% of the Area of the Parcel on the first storey.

(c) Principal Buildings shall not cover more than 24% of the Area of the Parcel for each storey of a Building that is located above the second storey.

(9) **Siting Requirements (As illustrated on Figure 1.)**

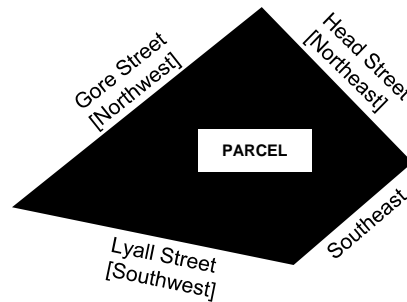


Figure 1.

(a) **Principal Buildings:**

(i) No Principal Building shall be located within 5.8 metres of the northeastern Lot Line.

(ii) No part of a Principal Building located above the second storey shall be located within 18 metres of the northeastern Lot Line.

(iii) No Principal Building shall be located within 3.2 metres of the northwestern Lot Line.

(iv) No part of a Principal Building located above the second storey shall be located within 12 metres of the northwestern Lot Line.

(v) No Principal Building shall be located within 0.9 metres of the southeastern Lot Line.

(vi) No Principal Building shall be located within 5.9 metres of the southwestern Lot Line.

(vii) That part of a Principal Building located above the second storey shall be located within 30 metres of the southwestern

Lot Line.

(b) **Accessory Buildings:**

- (i) No Accessory Building shall be located within 52 metres of the northeastern Lot Line.
- (ii) No Accessory Building shall be located within 9 metres of the northwestern Lot Line.
- (iii) No Accessory Building shall be located within 16 metres of the southwestern Lot Line.
- (iv) No Accessory Building shall be located within 0.1 metres of a Principal Building.

(10) **Siting Exceptions**

Within the CD-98 zone, the minimum distance to a Lot Line may be reduced by not more than the following distances to accommodate exterior canopies, attached to and forming part of a Principal Building:

- (a) Northeastern Lot Line: 3.0 metres
- (b) Northwestern Lot Line: 0.5 metres
- (c) Southwestern Lot Line: 0.9 metres
- (d) Southeastern Lot Line: 0.9 metres

(11) **Useable Open Space**

Useable Open Space shall be provided in an amount not less than 7.5% of the Area of the Parcel, including open space located over a parking structure.

(12) **Fencing**

Subject to Part 4, Section 22, no fence shall exceed a Height of 1.2 metres nor be located within 5.8 metres of a Lot Line abutting a Highway.

(13) **Off-Street Parking**

Off-street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011(as amended).

- (4) by changing the zoning designation of each of the following parcels, shown cross-hatched on Schedule 'A', attached hereto, from their current zoning designation, as detailed on Schedule 'B', attached hereto, to CD No. 98 [Comprehensive Development District No. 98]:

- a) PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];
 - b) PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];
 - c) PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];
 - d) PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];
 - e) PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];
 - f) PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];
 - g) PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];
 - h) PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street]; and
 - i) PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].
- (5) by changing Schedule 'A' Zoning Map, attached to and forming part of "Zoning Bylaw, 1992, No. 2050" to show the changes in zoning classification effected by this bylaw.

READ a first time by the Municipal Council on the 16th day of May, 2016.

READ a second time by the Municipal Council on the 16th day of May, 2016.

A Public Hearing was held pursuant to Sections 464, 465, 466, and 468 of the *Local Government Act* on the 20th day of June, 2016.

READ a third time by the Municipal Council on the 20th day of June, 2016.

RESCIND third reading by the Municipal Council on the 12th day of September, 2016.

AMEND AND READ ANEW at second reading by the Municipal Council on the 12th day of September, 2016.

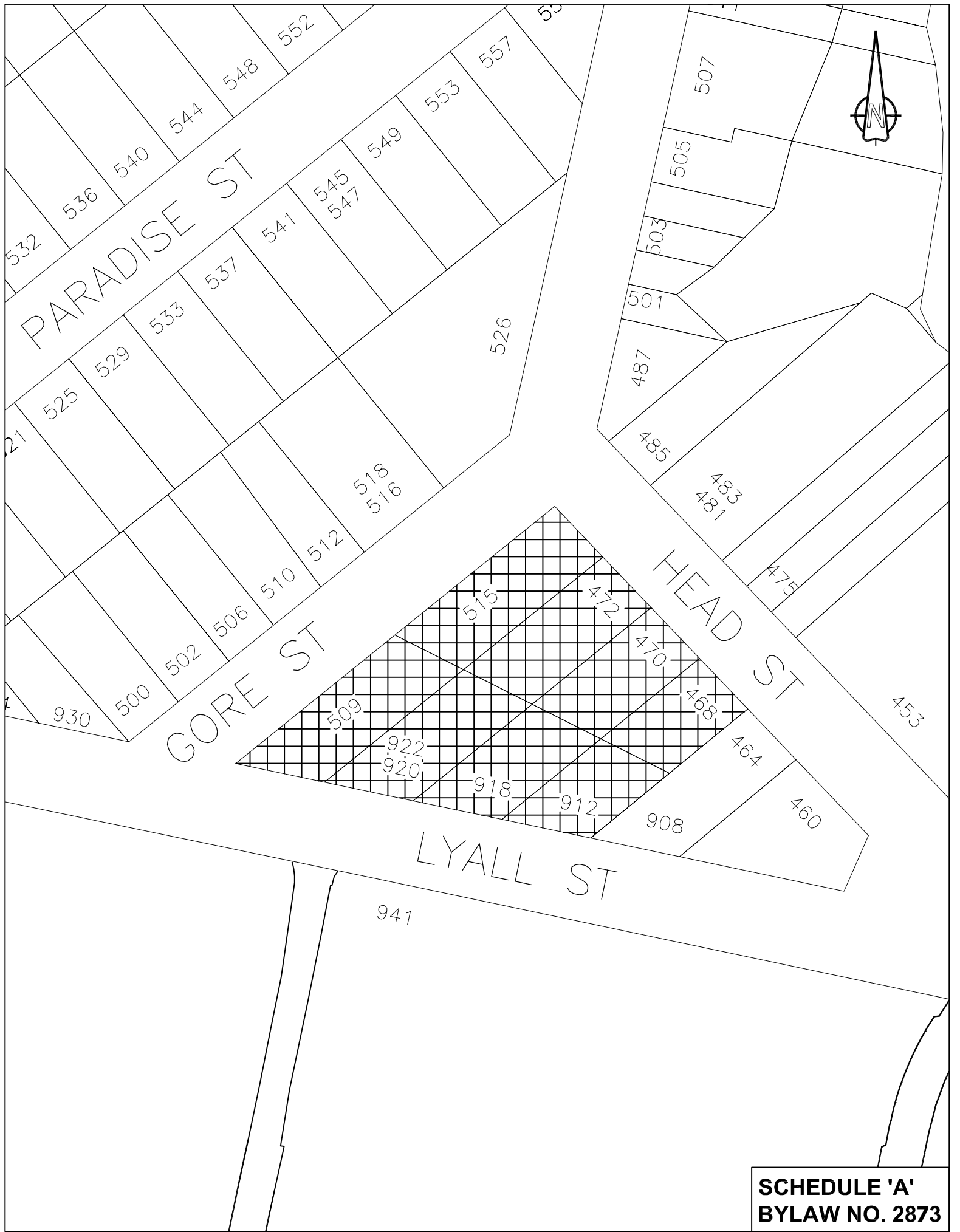
A Public Hearing was held pursuant to Sections 464, 465, 466, and 468 of the *Local Government Act* on the 3rd day of October, 2016.

READ a third time by the Municipal Council on the 3rd day of October, 2016.

ADOPTED by the Municipal Council on the ---- day of ----, 2016.

BARBARA DESJARDINS
MAYOR

ANJA NURVO
CORPORATE OFFICER



SCHEDULE 'A'
BYLAW NO. 2873



LEGEND

- ZONE DESIGNATIONS
- PARCELS

**SCHEDULE 'B'
BYLAW NO. 2873**



West Bay Neighbourhood Design Guidelines



We would like to thank the following members of the West Bay Design Guidelines Stakeholders Group whose local insight and involvement in the process was of great benefit to the development of these design guidelines.

West Bay Design Guidelines Stakeholders Group:

Brian Emmett
Kris Samuels
Lieutenant-Colonel Ulpiano Honorio
Carolyn Gisborne
Christina Clarke
Laurie Hurst
Jeff Miller
Janice Rose
Scott Hartman
Liz Dill
Jim Witter
Peter Hardcastle
Mark Lindholm
Sally Reid
Katrina Dwulit
Julie Flatt
Carole Witter

Submitted: 10 September, 2015.

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1. Purpose & Overview

Purpose and Overview

These design guidelines were developed through a collaborative, community-based process to ensure new development enhances rather than detracts from West Bay's unique and rich character and identity. These design guidelines focus on the interface between and integration of public and private lands towards the preservation and enhancement of the identity, social vitality and overall liveability that characterizes the West Bay neighbourhood. This includes integration with federal Department of National Defense (DND) lands, integration of local ecosystems, and connection to the harbourfront and the range of recreational and other activities associated with the working harbour.

This document includes:

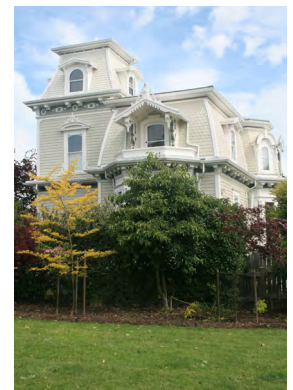
- An inventory and analysis of neighbourhood design characteristics;
- A vision statement and set of design principles as the rationale for the design guidelines;
- Development Permit Area design guidelines for Form and Character;
- A set of general design guidelines and recommendations for public realm design and improvements; and,
- Specific guidance on site planning and design for development opportunity sites along Head Street south adjacent to the harbourfront and including the "Triangle Lands", are also included.

The Spirit of West Bay

During the West Bay Design Guidelines Stakeholder Workshop, we asked: "What words capture the spirit of West Bay?" The following is a summary of responses:



Neighbourhood Characteristics: Photo Essay



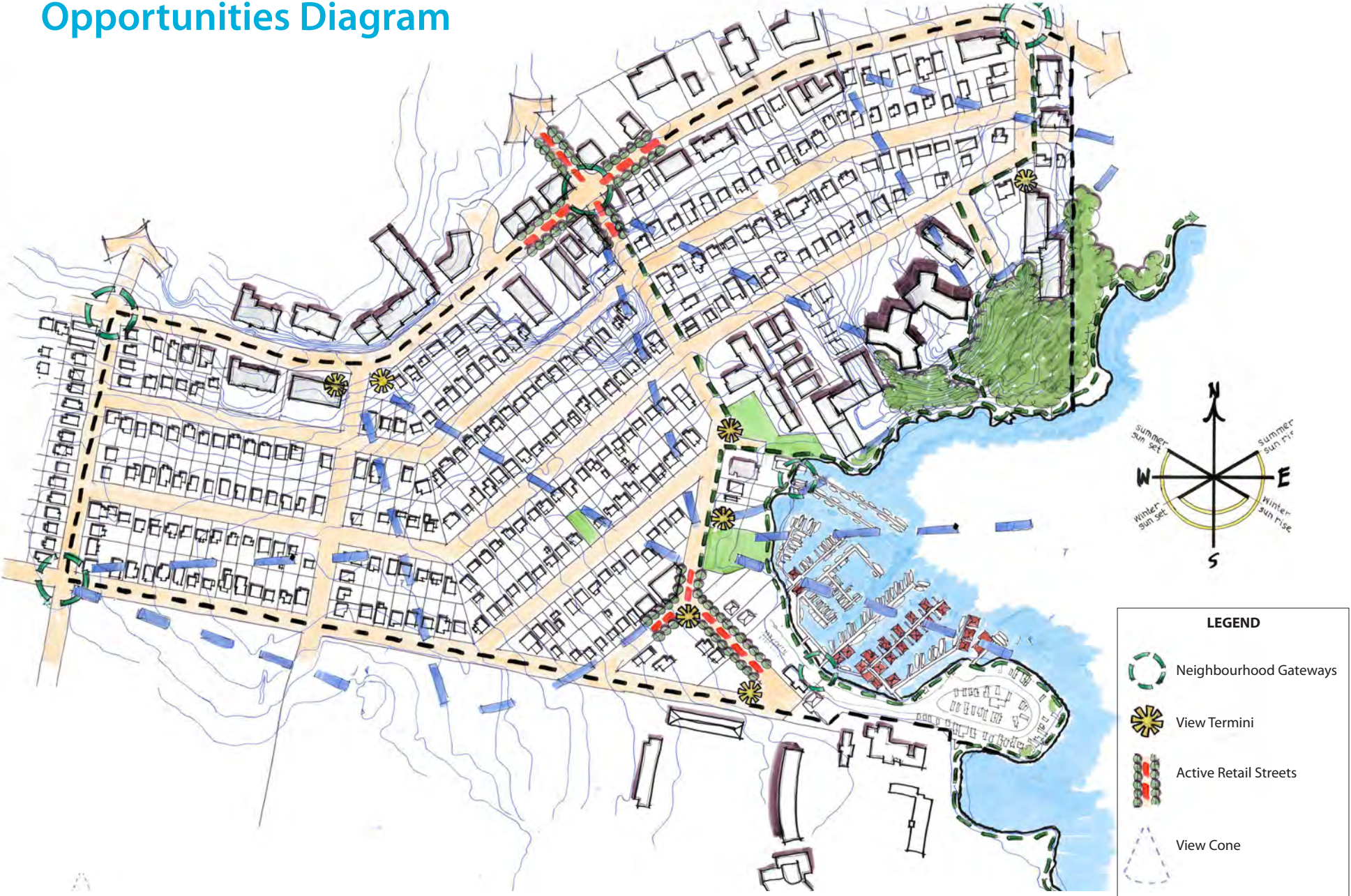
West Bay Ortho Photo and Study Area



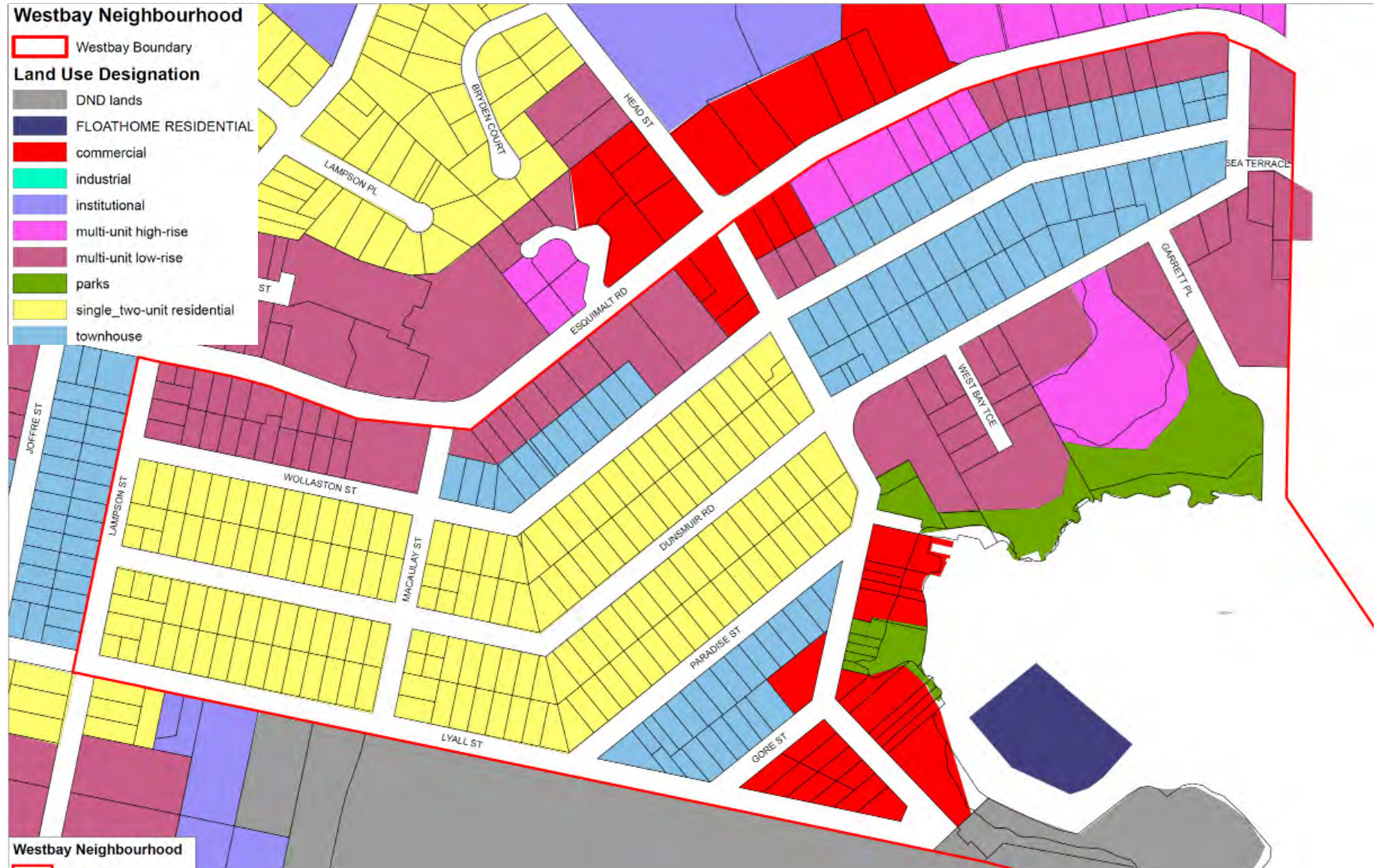
Neighbourhood Characteristics: Topography and Building Massing (Existing)



Neighbourhood Characteristics: Opportunities Diagram



West Bay Neighbourhood - OCP Land Use



2. Design Vision & Principles

Vision

West Bay is a thriving, attractive and walkable harbourfront neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbourfront recreation, and sunny open spaces with strong connections to the harbour, make this a truly liveable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.



Photo courtesy: Josh McCulloch, PictureBC

Design Principles

The following design directions for the public realm were identified through the workshop to help maintain and enhance the unique identity of West Bay:

- **Strong connection to the harbourfront:** Maintain and enhance visual and physical connections to the harbour and harbourfront trail network.
- **A walkable neighbourhood:** Publicly accessible buildings present a friendly face to streets and open spaces to make them active, attractive and inviting to pedestrians. Make connections within and to adjacent neighbourhoods to make walking easy.
- **Human scale:** Use architectural features, details, and site design elements that are of human proportion and clearly oriented for public, pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it on foot.
- **Marine ecology:** Enhance, restore and celebrate the unique marine and upland ecology. Layer and integrate mobility, recreation, stormwater, habitat and ecological systems in the context of the working harbour.
- **Harbourfront community & identity:** Ensure building and open space design that supports and enhances the unique sense of community and identity, including its architectural and marine heritage. More broadly, this includes the rich natural and human history of the area and associated expression of local culture, festivals, and gathering at and near the harbour front and water's edge.
- **Neighbourliness:** Ensure new development responds positively to the existing context by ensuring a sensitive transition in scale, by minimizing view and shadow impacts, and by responding to the positive design characteristics, if only subtly, of adjacent development.

3. Design Guidelines

Overview

Urban design is the comprehensive and cohesive combination of buildings, streets, open spaces and the natural environment and has, as its objective, the creation of memorable public spaces.

The essence of good urbanism is determined by the relationship between:

- the built and natural environments, and
- the public and private realm - the areas between buildings and public open spaces - at street level.

Buildings, streets, and other public open spaces scaled for human comfort and use are essential to the creation of a functional, aesthetically rich and vibrant neighbourhood. Building fronts and their orientation to streets and public open spaces are therefore the strongest determinants of the character and quality of neighbourhoods.

The design elements and approaches on the following pages provide a starting point and preliminary framework in maintaining and enhancing what is special, unique and wonderful about West Bay. They also provide a foundation to ensure new development and public realm improvements contribute to a vibrant, safe and accessible neighbourhood environment that encourages walking and street life while creating a unique social and physical environment that celebrates the beautiful natural setting of West Bay.



Photo: Joaquin Karakas



Photo: Joaquin Karakas

A Friendly Face to the Street

INTENT

- To define residential and commercial streets with active and attractive building fronts that have a positive orientation to streets and other public open spaces.
- To ensure buildings are sited and designed to be welcoming, and to encourage street vitality, visual interest, and safety.

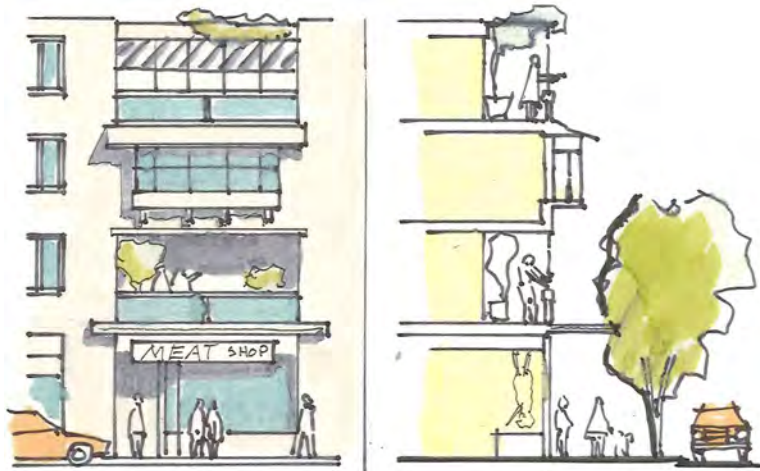
GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

- Locate publicly oriented active uses at grade and at or near the sidewalk edge.
- Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;
- A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.

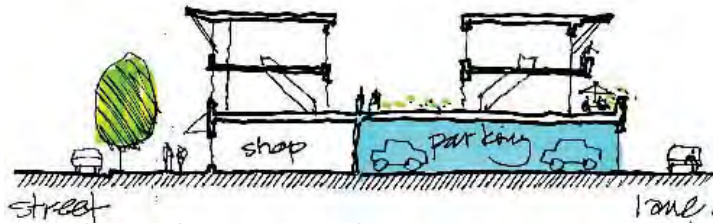


Active uses oriented to public streets and openspaces creates vibrant and attractive neighbourhoods (above, below and below left)





Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.

- Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.
- On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Locate on-street parking at the curb in front of shops.
- Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
- Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



On corner sites, develop street-facing façades for both streets. .

- Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.
- Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
- Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Photo: Joaquin Karakas

Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above and right)



Photo: Joaquin Karakas

Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

Photo: Dan Burden



Incorporate plantings and other landscape details such as banners.



Screen



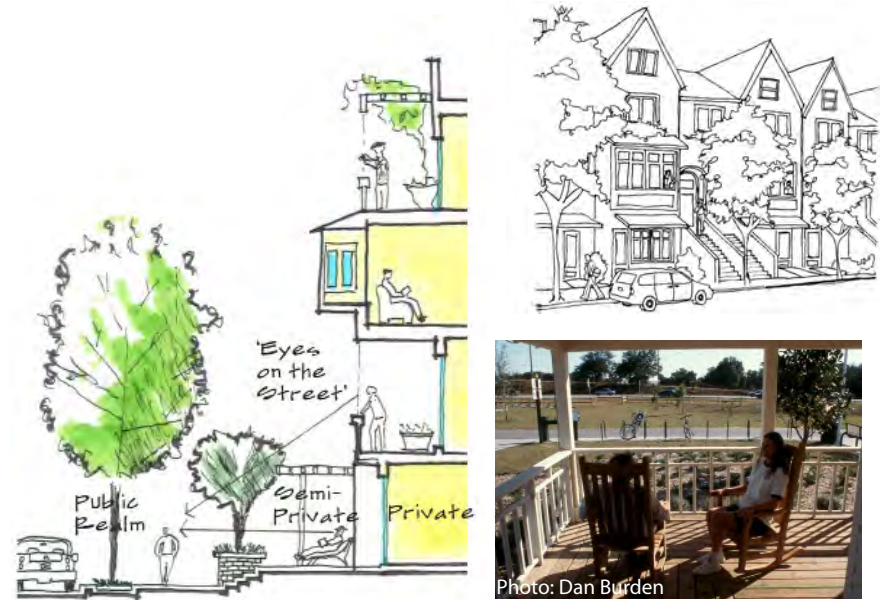
When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.

- Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:
 - » Install a vertical trellis in front of the wall with climbing vines or other plant material
 - » Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
 - » Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
 - » Employ quality materials of different textures and colours to make the wall more interesting visually
 - » Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
 - » Incorporate walls into a patio or sidewalk café space
 - » Terrace (step down) retaining walls



GUIDELINES: RESIDENTIAL BUILDINGS

- Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
- incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
- Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
- Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
- Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.



Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



Photo: Dan Burden



Photo: Joaquin Karakas



Photo: Joaquin Karakas



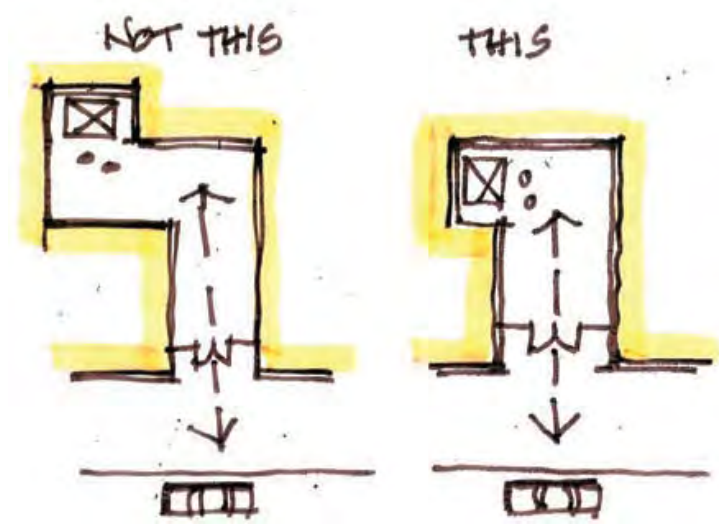
Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street. .



Photo: Joaquin Karakas

Consider using a landscape transition zone between entryways and public sidewalks.

- Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
- A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
- Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Direct sight lines into elevator lobbies are safer

VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
- New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.
- Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour
- Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's
- Mark/celebrate corners and street-end views through building and open space design.
- Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

- New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. "Flood Hazard Area Land Use Management Guidelines" (2004) (http://www.env.gov.bc.ca/wsd/public_safety/flood/pdfs_word/guidelines-2011.pdf).



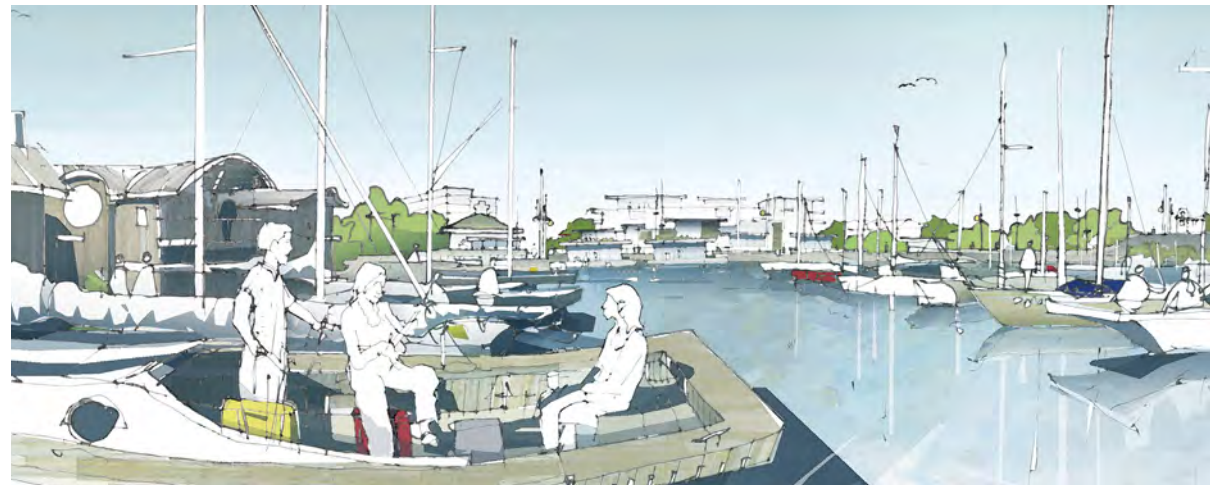
Photo: Bill Brown

Maintain street-end views to the harbour-front



Photo: Franc D'Ambrosio Architecture and Urbanism

Create new public connections to harbourfront uses and activities.

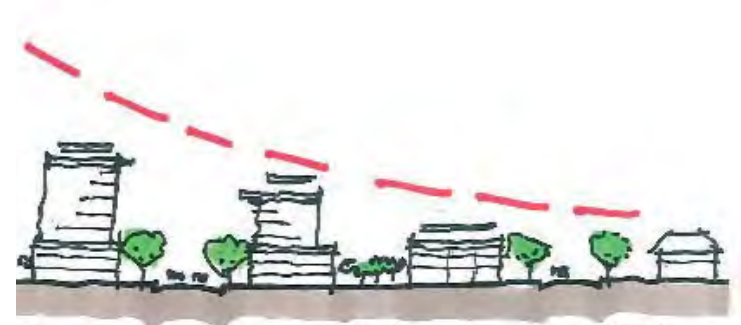


Neighbourliness

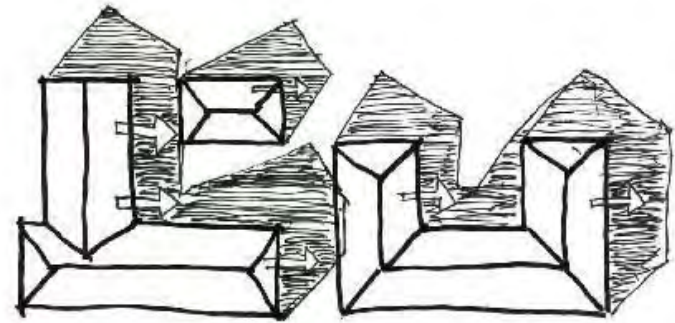
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

GUIDELINES

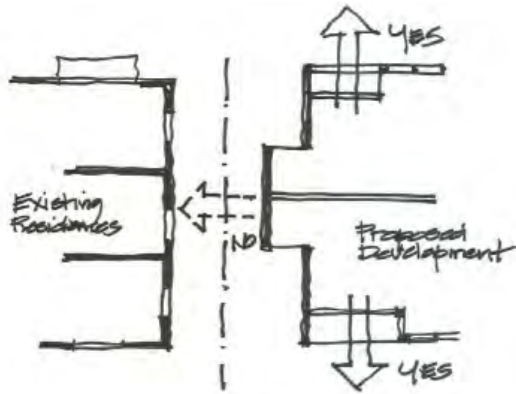
- New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.
- Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
- In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.
- Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
- Locate development to minimize view impacts on existing and planned future development.
- Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.
- Sun shade diagrams for the Winter Solstice will be required only when significant impacts are expected such as shading solar panels
 - » Winter Solstice: 9 a.m., 12 noon, 3 p.m.



Transition in scale to existing context



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and buildings



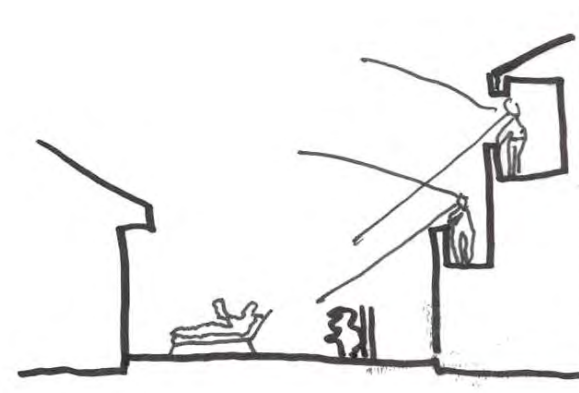
Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.

Photo: Joaquin Karakas



This building steps down to the slope to reduce the visual impact from it's massing

- Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
- Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
- Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
- Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 - » Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 - » Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 - » Stagger windows to not align with adjacent, facing windows.
 - » Primary windows into habitable spaces should not face interior side-yards



Minimize overlook into adjacent private yards

Architectural Concept: Achieving a Human Scale

OVERVIEW AND INTENT

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

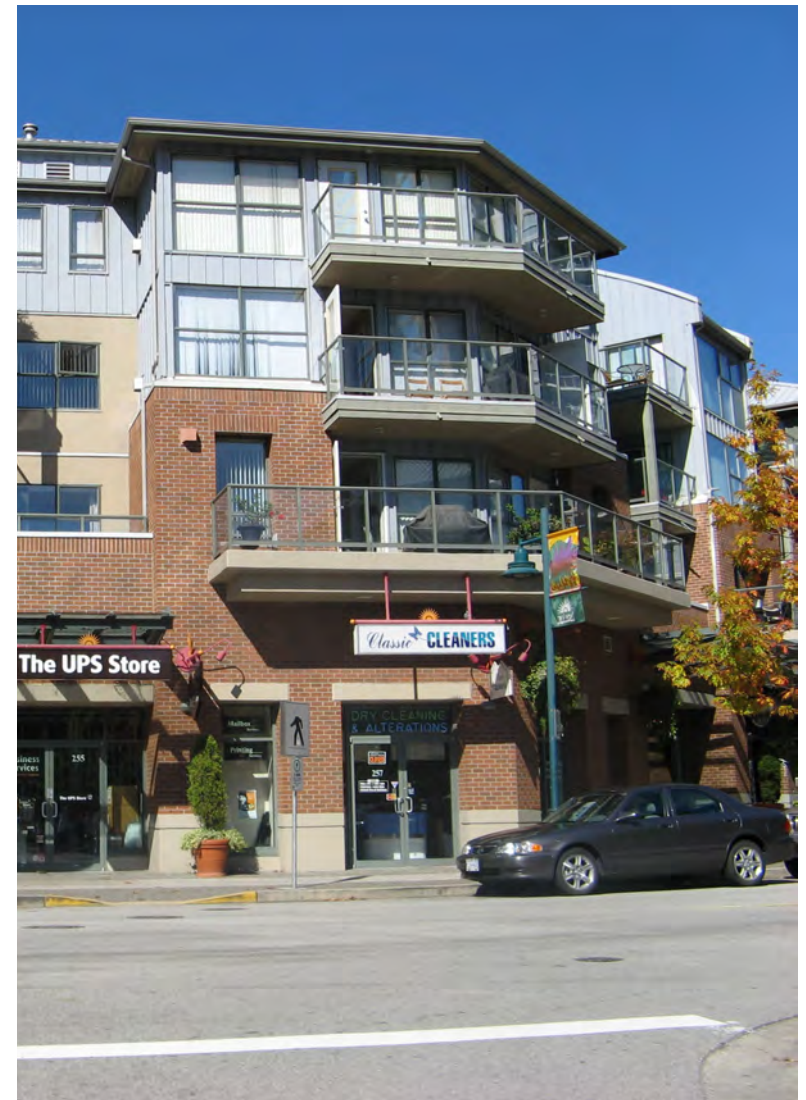
Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in a modern composition.

