



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**DESIGN REVIEW COMMITTEE
AGENDA**

**THURSDAY, FEBRUARY 9, 2017
2:00 P.M.
ESQUIMALT COUNCIL CHAMBERS**

MEMBERS: Roger Wheelock (Chair) Wendy Kay
Ally Dewji Richard Iredale
Paul De Greeff Jill Singleton
Robert Schindelka

RESOURCE MEMBER: Cst. Franco Bruschetta [Non-Voting]

COUNCIL LIAISON: Councillor Beth Burton-Krahn
Councillor Olga Liberchuk

STAFF LIAISON: Bill Brown, Director of Development Services

SECRETARY: Pearl Barnard

- I. **CALL TO ORDER**
- II. **LATE ITEMS**
- III. **ADOPTION OF AGENDA**
- IV. **ADOPTION OF MINUTES – JANUARY 11, 2017**
- V. **STAFF REPORT**

**(1) DEVELOPMENT PERMIT APPLICATION
“REVIEW OF DESIGN REVISIONS FOR THE CORE AREA
WASTEWATER TREATMENT PLANT AT MCLOUGHLIN POINT”
337 Victoria View Road
Lot 1; Section 11; Plan EPP36468**

PURPOSE OF APPLICATION:

The purpose of this application is to ensure that the applicant’s intentions are consistent with the Zoning Bylaw and the design guidelines for Development Permit Area No. 3 “Industrial”. The comments of the Design Review Committee are invaluable in helping staff in reviewing the application and Council in their decision making process vis a vis the application.

RECOMMENDATION:

The Esquimalt Design Review Committee recommends that the development permit application for the Core Area Waste Water Treatment Plant be forwarded to Council with a recommendation to **approve, approve with conditions, or deny the application including reasons for the chosen recommendation. Alternatively, if the Design Review Committee does not feel that the design has been adequately resolved or more information is required, it can request that the applicant return to the next meeting with revised drawings and additional information.**

(2) REZONING APPLICATION

“Marinaview”

460 Head Street [Lot 10, Block H, Section 11, Esquimalt District, Plan 292]

464 Head Street [Lot 9, Block H, Section 11, Esquimalt District, Plan 292]

PURPOSE OF APPLICATION:

The applicant is requesting a change in zoning from the current mix of RS-1 [Single Family] and C-7 [West Bay Commercial] zoning which currently regulates the two subject properties to a Comprehensive Development zone [CD] to facilitate the consolidation of the subject properties and authorize a new five storey, commercial/ residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, siting, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan and the West Bay Neighbourhood Design Guidelines [attached], endorsed by Esquimalt Council on November 16, 2015 [attached].

Specific form and character issues relating to the aesthetics of the building, such as cladding materials, window materials, doorways, streetscape improvements and landscaping will be evaluated in a separate application for Development Permit should this rezoning application be approved by Council.

RECOMMENDATION:

The Esquimalt Design Review Committee recommends that the application for rezoning to facilitate consolidation of two properties located between Head Street and Lyall Street to permit a new five storey, commercial/ residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units, and incorporating siting, height and massing consistent with architectural plans prepared by Hillel Architecture, stamped “Received January 18, 2017” be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

(3) DEVELOPMENT PERMIT APPLICATION

“West Bay Triangle”

468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]

470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]

472 Head Street [Lot 4, Block H, Section 11, Esquimalt District, Plan 292]

515 Gore Street [Lot 1, Block H, Section 11, Esquimalt District, Plan 292]

509 Gore Street [Lot 2, Block H, Section 11, Esquimalt District, Plan 292]

922 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]

920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]

918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]

912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]

PURPOSE OF APPLICATION:

The applicant is proposing to construct a the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units on a consolidation of nine subject properties to be regulated by Comprehensive Development district No. 98 [CD-98].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application meets the intent of the design guidelines contained in Section 9.4 of the Township’s Official Community Plan. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines [attached] as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit. Evaluation of this application should focus on issues respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of buildings and other structures.

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received January 20, 2017”, and the landscape plan prepared by Small and Rossell Landscape Architects, stamped “Received January 20, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received February 3, 2017” for the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, proposed to be located on the “West Bay Triangle” lands, be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

VI. STAFF LIAISON STATUS REPORT

VII. NEW BUSINESS

VIII. NEXT REGULAR MEETING
March 8, 2017

IX. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT
ADVISORY DESIGN REVIEW COMMITTEE MEETING MINUTES
HELD
JANUARY 11, 2017
ESQUIMALT COUNCIL CHAMBERS

MEMBERS PRESENT:	Roger Wheelock (CHAIR) Wendy Kay Jill Singleton Cst. Franco Bruschetta	Richard Iredale Paul De Greeff Ally Dewji
REGRETS:	Robert Schindelka	
STAFF LIAISON:	Bill Brown, Director, Development Services	
COUNCIL LIAISON:	Councillor Beth Burton-Krahn Councillor Olga Liberchuk	
SECRETARY:	Pearl Barnard	

I. CALL TO ORDER

The Chair called the meeting to order at 3:04 p.m.