GENERAL GUIDELINES

- The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- Design buildings to express their internal function and use.
- Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.
- Examples of architectural features include:
 - » Building height, massing, articulation and modulation
 - » Bay windows and balconies
 - » Corner features accent, such as turrets or cupolas
 - » Decorative rooflines and cornices
 - » Building entries
 - » Canopies and overhangs
- Examples of architectural details include:
 - » Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
 - » Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
 - » Articulation of columns and pilasters
 - » Ornament or integrated artwork
 - » Integrated architectural lighting
 - » Detailed grilles and railings
 - » Substantial trim details and moldings
 - » Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians



Photo: Franc D'Ambrosio Architecture and Urbanism

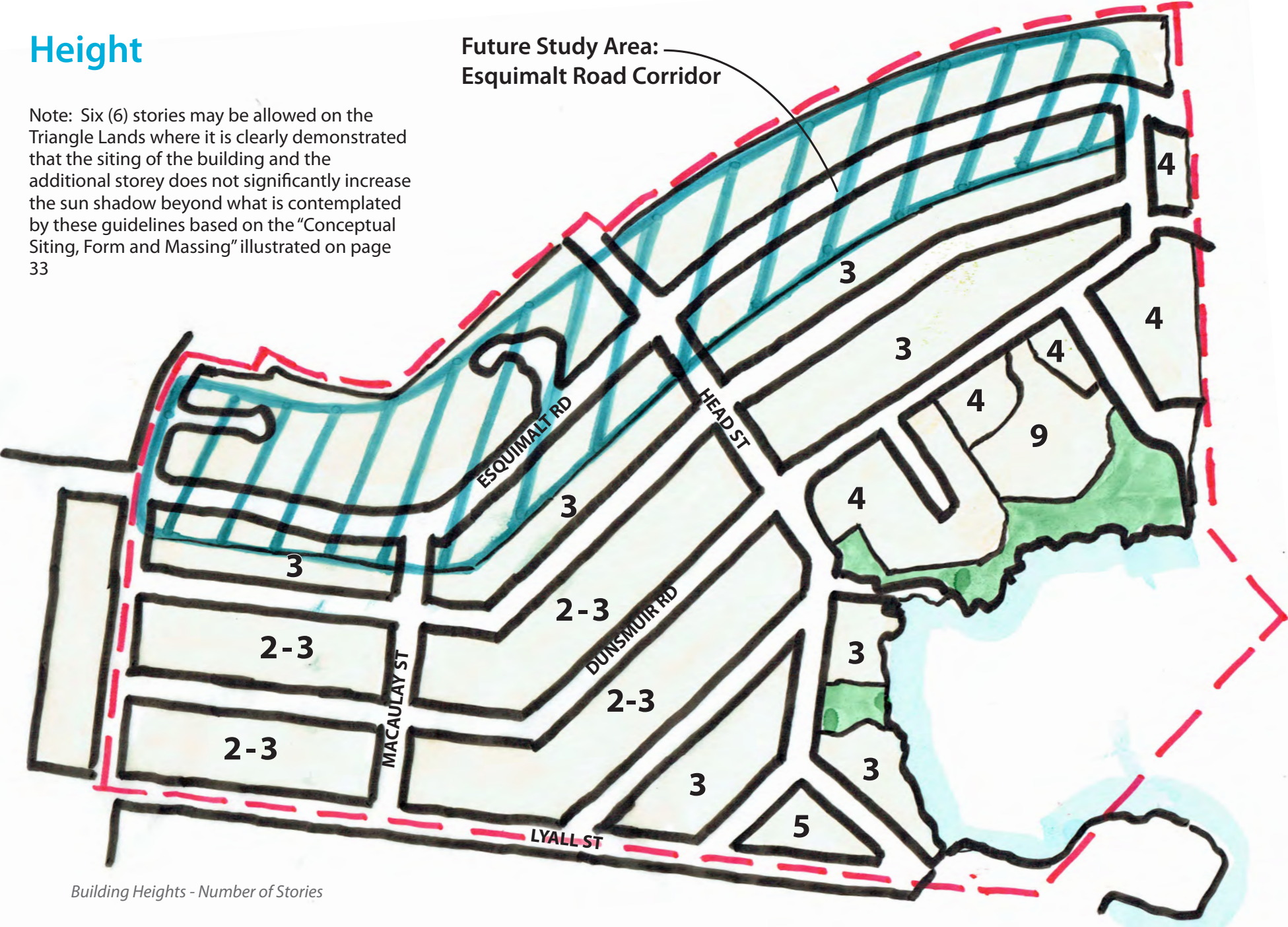
Design entrances to distinguish between individual commercial and residential.

- Locate and design entrances to create building identity and to distinguish between individual commercial and/or residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
- Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
- Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
- Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - » Are vertically proportioned and approximately the size and proportion of a traditional window
 - » Include substantial trim or molding
 - » Are separated from adjacent windows by a vertical element
 - » Are made up of small panes of glass
 - » Are separated with moldings or jambs but grouped together to form larger areas of glazing
- The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
- In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
- Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.

Height

Note: Six (6) stories may be allowed on the Triangle Lands where it is clearly demonstrated that the siting of the building and the additional storey does not significantly increase the sun shadow beyond what is contemplated by these guidelines based on the "Conceptual Siting, Form and Massing" illustrated on page 33

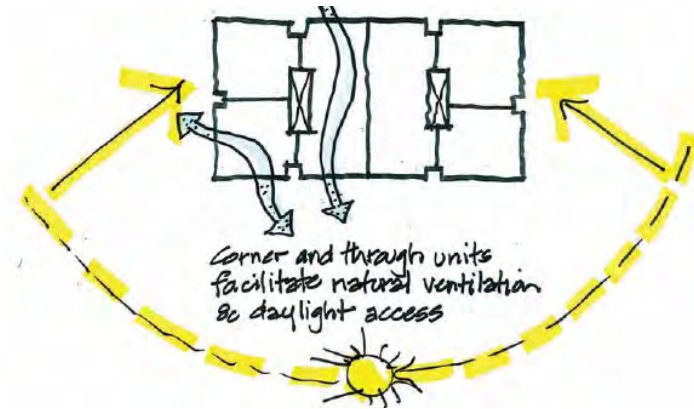
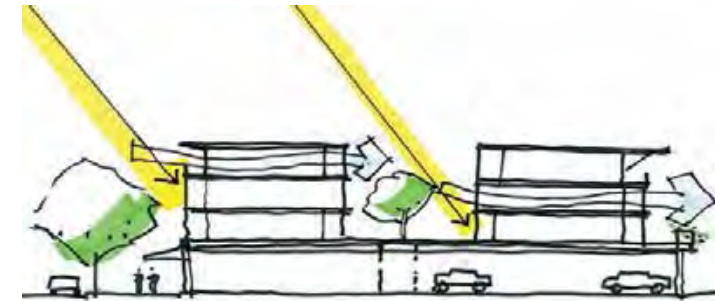
Future Study Area:
Esquimalt Road Corridor



Building Heights - Number of Stories

Green Healthy Buildings & Open Spaces

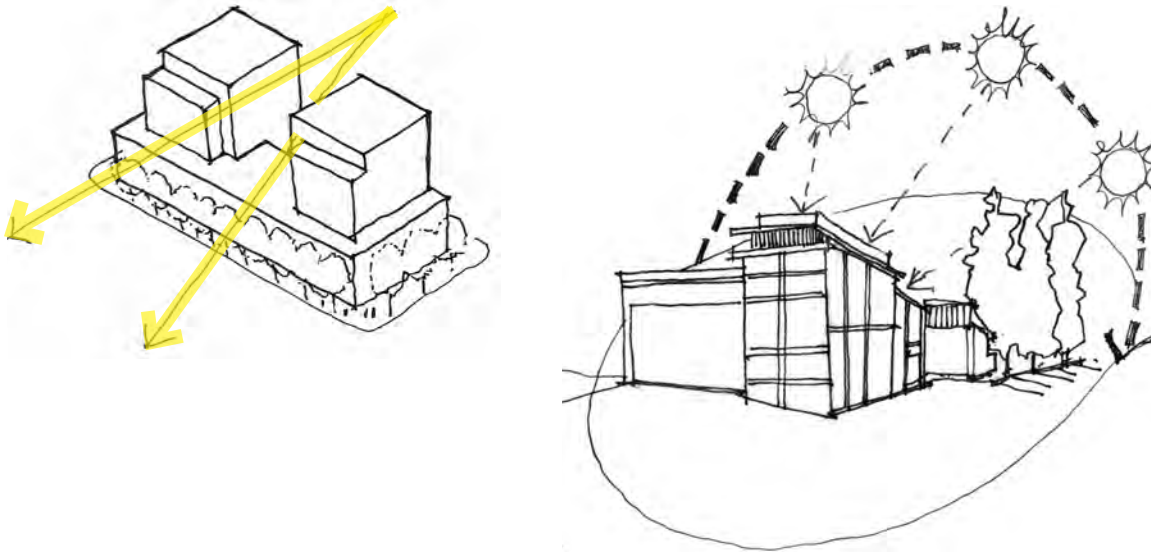
- Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.
- Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
- Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.



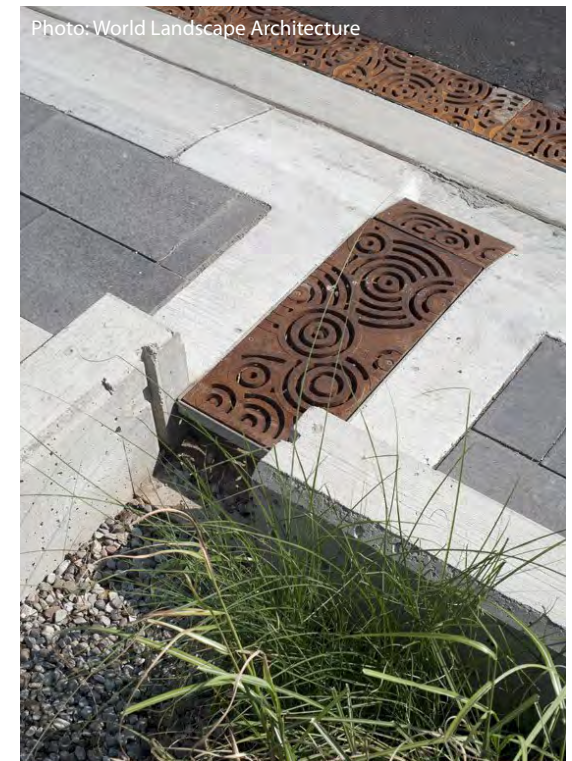
Incorporate common areas as defining elements of projects.



A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.



- New buildings should not block significant views or solar access to adjacent buildings and open spaces.
- Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
- Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
- Retention and infiltration best management practices for rainwater should be used as appropriate.
- Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.



Use best management practices for rainwater where appropriate.

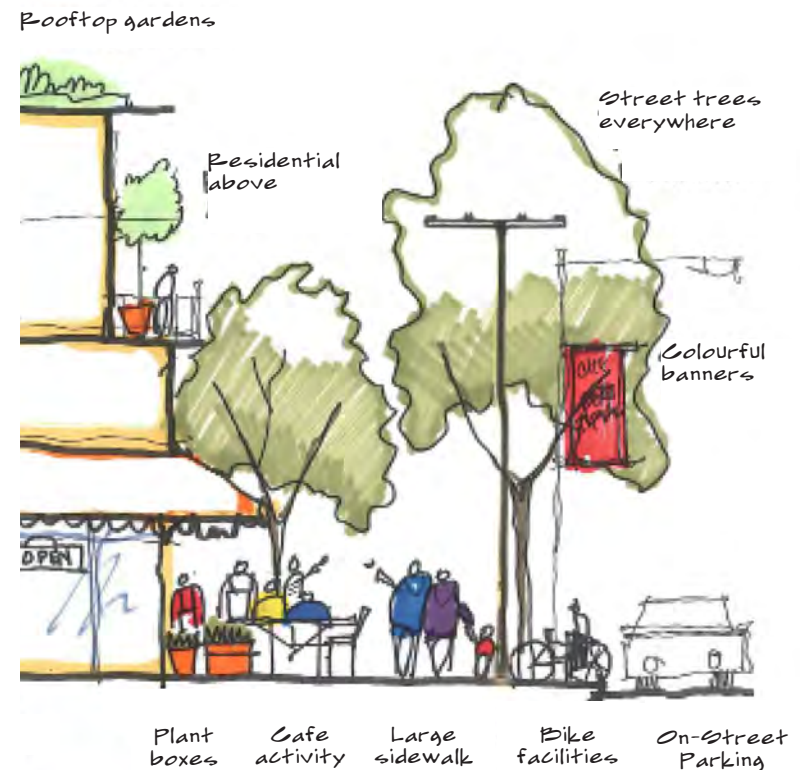
Public Realm Design: Pedestrian Comfort & Safety

INTENT

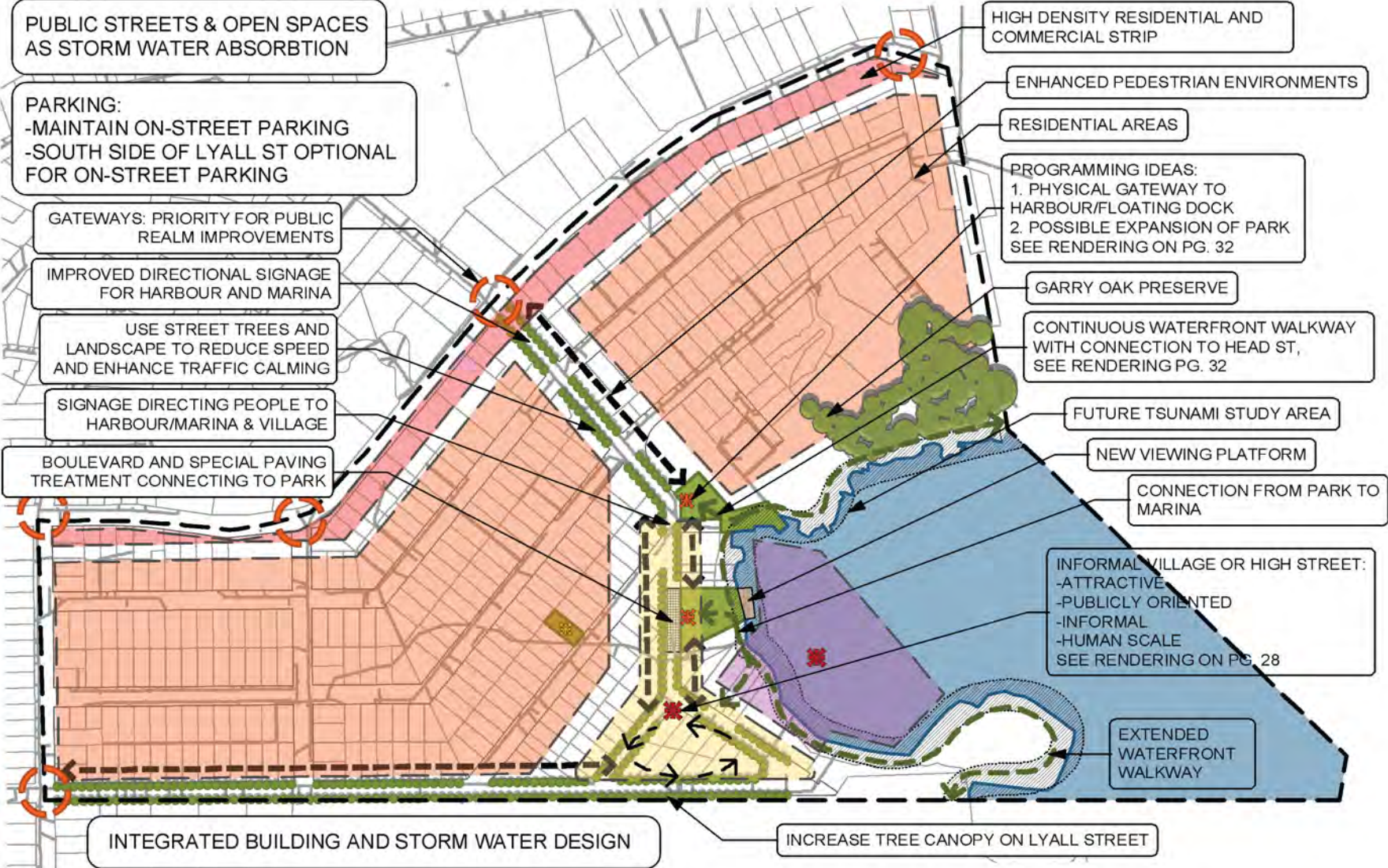
To ensure that the design of streets and open spaces creates visual interest, comfort, and safety for pedestrians and contributes to a unique local identity and sense of place for West Bay.

GUIDELINES

- Provide a continuous planting of street trees along both sides of neighbourhood streets, with priority for the Head Street South Village High Street and Esquimalt Road as indicated in the Recommended Public Realm Improvements map on page 25.
- Provide zebra- or ladder-painted crosswalks, or crosswalks made of special paving materials, at all key pedestrian crossings to increase driver awareness.
- Incorporate corner bulges into streetscape design in key pedestrian areas to enhance pedestrian crossings and provide space for landscaping, seating, rain gardens and public art.
- Provide adequate public streetscape amenities including benches, planters, garbage receptacles, bike racks, and distinctive bus shelters in areas with high pedestrian activity.
- Café tables are permitted and encouraged on public sidewalks in retail areas
- Pedestrian-oriented lighting should be provided with priority for high-activity pedestrian areas such as along Head Street and Esquimalt Road.
- Small plazas and squares should be incorporated in prominent locations with good solar gain, such as at Lyall and Head Street.
- Public art should be incorporated into parks, plazas and other key activity areas, especially at gateways and entry points, to enhance the sense of identity and entry into the neighbourhood.



Recommended Public Realm Improvements



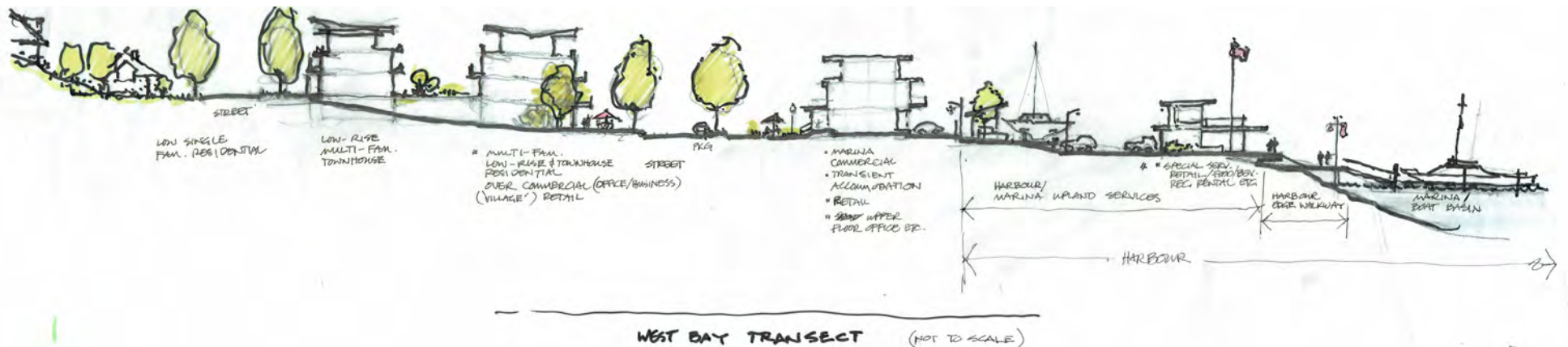
4. Site Specific Guidelines: Harbourfront Urban Village High Street

An important objective in the development of the design guidelines to address local concerns about density, height and massing, shadowing and view impacts, as well as the interface between federal lands and local ecosystems, specifically, in the harbourfront urban village area centred on Head Street South between Lyall and Paradise Streets. To this end, these guidelines were developed with input and feedback from neighbourhood stakeholders. This included an integrated design workshop (charrette) where design concepts and approaches for both public and private realms were developed. The sketch examples on this page were produced during this charrette.

This section is intended to provide site planning and design guidance and recommendations for this part of the West Bay Neighbourhood, based on stakeholder input. It is important to note that the concepts and illustrations on the following pages do not represent a development proposal, but rather a conceptual implementation of the preceding West Bay Neighbourhood Design Vision, Principles and Guidelines.



Sketches from the West Bay Design Workshop (above and below)



URBAN VILLAGE HIGH STREET LOOKING NORTH-WEST AT CORNER OF HEAD AND LYALL STREET

Maximum 5 storey building height



Urban Village high street defined by low scale buildings with active fronts and lush street tree canopy

Harbourfront retail, food and beverage uses

New public connection to harbourfront and Hidden Harbour Marina

Landmark corner plaza located at south gateway to urban village with views to harbourfront

URBAN VILLAGE HIGH STREET (HEAD STREET SOUTH) LOOKING SOUTH

Street trees, pedestrian oriented lighting and banners line either side of the street.

Continuous planting of street trees along south-west side of Head Street

Maximum 3 storey street wall along Head Street south

A rhythm of pedestrian oriented shop fronts and entryways line the street with sidewalk cafes and other uses spilling out onto the sidewalk

Landscaped bulb-outs and parking bays on north-east side of Head Street



Marked (zebra striped) pedestrian crossings at Gore and Lyall Streets

LOOKING NORTH TO HARBOUR AND URBAN VILLAGE HIGH STREET

*Publicly oriented active uses
activate the harbourfront*



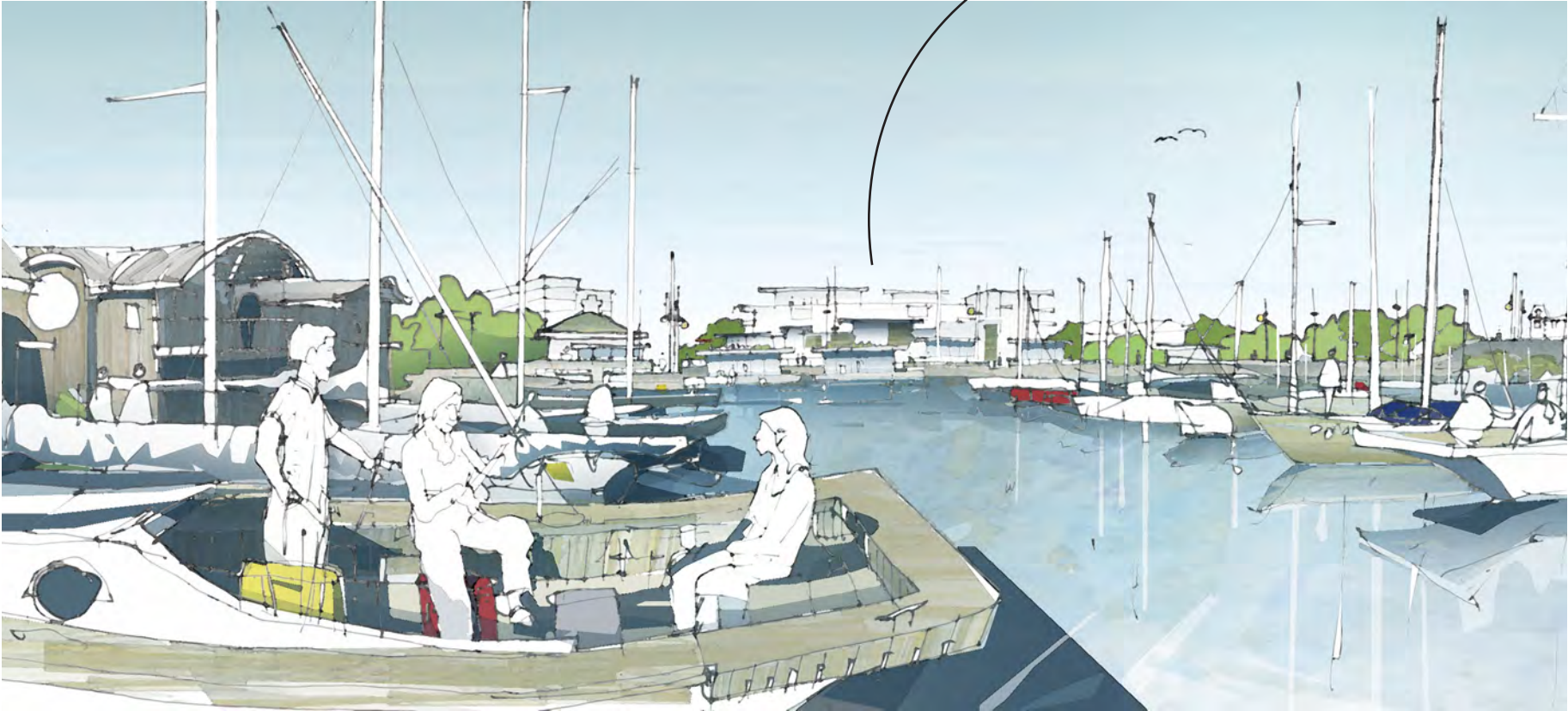
*Parking areas along the
harbourfront represent great
redevelopment opportunities where
appropriate*

*Continuous walking connecting
Songhees to West Bay Urban
village/marina and beyond*

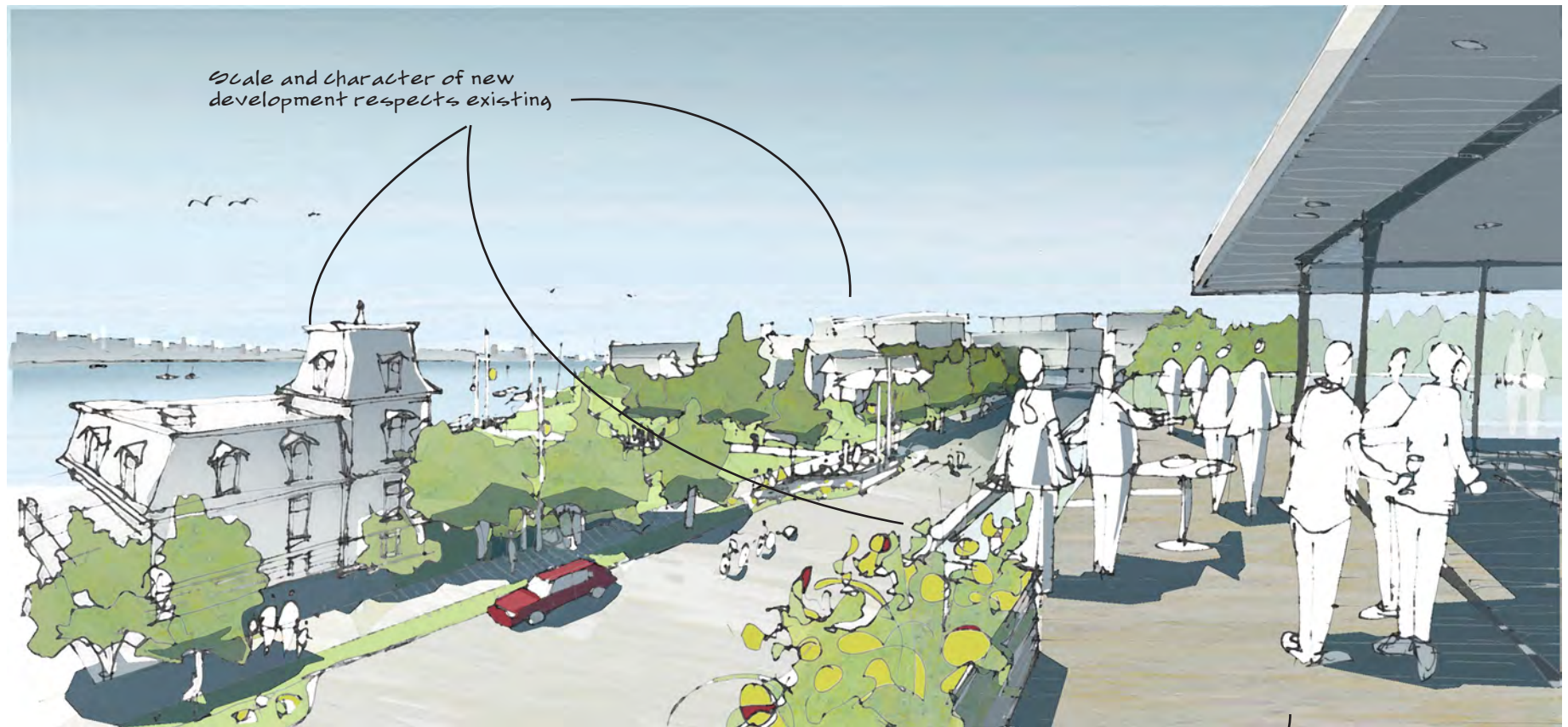
VIEW TO TOWARDS URBAN VILLAGE HIGH STREET FROM THE HARBOUR LOOKING WEST

Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity.

New development scaled to be compatible with, and not overwhelm, harbourfront character and identity. Also exhibits the transition massing from existing 7 storey on DND land.



VIEW FROM FUTURE BUILDING LOOKING SOUTH-EAST TO HARBOUR ACROSS URBAN VILLAGE HIGH STREET AND CAPTAIN JACOBSON PARK



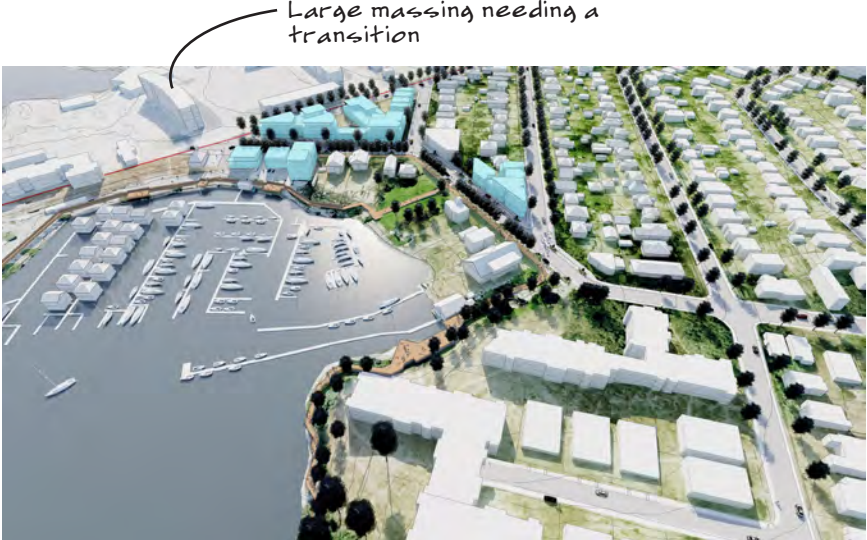
Scale and character of new development respects existing

Upper storeys step back to let sun onto urban village high street

HEAD STREET LOOKING SOUTH TOWARDS GORE AND LYALL

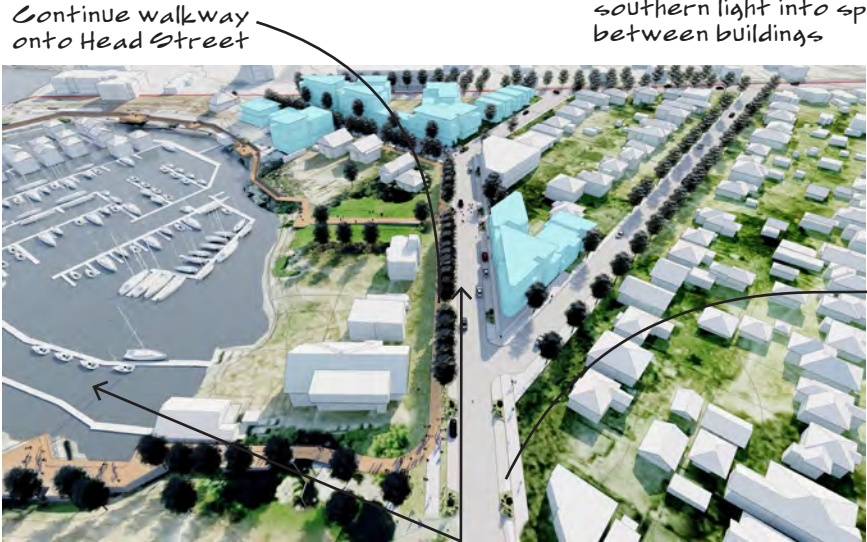


URBAN VILLAGE HIGH STREET: CONCEPTUAL SITING, FORM AND MASSING



Large massing needing a transition

- Legend**
- Conceptual massing
 - Continuous Boardwalk/Walkway



Continue walkway onto Head Street

Masses oriented to let southern light into spaces between buildings



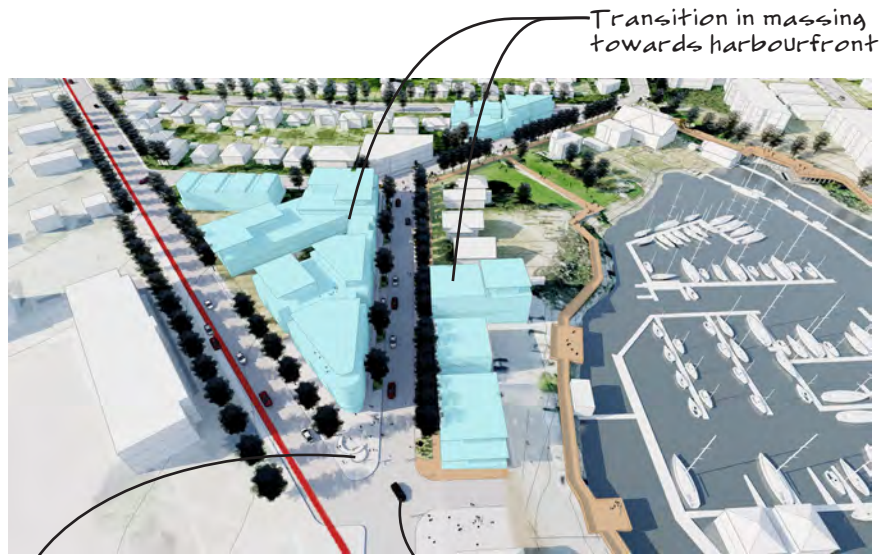
Continuous walkway

Public realm enhancements opportunity areas

New intersection

Improved view corridor

View on page 32.



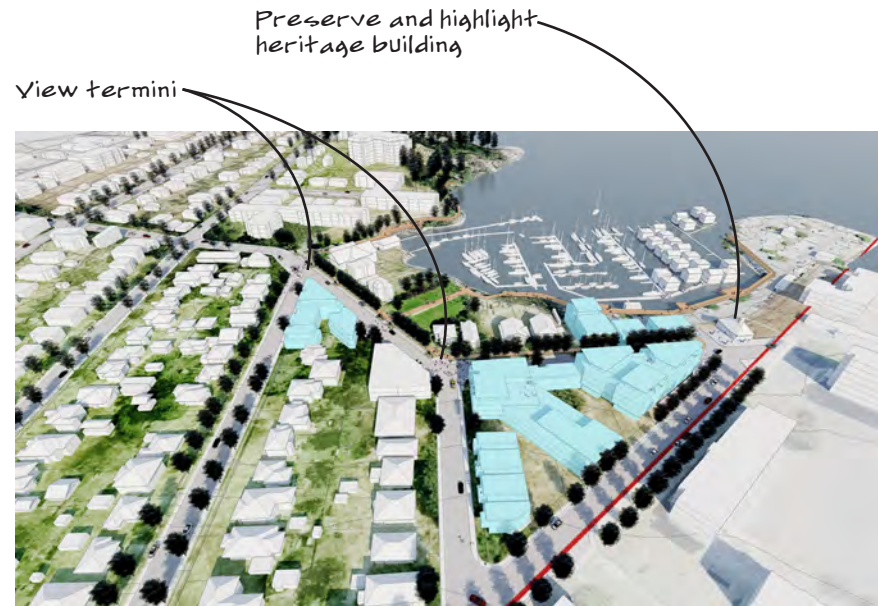
Transition in massing towards harbourfront

Landmark corner plaza located at South gateway to urban village, with views to harbourfront. See sketch on page 27.

New intersection



View on page 28



Preserve and highlight heritage building

View termini

Streetscape Guidelines

OVERVIEW

The design, construction and maintenance of streetscapes in the West Bay Neighbourhood should serve to reinforce its unique identity by incorporating:

1. Strong connection to the harbourfront;
2. A walkable neighbourhood;
3. Human scale;
4. Harbourfront ecology;
5. Harbourfront community & identity, and
6. Neighbourliness.

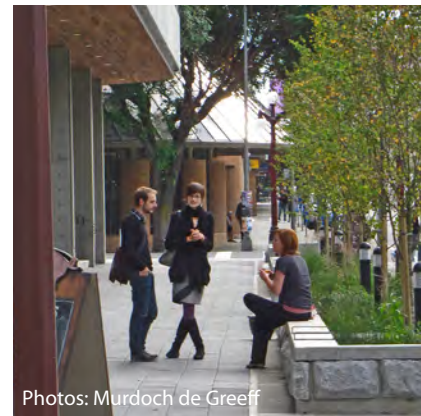
To illustrate the integration of these principles four typical streetscape sections and plans are presented below. These include Head Street North, Head Street South, Lyall Street West, and Lyall Street East. The sections focus on Public Realm improvements within two distinct areas of the West Bay Neighbourhood; West Bay Urban Village Gateways and West Bay Urban Village.

WEST BAY URBAN VILLAGE GATEWAY

West Bay Urban Village is accessible by Lyall Street and Head Street. Both streets currently have 18m wide Right of Ways (R.O.W) and accommodate two way traffic in standard 4.25m wide lanes. Objectives for the Lyall Street and Head Street Gateways include: improving the pedestrian environment, maintaining on-street parking, improved wayfinding, increased tree canopy, and integrated stormwater management. The Lyall Street West section shows a typical treatment through an existing residential lot on Lyall across from the Department of Defense (DND) lands. The Head Street North section shows a typical treatment near the intersection of Head and Esquimalt.

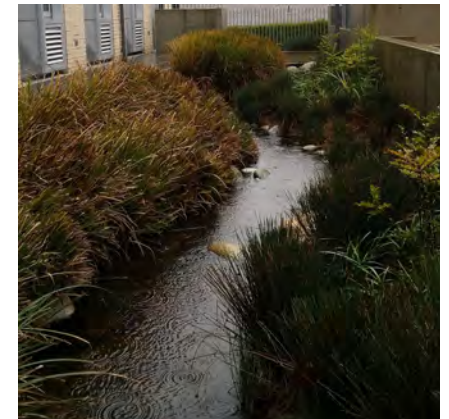
WEST BAY URBAN VILLAGE

West Bay Urban Village is located on the harbour front adjacent to the Marina. The area is identified for potential future development with a primary focus on the 'Triangle Property'. Some of the objectives for the Urban Village include: increased density, creation of places to gather, an inviting and active public realm, and reduced speed/traffic calming. Both the Lyall Street East section and Head Street South section show typical treatments at the triangle property and suggest a 'Skinny Streets' approach with reduce travel lane widths (3.0m)



Photos: Murdoch de Greeff

Integrated Storm Water Management, The Atrium building, Victoria BC



Integrated Storm Water Management, Reliable Controls building, Victoria BC

Integrated Storm Water Management

Integrate rain gardens, swales, tree planting, and other suitable BMP's to capture, slow, and treat storm water before it enters the harbour.



Photos: Murdoch de Greeff

Storm Water Plaza,
Cecelia Road, Victoria BC



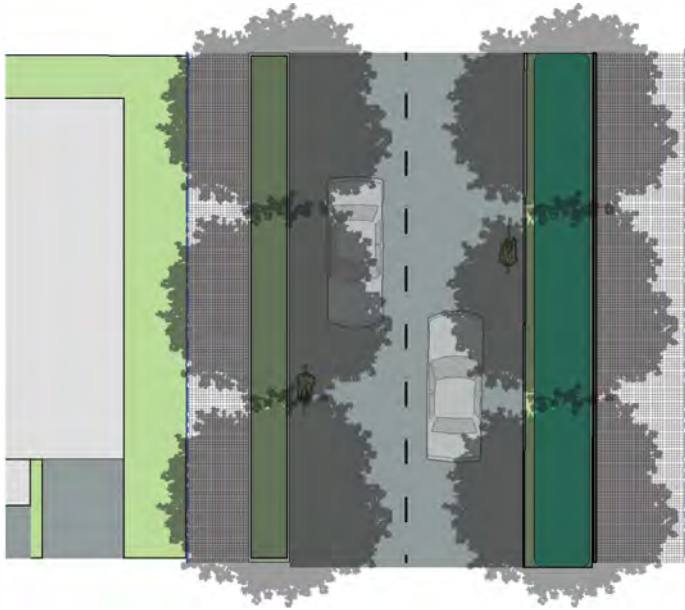
Photo: Paul de Greeff

Storm Water Streetscape,
Portland, Oregon



Photo: Murdoch de Greeff

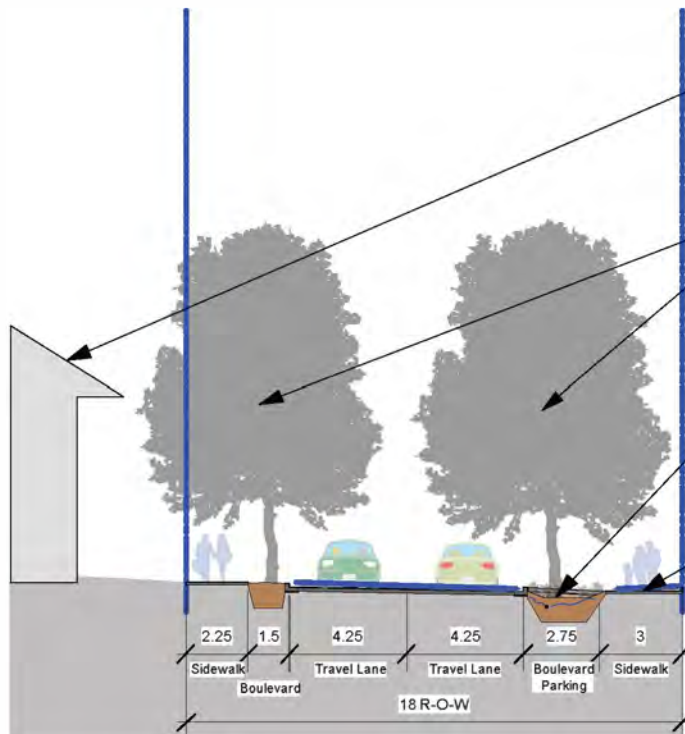
Storm Water Traffic Bulge,
Trent St., Victoria BC



Head Street North

Head street North, near the intersection of Head and Esquimalt Road, is intended to act as a Gateway to the Village, Harbour, and Marina. The intent on this section of Head is to increase the appeal of the pedestrian environment, employ clear wayfinding strategies to direct traffic towards the Village and/or Harbour/Marina.

Strategies to achieve this include: Increased sidewalk width to encourage walking, a double row of trees to accentuate Head as a linear corridor to the harbour/village, source controls such as onstreet rain gardens to treat stormwater, and similar building scales to reinforce the street end/harbour as a focal point. Development fronting onto the east side of this section of Head street should maintain physical and visual permeability to the waterfront by integrating paseos and achieving a minimal building separation of 6 metres



Neighbourliness-

Transition building heights from shoreline to upland areas to minimize shadow and view impacts.

Human Scale-

Utilize a double row of trees to accentuate the linearity of Head Street as a Gateway and the Village as a destination at the terminus.

Upland Ecology-

Install onstreet rain gardens to capture, treat and slow street run off before it enters the harbour downstream.

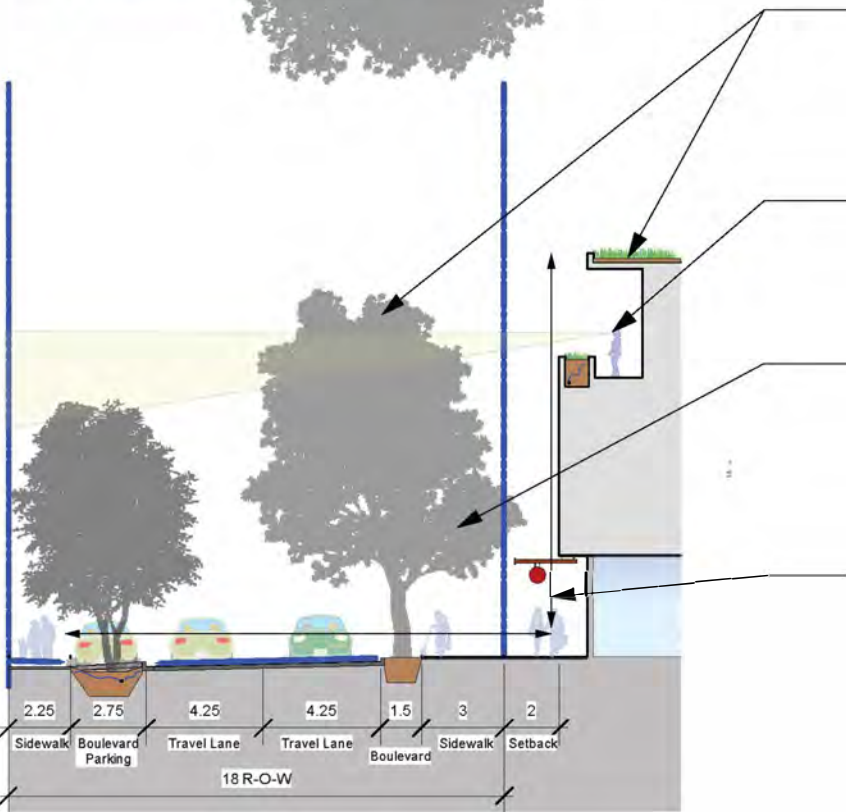
Walkable Neighbourhood-

Utilise vegetated boulevard strips to provide separation from vehicular traffic and increase sidewalk widths on gateway streets to promote wayfinding while contributing to attractive and inviting pedestrian environments.

Head Street South

Head street South, is intended to act as the communities Village or High Street. The intent on this section of Head is to provide public open spaces that contribute to a sense of being on the waterfront while supporting the commercial viability of the area. The *West Bay Village* public space should include but is not limited to waterfront access, space for gathering and festivals, and an attractive pedestrian oriented environment.

Strategies to achieve this include: *Distance to Building Height Ratios* of between 1:1 and 3:1, use of green building techniques to preserve the health and visual aesthetic of the shoreline, and transitions in scale to perserve upslope and adjacent views.



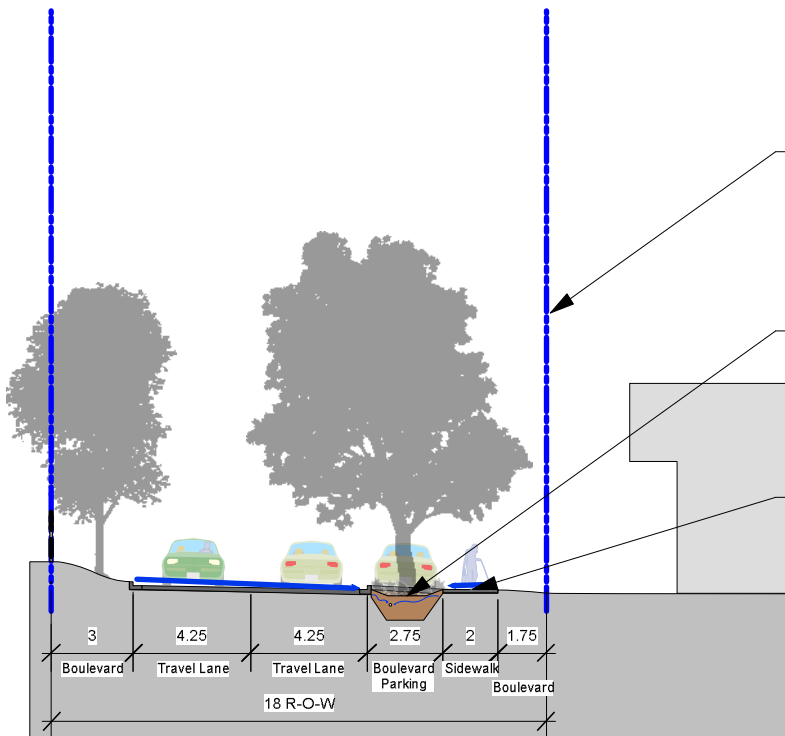
- Waterfront Community and Identity-**
Green Roofs, street trees and building heights support each other to reinforce the identity of West Bay as a Green Waterfront Community. Designs acknowledge views both to and from the inner harbour.
- Strong Connection to the Waterfront/Neighbourliness-**
Buildings step back and transition in scale to maximize potential for harbour views while minimizing shadow impacts on adjacent properties.
- Waterfront Ecology-**
Tree plantings intercept rainwater, reduce storm water flows through evapotranspiration, and provide valuable bird habitat contributing significantly to the shoreline ecology.
- Human Scale-**
Buildings should be between 1:1 and 3:1 *Distance to Building Height Ratio* to create intimate and inviting public open spaces.

Head Street South
Scale: 1:200



Lyall Street West- Existing Residential

West Lyall Street acts as an important connection from the harbour to the surrounding residences, Esquimalt village, and the Esquimalt Rec. Center. Future developments along this corridor should focus on improving the pedestrian environment to increase the walkability of the neighbourhood. Strategies to achieve this include: Tree planting to reduce the visual scale of the street and provide shade, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour downstream, and separated boulevards to increase the attractiveness of the pedestrian environment.



Human Scale-

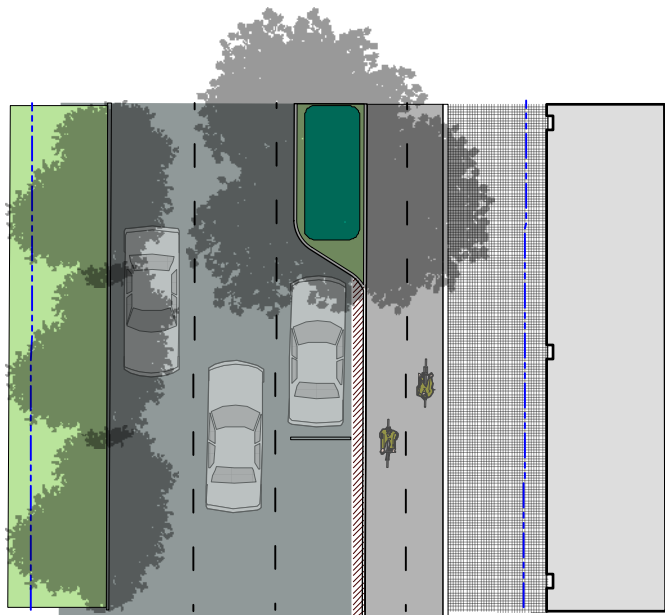
Tree planting can reduce the visual scale of the street while offering summer shade for the street and sidewalk.

Waterfront Ecology-

Integrated rain gardens to treat stormwater and provide habitat for birds and pollinators.

A Walkable Neighbourhood-

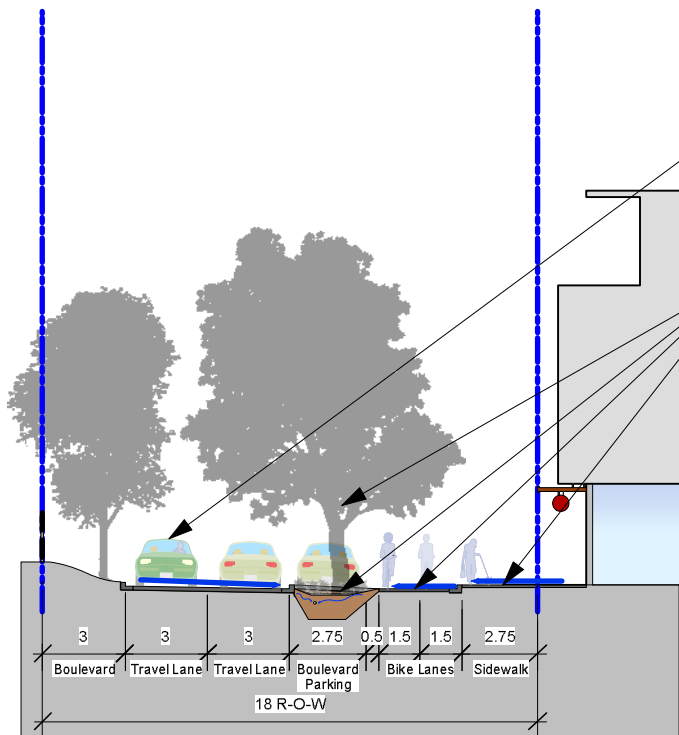
Use separated boulevards to remove sidewalks from vehicular traffic and increase the attractiveness of the pedestrian environment.



Lyall Street East- West Bay Village

East Lyall Street at the harbour interface is meant to act as part of the *West Bay Village*. The intent here is to contribute to the sense of a waterfront village while providing an attractive pedestrian environment with places for festivals and gatherings. Located adjacent to the DND lands Lyall street, in general, should focus on pedestrian improvements on the North side which is more heavily used by the public.

Strategies to achieve this include: Tree planting to reduce the visual scale of the street, vehicular travel lane reductions to provide space for bike lanes and encourage alternative modes of transportation, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour, and seperated boulevards to increase the attractiveness of the pedestrian environment.



Walkable Neighbourhood

Skinny Streets help to maximize pedestrian oriented space and allow for increased sidewalk widths as well as seperated bike lanes.

Waterfront Ecology-

Significant tree planting, green infrastructure, bike lanes, access corridors to waterfront recreation, clear wayfinding, and attractive pedestrian spaces should be layered and integrated to create a vibrant community, ecologically healthy shoreline, and healthy upland ecology.

20 January 2017

Mayor and Council
c/o Trevor Parkes, Senior Planner
Development Services
Township of Esquimalt

RE: Westbay Triangle Development Permit
Redevelopment of 468 Head Street, Westbay, Esquimalt, BC

Hillel
architecture



101 1831 Oak Bay Avenue
Victoria BC V8R 1C3

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Mayor and Council, c/o Trevor Parkes, Senior Planner

It is a pleasure for Hillel Architecture to enclose herein a Development Permit Application for the Westbay Triangle for the following independent land titles being amalgamated under this Development proposal into a single land holding:

468 Head Street [Triangle Estates],

PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];

PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];

PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];

PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];

PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];

PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];

PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];

PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street];

and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].

Enclosures

3 copies 24x36" copy of The Triangle at Westbay

1 copy 11x17" copy of The Triangle at Westbay

digital copies of same

digital copies of a Green Building Checklist (original submission reproduced)

digital copies of this DP application cover letter.



Hillel Architecture was retained in August of 2014 to chart a new course for the Development of the Westbay Triangle. Hillel Architecture engaged in a comprehensive community consultation process, involving neighbouring residents, Esquimalt Planning and Development staff, Esquimalt Engineering staff, the Esquimalt Chamber of Commerce, and numerous one on one interviews which defined an outcome for the Westbay Triangle as a whole. This consultation process resulted in a program document and an illustration of a concept recorded in our Westbay Triangle Development Guidelines. From this body of work, and within that consultation process, a rezoning and development proposal was illustrated for one portion of the land holdings known as the Westbay Triangle, and partially developed for the remaining half to be submitted at a later date, now known as Westbay Marinaview.

Westbay Triangle Rezoning Application

The Rezoning Application received broad based support from residents, local businesses, local off shore residents and patrons of the Westbay Marina, Esquimalt Planning and Economic Development Staff. At that same time Esquimalt engaged in a separate and independent neighbourhood consultation to create the Westbay Community Development Guidelines for the community as a whole. As a new document, it was expected that the Westbay Triangle Development Proposal would also conform to that independently developed guideline. Planning Staff noted in their project analysis that the Rezoning Submission did conform to that document expressing the desires of the neighbourhood.

Through its own consultation with Neighbours, Esquimalt Staff, and later with the members of APC, DRC, the proposal for the Westbay Triangle evolved subtly, each time improving, and each time not wavering from its original form as developed with, and supported by those earlier participants: the neighbours. The Westbay Triangle went on to receive its Public Hearing in the summer of 2016, without one voice of objection.

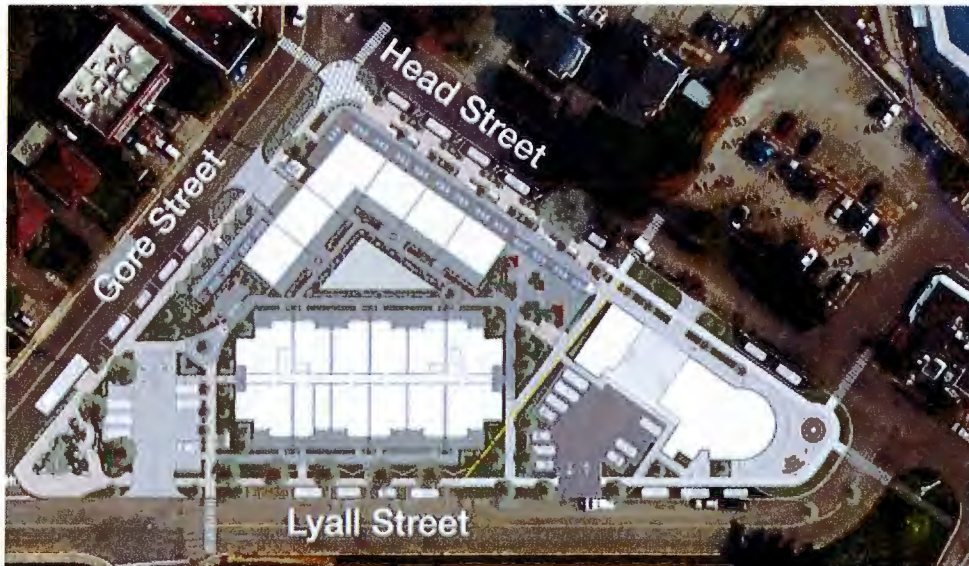
As that Rezoning successfully approached its conclusion, the second half of that same land holding, known as Westbay Marinaview, was developed further to meet the requirements of its own independent rezoning application. This submission is in keeping with - and conforming with - those same principals established in the Westbay Community Development Guidelines produced by Esquimalt, and through Hillel Architecture's own consultation process. The owners of Westbay Marinaview applied for this rezoning in January 2017.



Westbay Triangle Development Permit Defining Character and Role

The Westbay Triangle Development Permit application drawings present a building unchanged from its initial rezoning application drawings. It was paramount to the consultant and the owners, that the building submitted for Development Permit honoured that broad community support, staff support, respected that it received thoughtful review from the DRC, Esquimalt Staff, and concluded with Council approval at its rezoning stage.

The Triangle also can now be reviewed with the total composition for the Triangle Lands. Marinaview and the Triangle play differing roles in the definition of the new Westbay Village core. The Triangle defined the dynamic village centre. The approximate length of this village core aligns with the locations of heritage style buildings on the opposing side of this village streetscape.



The project's most visible corner, that at the intersection of Gore and Head Street - the new "main street" - is designed to stimulate growth along this portion of Head Street, and re-invigorate, re-energize these buildings and businesses further along Head Street towards Captain Jacobson Park and onwards to the terminus of the Songhees Walkway. This public space, with its integral compass rose, is intended to attract pedestrians, provide seating and visual cues this this is the center of the village core.

Subtle design cues create a perception that the village setting does not extend up Gore Street. The exterior stairwell, the vehicle entry, and some significant planting beds serve to protect Gore from the dynamics of the village propagating up Gore Street. These aid in keeping this a more quiet residential side street as it is preferred.

Further along Head street towards the Marina, the character changes. The village setting captured between the Triangle and the existing heritage style buildings comes to a natural conclusion at the transition between the Triangle and Marinaview.

In the Triangle's village area the focus is internal, on the dynamics of a shopping setting. People watching. Pedestrians shopping. Socializing and perusing. Interesting buildings. Interesting people. Interesting things. The character changes to one of a more open view towards the water. An ocean view. A Marina view. This is not the end of the village, it is a change in character and perspective.

As you walk past the Triangle, the shopping character changes, canopies lower in scale, and projection lessens. In front of the second phase of the Triangle lands the enclosed village evolves into the perspective of open, bright, ocean views and marina views. This is the home of Marinaview. Marinaview is slightly more modern, a little more crisp, its finishes slightly more refined in tone. Where the Triangle needs to be comfortable with the existing heritage style buildings, Marinaview adds another layer of fit and finish and relates to the open sightlines, the modern lines of boats, the decidedly more contemporary float homes visible in the background. Although further description of Marinaview's roles should not be outlined here. It is important that the reader know, that the same level of thought that defined the character and finishes of the triangle are equally at work in Marinaview, only now just entering its rezoning phase; the younger sibling. Its more contemporary lines and use of materials permits the next contributions to the neighbourhood's composition to be truly modern if that is those designers intent. The stage has been set.

Defining Site and Setting

The total composition, of both Phase I; the Triangle, and phase II; Marinaview contribute three distinctly differing corner public spaces at the road intersections. The dynamic center village intersection of Gore and Head Street with its very public setting combined with people, seating, shop front displays, and attractive landscaping. The sites and sounds of the village core.



Along Head Street the village setting at Lyall has a bright open public space, with long sightlines over the water, plants and seating in the open all day long, sun washed, public social space. Here another food and beverage company gets to offer a fuller menu, a licensed eatery, for longer conversations. This portion developed within Westbay's Marinaview



Away from the water, further up Lyall Street, the Triangle concludes with a quiet, restful, lush garden setting encouraging of quieter reading, quieter conversation, and protection from the on shore breezes. Here the location is a quiet residential neighbourhood. Here the last orchard tree of the original local farm is protected, for it to serve out its years in an organic composition and natural setting.



Along each of these three street fronts, each street has its own character respected, each is deserving of being a front yard, and each has a substantial front yard setback now incorporated in the zoning description. These set backs provide the physical distance to achieve an outdoor dynamic market place one side, privacy protected residences on the other two street fronts. Each generously landscaped to suit their differing roles and characters



The greenspaces

The Triangle design creates three distinct elevated private greenspaces, 3 differing characters, distributed to permit some in sun and some in shade at all times.

One with views out over the water and suitable for larger social gatherings;



One breeze protected and internal in focus, linear in nature with small seating elements; and finally,



one overlooking a quiet residential community with a more plant oriented, green and natural setting. Surrounding the property, planting areas contribute purposefully differing settings.



There are areas designed for the enjoyment of people and socializing, while others exclude people to increase privacy, increase the abundance of, and diversity of flora and fauna around the Triangle

Defining a neighbourhood

The Triangle offers a range of residential unit sizes, views, and features. Residences are designed to suit a range of incomes, a range of lifestyles, a range of family sizes, and to suit a range in ages. This diversity aids in creating a full neighbourhood, a complete community. This extends to the business units that are small in scale, ensuring their shop fronts match the scale of those already present, and their stage set invigorates the streetscape. The outside realm encourages uses, permits retail product display and / or patrons seating, all essential to a dynamic village setting.

Architectural Expression – The village

A palette of materials, and building details have been considered that define the character and aesthetics of the Triangle, both the village setting, and above, for the residences. The materials are used in a manner that is honest, practical, and consistent with hardworking marine buildings along our coastlines.

On the ground floor the doors and entryways of the commercial shop fronts are serviced by openings configured to serve this location appropriately: pedestrian doors for patrons, and larger sliding wood door assemblies permitting the passage of larger retail items, and for openness on fair weather days. These larger wood panel, glazed doors are defined with surface mounted hardware of a scale appropriate to these marine based buildings. The DRC could discuss the use of black framing to these commercial openings, which would also differentiate the commercial building portion from its lighter residential character above.



Overhead canopies, significant in span (3m), and scale, protect outdoor display and furnishings space. That interstitial space between the public realm and the shop itself. A dynamic multi-use space providing protection from heat, sun, rain, sound from travelling vertically to residences above, and to define that stage setting for the shop front. These canopies change in scale and span to communicate when the village setting is changing as you approach Marinaview along Head Street.



The village core features real stone column facings, heavy timber bracketing supporting exposed heavy timber framing above. Solid wood decking is visible on the underside of the canopy deck, and cedar siding finishes the upper portion of the shop front wall. This also ensures that wood finishes mostly removed from the affects of rain, increasing their longevity, and minimizing their maintenance.



Steel detailing, structural supports, and exposed structural brackets are fashioned from hot dipped galvanized steel for a maintenance free, finish free, industrial aesthetic. This is matched with light fixture enclosures and mountings.

Whereas in the finer detail of signage and its mounting, lettering, and building addressing, these items can be marine fittings and with finishes appropriate to the marine industry and including polished stainless steel.

Door hardware and hinges that occupy the scale between these two extremes are chosen on grounds of robust design, and corrosion free materials, and forged steel aesthetic.



Architectural Expression – The Residences

On the upper floors, the residential building layers are enclosed with the same materials executed in a lighter, more contemporary form. Where the Shop fronts below define an industrial like character rich village core at home with heritage style buildings of the streetscape, the upper floors communicate a slightly more contemporary form. Not modern, but sufficient to permit the next buildings added to this community to be so. The Triangle bridges that character change. The Triangle forever being the center of the village core, and the older sibling. The Lyall Street frontage of the residential building contains similar structural members, in a similar but more refined surface and scale. Here clear anodized framing defines entries and window systems.



Spandrel glass in appearance, glazed panels between windows aid in breaking exterior finishes into pairs of floors, as does stucco, and stone appointments – each running in differing twin storey arrangements. Together the materials brighten with height, lightening the visual nature of the building, lessening the perception of individual floors. The residential building concludes with a well setback, less visible penthouse floor.

This penthouse, when visible from distance is a distinct richer colour palette, complete with its own significant canopies from overhanging cedar tone finished soffits. Here where distance permits, and codes require, surfaces are non-combustible finishes appearing as cedar from below and matching in tone and character actual cedar used in the lower stories of the village level. The overhangs of canopies in this upper most floor tie the character to that of the lowest floors.

Summary

The enclosed building conforms with statements within Comprehensive Development District No. 98 [CD NO. 98] without variances. The development permit application is an extension of the rezoning process without building redefinition to suit a new preference or new intent. The Building represents that which was previewed by the neighbourhood, and in commitment to that neighbourhood's support, it is submitted here unchanged in form, character, or materials.

Throughout the rezoning process of this project, these building elements have received thoughtful commentary from the DRC, Planning, members of the Chamber of Commerce and those members of the community, and support by Mayor and Council. This submission honours that input and the rezoning that resulted. The drawings being only being more comprehensively labelled as is required by an escalating level of detail on its way towards construction documents.

Regards
Peter Hardcastle

Hillel Architecture Inc.

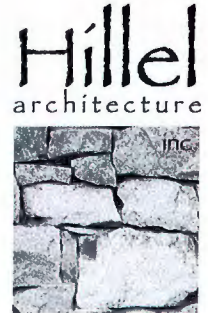


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20 January 2017

Mayor and Council
c/o Trevor Parkes, Senior Planner
Development Services
Township of Esquimalt



101 1831 Oak Bay Avenue
Victoria BC V8R - 1C3
phone 250 . 592 . 9198
fax 250 . 592 . 9178

RE: Westbay Triangle Development Permit
Redevelopment of 468 Head Street, Westbay, Esquimalt, BC

Design Review Committee, c/o Trevor Parkes, Senior Planner

The Design review Committee meeting held February 9th, 2017 reviewed the materials presented for the Development Permit application for the Westbay Triangle, located:

- 468 Head Street [Triangle Estates],*
- PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];*
- PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];*
- PID 000-036- 722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];*
- PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];*
- PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];*
- PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];*
- PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];*
- PID 009- 175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street];*
- and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].*

Enclosures

- 1 copy 11x17" complete printout of The Triangle at Westbay
- digital copies of same
- DRC response cover letter.

Hillel Architecture was pleased to hear the compliments expressed by members of the design review committee on the development as a whole, general statements regarding form and character, and a few mentions of appreciation for specific elements of the Westbay Triangle. The panel members also expressed three specific items, that they felt were worthy of further review or design editing for presentation again to panel members in their next regularly scheduled meeting to be held March 8th, 2017.

Those items to be further developed, discussed or redesigned in response to DRC commentary were summarized to the following three items;

- that consideration be given to the elimination of the surface parking at Gore and Lyall Street,
- that the landscape plan be amended to include 11 stated mandatory street trees,
- that consideration be given to enclosing the Triangle portal to permit enhanced green space.

Consideration of Surface Parking

The area in question is the surface parking area located within the triangle at the intersection of Gore Street and Lyall Street, located behind an area of significant plantings introduced during the DRC review process at the rezoning application of this proposal.

This surface parking area was formerly enclosed with low perimeter walls and DRC felt at that time that the parking was best openly expressed, with an increase in perimeter landscaping elements permitting a greening of this vicinity. DRC members are reminded that below this area is a military services culvert over which we cannot build nor prevent access preventing further development potential below grade for this parking concealment.

Additional commentary at that time lead to the removal of two surface parking stalls, and the introduction of significant scale tree species to aid in the transitioning from lower single family homes further up Lyall Street to the multifamily building. In addition, the view corridor was to benefit from that significant tree canopy serving to further conceal the multifamily building from view. With these two items amended the project continued to its public hearing and ultimately received its rezoning in this configuration. That rezoning now contains a significant number of legal documents based on this parking location, and by that same rezoning submission, including a survey of exterior building envelop setbacks and overhangs to each floor, all of which would be amended should an attempt to further enlarge any portion of the building shell to conceal a portion of those 12 parking stalls and their attendant manoeuvring aisles. It is felt that any redesign in this vicinity risks developing a solution that may trigger a new rezoning application.

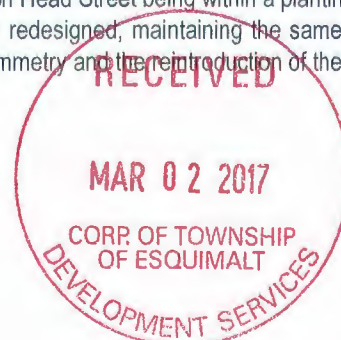
In consideration of the guidelines that discourage visible surface parking, in a typical "building" development this is welcomed. In developing a village however, a larger composition, and one designed to attract newcomers to this new town centre consideration must be given to providing hints as to the location of parking. Portions of this public guest parking have been concealed from these streetscapes by their location within the development, accessed through its port-cochere or pass through. Portions of this guest parking are purposefully placed in close proximity to the HC entry of the multifamily building and the terrace park level permitting access to all site pathways.

In response to the concerns noted, Hillel Architecture, Esquimalt Planning, Esquimalt Civil Engineering, and Esquimalt road and Traffic Engineering reviewed Lyall Street road design which has resulted in approximately 2.3 meters of additional landscaping buffer space be placed between the road and this surface parking. More of this significant road redesign is disclosed below under landscaping and street tree design amendments.

Consideration of

The notes from the planning staff had drawn DRC members attention to the requirement for the site plan to be amended to provide the mandatory 11 street trees, with their requisite tree grates and tree collars. In response to the concerns noted, Hillel Architecture, Esquimalt Planning, Esquimalt Civil Engineering, and Esquimalt Road and Traffic Engineering reviewed the design parameters of all three streets, which included sidewalk and road edge locations on all three sides, but significantly on the Lyall Street road design.

- Lane width, lane locations, and resultant parallel parking lane placement and sidewalk placement edited on all three road frontages.
- Sidewalks on all three sides were increased to in excess of the 2m standard referred to in the WBCDG.
- along each street face municipal sidewalks are placed against the property line in response to Esquimalt engineering commentary.
- 12 street trees are now demonstrated with 11 being placed in tree collars, with tree grates as now bound by legal agreement, with the 12 tree on Head Street being within a planting bed.
- Gore and Head Street sidewalk meeting is redesigned, maintaining the same design elements in principal, however graphically rebuilt with symmetry and the reintroduction of the compass rose.



•Along Lyall Street the removal of public parking along the property edge of the military base, permitted Lyall Street traffic lanes and the parallel parking along this property edge to move over $\pm 2.6\text{m}$. This results in a significantly greener edge along all of Lyall street, and predictably increasing the greenery between driver and the surface parking in question in this vicinity.

This same benefit then also extends to the surface parking at marinaview.

• with permission from Esquimalt, each parking entry will now have a twin of street trees and low maintenance planting bed to each side of vehicle entries, increasing their attractiveness, yet also providing screening from view of vehicles.

• The enclosed organic shape protecting the heritage tree at Gore and Lyall Street was edited to respect preliminary SRW agreements now signed and legally binding.

• on site "mini triangle" at the back of surface HC stalls at the confluence of three pathways was edited to improve geometry, and permit three lines of more equal passage.

Consideration for the infill of the Triangle

The open triangle has been a design feature since this projects inception, and has proven to be a valued design consideration throughout its various presentations (17) with the exception of both its presentation at the DRC initially during the rezoning stage, and again at the DRC under this DP application stage.

Rezoning application is based on this triangle being open, and disclosed data, agreed to at the public hearing, would significantly change with this enclosure. This roof enclose will result in building area growing 691.8 m^2 or 7437 ft^2 , generating an increase in site coverage, building area, and a change to the projects calculated Floor Space Ratio. Any one of which triggers a rezoning application as at development permit stage these items are not permitted to increase those approvals in place.

This long span structure would impose significant costs by itself, and in addition would create the need for CO2 monitoring, and exhaust systems serving that newly created 7437 ft^2 which would be located above in the central triangle. This joining of two independent but adjoining buildings into one floor plate risks the exterior finishes along Head Street being changed to non-combustible finishes greatly affecting the very design elements most valued by all along this Head Street commercial frontage.

The design concern however is still a valued one to hear and respond too. From the parkside townhouses, or from the slightly lower elevation of the ground floor of the multifamily units this portal is not visible. From the floors above this portal plays a lesser role in the units views in general, however their sightlines would include the views down through this portal. From this direction fabric sails have been added to effectively screen this opening, maintain its open nature and permit its earlier design objectives to also be met.

Summary

We appreciate the thoroughness and care taken to review this Development Permit and look forward to its review.

Regards
Peter Hardcastle
Hillel Architecture Inc.