II. ELECTION OF CHAIR

Nominations were called for Wendy Kay nominated Roger Wheelock, seconded by Paul De Greeff. Roger Wheelock was elected by acclamation as Chair for the year 2017.

III. ELECTION OF VICE CHAIR

Nominations were called for and Wendy Kay nominated Paul De Greeff, seconded by Jill Singleton. Paul De Greeff was elected by acclamation as Vice Chair for the year 2017.

IV. LATE ITEMS

No late items

V. ADOPTION OF AGENDA

Moved by Jill Singleton, seconded by Wendy Kay: That the agenda be adopted as distributed. **Carried Unanimously**

IV. ADOPTION OF MINUTES – November 9, 2016 Meeting

Moved Jill Singleton, seconded by Richard Ireland: That the minutes of November 9, 2016 be adopted as distributed. **Carried Unanimously.**

V. STAFF REPORTS

DEVELOPMENT PERMIT APPLICATION
“Core Area Waste Water Treatment Plant”
337 Victoria View Road
Lot 1; Section 11; Plan EPP36468

PURPOSE OF APPLICATION:

To ensure that the developer's intentions are consistent with the Zoning Bylaw and the design guidelines for Development Permit Area No. 3 - Industrial.

Bill Brown outlined that the Development Permit Application is for the Core Area Waste Water Treatment Plant at McLoughlin Point. Mr. Brown explained that staff have listed their concerns in the staff report and are interested in hearing any comments that the Design Review Committee members may have.

In attendance:

Harbour Resource Partnership

Bree Milne
Peter Gawlick
Jeremy Klarenbach
Jim Mann
Ernie Maschner
Jim Aalders
Bev Windjack

Capital Regional District Project Board

Dave Clancy
Jane Bird

Cavello Law Corporation

Lui Cavello

Jim Mann gave a PowerPoint presentation outlining the proposal for the Core Area Waste Water Treatment Plant Project. Mr. Mann gave a brief history of the site and an overview of the site plan, design approach, massing, colours, materials, and parking for the site.

Beth Windjack gave an overview of the proposed landscape plan for the site. Ms. Windjack explained that the landscape plan was heavily prescribed by the design guidelines. She then outlined that the plantings are primarily in place to buffer the building, to make it as invisible as possible from the harbour. The plantings will also improve the ecological integrity on the site and enhance the streetscape along Victoria View Road. She then gave a brief overview of the tree species and plant selection for the site and advised that the landscaping will be irrigated with treated water from the plant.

The Chair thanked the applicant for their presentation.

Committee Members had the following questions and comments:

- A concern was raised that the big retaining wall could be a potential site for graffiti. Consider anti graffiti coating and a Closed Circuit Television [CCTV] system. Another suggestion was for a mural or some sort of art work to be put on the wall; something that would be a showcase when the cruise ships or planes come into the harbour. Mr. Mann advised that there could be something done with the wall. Jane Bird advised that public art is currently under discussion with the Township of Esquimalt.

Cst. Franco Bruschetta left the meeting at 3:46 p.m.

- Is there a lighting plan for the site? Mr. Mann advised that a lighting plan had been submitted with the Development Permit. He then outlined that they are following the design guidelines which recommends minimal lighting for the site.

- A member asked about the green building components for the project. Mr. Mann advised that the operations and maintenance building will be designed and built to the level of LEED 2009 Gold, but will not be certified. Member then asked why LEED Gold 2009 and not the latest version. Mr. Brown clarified that LEED Gold 2009 is referred to in the Zoning Bylaw.
- A member asked about interpretive signage for the site. Since the treatment plant is not publicly accessible, will there be an opportunity to put interpretive signs at the street level? Mr. Mann advised that at the present time the interpretive signs will be located on the observation deck, but other options could be looked at.
- Members had the following comments about the south side of the site; looks very industrial, needs something more, seems very lacking in beauty and does not blend into the environment.
- Members had the following comments about the landscaping design for the site:
 - Two metre tall trees on install are really insufficient given the height of the building. Consider larger trees.
 - More information is needed on how the vines growing on the second tier of the building will be supported, and how that green mass is going to be achieved.
 - Consider planting the native Arbutus tree (*Arbutus menziesii*) as it is a prominent species on our shorelines.
 - Enhance the landscaping on the south end to screen the tertiary building.
 - Consider increasing the green roof area on the treatment plant.
 - The buffer edge around the front of the building needs to be rethought.
 - The irrigation is fantastic.
- COR-TEN steel was discussed. Members felt more information was needed. Ms. Bird advised that to address staff's concerns they are looking at other options.
- A concern was raised that generator boxes on the roof look awkward.
- In response to staff's questions: Does the design respect the site. A member commented that it is a real squeeze; part of the problem is lack of space. Is the galvanized metal appropriate treatment for the odour control stacks? Member suggested something darker; so it blends in with the trees.
- What is the life span of the design of the plant? Mr. Maschner advised 50 years plus, ultimately 2065 and the capacity is designed to 2040.
- A concern was raised that the tsunami and retaining walls are so close to the ocean that there won't be any space to accommodate a public access or future walkway. The tertiary treatment plant is a bit of a challenge, an ugly box as you come into the harbour. Can it be pushed back?
- A member also commented that the overall disposition of massing seems like all the love went into the east elevation and yet the south elevation is the entry into the city, and what we all have to constantly look at.
- Consider reducing the onsite parking and moving the building back to zero lot line. Members felt it would improve the look for the rest of the city, relieve the crowding along the foreshore and provide more room for enhanced landscaping.