DRAWING LIST

Architectural Drawings Enclosed

- A1.1 Existing Site and Site Data
- A1.2 Graphic Project Summary
- A1.3 Site Development Layout
- A1.4 Site Development Finishes
- A1.5 Graphic Summary of Setbacks

- A2.0 Underground Parkade Layout
- A2.1 Bldg 1 - Ground Floor @ Head Street Layout
- A2.2 Bldg 1 - Ground Floor @ Head Street Finishes
- A2.3 Bldg 1 & 2 Ground Floor @ Lyall Street Layout
- A2.4 Bldg 1 & 2 Ground Floor @ Lyall Street Finishes
- A2.5 Bldg 2 Floors 2nd through 5th floors
- A2.6 Bldg 2 Penthouse (Sixth) Floor Plan

- A2.7 Building 1 - Suite Floor Plans
- A2.8 Building 1 - Suite Floor Plans
- A2.9 Building 2 - Suite Floor Plans
- A2.10 Building 2 - Suite Floor Plans
- A2.11 Building 2 - Suite Floor Plans

- A3.0 Proposed Project Perspectives
- A3.1 Neighbourhood Perspectives
- A3.2 Building 1 - Head Street [northeast] & Gore Street [northwest] Elevations
- A3.3 Building 1 - [southeast] & [southwest] Elevations
- A3.4 Building 1 - Detail Elevations and Finishes

- A3.5 Building 2 - Lyall Street [south] Elevation
- A3.6 Building 2 - North Elevations
- A3.7 Building 2 - Gore Street [west] & East Elevations
- A3.8 Building 2 - Detail Elevations and Finishes

- A4.1 Building 1 & 2 - Section Facing [northwest] Gore Street
- A4.2 Building 1 - Section Facing [south] Lyall Street
- A4.3 Building 2 - Section Facing [northeast] Head Street

- A4.4 Building 1 - Colour & Material Board
- A4.5 Building 2 - Colour & Material Board

Landscape Drawings Enclosed

- L1 Landscape Concept
- L2 Landscape Sections & Planting

CONTACT LIST

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THE TRIANGLE AT WESTBAY

ESQUIMALT, BRITISH COLUMBIA





1 Existing Site
A1.1 Scale 1:500

CIVIC ADDRESS

CIVIC ADDRESS
472, 470, 768 Head Street. 515, 509 Gore Street. 922, 920, 918, 912 Lyall Street.
Please note; 464, 480 Head Street are not included in this submission. Separate ownership.

PROJECT DATA

ZONING: PROPOSED PROJECT

DENSITY

Site Area (m²) 4,075m² (43,865 R2)
 Lot Coverage % 54.8% (2235.3 m² measured to outside face, buildings & all canopies)
 Floor Space Ratio 63% (2572.7m² as measured above and including central park)
 1.34 (5420 m² developed / 4075 site m² / January 2017)
 Building Footprint Building 1 and Building 2 combined: 2235.3 m²
 • areas based off exterior face of exterior walls
 • includes all exterior decks, canopies, & cantilevers
 Building 1 and Building 2 combined: 2572.7 m²
 • includes all noted above plus interior elevated parks / terraces
 Total Floor Area Building 1: Esquimalt definition for floor space ratio calculations
 See diagrams A1.2 Ground Floor: 632.1 m²
 2nd Floor: 400.0 m²
Total Floor Area: 1,092.1m²

Building 1 Ground Floor breakdown:	G101: 62m ² d / 59.5m ² inside	Building 1 Second Floor breakdown:
CRU1: 102m ² d / 97m ² inside	Group C, D (live work)	P201: 59.5m ² d / 59.5m ² inside
Group A, Group D, Group E	2 bedroom residential suite	Group C, Group D
CRU2: 62m ² d / 59.5m ² inside	G102: 62m ² d / 59.5m ² inside	P202: 97m ² d / 59.5m ² inside
Group D, Group E	Group C, D (live work)	Group C, Group D
CRU3: 62m ² d / 59.5m ² inside	G103: 62m ² d / 59.5m ² inside	P203: 97m ² d / 59.5m ² inside
Group D, Group E	Group C, D (live work)	Group C, Group D
CRU4: 62m ² d / 59.5m ² inside	G104: 62m ² d / 59.5m ² inside	P204: 97m ² d / 59.5m ² inside
Group D, Group E	Group C, Group D	Group C, Group D
CRU5: 62m ² d / 59.5m ² inside	G105: 62m ² d / 59.5m ² inside	P205: 97m ² d / 59.5m ² inside
Group D, Group E, Group C	Group C, Group D	Group C, Group D
Live or work unit	Live or work unit	Live or work unit
CRU6: 62m ² d / 59.5m ² inside	G106: 62m ² d / 59.5m ² inside	P206: 97m ² d / 59.5m ² inside
Group D, Group E, Group C	Group C, Group D	Group C, Group D
Live or work unit	Live or work unit	Live or work unit

Building 2:
 Level 1 714.8m²
 Level 2 - 5 756.7m² Each of 4 floors
 Level 6 585.9m²
Total Floor Area: 4327.5 m²
 (areas based off definition: inside face of exterior walls + deductions)

Maximum Height Building 1: 21.75m ± above average grade 7.4m (7.3875m geodetic)
 Building 2: 7.6m ± above average grade 7.4m (7.3875m geodetic)
 Maximum Storeys Building 1: 2 storeys
 Building 2: 6 storeys

SITE SETBACKS

Head Street •2.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 38
 Drawings demonstrate ±5.8m to bldg face (±6m to grid)
 Gore Street •illustrated similar to 2.0m page 38 of development guidelines
 Drawings demonstrate ±5.8m to bldg face (±6m to grid), 3.0m to Exit Stairs
 Lyall Street •1.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 40
 Drawings demonstrate ±5.8m
 Interior Side Yard •0.5m demonstrated to internal sideyard

RESIDENTIAL UNITS

No. of Residential Units **73 total residential units**
 Building 1:
 Ground Floor: 2 units Level 6.2m facing Gore Street
 2nd Floor: 6 units Level 9.2m facing Park Terrace
8 total
 Building 2:
 Ground Floor: 11 units
 2nd Floor: 12 units
 3rd Floor: 12 units
 4th Floor: 12 units
 5th Floor: 12 units
 6th Floor: 6 units
65 total
 Building 1:
 8 - two bedroom residential units
 Building 2:
 17 - two bedroom residential units
 48 - one bedroom residential units

LEGAL ADDRESS

488 Head Street (Triangle Estates),
 PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 (488 Head Street);
 PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 (470 Head Street);
 PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 (472 Head Street);
 PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 (515 Gore Street);
 PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 (509 Gore Street);
 PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (522 Lyall Street);
 PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (520 Lyall Street);
 PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (918 Lyall Street);
 and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 (912 Lyall Street).

PARKING (assignment of parking allocations shown sheet A2.0)

CRU 1 @ 14m ² / 1 stall / 5 seats	12 stalls (inside + outside seating)
CRU 2 @ 25m ²	2.38 stalls (highest parking use assumed)
CRU 3 @ 25m ²	2.38 stalls (highest parking use assumed)
CRU 4 @ 25m ²	2.38 stalls (highest parking use assumed)
CRU 5 @ 25m ²	2.38 stalls (highest parking use assumed)
CRU 6 @ 25m ²	2.38 stalls (highest parking use assumed)
Total commercial:	23.8 stalls (20-24 stalls / leasing dependent)
-3 stalls equivalent deducted for parking bylaw item 11.2 (-15% commercial stalls permitted to share residential guest stalls) -2 stalls deducted for parking bylaw item 13.5 (providing bicycle facilities) 18.9 commercial stalls req'd - 22 provided	
3 spaces in excess of Bylaw based on predicted tenancies and their parking needs assessed by space use.	
Total Residential 73 x 1.3 each:	94.9 stalls req'd - 97 stalls provided.
Residential Guest Parking	25% assigned as visitors - 24 stalls 2 spaces in excess of Bylaw
Total Project provided:	119 stalls provided (5 in excess of Bylaw requirements) (assignment of parking allocations shown sheet A2.0)

Parking Bylaw Standards referenced:
 Cafe @ 14m²
 or 5 seats per stall
 Grocer @ 35m²
 Retail @ 25m²
 Office @ 30m²

15% of required commercial parking can be met by use of residential guest parking stalls during business hours

Residential 1.3 each
 25% to be assigned as visitors stalls

Commercial Loading Stall Not required / less than 700 m² commercial space

parking stalls on site 15 parallel street parking stalls provided on municipal property and not counted

BIKE PARKING

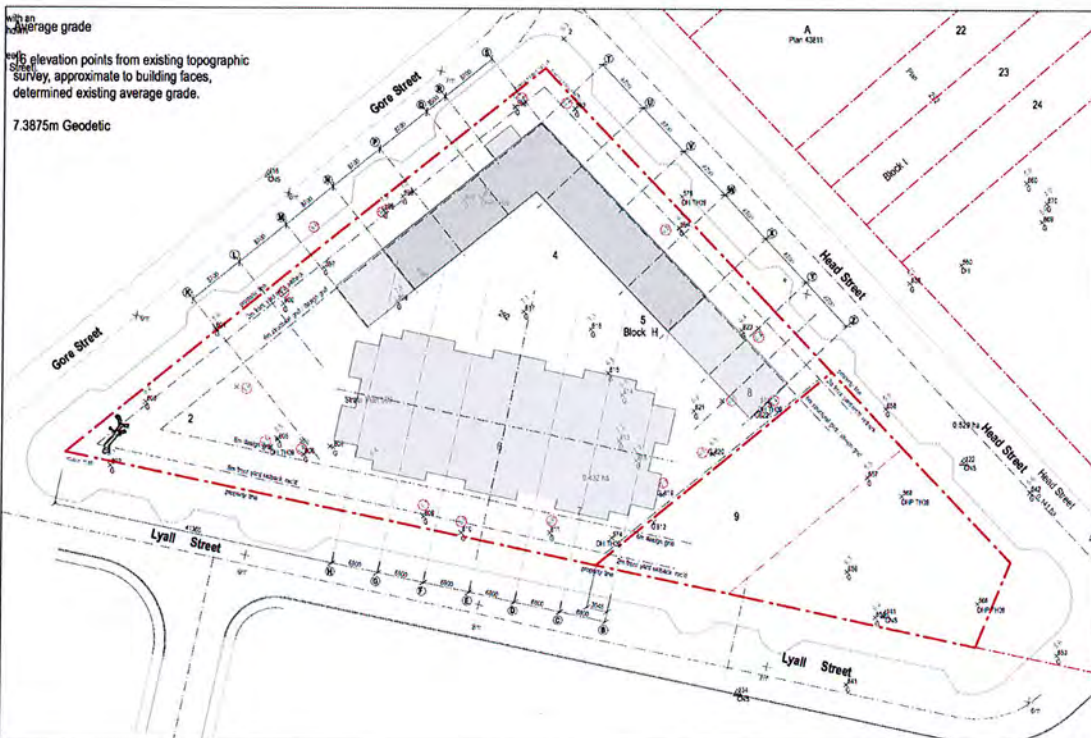
Bicycle parking 110 min req'd (73 x 1.5 per residential unit) 116 shown. Exceeds:
 Esquimalt OCP, Section 2, Managed growth, 2.2.4.1, page 16
 Commercial bicycle parking 9 provided (1.5 per commercial tenant) exceeds:
 Class I (enclosed in bldg) Esquimalt OCP, Section 2, Managed growth, 2.3.1.1, page 18
 Electric bicycle charging 2 locations provided for residents, exceeds Bylaw
 Class I (enclosed in bldg)
 Motorcycle / scooter parking (gas fueled scooters / motorcycles) • covered or enclosed
 • 9m x 1.8m stall : 19 stalls (exceeds Bylaw requirement)
 surface public parking 4 stalls
 parkade level P1: 8 stalls
 parkade level P2: 7 stalls
 total: 19 stalls
 Electric motorcycle / scooter charging (rechargeable battery operated) 3 locations provided, exceeds Bylaw
 Guest bicycle parking: Residential Class II (outdoor racks) 6 guest bikes at residential entry. As per:
 Esquimalt OCP, Section 2, Managed growth, 2.2.4.1, page 16
 Guest bicycle parking: Commercial Class II (outdoor racks) 6 guest bikes at Head and Gore intersection. Exceeds:
 Esquimalt OCP, Section 2, Managed growth, 2.3.1.1, page 18
 Bike and Motorcycle user facilities (showers, laundry) Bikers and Mariners shared laundry and showering facilities (commercial employees, residents, marine lease holders only)

BUILDING STORAGE

Residential Storage 75 residential storage lockers provided (min 1 per residential unit)
 Additional Storage Rooms - 6 units
 23.5m² storage room 1 (parkade P2 level 2.9m grade)
 23.5m² storage room 2 (parkade P2 level 2.9m grade)
 (revenue producing storage units, for 6 commercial tenants on ground floor adjacent to commercial use spaces)
 28.5m² storage room 3 (parkade P1 level 5.9m grade)
 28.5m² storage room 4 (parkade P1 level 5.9m grade)
 28.5m² storage room 5 (parkade P1 level 5.9m grade)
 28.5m² storage room 6 (parkade P1 level 5.9m grade)

488 Head Street (Triangle Estates),
 PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 (488 Head Street);
 PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 (470 Head Street);
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 PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (522 Lyall Street);
 PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (520 Lyall Street);
 PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (918 Lyall Street);
 and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 (912 Lyall Street).

Original Survey Source
 McIlvaney Riley Land Surveying Inc.
 #113 - 2244 Sooke Road
 Victoria, B.C. V9B 1X1
 (250) 474-5538
 www.mrls.ca



1 Existing Site
A1.1 Scale 1:500



12 March 2017	Approved by SDC Committee
16 January 2017	Development Permit Application
05 April 2016	Planning Committee
06 December 2015	Final Reg. Submission
June 2014	Final Reg. Submission
2014	2014
2014	2014

Permission is granted for these drawings to be reprinted by Lindholm Land and Investment Corporation

Hillel architecture

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 Victoria BC V8R - K3
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 fax 250-592-998

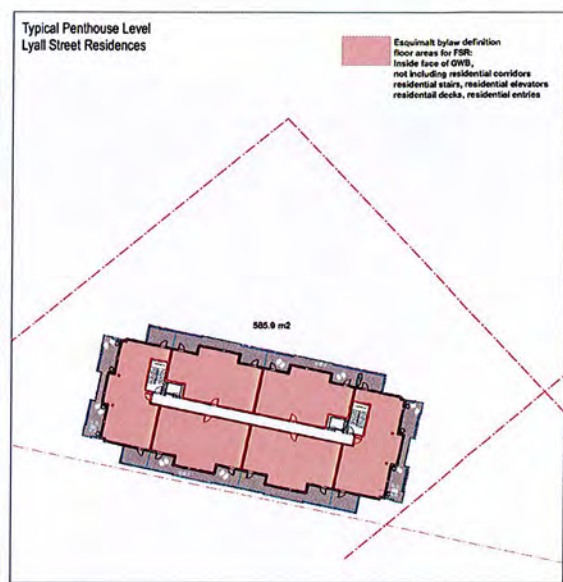
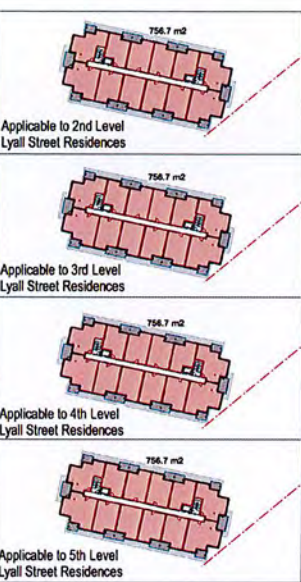
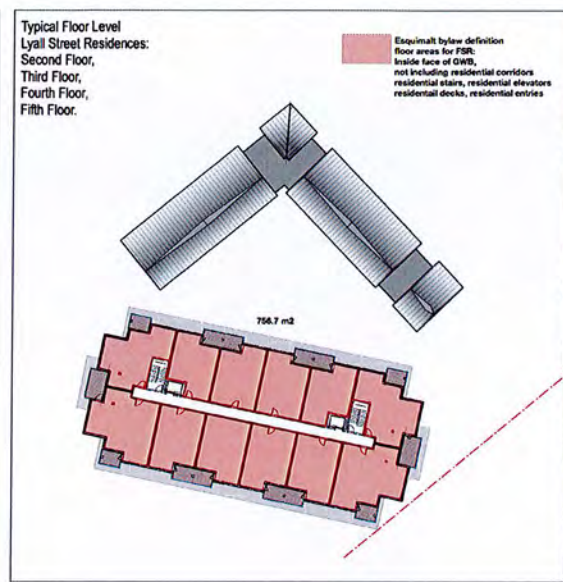
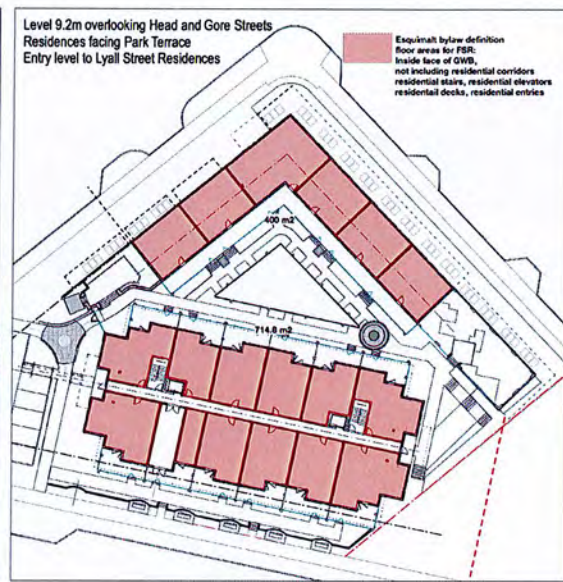
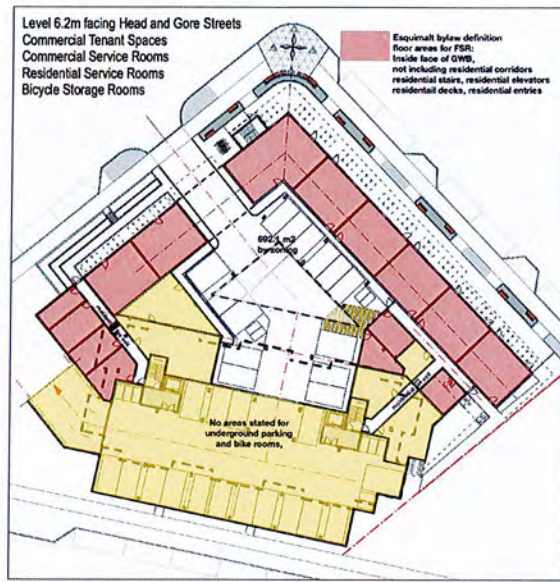
Subject: THE WESTBAY TRIANGLE
 HEA, GORE & LYALL ST, ESQUIMALT

Drawing title: Existing Site & Site Data

Scale: A1.1

THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA



ZONING BYLAW 1992, NO. 2050
Amendment Bylaw No. 2861

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area" when used in reference to a floor, Storey or Building means the entire area which in plan is enclosed by the exterior walls of the Storey or floor level, calculated in accordance with Section 14.

(9) at PART 4 - GENERAL REGULATIONS Section 14.
NEW DEFINITION USED IN THESE DOCUMENTS:
14. CALCULATION OF FLOOR AREA AND FLOOR AREA RATIO

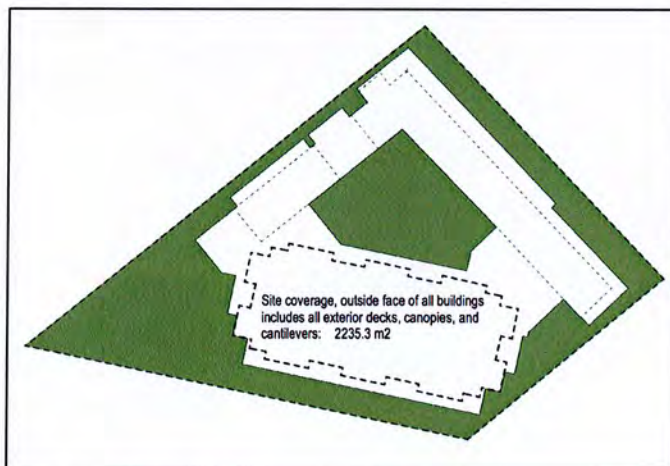
(1) The following shall not be included as Floor Area:
(a) balconies,
(b) open decks and sun decks,
(c) unenclosed porches and verandas,
(d) chimneys forming part of an exterior wall,
(e) projecting bay windows where the bottom of the window is located a minimum 0.3 metres above the floor, and no area of the bay window in plan exceeds 1.0 square metre.

(2) In addition, the following shall not be included as Floor Area in Multiple Family Dwellings and in residential portions of Mixed Commercial Residential Buildings:
(a) the area of the floor of any and all portions of the Building used or intended to be used for the parking or movement of vehicles,
(b) secure bicycle storage facilities (bike lockers),
(c) the area of the floor of any and all portions of the Building intended to be used by all residents of the building, including but not limited to the following:
(i) mezzanines,
(ii) corridors and hallways,
(iii) landings and foyers,
(iv) stairwells and stairwells, and
(v) elevator shafts.

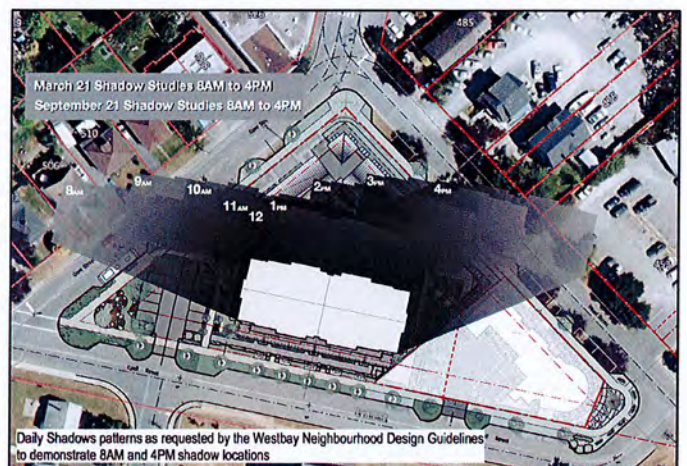
(3) In addition, the following shall not be included as Floor Area for the purposes of computing Floor Area Ratio:
(a) Any portion of a penthouse containing elevator or ventilating machinery,
(b) Any portion of a Building where the ceiling height is less than 1.2 metres above the floor.

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area Ratio" means the figure obtained when the Floor Area of all floors and Storeys in all Buildings on a Parcel is divided by the Area of the Parcel, calculated in accordance with Section 14.

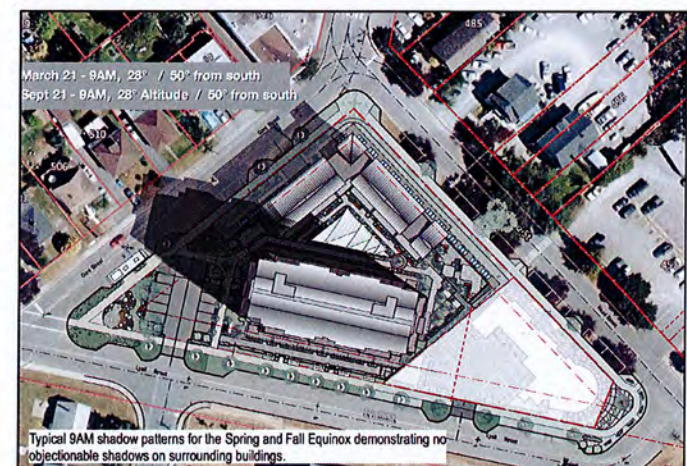
1 Floor Area Graphic Summary
Scale 1:500



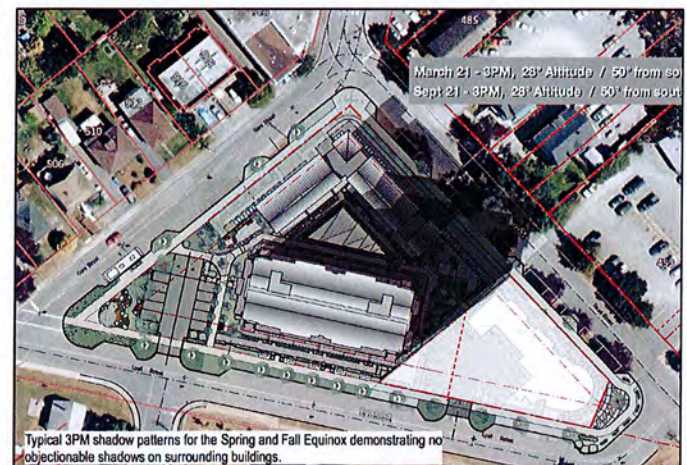
2 Site Coverage Graphic Summary
Scale 1:500



3 Daily Shading Summary: March, June, Sept.
Scale 1:1900



4 Shadows / 9AM and 3PM, March 21st, Sept. 21st
Scale 1:1900



Date	Event	Status
09 March 2017	Approved to OBC Committee	Approved
16 January 2017	Developer Permit Application	Approved
06 April 2016	Planning Committee	Approved
08 December 2015	Hearing Submission	Approved
16 June 2014	Site Plan	Approved

Permission is granted for this drawing to be reprinted by Lindholm Land and Investment Corporation

Hillel architecture

project: THE WESTBAY TRIANGLE
HEAD, GORE & JALL STREETS, ESQUIMALT

drawing title: Graphic Project Summary

phone: 250-592-9198
fax: 250-592-9178

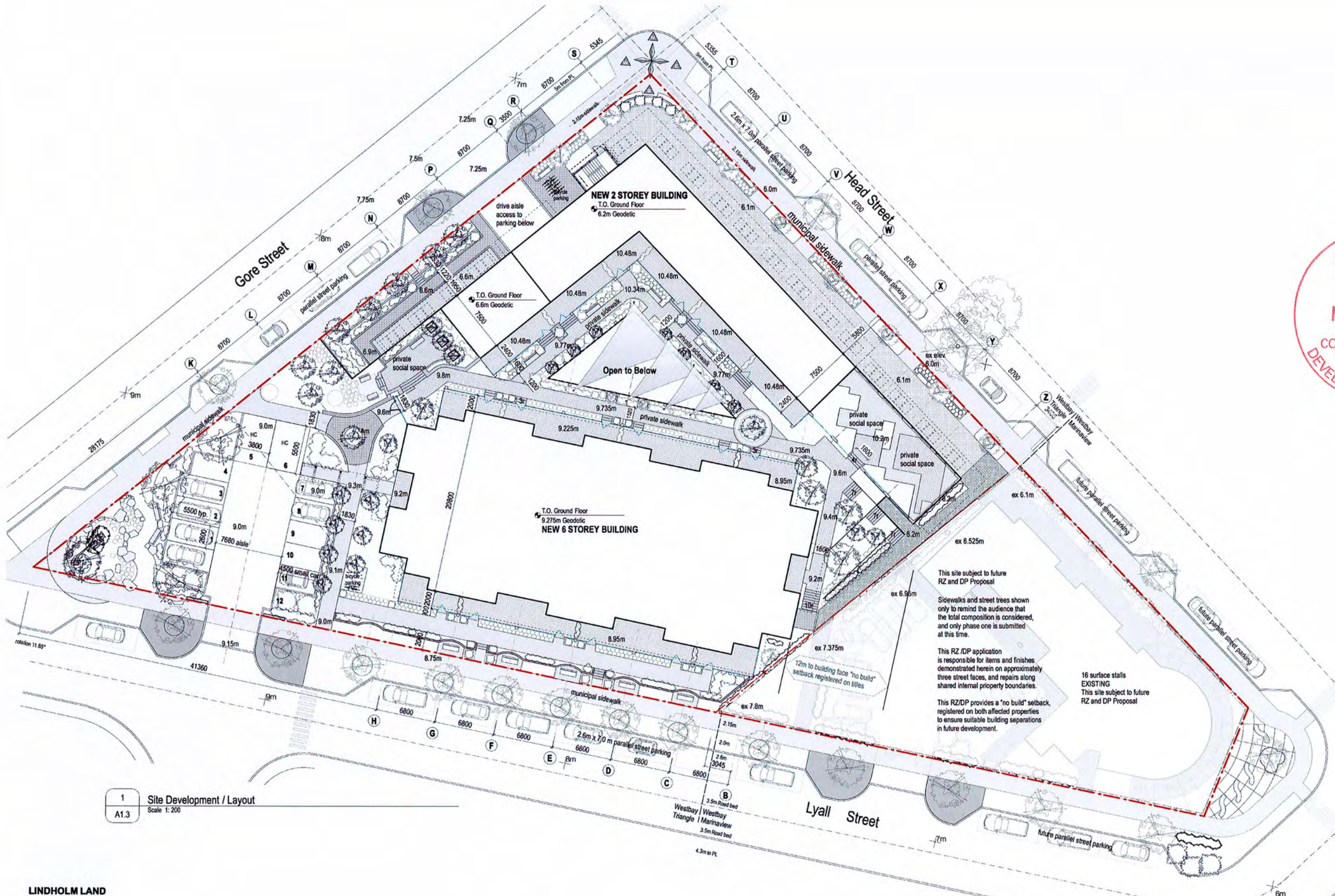
sheet no: A1.2

LINDHOLM LAND & INVESTMENT CORPORATION

7081 Central Saanich Road
Victoria, British Columbia
Canada V8M 1Y3
mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY

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 DEVELOPMENT SERVICES

1 Site Development / Layout
 A1.3 Scale 1:200

THE TRIANGLE @ WESTBAY

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 phone 250-592-9998
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Project: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

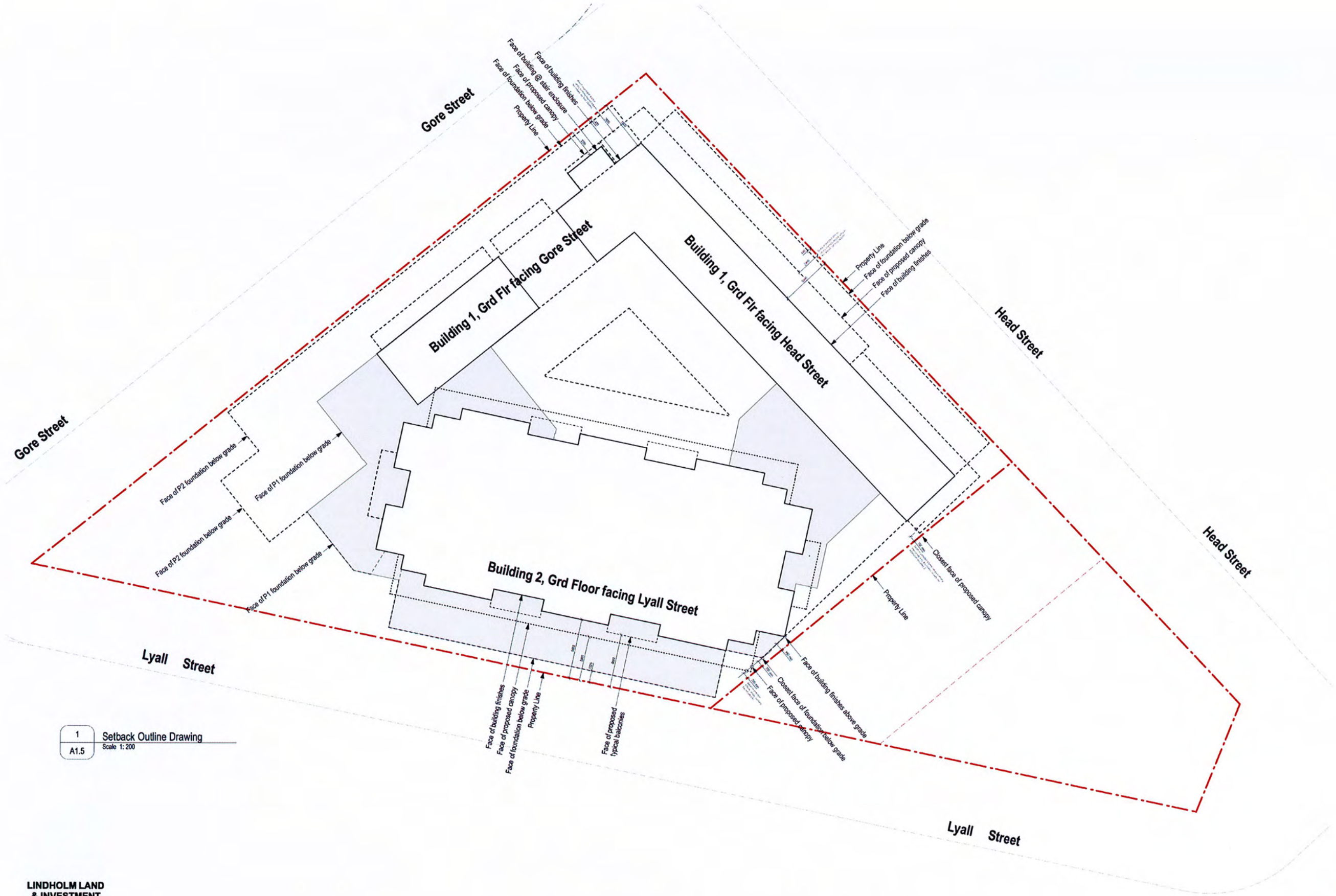
Site Development Layout

11 March 2017
 14 January 2017
 26 April 2016
 08 October 2015
 June 2014

Approved in E/C Comment
 Dev/land Permit Application
 Planning Committee
 Hearing Submission

2014.05

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1 Setback Outline Drawing
 Scale 1:200
 A1.5

03 March 2017	Approved in OHC Committee
16 January 2017	Development Permit Application
05 April 2016	Planning Consultation
08 December 2015	Requesting Submission
June 2016	2014.30

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 mark.lindholm@lindholm.ca

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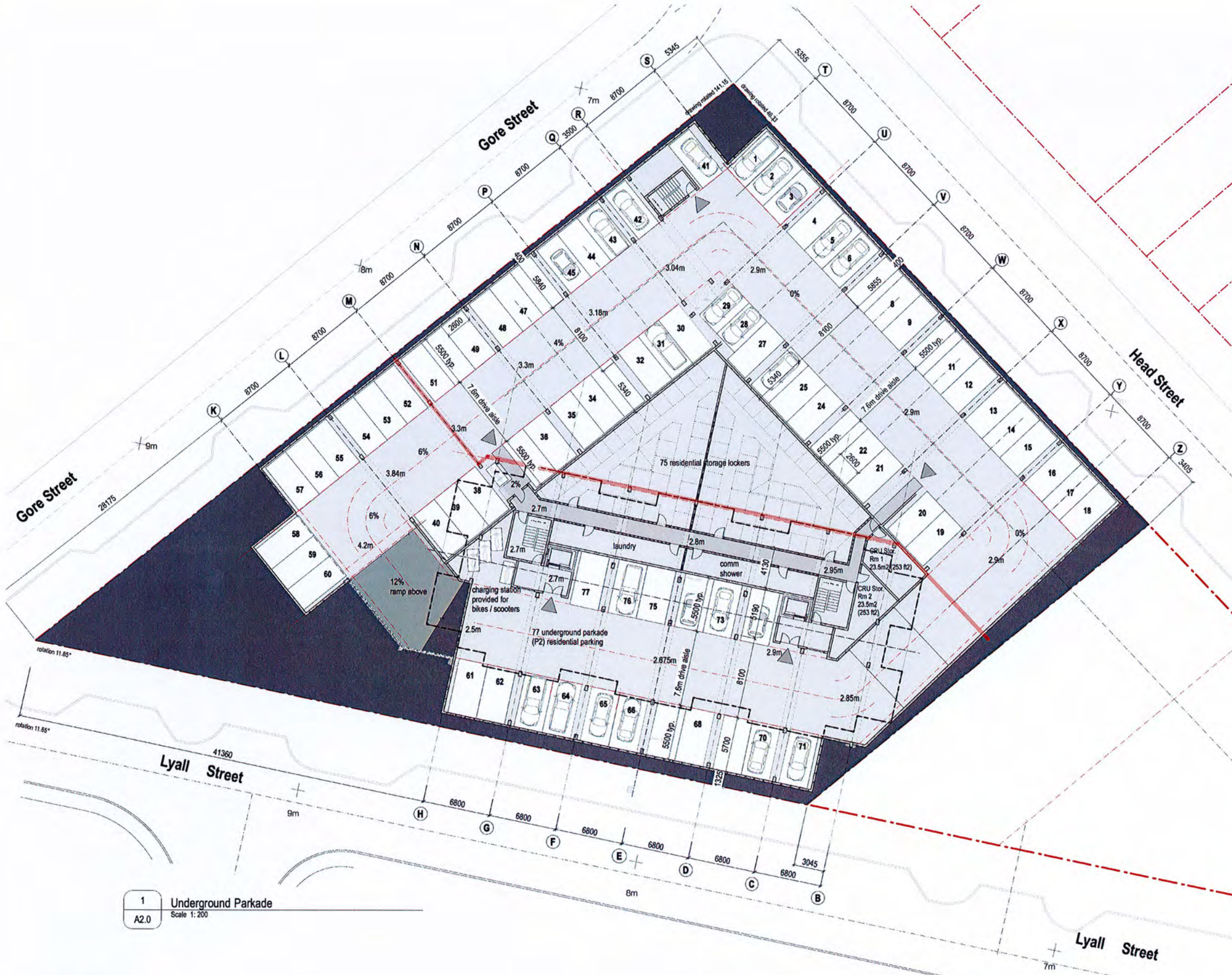
Hillel architecture

PROJECT: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STS, ESQUIMALT

101 45th Chalk Bay Avenue
 Victoria BC V8R 1C3

phone 250-592-9798
 fax 250-592-9778

Graphic Summary of Setbacks
 drawing no. A1.5



19 comm total min required
 +3 beyond bylaw req's
 22 illustrated under
 commercial strata ownership

95 residential min. required
 24 stalls for visitors
 71 stalls for owners
 52 beyond bylaw req's
 97 illustrated under
 residential strata ownership

- RG 4 residential short term guest stalls
- CP 8 commercial public parking stalls
- 14 street side parallel parking spaces not counted

Note: 3 residential guest stalls permitted to be commercial use daytime as per bylaw. Commercial parking becomes residential guest stalls after hours

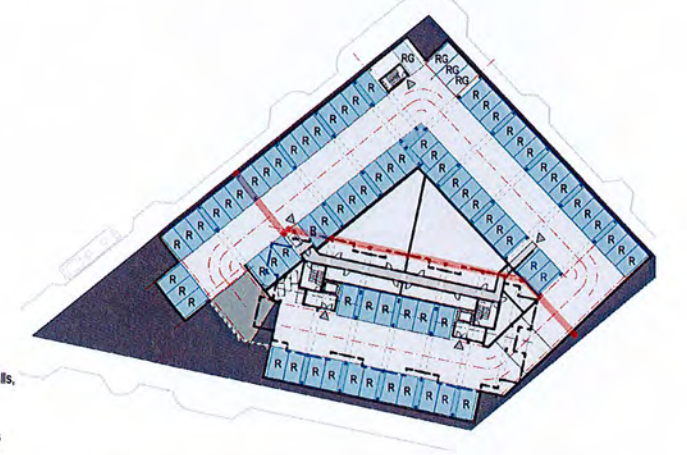
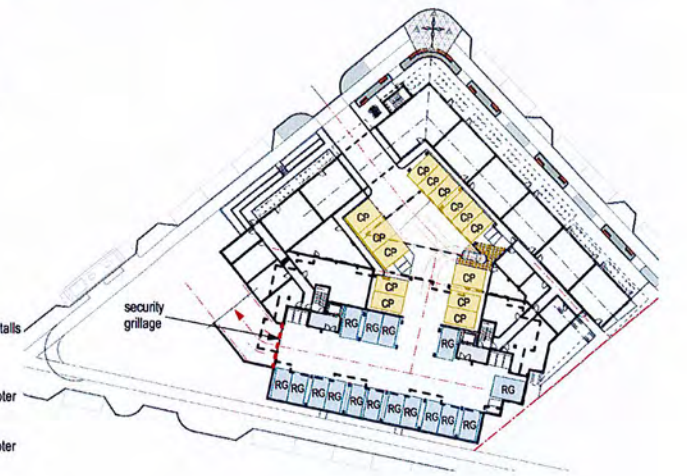
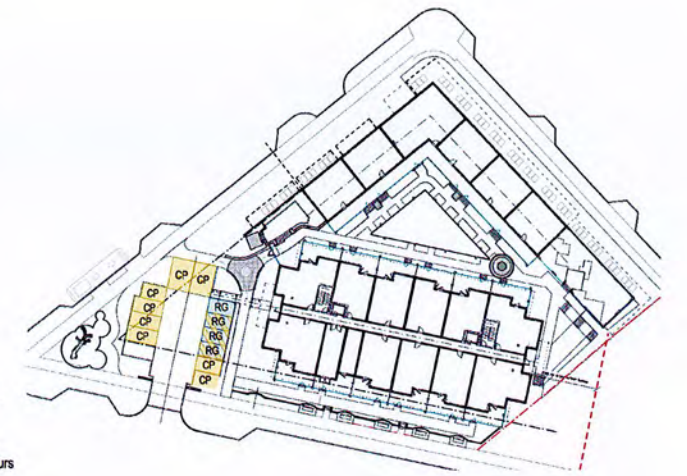
Note motorcycle, scooter, electric motorcycle, electric scooter not coloured in these diagrams, however these items are distributed throughout parking facilities to similarly provide these items in each parking category:

- long term residential guest,
- short term residential guest,
- residential homeowner,
- comm public parking
- comm leaseholder parking

- RG 16 Residential Guest stalls
- CP 14 commercial public parking stalls

Note: Residential guest stall area includes guest motorcycle / scooter parking in excess of bylaw. Commercial public stall area includes guest motorcycle / scooter parking in excess of bylaw.