RECOMMENDATION:

Moved by Jill Singleton seconded by Paul De Greeff: The Esquimalt Design Review Committee recommends that the Development Permit Application for the Core Area Waste Water Treatment Plant **return to the Design Review Committee with revised drawings. The Motion Carried Unanimously**

VI. STAFF LIASON STATUS REPORT

1. There will be an Open House for the Esquimalt Road Urban Design Guidelines on Wednesday, January 18th from 4:00 p.m. to 7:00 p.m. in Council Chambers.
2. The CRD will be hosting an Open House for the Core Area Waste Water Treatment Plant, January 12th & 14th.
3. The Development Permit for the Esquimalt Town Square will be coming in March.
4. Staff are working on the Official Community Plan Review.

VII. NEW BUSINESS

- The Chair welcomed the two new Council Liaisons and thanked Jill Singleton for being the Chair for the past year.

VIII. NEXT REGULAR MEETING

Wednesday, February 8, 2017

IX. ADJOURNMENT

On motion the meeting adjourned at 4:50 p.m.

CHAIR, DESIGN REVIEW COMMITTEE

THIS 9th DAY OF FEBRUARY 2017

ANJA NURVO,
CORPORATE OFFICER



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: February 9, 2017

STAFF REPORT

DATE: February 3, 2017

TO: Chair and Members of the Design Review Committee

FROM: Bill Brown, Director of Development Services

SUBJECT: **DEVELOPMENT PERMIT APPLICATION**
“REVIEW OF DESIGN REVISIONS FOR THE CORE AREA
WASTEWATER TREATMENT PLANT AT MCLOUGHLIN POINT”
337 Victoria View Road
Lot 1; Section 11; Plan EPP36468

RECOMMENDATION:

The Esquimalt Design Review Committee recommends that the development permit application for the Core Area Waste Water Treatment Plant be forwarded to Council with a recommendation to **approve, approve with conditions, or deny the application including reasons for the chosen recommendation. Alternatively, if the Design Review Committee does not feel that the design has been adequately resolved or more information is required, it can request that the applicant return to the next meeting with revised drawings and additional information.**

BACKGROUND:

Purpose of the Application

The purpose of this application is to ensure that the applicant’s intentions are consistent with the Zoning Bylaw and the design guidelines for Development Permit Area No. 3 “Industrial”. The comments of the Design Review Committee are invaluable in helping staff in reviewing the application and Council in their decision making process vis a vis the application.

Context

The McLoughlin Point site is a 14,213 m² parcel situated on the west side of the entrance to Victoria Harbour. Because of its prominent location, it is imperative that the plant exhibit exemplary design. Approximately 240 cruise ships carrying over 500,000 passengers are scheduled pass by the plant on their way to dock at Ogden Point in 2017. It is crucial that these visitors get a good first impression of Esquimalt. Also, the plant will lie in one of the float plane flight paths to the harbor. In addition to the need to ensure that the design of the plant reflects its prominent location, the design also needs to respond to the fact that it is located near one of the most seismically active areas in Canada and, given its proximity to the shore, subject to potential Tsunamis.

Applicant/Owner: Capital Regional District

Architect: HDR|CEI Architecture

Property Size: Metric: 14,213 m²

Existing Land Uses: Vacant

Surrounding Land Uses:

North: CFB Esquimalt.

South: Strait of Juan de Fuca.