- RG 4 long term residential guest stalls, entry permit / entry card req'd
- R 73 Residential stalls for 73 units



1 Underground Parkade
 A2.0 Scale 1:200

2 Parking Stall Assignments
 A2.0 Scale 1:200



THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA



Date	By	Description
02 March 2017	Approved by DRC Committee	
16 January 2017	Development Permit Application	
01 April 2016	Planning Committee	
08 December 2015	Planning Committee	
14 April 2014	Planning Committee	
12 June 14	Planning Committee	
22 June 14	Planning Committee	
2014.33	2014.33	

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Hillel architecture

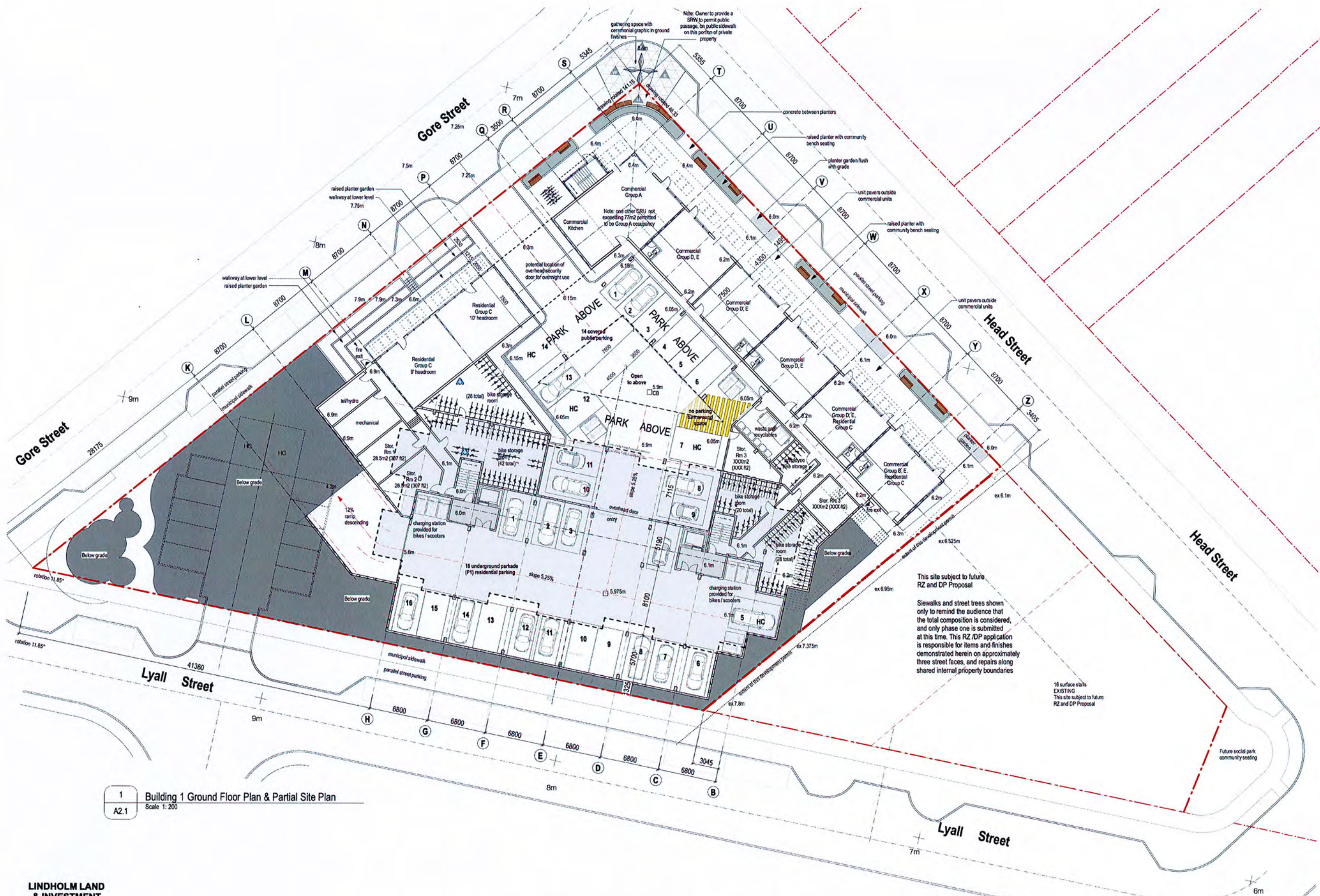
101 851 Oak Bay Avenue
 Victoria BC V8R - K3

phone 250.992.9998
 fax 250.992.9978

PROJECT: THE WESTBAY TRIANGLE
 HEAD, GORE, & LYALL STREETS, ESQUIMALT

drawing title: Underground Parkade Layout

revision no. A2.0



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DEVELOPMENT SERVICES

This site subject to future RZ and DP Proposal

Siewalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ /DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

16 surface stairs EXISTING
This site subject to future RZ and DP Proposal

Future social park community seating

1 Building 1 Ground Floor Plan & Partial Site Plan
A2.1 Scale 1:200

Date	By	For
12 March 2017	Approved in DMC Commentary	
16 January 2017	Development Permit Application	
06 April 2016	Planning Commentary	
08 December 2015	Revising Submission	
June 2014		
2014.33		

Permission is granted for these drawings to be reprinted by Lixholm Land and Investment Corporation.

Hillel architecture

Project: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

Drawing title: Bldg 1 - Ground Floor @ Head Street - Layout

Client: 1011 Oak Bay Avenue Victoria BC V8R 1K3
Phone: 250.592.9798 Fax: 250.592.9798

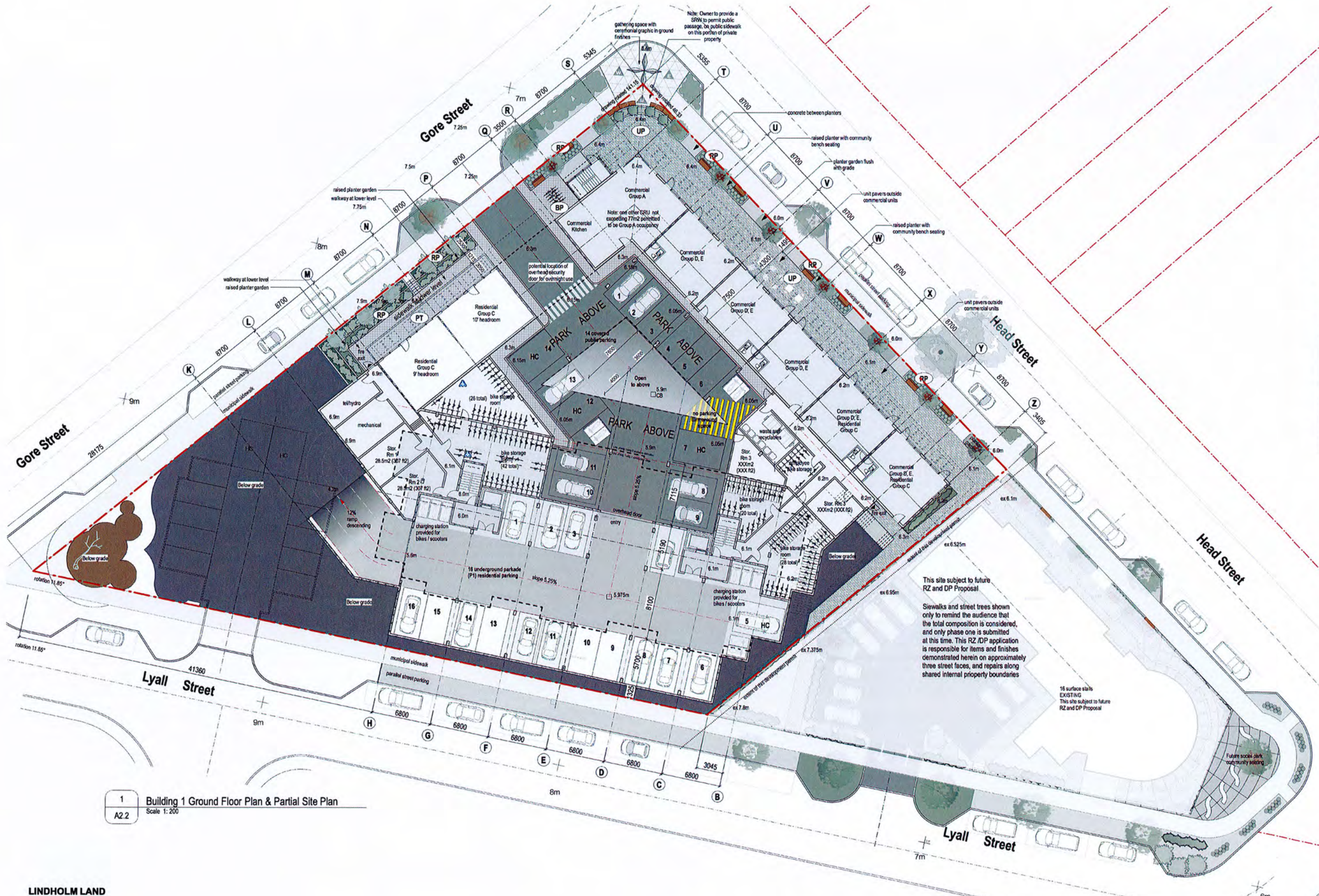
Scale: A2.1

LINDHOLM LAND & INVESTMENT CORPORATION

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Victoria, British Columbia
Canada V8M 1Y3
mark.lindholtm@linholm.ca

THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA



Site Finishes Legend
List of Finishes typical of all elevations

- ET existing trees to be undisturbed during construction
- CP common planting area - see landscape plan for details
- SA seeded or sodded grass area - see landscape plan for details
- NT approximate location of new tree planting area - see landscape plan for details
- RF rock feature - see landscape plan for details
- RP raised concrete planter - see landscape plan for details
- UP unit paver area - pathways and patios
- APS new asphalt surface
- MS municipal sidewalk - poured concrete finish
- PS private sidewalk - coloured poured concrete finish
- PW private walkways / suite entry features - coloured poured concrete finish
- PT private terrace - unit pavers finish
- RT roof top terrace - unit pavers finish
- PRP private residents park - unit pavers finish with bench seating
- SC site privacy screening - cedar fence - see landscape plan for details
- LPS landscape privacy screening - see landscape plan for details
- RW cultured stone veneer on concrete retaining wall with a concrete cap
- CW concrete retaining wall with a concrete cap
- BP six bike class 2 visitors bike parking / lockable
- VP asphalt surface vehicle parking stalls
- PB precast concrete parking blocks, typical all parking stalls
- ART public art
- PL parking / site lighting

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 MAR 02 2017

 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

This site subject to future RZ and DP Proposal
 Sidewalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ /DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

16 surface stalls EXISTING
 This site subject to future RZ and DP Proposal

1 Building 1 Ground Floor Plan & Partial Site Plan
 A2.2 Scale 1:200



THE TRIANGLE @ WESTBAY

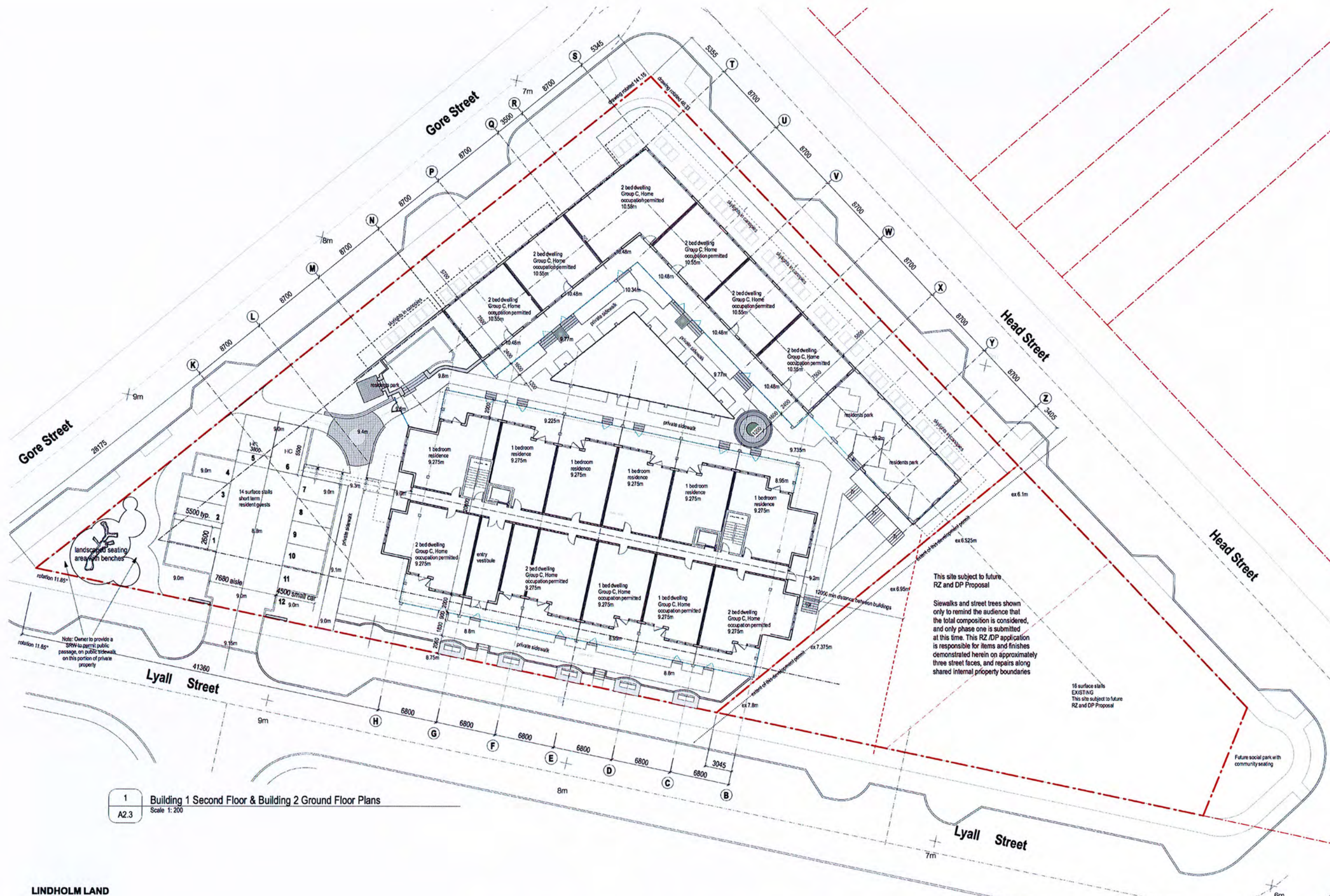
ESQUIMALT • BRITISH COLUMBIA

12 March 2017
 16 January 2017
 26 April 2016
 08 December 2015

Approved by DRC, Commission
 Development Permit Application
 Planning Committee
 Hear, etc. Submission

Project: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 Drawing: Bldg 1 - Ground Floor @ Head Street - Finishes
 Drawing No: A2.2

Hillel architecture
 101 851 Oak Bay Avenue
 Victoria BC V8R 1K3
 phone 250-592-9998
 fax 250-592-9978



This site subject to future RZ and DP Proposal

Sieiwalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ/DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

16 surface stalls EXISTING
This site subject to future RZ and DP Proposal



1 Building 1 Second Floor & Building 2 Ground Floor Plans
A2.3 Scale 1:200

REV	DATE	DESCRIPTION
02	March 2017	Adjusted to DRC Commentary
01	16 January 2017	Development Permit Application
00	06 April 2016	Planning Commentary
00	08 December 2015	Hearing Submission
00	24 February 2014	Final Design
00	10 October 2013	Final Design
00	10 October 2013	Final Design

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Hillel architecture

PROJECT: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

DRAWING NO: Building 1 & 2 Ground Floor @ Lyall Street - Layout

DATE: 2014.10.10

SCALE: 1:200

DESIGNED BY: [Signature]

PHOTOGRAPHY: [Signature]

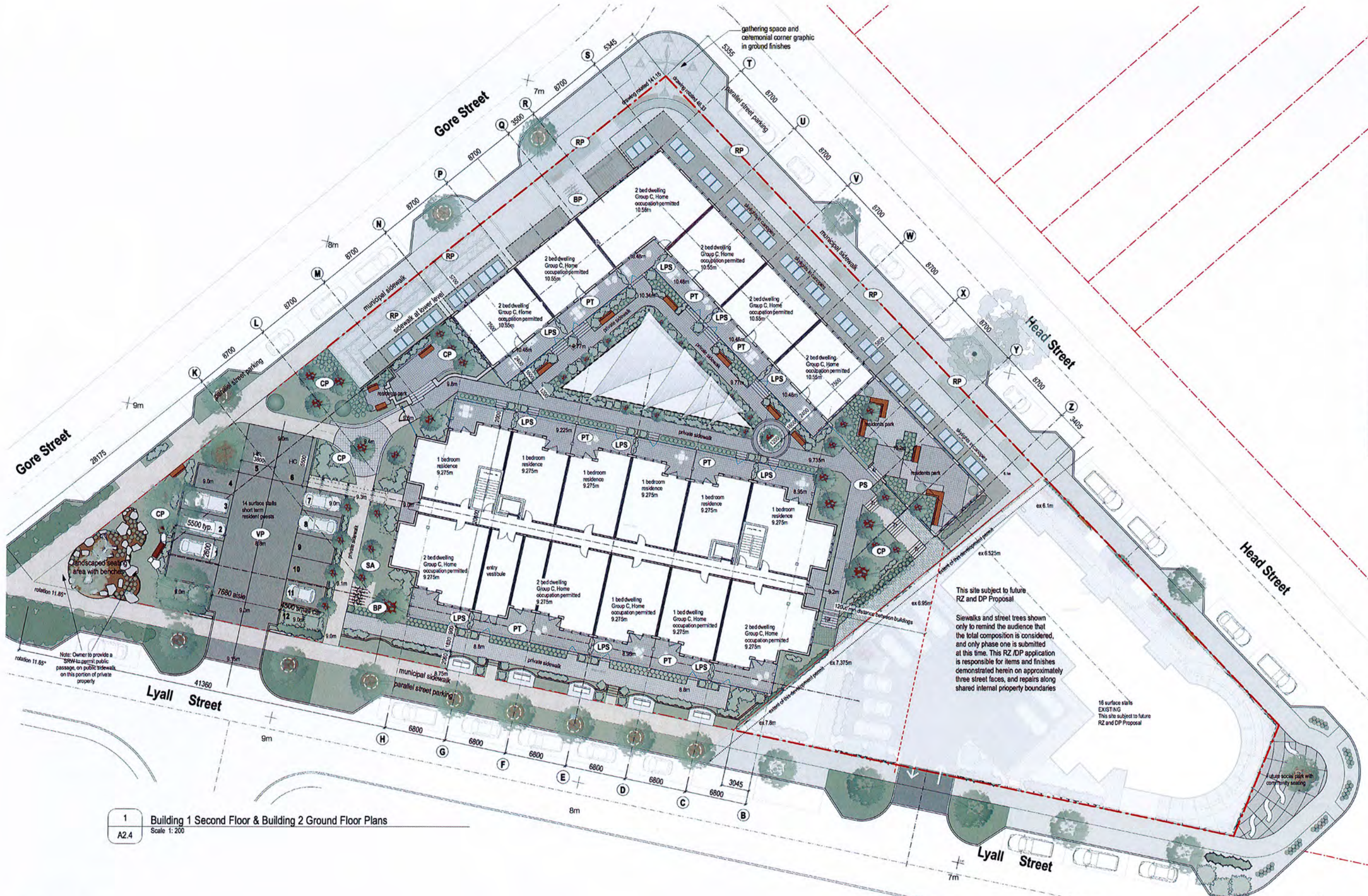
101 851 Oak Bay Avenue
Victoria BC V8K - K3
phone 250.992.9998
fax 250.992.9978

LINDHOLM LAND & INVESTMENT CORPORATION

7081 Central Saanich Road
Victoria, British Columbia
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mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY

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Site Finishes Legend

List of finishes typical of all elevations

- ET existing trees to be undisturbed during construction
- CP common planting area - see landscape plan for details
- SA seeded or sodded grass area - see landscape plan for details
- NT approximate location of new tree planting area - see landscape plan for details
- RF rock feature - see landscape plan for details
- RP raised concrete planter - see landscape plan for details
- UP unit paver area - pathways and patios
- APS new asphalt surface
- MS municipal sidewalk - poured concrete finish
- PS private sidewalk - coloured poured concrete finish
- PW private walkways / suite entry features - coloured poured concrete finish
- PT private terrace - unit pavers finish
- RT roof top terrace - unit pavers finish
- PRP private residents park - unit pavers finish with bench seating
- SC site privacy screening - cedar fence - see landscape plan for details
- LPS landscape privacy screening - see landscape plan for details
- RW cultured stone veneer on concrete retaining wall with a concrete cap
- CW concrete retaining wall with a concrete cap
- BP six bike class 2 visitors bike parking / lockable
- VP asphalt surface vehicle parking stalls
- PB precast concrete parking blocks, typical all parking stalls
- ART public art
- PL parking / site lighting



This site subject to future RZ and DP Proposal

Sidewalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ / DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

16 surface stairs EXISTING
This site subject to future RZ and DP Proposal

1 Building 1 Second Floor & Building 2 Ground Floor Plans
A2.4 Scale 1: 200

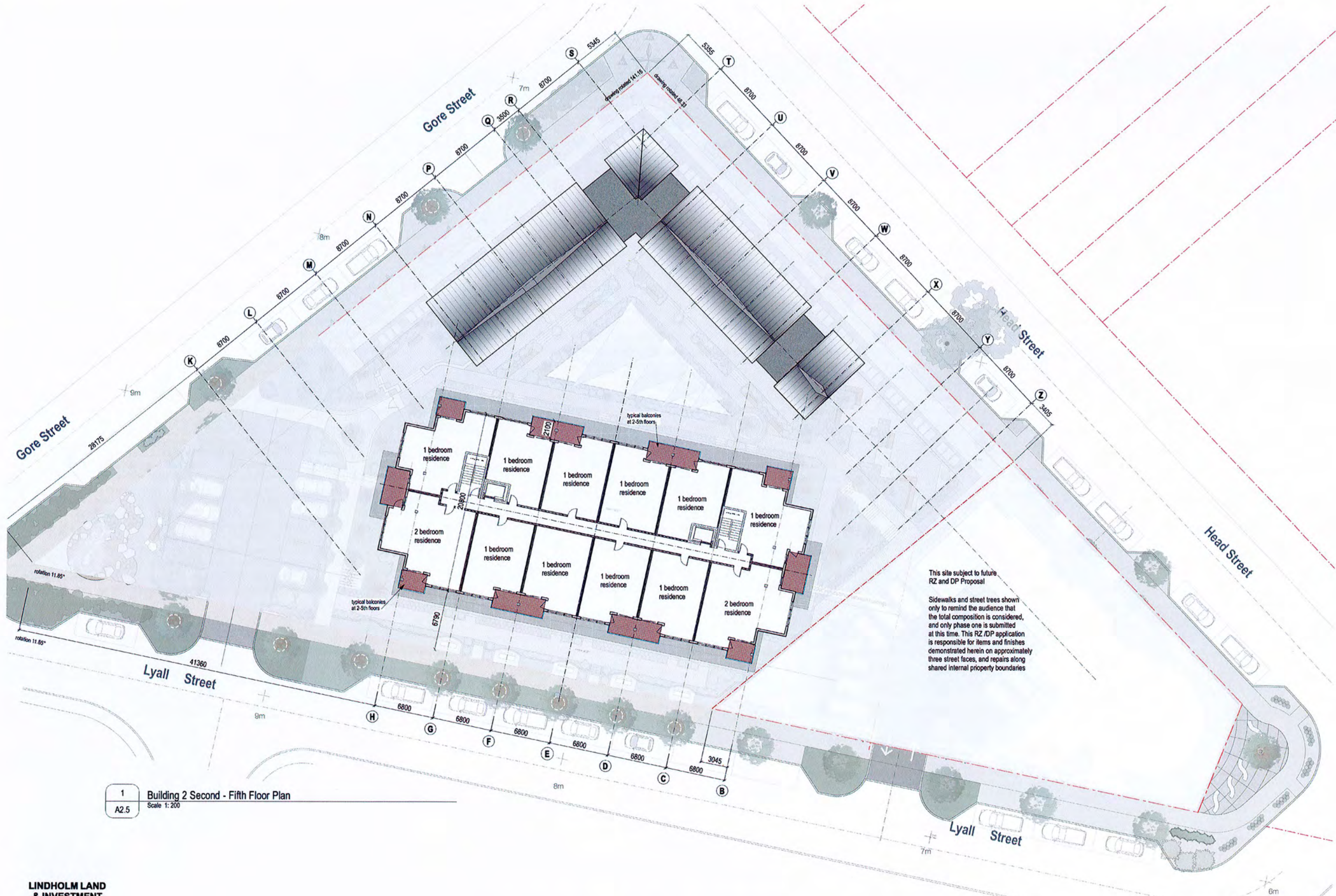


THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Date	By	Description
12 March 2017	Approved to DRG Committee	
16 January 2017	Development Permit Application	
16 April 2016	Planning Committee	
06 December 2015	May 2015 Submission	
18 June 2014	2014.174	
2014.174		
2014.174		

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 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

1 Building 2 Second - Fifth Floor Plan
 A2.5 Scale 1:200

Date	By	For
12 March 2017	Approved by DMC, Construction	
16 January 2017	Development Permit Application	
05 April 2016	Planning Committee	
08 December 2015	rezoning Submission	

Hillel
 architecture

THE WESTBAY TRIANGLE
 HEAD, GORE & LYLL STREETS, ESQUIMALT

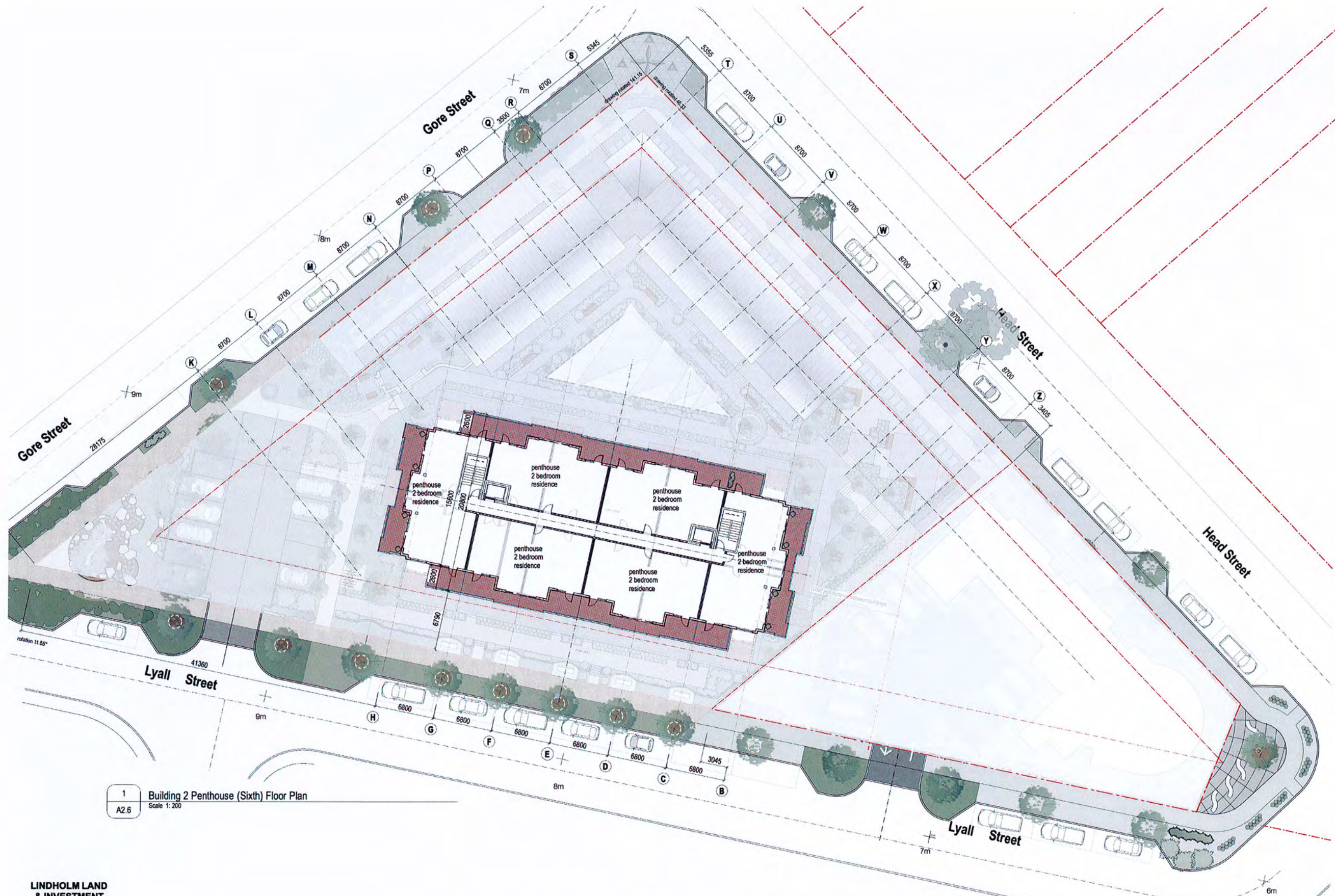
Building 2 - Typical Floor Layout
 2nd through 5th Floor Plans

A2.5

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THE TRIANGLE @ WESTBAY

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 DEVELOPMENT SERVICES

1 Building 2 Penthouse (Sixth) Floor Plan
 A2.6 Scale 1:200

03 March 2017	Advised to OBC Committee
16 January 2017	Development Permit Application
09 April 2016	Planning Committee
05 December 2015	Planning Committee
June 2015	Planning Committee
2014-15	2014-15
2014-15	2014-15

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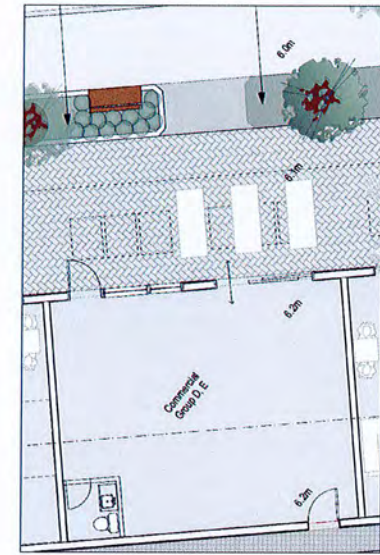
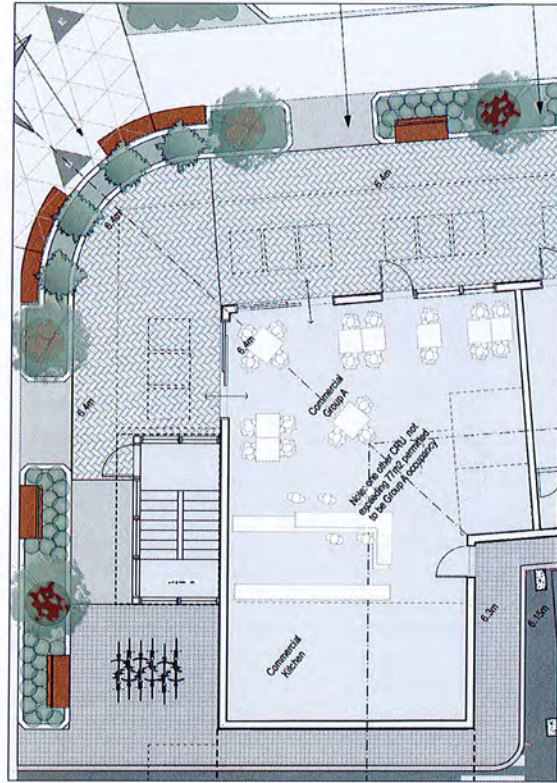
THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Hillel architecture

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 fax 250-992-9978

PROJECT: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS ESQUIMALT
 DRAWING NO: Bldg 2 - Penthouse (Sixth) Floor Plan
 DATE: 15 January 2017
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 SCALE: 1:200
 SHEET NO: A2.6

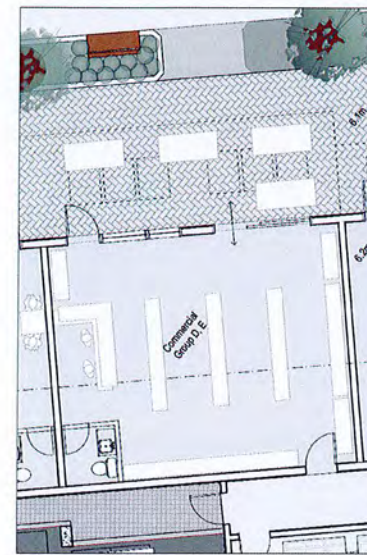


Suite CRU2 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 ft²
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38

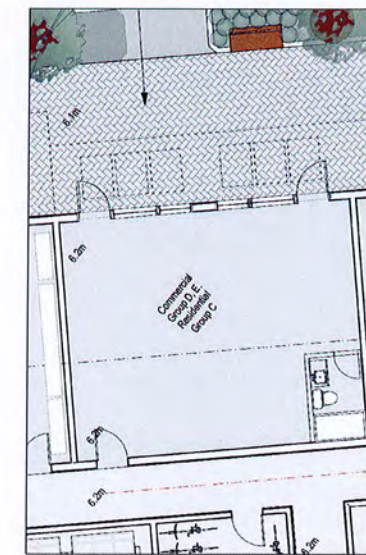
Suite CRU1
96.1m² inside GWB / 100.9 m² to cl / 1086 ft²
Commercial suite
Group A (less than 10% gross floor area, minor occupancy),
Group D, Group E
Parking 96.1m² @ 14m²/st = 6.8 stalls minimum
Parking @ 1 stall per 5 seats: 60 seats / 12 stalls req'd



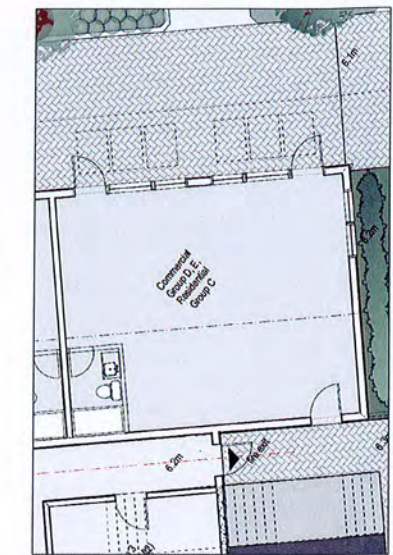
Suite CRU3 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 ft²
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38



Suite CRU4 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 ft²
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38



Suite CRU5 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 ft²
Commercial / Res suite
Group D, Group E,
Group C = Live or work unit
Parking 59.5m² @ 25m²/st = 2.38
As residence: 1.3 parking stalls

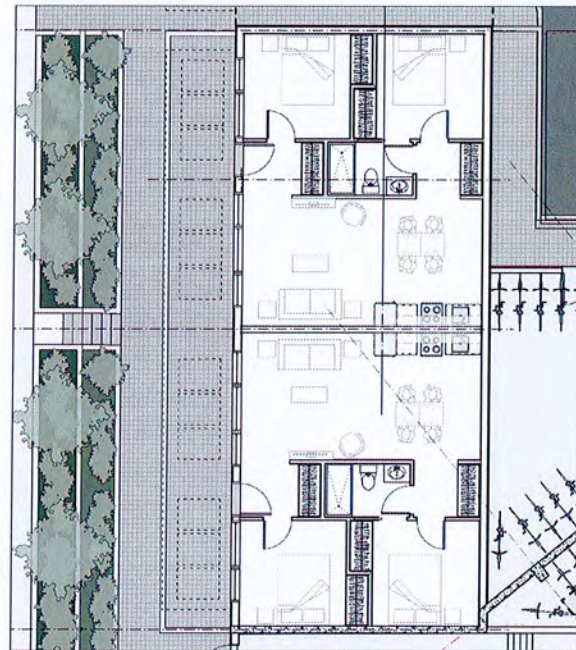


Suite CRU6 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 ft²
Commercial / Res suite
Group D, Group E,
Group C = Live or work unit
Parking 59.5m² @ 25m²/st = 2.38
As residence: 1.3 parking stalls

Please note:
Greatest horizontal building area by code definition, outside face of walls: 1731.2m² gross area.
Minor occupancies can not exceed 173.12m².

Building Code assumptions
CRU 1 - Group A Public Assembly Use unit, 96.1m²
This unit less than 10% greatest horizontal building area - minor occupancy permitted.

CRU 2 to CRU 6 permitted to be Group A Public Assembly Use provided
that the total of all public assembly use spaces combined do not exceed 173.12m².
Each CRU @ ±59.5m² permits one more commercial space to be assigned Group A use.
Project parking calculations permit this increase (5 stalls in excess of Bylaw demonstrated)



Suite G102 Residential suite
59.3 m² inside GWB / 62.9m² cl of walls / 677 ft²
2 bedroom residential dwelling
Group C
Home occupancy permitted
Parking: 1.3 stalls per dwelling

Suite G101 Residential suite
59.3 m² inside GWB / 62.9m² cl of walls / 677 ft²
2 bedroom residential dwelling
Group C
Home occupancy permitted
Parking: 1.3 stalls per dwelling

2 Building 1 Commercial Suite Plans - Ground Floor
A2.7 Scale 1:100

1 Building 1 Residential Suite Plans - Ground Floor
A2.7 Scale 1:100



REV	DATE	BY	DESCRIPTION
1	18 March 2017	AD	Adopted to BDC Connection
2	18 January 2017	AD	Development Permit Application
3	08 April 2016	AD	Planning Committee
4	08 October 2015	AD	Resolving Submission
5	June 2014	AD	Planning 14

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project
THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREET, ESQUIMALT

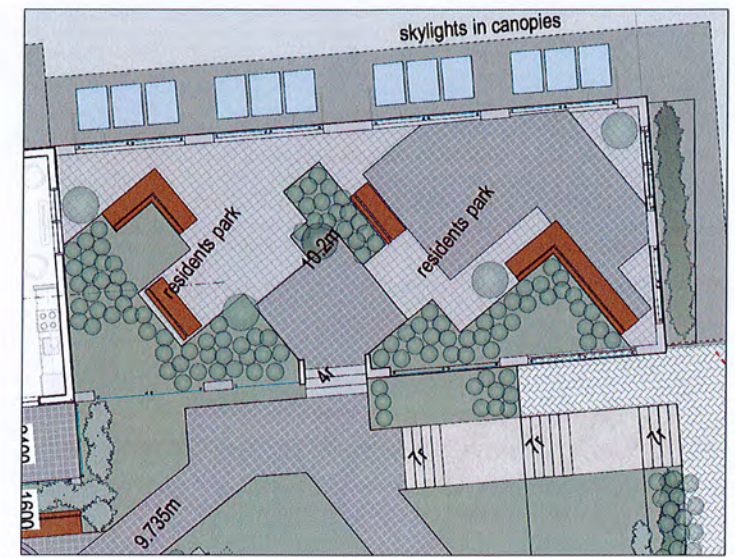
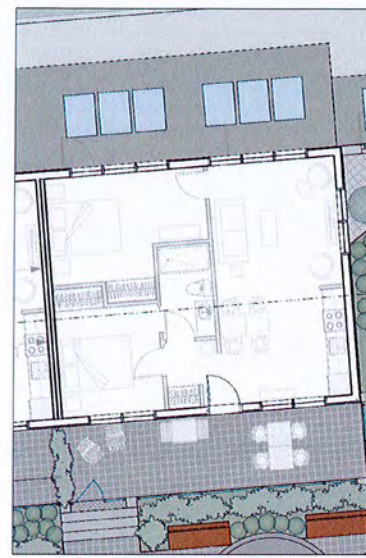
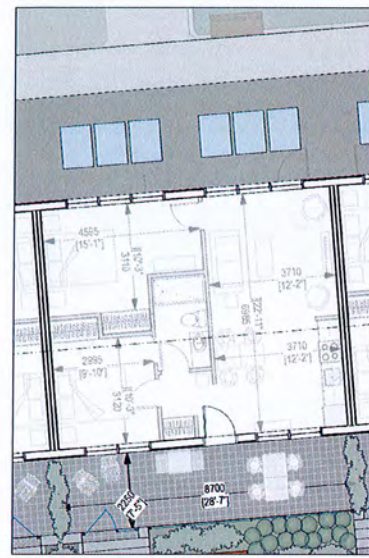
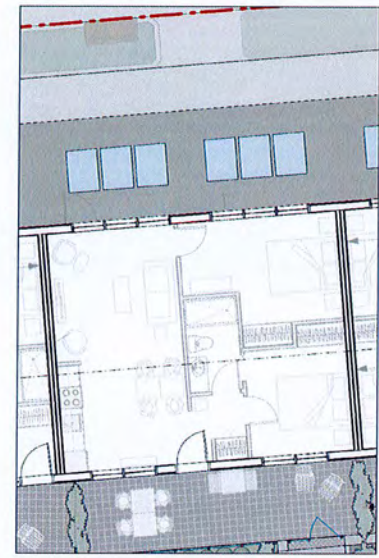
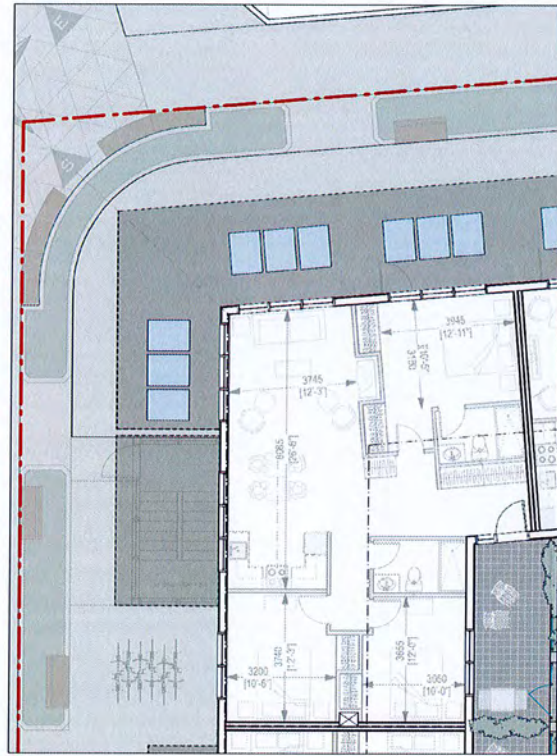
drawing title
Building 1 - Suite Floor Plans

drawing no.
A2.7

LINDHOLM LAND & INVESTMENT CORPORATION

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mark.lindholm@lindholm.com

THE TRIANGLE @ WESTBAY
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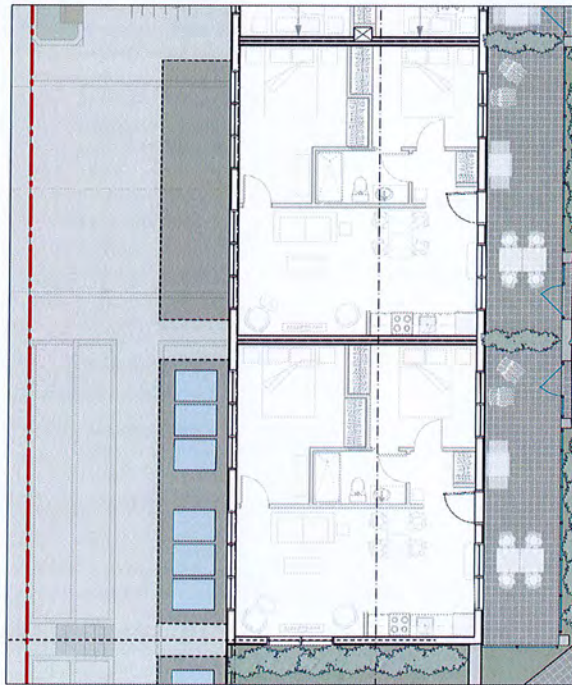
Suite P204 Residential Suite
63 m2 to cl walls / 678 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P203 Residential Suite
101.1 m2 to cl walls / ±1088 R2
3 bedroom dwelling
Group C Residential use only
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P205 Residential Suite
63 m2 to cl walls / 678 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P206 Residential Suite
63 m2 to cl walls / 678 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Roof Top Ocean view / Marina view Park
1280 R2 raised gardens, and container trees.
Fixed tables and chairs
Residential Amenity / residents only



Suite P202 Residential Suite
63 m2 to cl walls / 678 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P201 Residential Suite
63 m2 to cl walls / 678 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

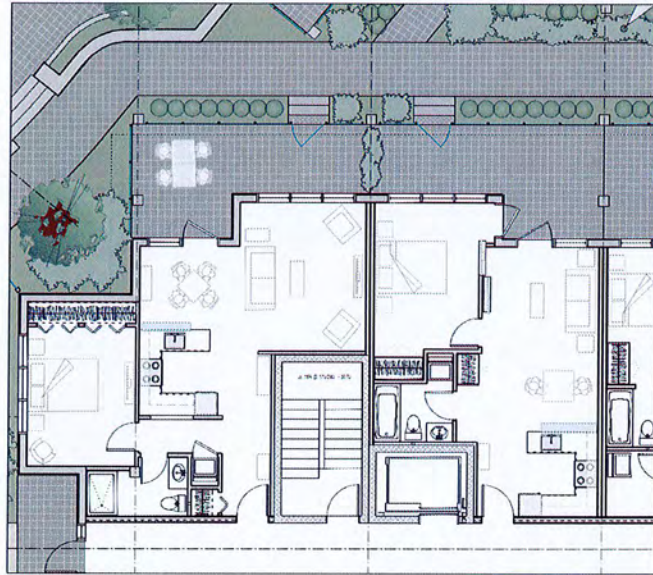
2 Building 1 Suite Plan - Second Floor
Scale 1:100
A2.8

1 Building 1 Suite Plan - Second Floor
Scale 1:100
A2.8



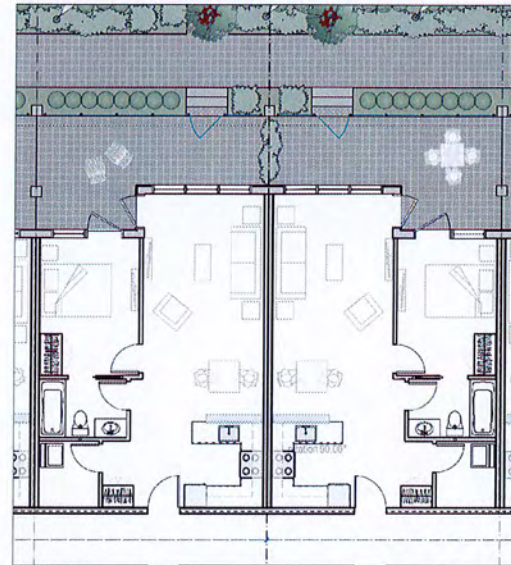
02 March 2017		Adopted by SMC Committee	
NO.	16	DATE	18 January 2017
BY	SMC	FOR	Development Permit Application
NO.	08	DATE	08 April 2016
BY	SMC	FOR	Planning Committee
NO.	08	DATE	08 December 2015
BY	SMC	FOR	Resolving Submission
NO.	08	DATE	June 2014
BY	SMC	FOR	Final Review
NO.	08	DATE	2014.20

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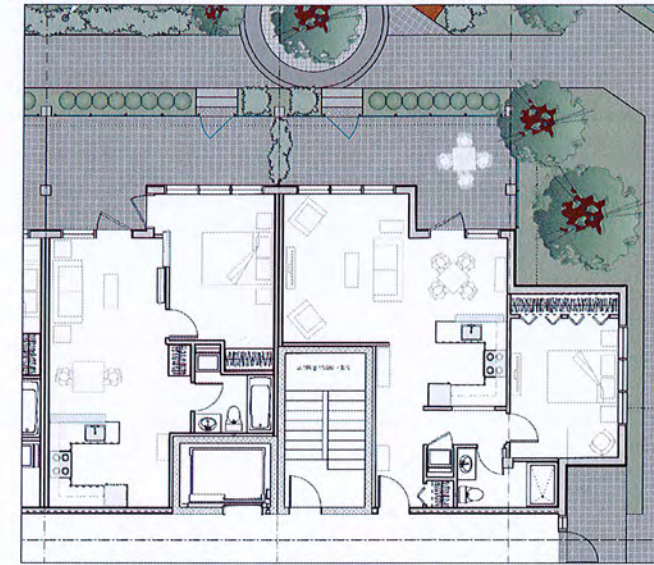
Suite 112 Residential Suite
65.8 m² / 708 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite 111 Residential Suite
53.5 m² / 576 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Suite 110 Residential Suite
59.6 m² / 642 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted

Suite 109 Residential Suite
59.6 m² / 642 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted



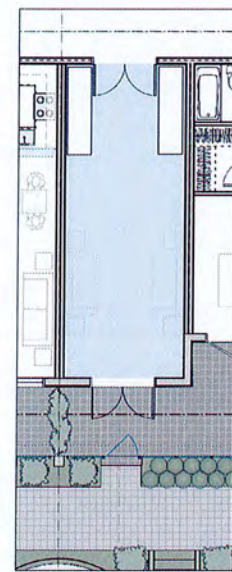
Suite 108 53.5 m² / 576 ft²
1 bedroom Residential suite
Group C Residential use
Residential use only

Suite 107 65.8 m² / 708 ft²
1 bedroom Residential suite
Group C Residential use
Residential use only

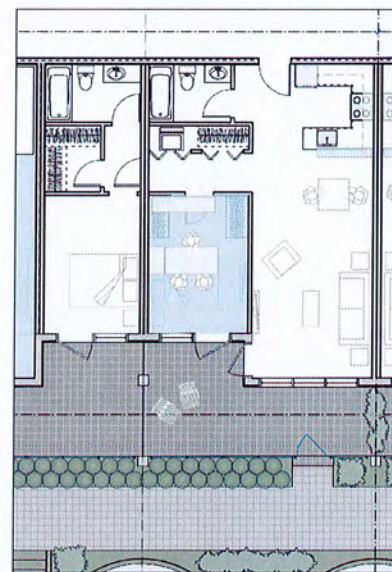
1 Building 2 Residential Suite Plans - Ground Floor
Scale 1:100
A2.9



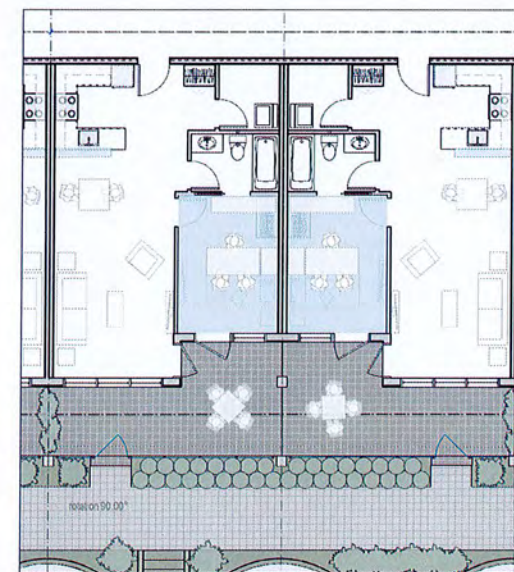
Suite 101 Residential Suite
78.1 m² / 840 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Residential Entry Foyer
Room 102



Suite 103 84 m² / 904 ft²
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Suite 104 59.6 m² / 642 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite 105 59.6 m² / 642 ft²
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

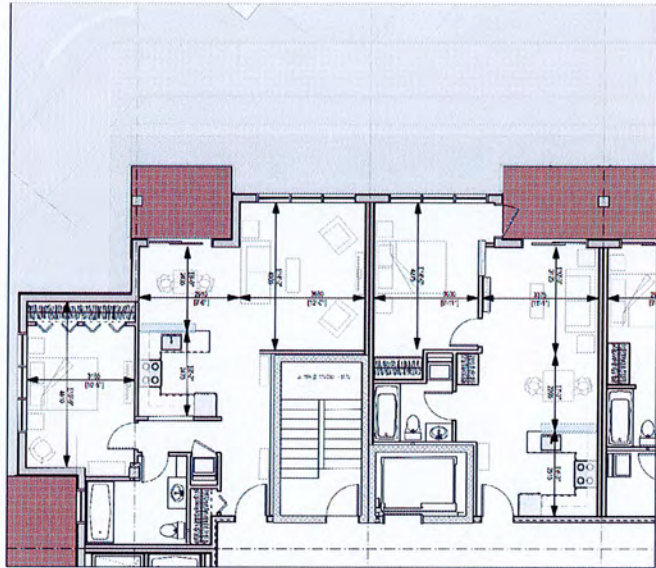


Suite 106 78.1 m² / 840 ft²
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

2 Building 2 Suite Plans - Entry Vestibule
Scale 1:100
A2.9

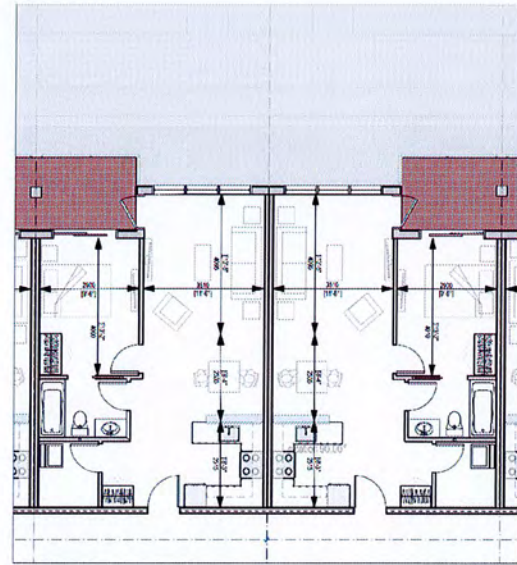
DATE	DESCRIPTION	BY	FOR
18 January 2017	Development Permit Application		
08 April 2016	Planning Committee		
05 December 2015	Resolving Submissions		
June 2014	Final Review		
2014.30			

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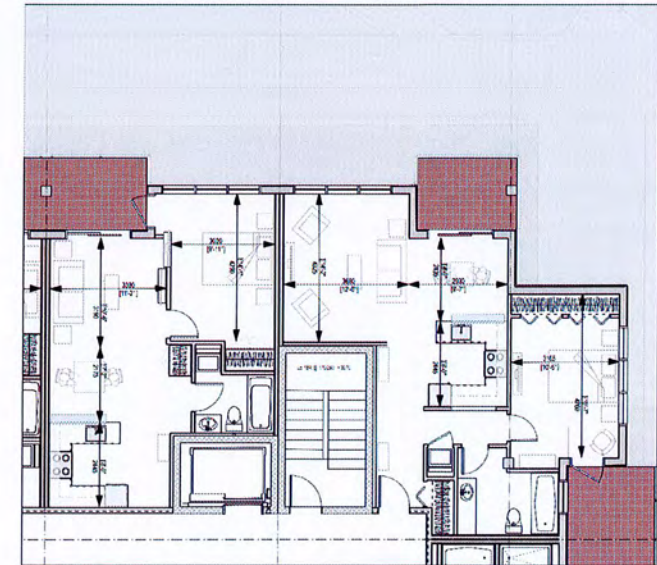
Suite 212 68.4 m2 / 736 ft2
1 bedroom Residential suite

Suite 211 53.5 m2 / 576 ft2
1 bedroom Residential suite



Suite 210 59.6 m2 / 642 ft2
1 bedroom Residential suite

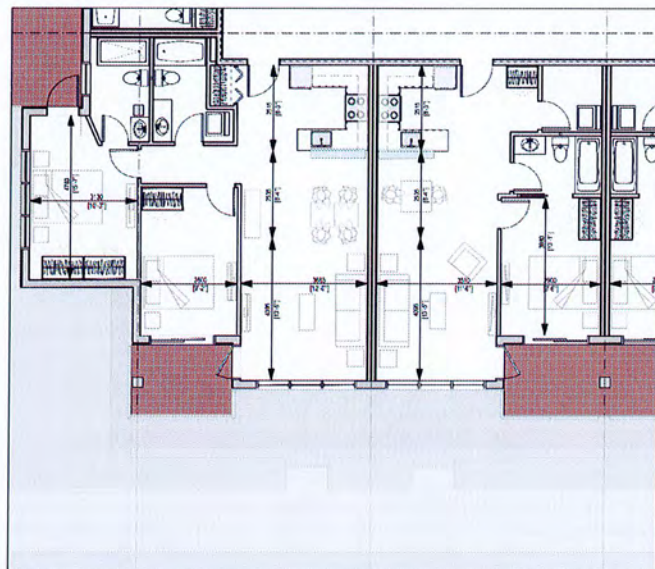
Suite 209 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 208 53.5 m2 / 576 ft2
1 bedroom Residential suite

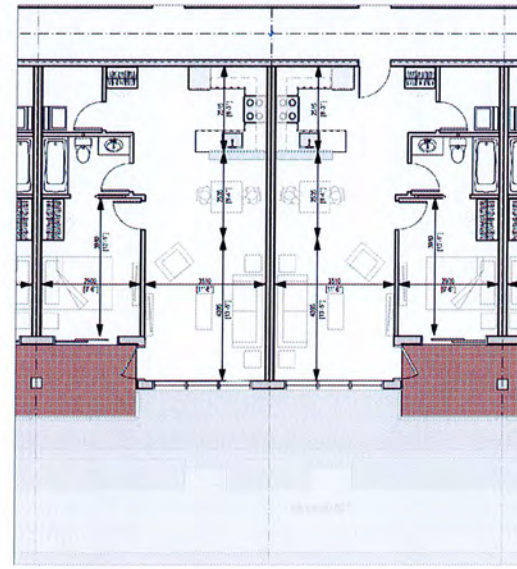
Suite 207 68.4 m2 / 736 ft2
1 bedroom Residential suite

1 Building 2 Suite Plan - 2nd to 5th Flrs
Scale 1: 100
A2.10



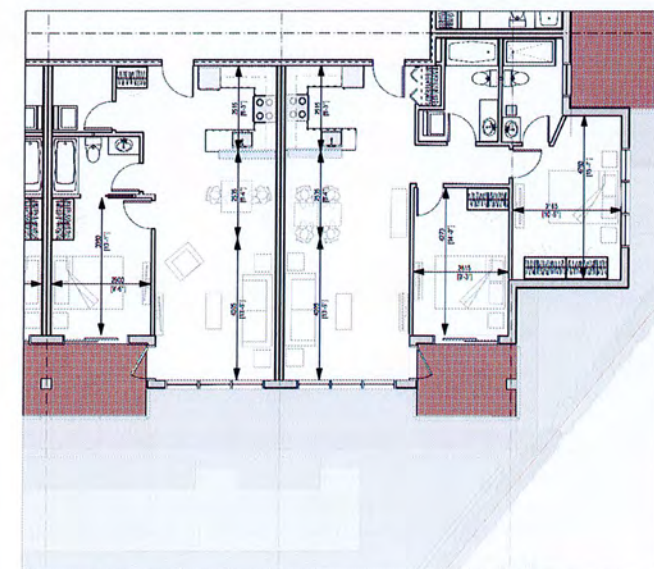
Suite 201 81.4 m2 / 876 ft2
2 bedroom Residential suite

Suite 202 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 203 59.6 m2 / 642 ft2
1 bedroom Residential suite

Suite 204 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 205 59.6 m2 / 642 ft2
1 bedroom Residential suite

Suite 206 81.4 m2 / 876 ft2
2 bedroom Residential suite

2 Building 2 Suite Plan - 2nd to 5th flrs
Scale 1: 100
A2.10

01 March 2017		Adopted by DSC Committee
Rev	15 January 2017	Development Permit Application
Rev	05 April 2016	Planning Committee
Rev	05 December 2015	Neighbour Submission
Rev	27 June 2015	
Rev	17 February 2015	
Rev	05 November 2014	
Rev	02 October 2014	

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THE TRIANGLE @ WESTBAY

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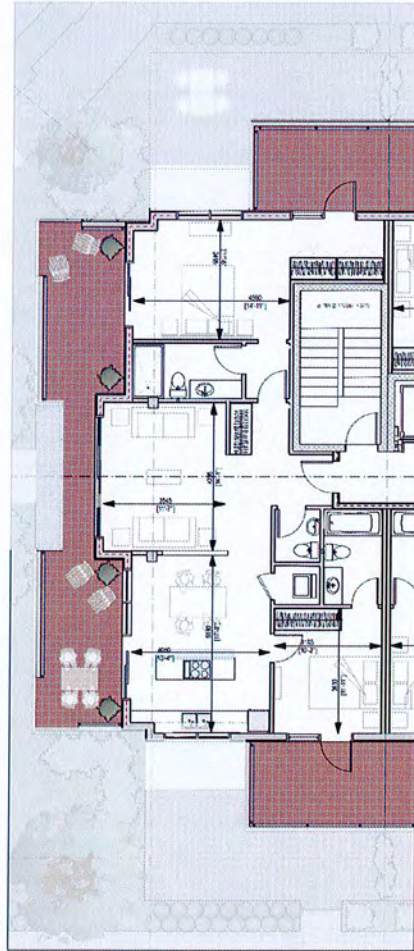


project
THE WESTBAY TRIANGLE
HEAF, GORE & LYALL STREETS, ESQUIMALT

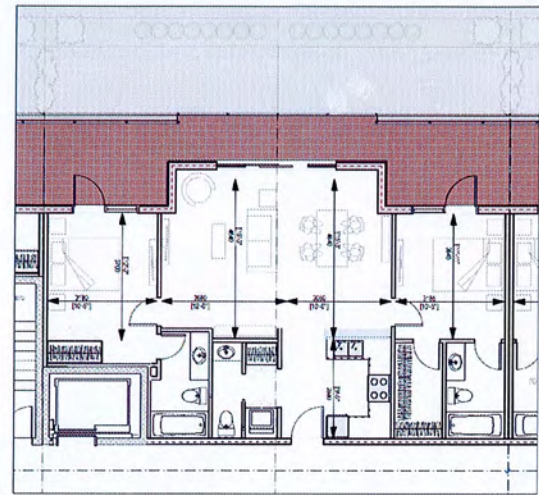
drawing title
Building 2 - Suite Floor Plans

revision no.
A2.10

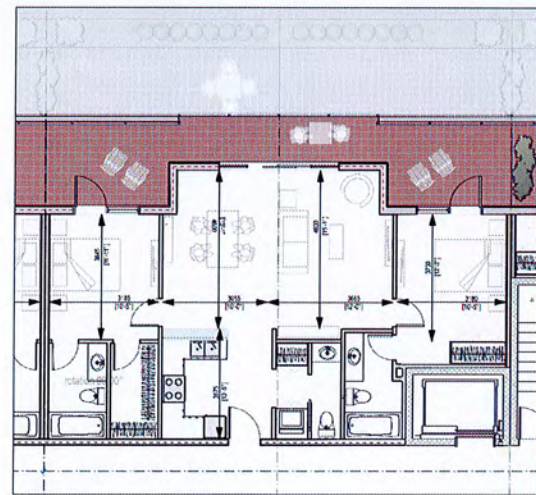
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 DEVELOPMENT SERVICES



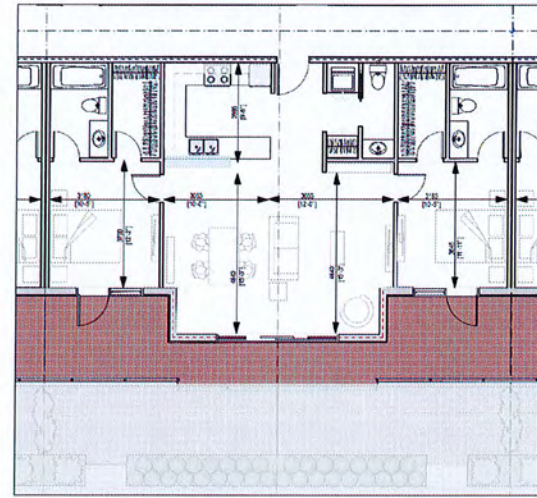
Penthouse Suite 601 97.5 m2 / 1050 ft2
 2 master bedroom Residential suite



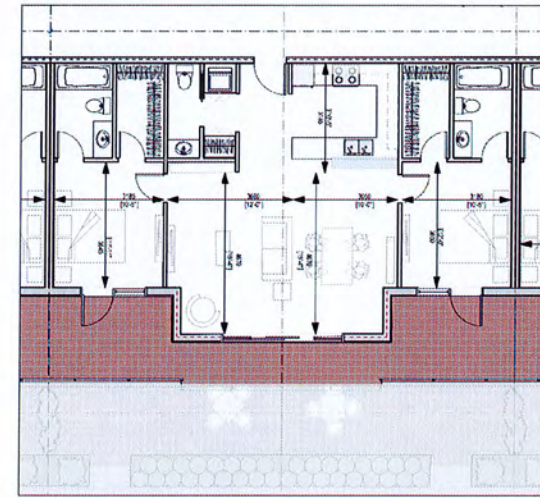
Penthouse Suite 606 91.3 m2 / 982 ft2
 2 master bedroom Residential suite



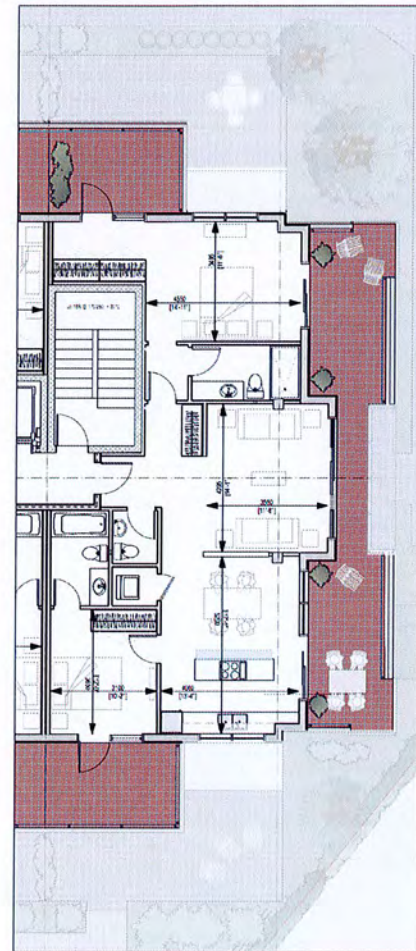
Penthouse Suite 605 91.3 m2 / 982 ft2
 2 master bedroom Residential suite



Penthouse Suite 602 97.9 m2 / 1053 ft2
 2 master bedroom Residential suite



Penthouse Suite 603 97.9 m2 / 1053 ft2
 2 master bedroom Residential suite



Penthouse Suite 604 97.5 m2 / 1050 ft2
 2 master bedroom Residential suite

1 Building 2 Suite Plan - Penthouse
 A2.11 Scale 1:100

REV	DATE	BY	CHKD	DESCRIPTION
1	18 January 2017	ML	ML	Development Permit Application
2	09 March 2017	ML	ML	Planning Committee
3	08 December 2016	ML	ML	Meeting Submission
4	June 2014	ML	ML	Final

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 mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY
 ESQUIMALT • BRITISH COLUMBIA

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project	THE WESTBAY TRIANGLE HEAD, GOPE & LYALL STREETS, ESQUIMALT
drawing title	Building 2 - Suite Floor Plans
revision no.	A2.11



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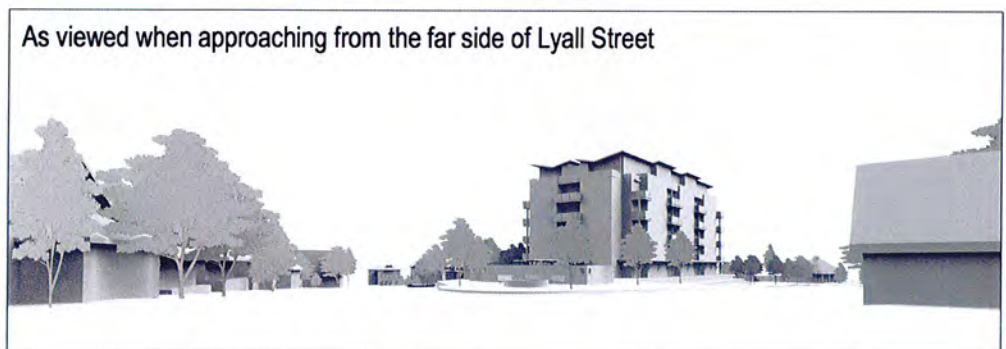
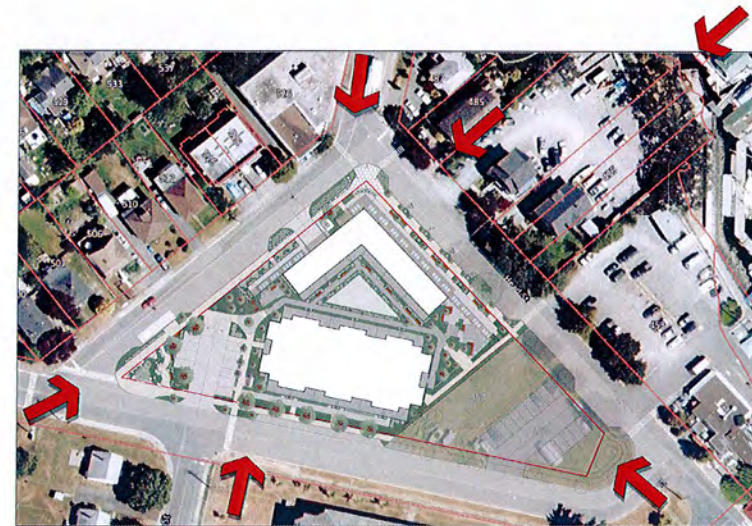
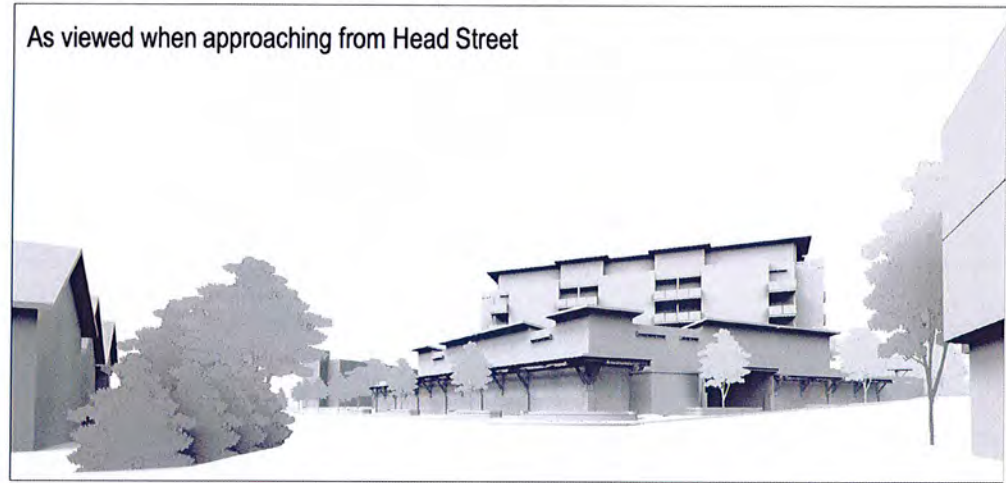
THE TRIANGLE @ WESTBAY
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18 January 2017	Development Permit Application
05 April 2016	Planning Committee
05 December 2015	Neighbour Submission
June 2014	Site Plan
2014.30	

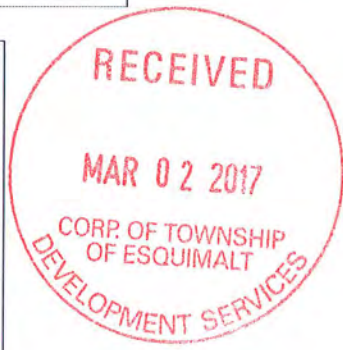
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Hillel
 architecture
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 phone 250.592.3978
 fax 250.592.3978

PROJECT: THE TRIANGLE @ WESTBAY
 HEAD CORNER LYALL STREET & ESQUIMALT
 DRAWING NO: Project Perspectives
 DATE: 18 January 2017
 DRAWING NO: A3.0



Note: This sheet shows artistic renderings only. For exterior finishes, dimensioned elevations, and their relationship to neighboring buildings see elevation sheets A3.2 - A3.8



THE TRIANGLE @ WESTBAY
 ESQUIMALT • BRITISH COLUMBIA

DATE	19 January 2017	DESCRIPTION	Development Permit Application
DATE	08 April 2016	DESCRIPTION	Planning Commentary
DATE	18 September 2015	DESCRIPTION	Revising Submission
DATE	JULY 2014	DESCRIPTION	Revising Submission
DATE		DESCRIPTION	
DATE		DESCRIPTION	

Permission is granted for these drawings to be updated by Lindholm Land and Investment Corporation

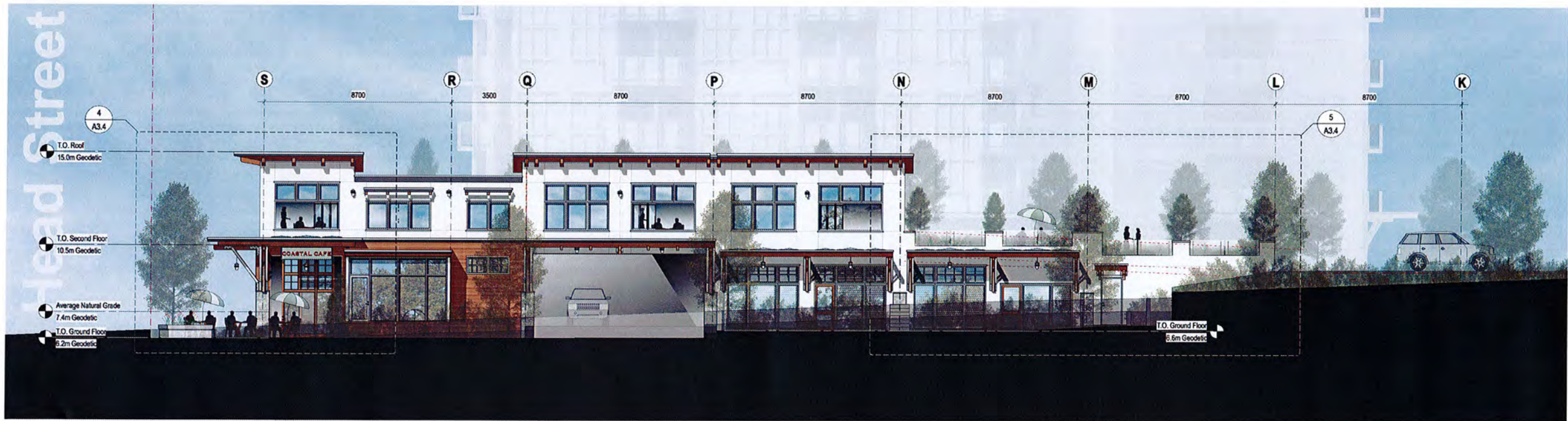
Hillel architecture
 101 891 Oak Bay Avenue
 Victoria BC V8R 4K3
 phone 250.592.9998
 fax 250.592.9978

PROJECT: THE TRIANGLE @ WESTBAY
 HEAD, GOWE & LYALL STS. ESQUIMALT

DRAWING NO: Neighbourhood Perspectives
 DRAWING NO: A3.1



1 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:100
A3.2



2 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:100
A3.2

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CORP OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES

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mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA

18 January 2017	Development Permit Application
05 December 2015	Recording Submission
25 October 2014	Preliminary Review
June 2014	Final Review

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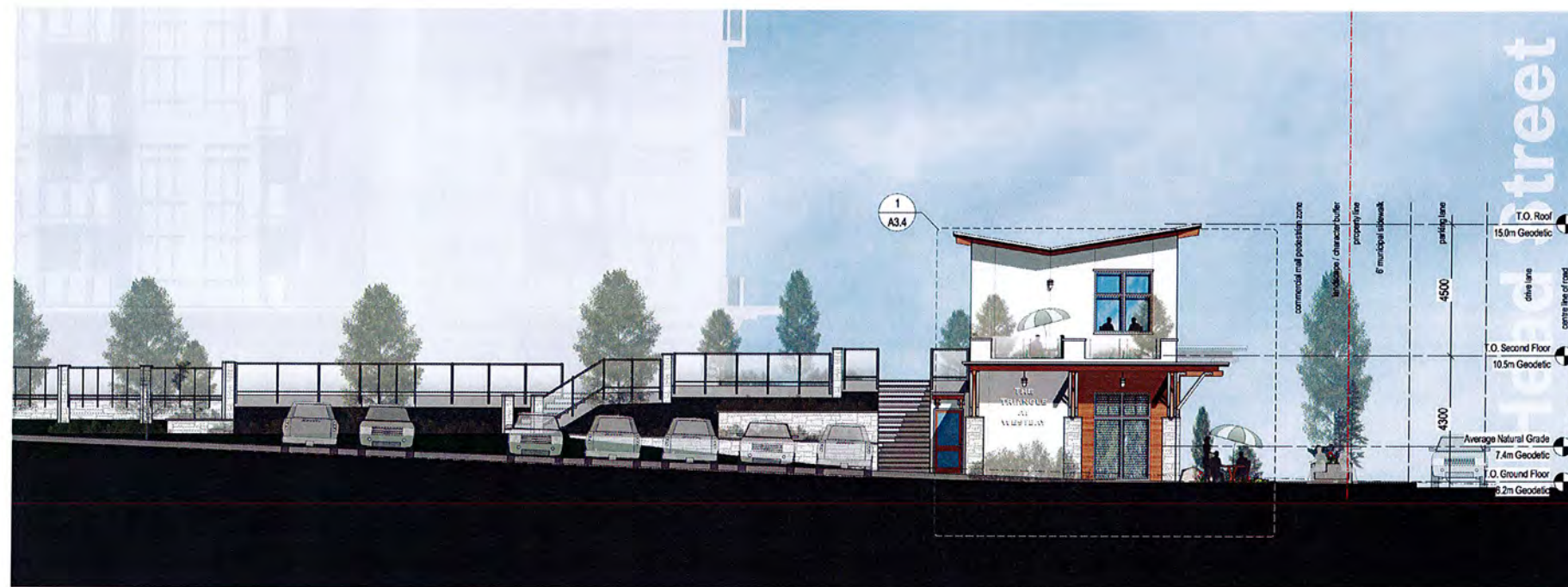
Hillel architecture

PROJECT: THE TRIANGLE AT WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT

Building 1 - Head Street [northeast] & Gore Street [northwest] Elevations

phone 250-992-9998
fax 250-992-9978

drawing no. A3.2



1 Building 1 [southeast] Exterior Elevation
Scale 1:100
A3.3

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikksens Cetol finished exposed roof timber framing
- 03 Wood tone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikksens Cetol finished fascia boards
- 05 Wood tone finished cementitious fascia boards as non combustible finish matching cedar finishes of building 1
- 06 Sikksens Cetol finished exposed cedar soffit
- 07 Wood tone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikksens Cetol stain finish. Timber columns extend down to stone veneer plasters
- 09 Framed sunshade canopy with matching roof flashing. Sikksens Cetol finished soffit over residential windows

Exterior Finishes

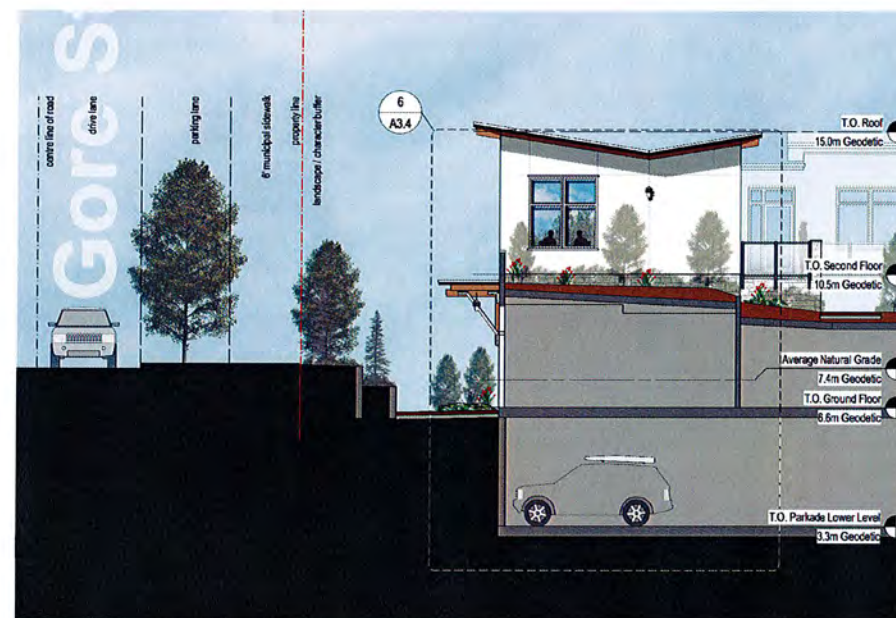
- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray. Colour matched to item 11 above.
- 14 Sikksens Cetol finished 1x4 T & Groove Cedar siding installed square edge out, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding (hardi product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations.

Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite gray painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikksens Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

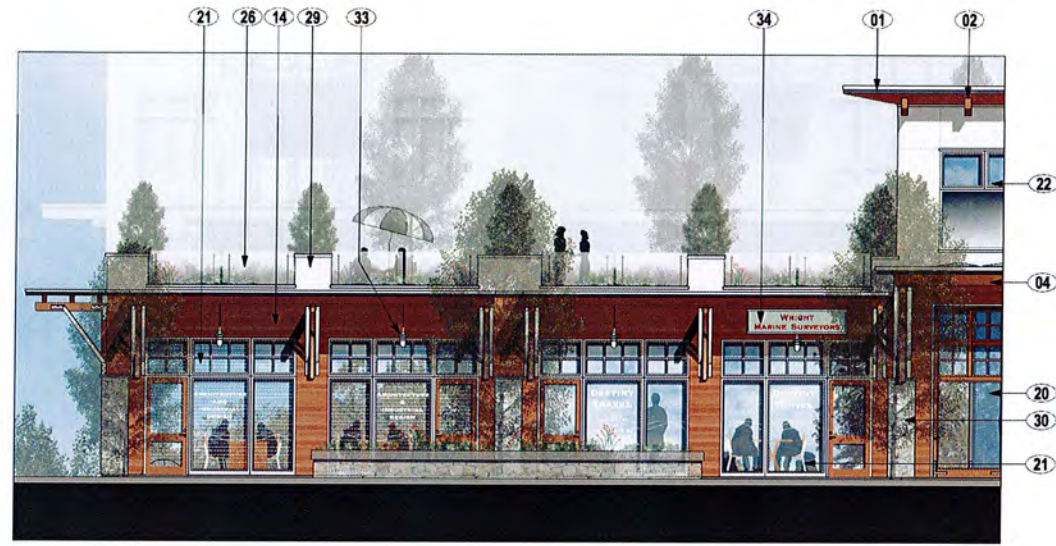
- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight



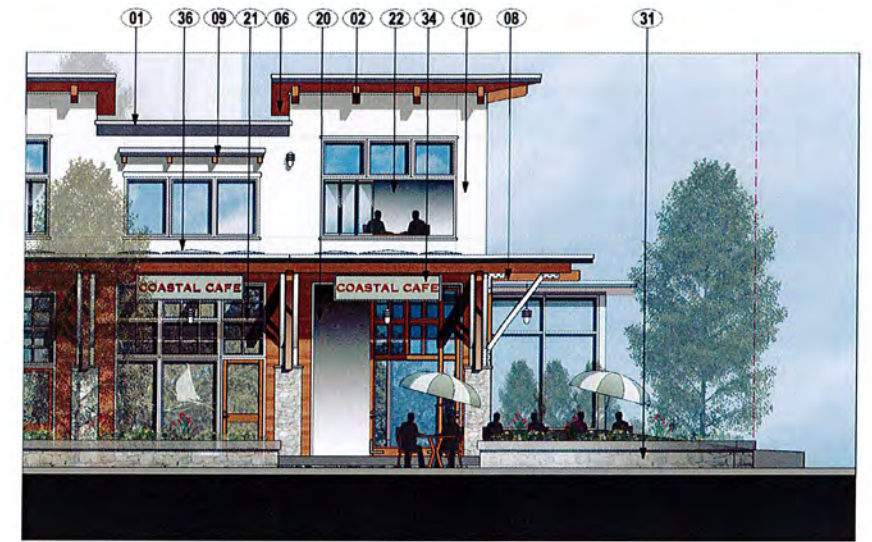
2 Building 1 [southwest] Exterior Elevation
Scale 1:100
A3.3



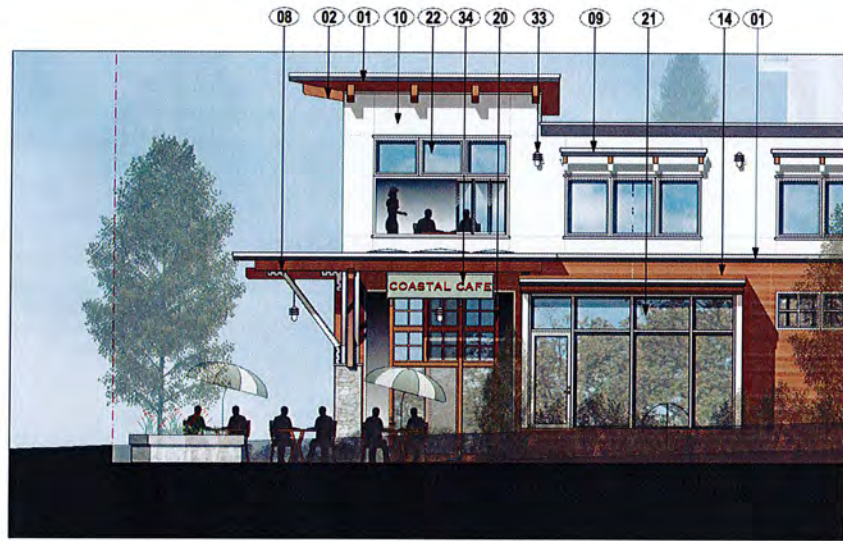
1 Building 1 [southeast] Exterior Elevation
Scale 1:75
A3.4



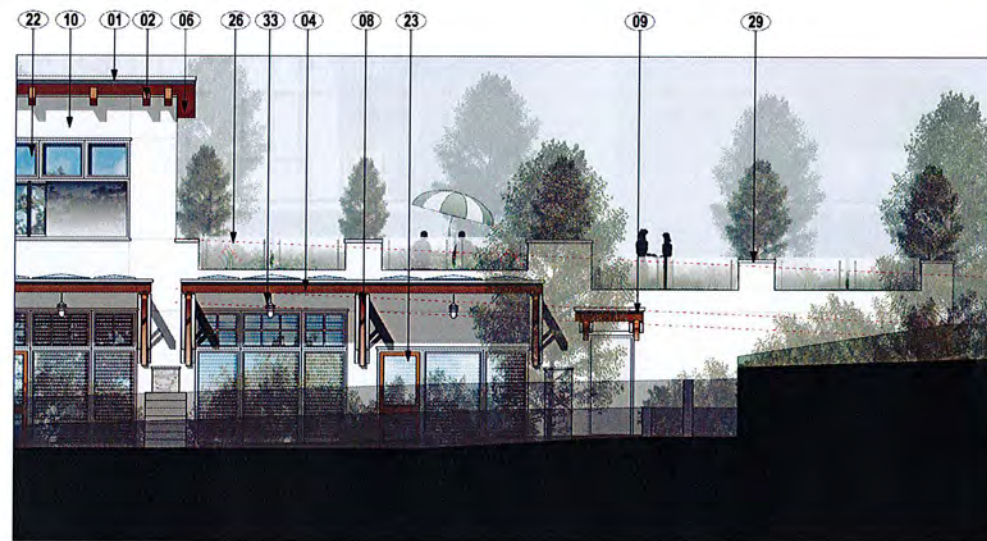
2 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:75
A3.4



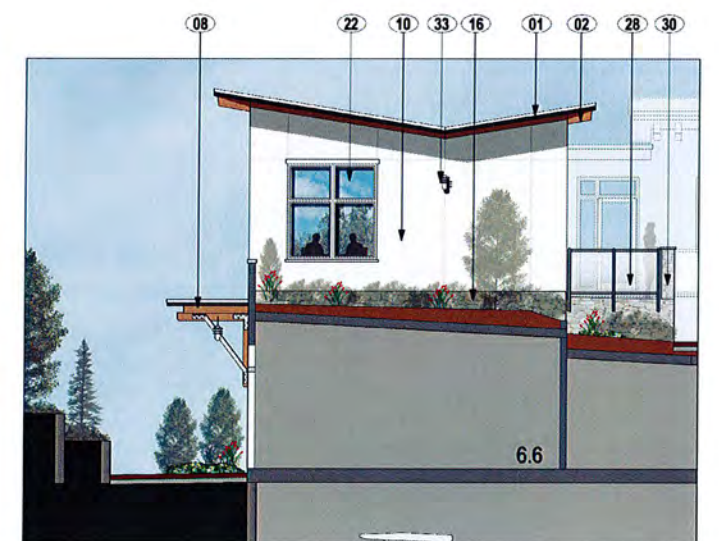
3 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:75
A3.4



4 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:75
A3.4



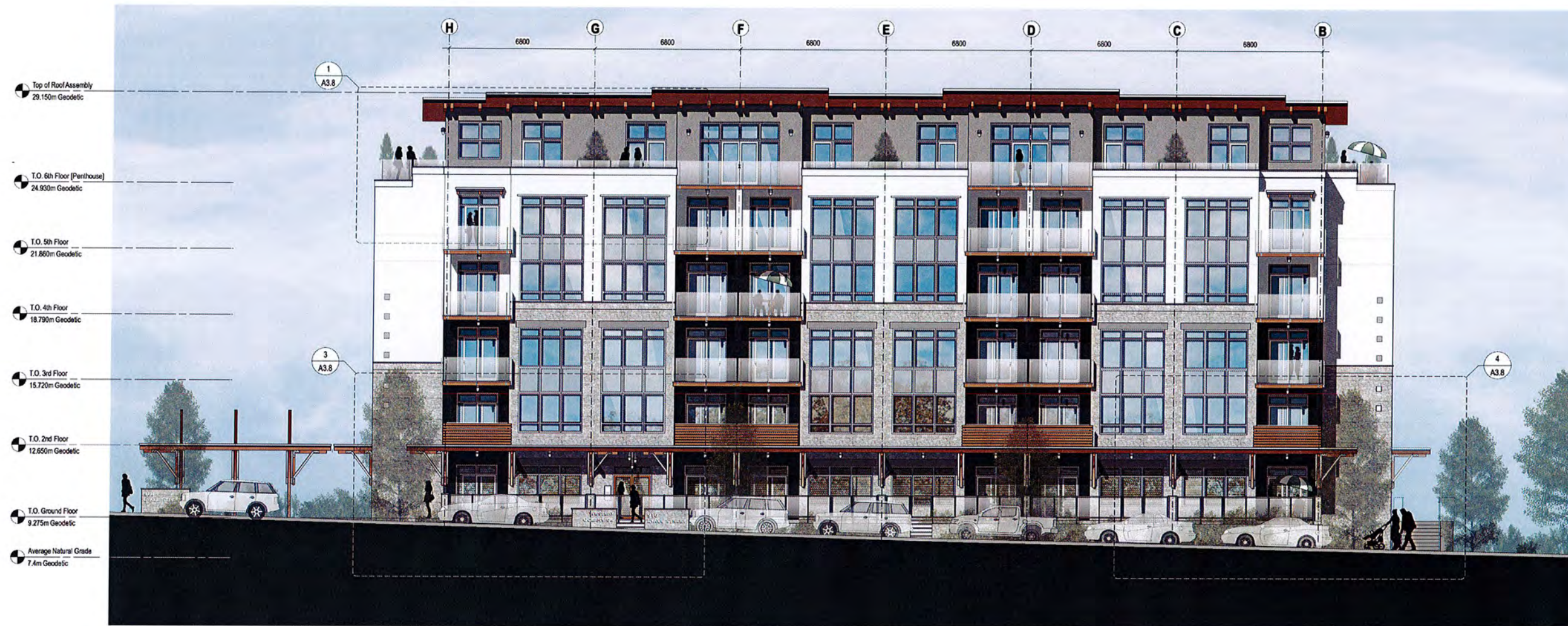
5 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:75
A3.4



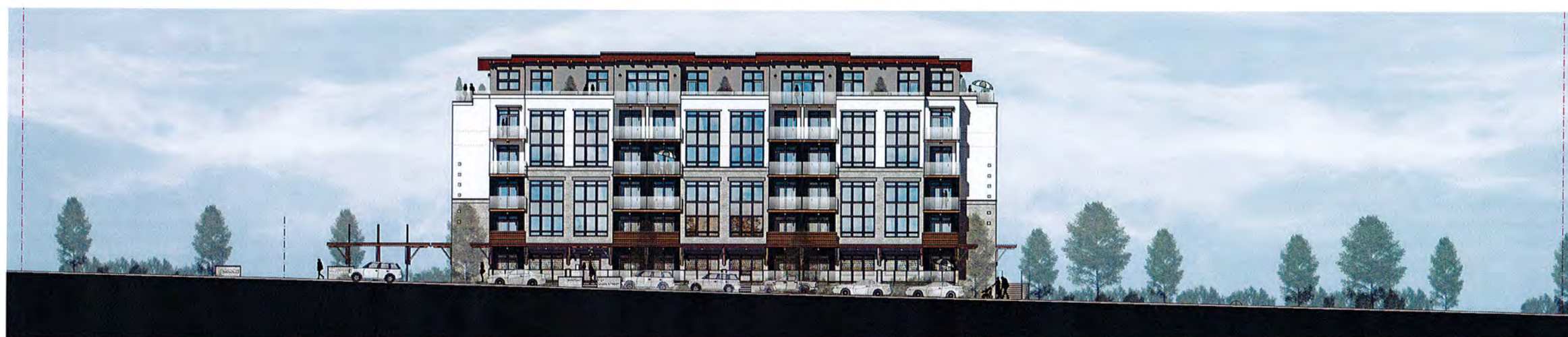
6 Building 1 [southwest] Exterior Elevation
Scale 1:75
A3.4



19 January 2017	Development Permit Application
05 December 2015	Recording Submission
25 September 2014	Public Review
June 2014	City Review



1 Building 2 - Lyall Street (south) Exterior Elevation
Scale: 1:100
A3.5



DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	05 April 2015	DESCRIPTION	Planning Committee
DATE	08 December 2015	DESCRIPTION	Reopening Submission
DATE	JUNE 2016	DESCRIPTION	Final Review
DATE	2014.30	DESCRIPTION	

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THE TRIANGLE @ WESTBAY

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project
THE TRIANGLE @ WESTBAY
HEAD, GORE & LYALL STREET, ESQUIMALT

drawing title
Building 2 - Lyall Street (south) Elevation

drawing no.
A3.5



1 Building 2 - [north] Exterior Elevation
 A3.6 Scale 1:100



DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	05 April 2018	DESCRIPTION	Planning Committee
DATE	05 December 2018	DESCRIPTION	Hearing Submission
DATE	June 2014	DESCRIPTION	
DATE	04 March 2014	DESCRIPTION	
DATE	04 March 2014	DESCRIPTION	

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PROJECT
THE TRIANGLE AT WESTBAY
 HEAD, GORE & LYALL STREETS, ESQUIMALT

DRAWING NO.
Building 2 - [north] Elevations

DRAWING NO.
A3.6

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 DEVELOPMENT SERVICES



1 Building 2 - [east] Exterior Elevation
 Scale 1:100
 A3.7



1 Building 2 - Gore Street [west] Exterior Elevation
 Scale 1:100
 A3.7

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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	06 April 2016	DESCRIPTION	Planning Commentary
DATE	05 December 2015	DESCRIPTION	Hearing Submission
DATE	June 2014	DESCRIPTION	Final Design
DATE	12 February 2014	DESCRIPTION	Final Design
DATE	04 October 2013	DESCRIPTION	Final Design
DATE	2014.03	DESCRIPTION	Final Design

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PROJECT
 THE TRIANGLE @ WESTBAY
 HEAD, GORE & LYALL STREETS, ESQUIMALT

DATE
 Building 2 - Gore Street [west] & East Elevations

PHONE
 250-592-9798

FAX
 250-592-9798

DATE
 2014.03

SCALE
 A3.7



1 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:75



2 Building 2 - Gore Street [east] Exterior Elevation
A3.8 Scale 1:75



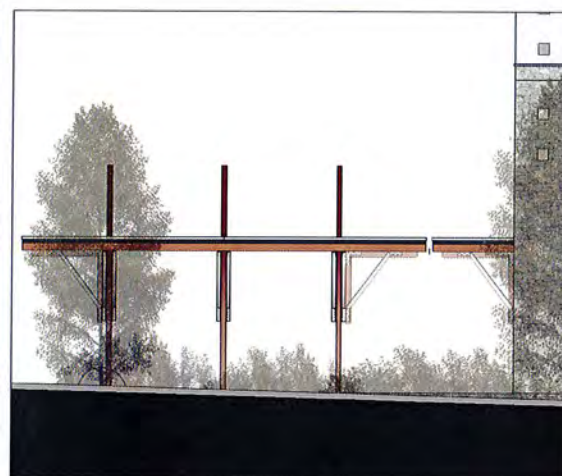
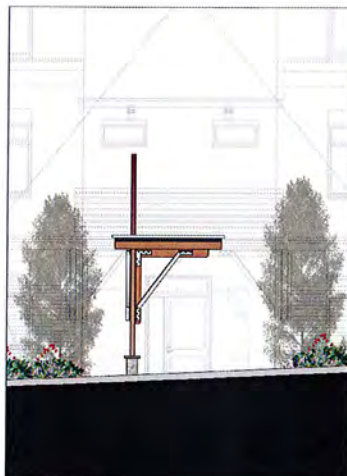
3 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:75



4 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:75



5 Building 2 - Gore Street [east] Exterior Elevation
A3.8 Scale 1:75



6 Building 2 - Side Entry Roof
A3.8 Scale 1:75

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikksens Cetol finished exposed roof timber framing
- 03 Wood tone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikksens Cetol finished fascia boards
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- 06 Sikksens Cetol finished exposed cedar soffit
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- 09 Framed sunshade canopy with matching roof flashing. Sikksens Cetol finished soffit over residential windows

Exterior Finishes

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- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Out Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations.

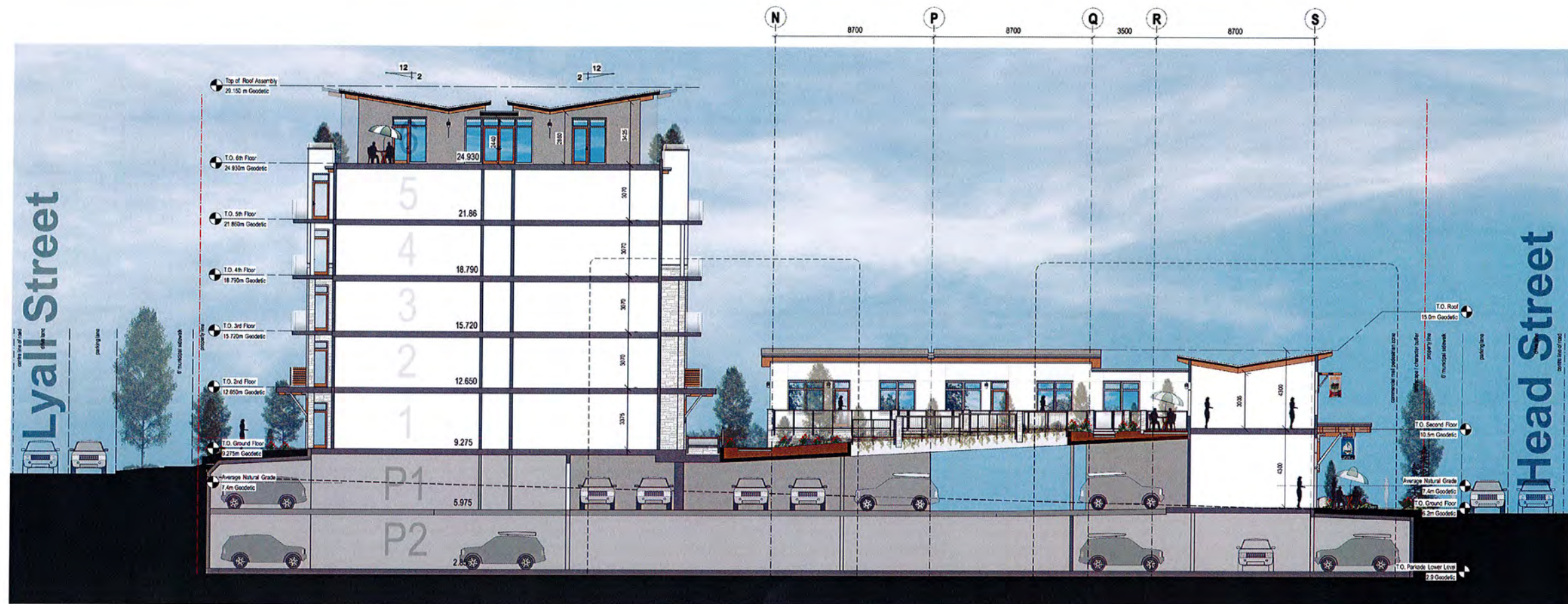
Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite grey painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikksens Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open traing's. Clear tempered glazed accent panels.
- 29 Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

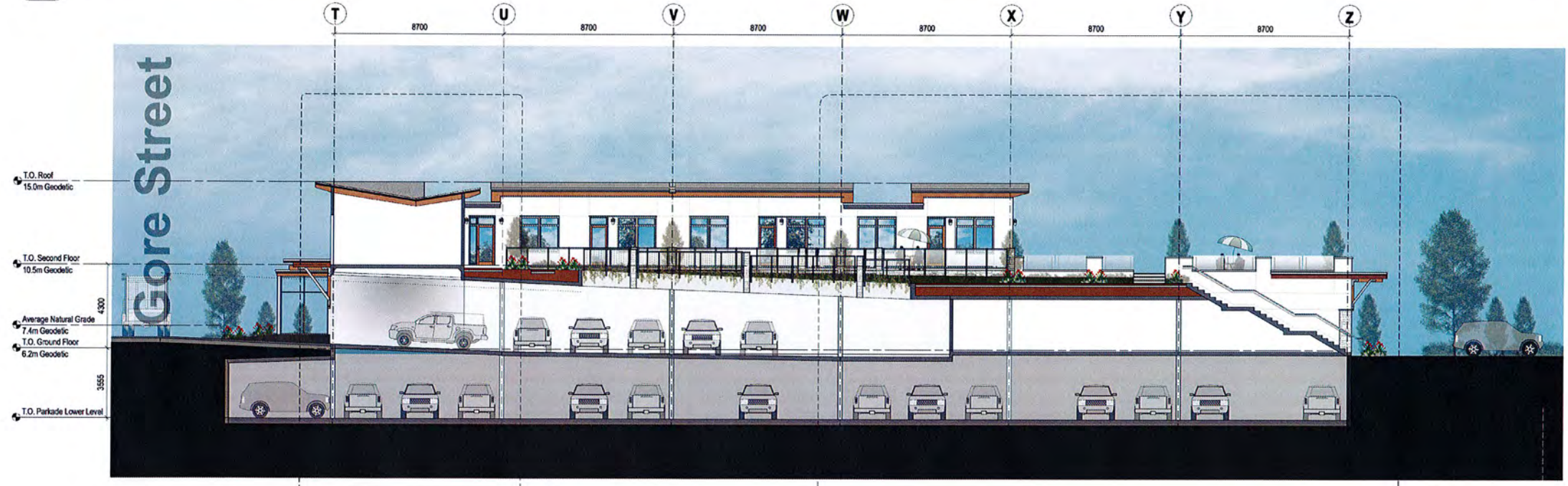
Exterior Features

- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Out Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Out Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight





1 Buildings 1 & 2 - Section Facing [northwest] Gore Street
 A4.1 Scale 1:125



1 Building 1 - Section Facing [northeast] Head Street
 A4.1 Scale 1:125



18 January 2017	Development Permit Application
06 April 2018	Planning Committee
08 December 2018	Resolving Submission
June 2024	Final Review

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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Hille architecture

project: THE TRIANGLE @ WESTBAY
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title: Building 1 & 2 - Section Facing [northwest] Gore Street

phone: 250-592-9938
 fax: 250-592-9978

drawing no: A4.1



1 Building 2 - Section Facing [south] Lyall Street
 A4.2 Scale 1:100

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 mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY
 ESQUIMALT • BRITISH COLUMBIA

DATE	14 July 2017	DESCRIPTION	Development Permit Application
DATE	06 April 2016	DESCRIPTION	Planning Committee
DATE	08 October 2015	DESCRIPTION	Receiving Submission
DATE	June 2014	DESCRIPTION	

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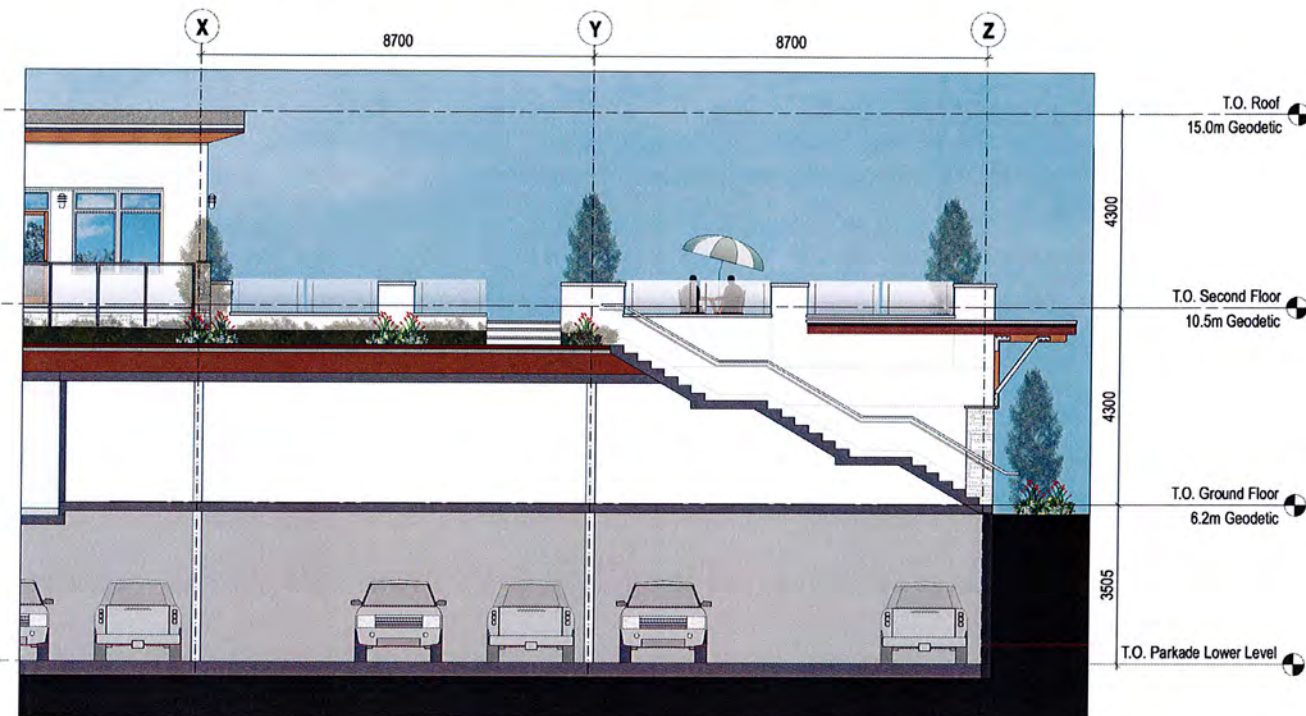
PROJECT
THE TRIANGLE @ WESTBAY
 HEAD GORE & LYALL STREETS, ESQUIMALT

DRAWING NO.
Building 2 - Section Facing [south] Lyall Street

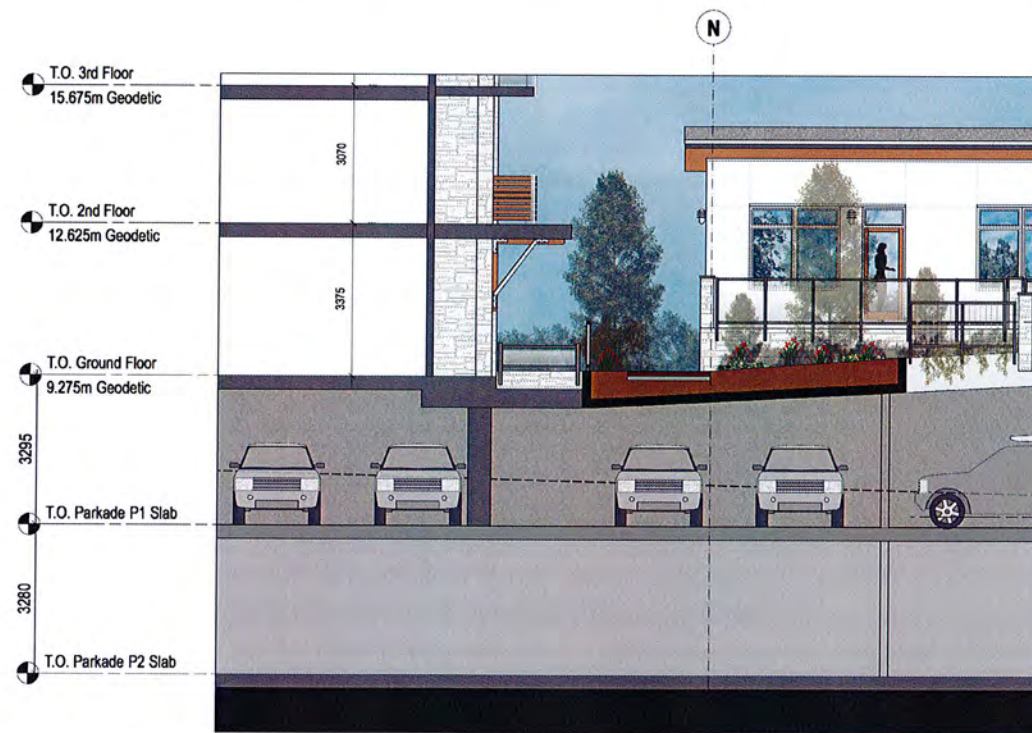
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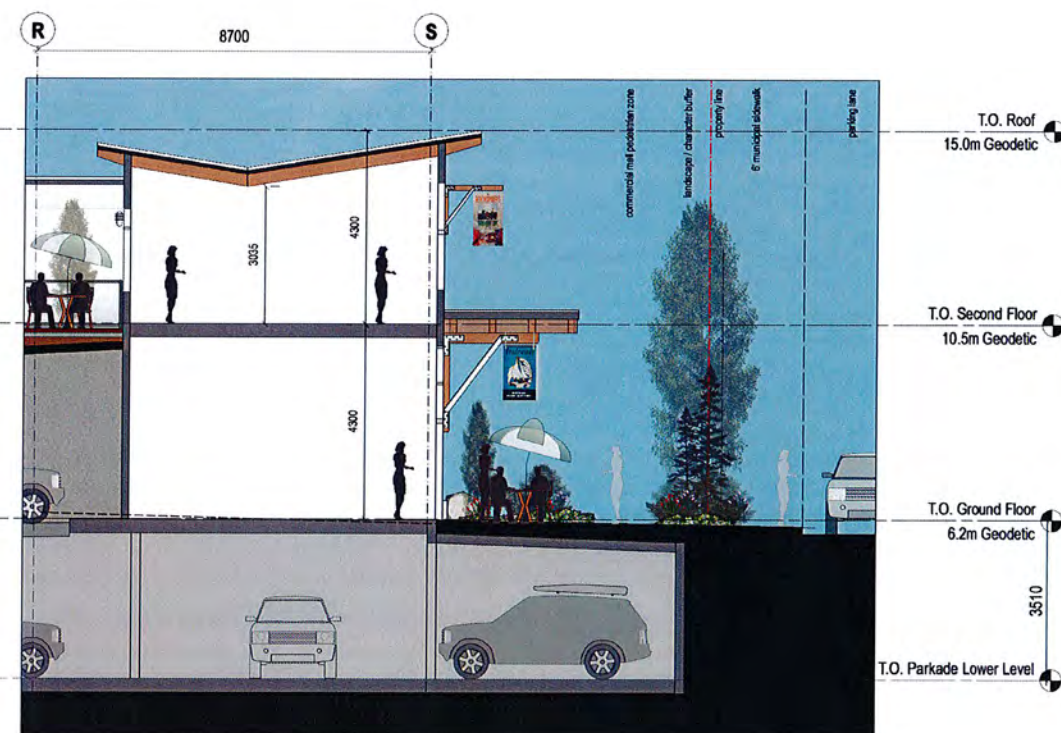
1 Buildings 1 - Detail Section
A4.3 Scale 1:75



2 Building 1 - Detail Section
A4.3 Scale 1:75



3 Buildings 1 & 2 - Detail Section
A4.3 Scale 1:75



4 Building 1 - Detail Section
A4.3 Scale 1:75

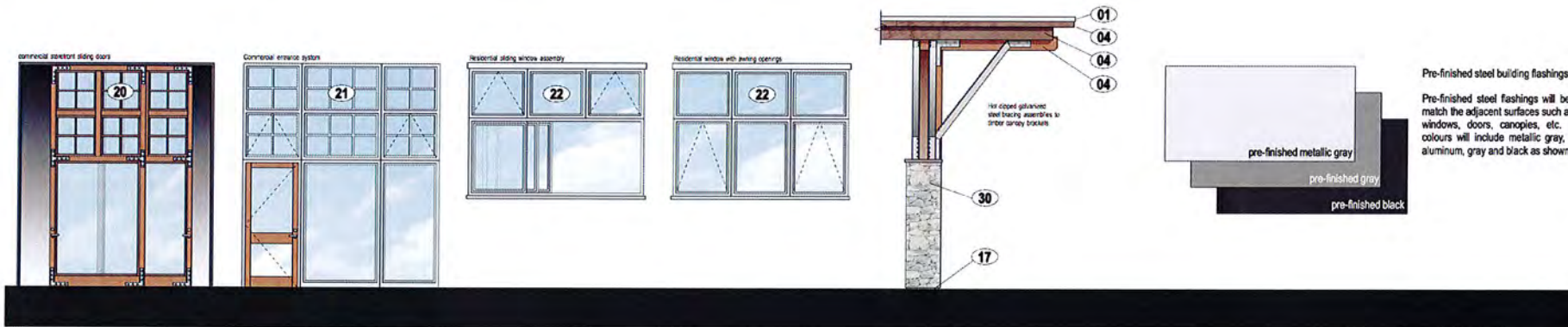
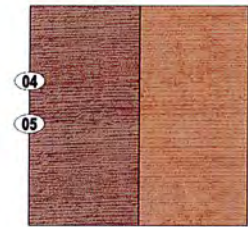
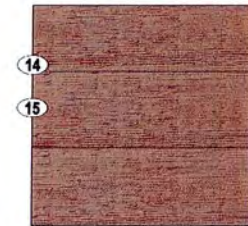
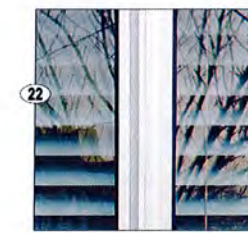
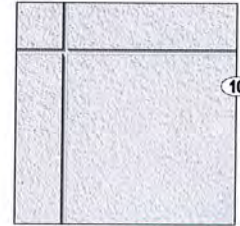


THE TRIANGLE @ WESTBAY

ESQUIMALT + BRITISH COLUMBIA

Rev	Date	Description
1	18 January 2017	Development Permit Application
2	06 April 2016	Planning Commentary
3	08 December 2015	Revisory Submission
4	June 2014	Rev 1
5		Rev 2
6		Rev 3
7		Rev 4
8		Rev 5
9		Rev 6
10		Rev 7
11		Rev 8
12		Rev 9
13		Rev 10
14		Rev 11
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95		Rev 92
96		Rev 93
97		Rev 94
98		Rev 95
99		Rev 96
100		Rev 97
101		Rev 98
102		Rev 99
103		Rev 100

Colour And Materials Palette - Building 1



Pre-finished steel building flashings:
Pre-finished steel flashings will be used to match the adjacent surfaces such as exterior windows, doors, canopies, etc. Multiple colours will include metallic gray, anodized aluminum, gray and black as shown.

Roofing

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Exterior Features

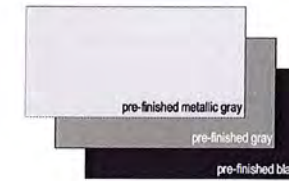
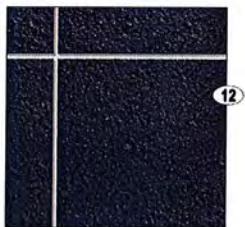
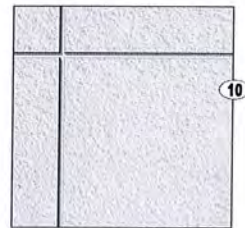
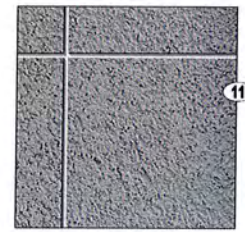
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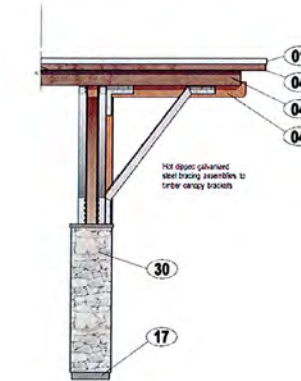
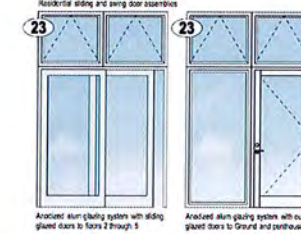
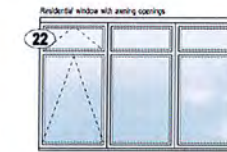
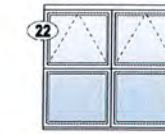
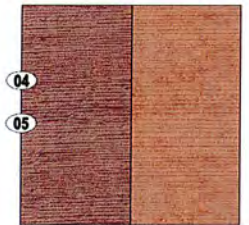
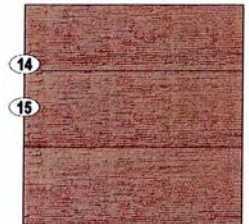
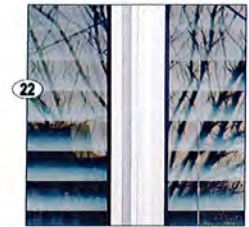
THE TRIANGLE AT WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Colour And Materials Palette - Building 2



Pre-finished steel building flashings:
Pre-finished steel flashings will be used to match the adjacent surfaces such as exterior windows, doors, canopies, etc. Multiple colours will include metallic gray, anodized aluminum, gray and black as shown.



Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikksens Cetol finished exposed roof timber framing
- 03 Wood lone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikksens Cetol finished fascia boards
- 05 Wood lone finished cementitious fascia boards as non combustible finish matching cedar finishes of building 1
- 06 Sikksens Cetol finished exposed cedar soffit
- 07 Wood lone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikksens Cetol stain finish. Timber columns extend down to stone veneer plasters
- 09 Framed sunshade canopy with matching roof flashing. Sikksens Cetol finished soffit over residential windows

Exterior Finishes

- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray. Colour matched to item 11 above.
- 14 Sikksens Cetol finished 1x4 T & Groove Cedar siding installed square edge out, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood lone 1x4 ship lap siding (hard product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations.

Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite grey painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikksens Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight



THE TRIANGLE AT WESTBAY

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	A CONCRETE PAVERS - ABBOTSFORD 'ESTATE STONE'
	B BROOM FINISH CONCRETE SIDEWALK
	C POURED CONCRETE, ALTERNATING FINE BROOM & EXPOSED AGGREGATE FINISHES
	D PRECAST 2" THICK CONCRETE SLABS
	E SANDWASH FINISH POURED CONCRETE
	F CONCRETE PAVERS OR STAMPED CONCRETE
	G PRECAST PAVER COBBLE STONES
	H ASPHALT
	I BROOM FINISH COLOURED CONCRETE SIDEWALK

RECEIVED
MAR 02 2017
 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT SERVICES



TRIANGLE CORNER DEVELOPED AS A GATHERING & CEREMONIAL SPACE. COMPASS POINTS DEMARCATED WITH TRIANGULAR CONTRASTING MARKER.

TERRACED PLANTERS

PRIVATE SOCIAL GARDEN SPACE ABOVE RESIDENCE

MATURE GARRY OAK IN BOULEVARD TO BE PROTECTED AND PRESERVED IF FEASIBLE. EXCAVATIONS IN SIDEWALK REQUIRED FOR U/G HYDRO

TRIANGLE CORNER DEVELOPED AS A LANDMARK PUBLIC SPACE

OLD APPLE TREE RECOGNIZED FOR IT'S ANCIENT GNARLED FORM TO BE SURROUNDED WITH NATURALISTIC LOW HEIGHT PLANTINGS TO DETER TEMPTATION TO PLAY ON FRAGILE TREE

PRIVATE SOCIAL GARDEN SPACE ABOVE RETAIL UNIT ORIENTED TO DIRECT VIEWS TOWARDS VICTORIA HARBOUR

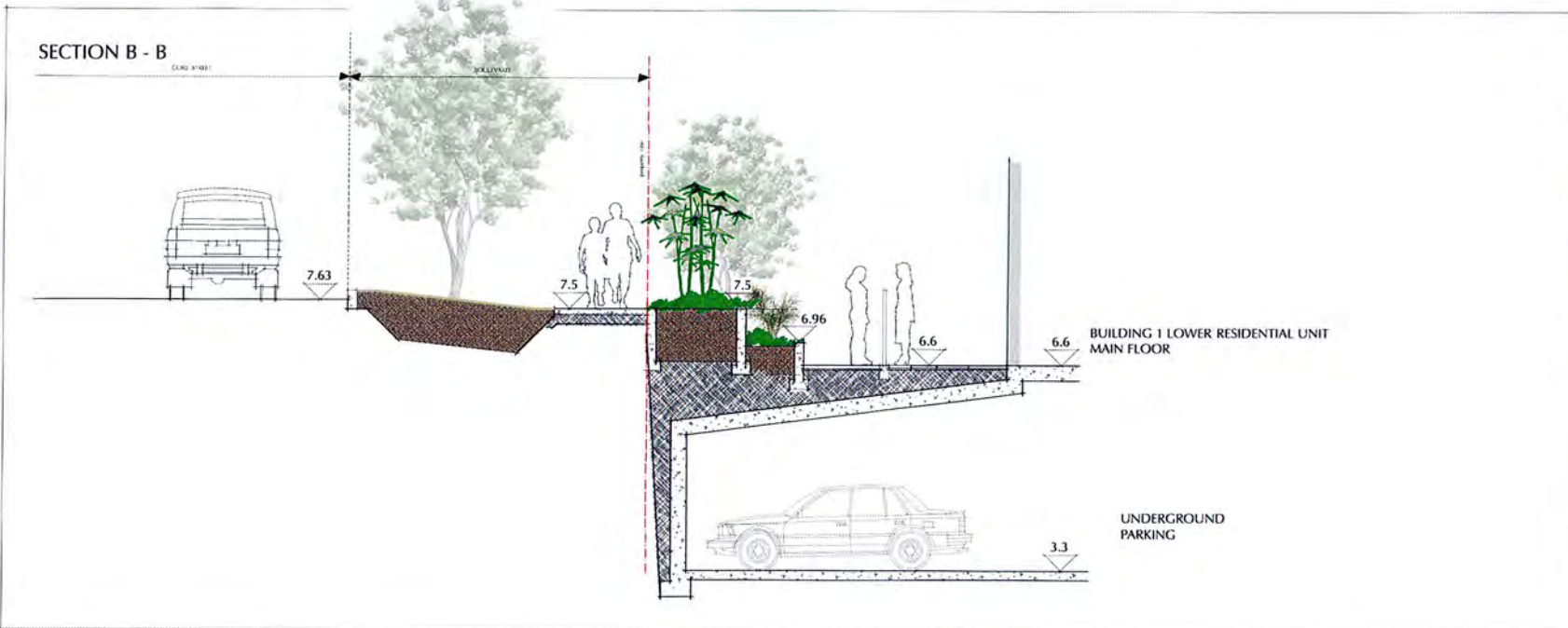
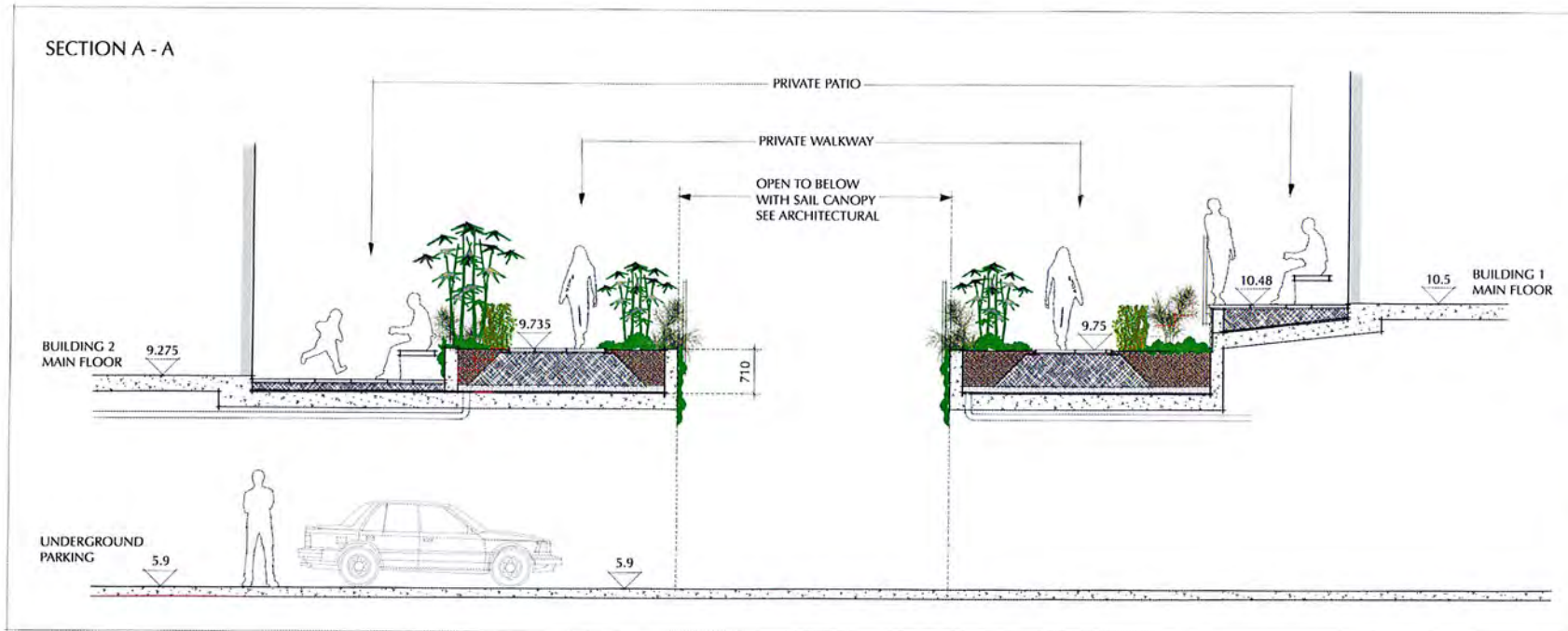
STREET TREES PLANTED IN MUNICIPAL BOULEVARD

THE TRIANGLE @ WESTBAY
 ESQUIMALT + BRITISH COLUMBIA

no.	rev.	description	date	author	checked
R1	1	20 Feb 2017	20 Feb 2017	MS	MS
R1	2	15 Jan 2017	15 Jan 2017	MS	MS
R1	3	15 Jan 2017	15 Jan 2017	MS	MS
R1	4	15 Jan 2017	15 Jan 2017	MS	MS
R1	5	15 Jan 2017	15 Jan 2017	MS	MS
R1	6	15 Jan 2017	15 Jan 2017	MS	MS
R1	7	15 Jan 2017	15 Jan 2017	MS	MS
R1	8	15 Jan 2017	15 Jan 2017	MS	MS
R1	9	15 Jan 2017	15 Jan 2017	MS	MS
R1	10	15 Jan 2017	15 Jan 2017	MS	MS
R1	11	15 Jan 2017	15 Jan 2017	MS	MS
R1	12	15 Jan 2017	15 Jan 2017	MS	MS
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R1	14	15 Jan 2017	15 Jan 2017	MS	MS
R1	15	15 Jan 2017	15 Jan 2017	MS	MS
R1	16	15 Jan 2017	15 Jan 2017	MS	MS
R1	17	15 Jan 2017	15 Jan 2017	MS	MS
R1	18	15 Jan 2017	15 Jan 2017	MS	MS
R1	19	15 Jan 2017	15 Jan 2017	MS	MS
R1	20	15 Jan 2017	15 Jan 2017	MS	MS

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project	THE WESTBAY TRIANGLE HEAD, GORE & LYALL STREETS, ESQUIMALT
drawing title	LANDSCAPE CONCEPT
client	LINDHOLM LAND & INVESTMENT CORPORATION
date	15 JAN 2017
revision no.	1
drawing no.	L1



PLANT SCHEDULE	BOTANICAL NAME	COMMON NAME	HEIGHT	CALIPER	ROOTBALL & BRANCHING	CONTAINER SIZE	SPACING	QUANTITY	QUALITY	INSTRUCTIONS
TREES										
a	ACER CIRCINATUM	VINE MAPLE	250-300 CM	4 CM	B&B, 60 CM DIAM. BALL, 3 STRONG STEMS		AS SHOWN	4	CNLA NO. 1 QUALITY	STAKED
b	CORNUS KOUSA SATOMI	SATOMI FLOWERING DOGWOOD	250-300 CM	4 CM	B&B, 60 CM DIAM. BALL, 3 STRONG STEMS		AS SHOWN	7	CNLA NO. 1 QUALITY	STAKED
c	CHAMAECYPARIS NODOSATA 'GREEN ARROW'	GREEN ARROW NODOSATA CYPRESS	300 CM		B&B, 130 CM DIAM. BALL		AS SHOWN	2	CNLA NO. 1 QUALITY	STAKED
d	CERCIS CANADENSIS 'RISING SUN'	RISING SUN REDBUD	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	2	CNLA NO. 1 QUALITY	STAKED
e	ACER RUBRUM 'OCTOBER GLORY'	RED MAPLE	300-425 CM	6 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	4	CNLA NO. 1 QUALITY	STAKED
f	CORNUS EDDIES WHITE WONDER	FLOWERING DOGWOOD	300-425 CM	5 CM	B&B, 60 CM DIAM. BALL		AS SHOWN	3	CNLA NO. 1 QUALITY	STAKED
g	GINKGO BILOBA 'AUTUMN GOLD'	GINKGO	300-425 CM	6 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	8	CNLA NO. 1 QUALITY	STAKED
h	MAGNOLIA 'GALAXY'	GALAXY MAGNOLIA	300-425 CM	6 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	6	CNLA NO. 1 QUALITY	STAKED
i	CHAMAECYPARIS OBTUSA GRACILIS	SLENDER HINKI FALSE CYPRESS	200 CM		CONTAINER	#15 POT	AS SHOWN	5	CNLA NO. 1 QUALITY	STAKED
j	PRUNUS X YEDOENSIS 'AKEDONO'	JAPANESE FLOWERING CHERRY	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	3	CNLA NO. 1 QUALITY	STAKED
k	CERCIS CANADENSIS 'APPALACHIAN RED'	CANADIAN REDBUD	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	2	CNLA NO. 1 QUALITY	STAKED
o	LIQUIDAMBER STYRACIFLUA WORPLESOON	AMERICAN SWEETGUM	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	3	CNLA NO. 1 QUALITY	STAKED
SPECIMEN LARGE SHRUBS										
l	ABRUTUS UNEDO	STRAWBERRY TREE	250-300 CM	MULTI	B&B, 50 CM DIAM. BALL		AS SHOWN	7	CNLA NO. 1 QUALITY	STAKED
m	MAGNOLIA X YULCAN	RED FL. MAGNOLIA	175-200 CM		B&B, 55 CM DIAM. BALL, 4 STRONG STEMS		AS SHOWN	3	CNLA NO. 1 QUALITY	STAKED
n	COTINUS COGGYGRIA	AMERICAN SMOKE TREE	60-100 CM		CONTAINER	#5 POT	AS SHOWN	4	CNLA NO. 1 QUALITY	
SHRUBS - TOTAL AREA, 23050 M. AVERAGE SPACING - 1 PLANT / SQ.M. = 230 SHRUBS.										
SHRUBS - NATIVE										
	ARCTOSTAPHYLOS UVA URSI 'VANCOUVER JADE'	KINKY KINK	15-30 CM		3 CANES	#1 POT	600		CNLA NO. 1 QUALITY	
	CORNUS STOLONIFERA 'KELSEY'	DWARF RED OSIER DOGWOOD	25-60 CM		4 CANES	#2 POT	1000		CNLA NO. 1 QUALITY	
	MAHONIA AQUIFOLIUM	TALL OREGON GRAPE	25-60 CM		4 CANES	#2 POT	1000		CNLA NO. 1 QUALITY	
	MAHONIA NERVOSA	DULL OREGON GRAPE	15-40 CM		3 CANES	#1 POT	600		CNLA NO. 1 QUALITY	
SHRUBS - ORNAMENTAL										
	HYDRANGEA BLUE BIRD	LACECAP HYDRANGEA	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO. 1 QUALITY	
	LEYCESTERIA GOLDEN LANTERNS	GOLDEN PHEASANT BERRY	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO. 1 QUALITY	
	MAHONIA X CHARITY	MAHONIA	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO. 1 QUALITY	
	NANDINA DOMESTICA	HEAVENLY BAMBOO	60-100 CM		SPREAD 35 CM	#5 POT	1000		CNLA NO. 1 QUALITY	
	PHILADELPHUS LIMONEL	MOCK ORANGE	40-100 CM		5 CANES	#5 POT	1500		CNLA NO. 1 QUALITY	
	PINUS MUGO 'PUMILIO'	MOUNTAIN PINE	50 CM		SPREAD 50 CM	#5 POT	1000		CNLA NO. 1 QUALITY	
	RUBUS CALYCINOIDES	CREeping RASPBERRY	15-30 CM		3 CANES	#1 POT	600		CNLA NO. 1 QUALITY	
	VACCINIUM CORYMBOSUM 'DUKE'	DUKE' EDBLE BLUEBERRY	25-60 CM		4 CANES	#2 POT	1000		CNLA NO. 1 QUALITY	
PERENNIALS - TOTAL AREA, 235 SQ.M. AVERAGE SPACING - 3 PLANTS / SQ.M. = 705 SHRUBS.										
PERENNIALS & ORNAMENTAL GRASSES										
	DIERAMA FL. CHERRIMA	ANGEL'S FISHING ROD	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	600		CNLA NO. 1 QUALITY	
	EPIMEDIUM RUBRUM	BARRENWORT	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	600		CNLA NO. 1 QUALITY	
	GERANIUM MACRORRHIZUM	CRANESBILL	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	600		CNLA NO. 1 QUALITY	
	HEMEROCALLIS 'STELLA D'ORO'	DAY LILY YELLOW FLS.	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	600		CNLA NO. 1 QUALITY	
	RIS ENSATA 'ORIENTAL LADY'	PINK JAPANESE RIS	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	600		CNLA NO. 1 QUALITY	
	MISCANTHUS 'GRACILLIMUS'	JAPANESE SILVER GRASS				#2 POT	1000		CNLA NO. 1 QUALITY	
HEDGE PLANTS										
	BUXUS SEMPERVIRENS	BOX	25-60 CM			#2 POT	600	400	CNLA NO. 1 QUALITY	
	LONICERA NITIDA 'RED TIPS'	CHINESE PRIVET	25-60 CM			#2 POT	750	9	CNLA NO. 1 QUALITY	
	ABRUTUS UNEDO	STRAWBERRY TREE	60-100 CM			#5 POT	1000	18	CNLA NO. 1 QUALITY	
	TAXUS X HICKSII	HICKS'S YEW	60-100 CM			#5 POT	750	28	CNLA NO. 1 QUALITY	
	TSUGA HETEROPHYLLA	HEMLOCK	60-100 CM			#5 POT	1000	27	CNLA NO. 1 QUALITY	
RAIN GARDEN PLANTS										
	CAREX ELATA 'AUREA'	GOLDEN SEDGE	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	500	80	CNLA NO. 1 QUALITY	
	CAREX ORNUPTA	SLOUGH SEDGE	15-18 CM		2 FAN. BLOOMING SIZE	#1 POT	500	80	CNLA NO. 1 QUALITY	
	CORNUS STOLONIFERA 'KELSEY'	DWARF RED OSIER DOGWOOD	50 CM		SPREAD 50 CM	#2 POT	1000	35	CNLA NO. 1 QUALITY	

NOTE: PLANT SPECIES, CULTIVARS, SIZES AND NUMBERS TO BE REFINED AT DETAIL DESIGN STAGE



THE TRIANGLE @ WESTBAY
ESQUIMALT + BRITISH COLUMBIA

Project: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT
Drawing title: LANDSCAPE SECTIONS & PLANTING
Drawing No: L2

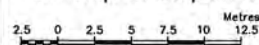
Revision table:
R1: 28 Feb 2017
R2: 15 Jan 2017
R3: 19 Jan 2017

Scale: 1:30

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Plan Showing Proposed Building Location
 Situated on Proposed Consolidation of:
 - 1, 2, 4, 5, 6, 7, 8, Block H, Plan 292;
 - Strata Plan 509;
 all of Section 11, Esquimalt District.

Township of Esquimalt



The intended plot size of this plan is 864 mm in width by 559 mm in height (D size) when plotted at a scale of 1:250

Legend:

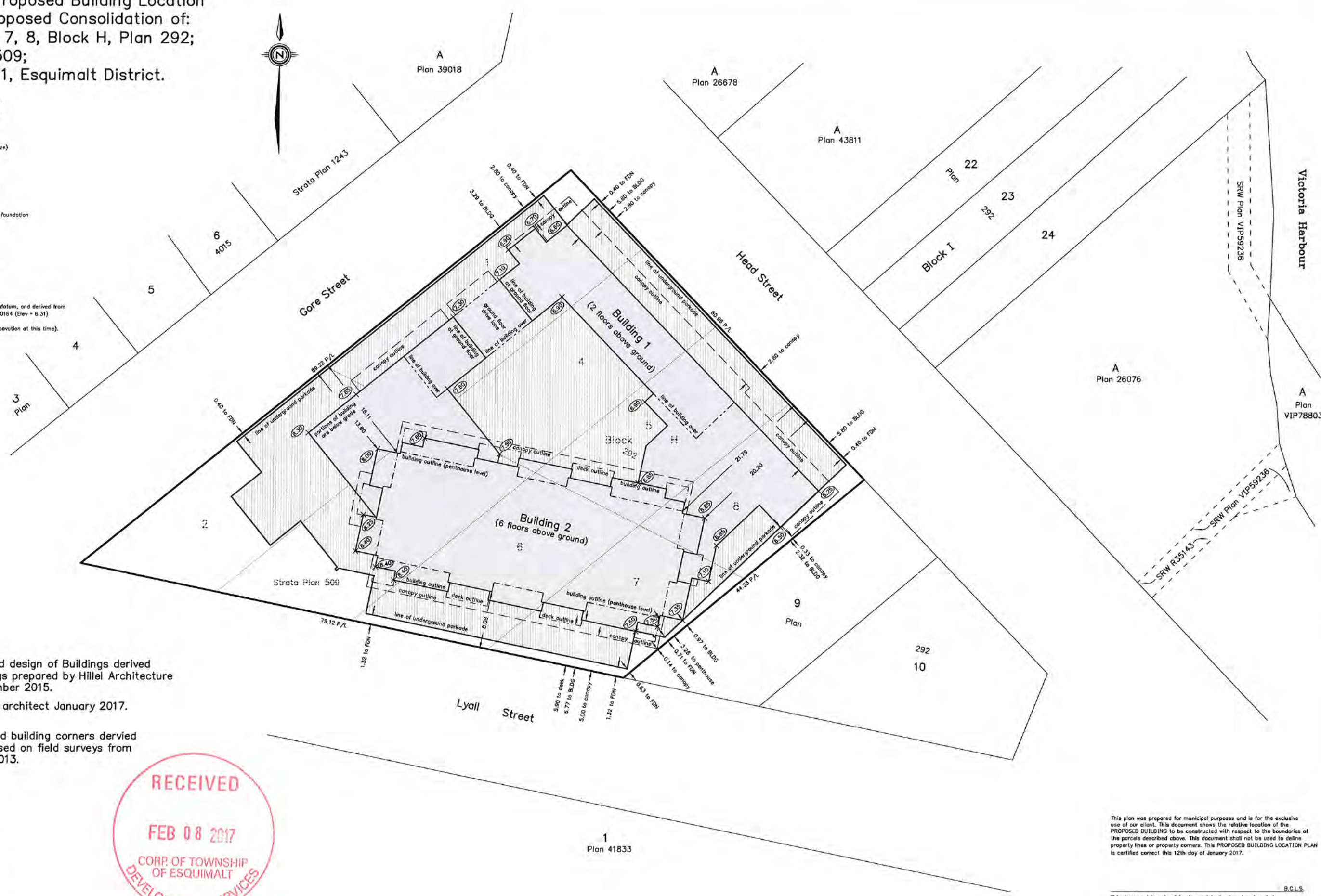
All distances are in metres and decimals thereof unless otherwise indicated.

- denotes parkade and underground foundation
- building footprint
- P/L denotes property line
- FDN denotes foundation underground
- BLDG denotes building above ground

Total Site Area = 0.408 ha

Elevations are in metres, based on geodetic datum, and derived from Bench Marks 84H0163 (Elev = 9.98) and 84H0164 (Elev = 6.31).

Elevation at Original Ground (no excavation at this time).



Note:
 Proposed location and design of Buildings derived from autocad drawings prepared by Hillel Architecture and received 7 December 2015.
 Plan amended as per architect January 2017.

Elevations at proposed building corners derived from interpolation based on field surveys from July 2012 and April 2013.