West: CFB Esquimalt (low density residential)

East: Entrance to Victoria Harbour

Existing Zoning: McLoughlin Point Special Use [I-3]

Existing OCP Designation: Industrial

Existing Development Permit Area: No. 3 Industrial

Design Overview

The Design Review Committee first reviewed the Development Permit application for the proposed Core Area Wastewater Treatment Plant at their January 11, 2017 meeting. At that time a number of issues were identified and the applicant was asked to consider the concerns of the Committee and return with a design that addresses the Committee's concerns. The applicant has refined the design which is attached to this staff report for the Committee's review. In particular, the applicant has addressed the Committee's concerns as follows:

DRC Concern	Applicant's Response
Can the building be moved to the west?	The building has been moved approximately 2.8 m to the west which allows for a 10.0 m setback from the High Water Mark.
Can the parking be put on Victoria View Road?	The revised plans show parking on Victoria View Road.
Can the trees being planted be taller than 2.0 m?	Landscape Architect to respond.
Will the Operations and Maintenance Building be LEED Certified?	At the last Committee meeting the applicant stated that the Operations and Maintenance Building would not be LEED Certified, however, the proposed amendment to the Zoning Bylaw will require the building to be LEED Certified.
Can anything be done to improve the south elevation?	The applicant has presented a more refined south elevation. There is significant use of translucent polycarbonate panel. In addition, precast concrete panels have been incorporated into the base of the south elevation of the tertiary treatment portion of the building to add articulation.
Will Cor-Ten be used as a material?	The applicant has removed all Cor-ten from the design. In its place, stained concrete panels will be used that have a similar design impact.
Chimney colour	The colours of the chimneys has been changed from galvanized steel to "grey metal"
Roof top generators	The roof top generators have been moved to the setback between the building and Victoria View Road.

In addition, the design team has made the following design enhancements:

Proposed Change	Comments
Parapet	A 1.0 m tall parapet has been added to the perimeter of the roof of the processing portion of the plant in order to screen items on the roof top.
Translucent Polycarbonate Panel	Extensive areas of translucent polycarbonate panel have been added to help “lighten” the building up and break up the mass of the building.
The exterior wall of the tallest portions of the treatment building have been treated in a lighter colour metal cladding.	The architects have replaced the dark metal cladding with light metal cladding on the upper parts of the building in order to reduce the perceived mass of the building.

The applicant’s architects have provided a revised “Design Guideline Response Report” which includes a narrative explaining their design approach and is attached to this report.

Having reviewed the applicant’s revised submission, staff have a number of questions that they would like the Design Review Committee to consider.

- 1) Staff would like to know if the DRC supports the use of translucent polycarbonate panels as proposed by the design team.
- 2) Staff would like to know if the DRC feels that the south elevation has been sufficiently resolved given the prominence of this site in relation to the cruise ship approach to Ogden Point.
- 3) The odour control room, which is located on the north east corner of the plant, has been raised from approximately 5.0 m in height to approximately 10.0m in height. This was possible when the plant was moved closer to Victoria View Road. Does the DRC have any concerns related to the increased height of this portion of the building?
- 4) In place of the generators, a Dissolved Air Flotation tank basin has been installed on the roof. Does the DRC feel that this new addition has been properly integrated into the overall design of the building?
- 5) Does the DRC have any concerns with the design of the stairwell enclosure that has been added to the roof of the tertiary treatment plant portion of the building?
- 6) Does the DRC have any other concerns with the proposed design of the building or the proposed landscaping?

Alternatives

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**
4. Request that the applicant return to a future meeting with revised drawings and additional information that responds to the concerns raised by the Design Review Committee.

Bill Brown
Director of Development Services



McLoughlin Point Wastewater Treatment Plant

HARBOUR RESOURCE PARTNERS PROJECT TEAM

AECOM – Engineering 3292 Production Way Burnaby BC V5Z 4R4 - Ernie Maschner
 Graham Construction 10840 27 Street SE Calgary AB T2Z 3R6 – Mark Livingston
 HDR|CEI 203-655 Tye Road Victoria BC V2A 6P6- Jim Mann
 LADR Landscape Design 495 Dupplin Rd #2b, Victoria, BC V8z 1B8– Bev Windjack

Google Maps



DRAWING LIST - ARCHITECTURAL

SHEET NUMBER	SHEET NAME
A0	COVER SHEET
A1	ARCHITECTURAL ROOF PLAN
A2	RETAINING WALL PLAN
A3	LEVEL 1
A4	LEVEL 2
A5	BUILDING AND SITE SECTIONS
A6	BUILDING ELEVATIONS
A7	BUILDING ELEVATIONS 2
A8	RENDERED VIEWS 1
A9	RENDERED VIEWS 2

DRAWING LIST - CONSULTANTS

SHEET NUMBER	SHEET NAME
010057964-01K01-003	PROPOSED BUILDING AVERAGE GRADES
L1	LANDSCAPE PLAN
L2	PLANT SCHEDULE
M-BB-C-004	TRUCK TURNING AND PARKING PLAN
M-BB-C-201	EARLY WORK AREA PLAN
M-BB-C-202	ELECTRICAL SITE LIGHTING PLAN

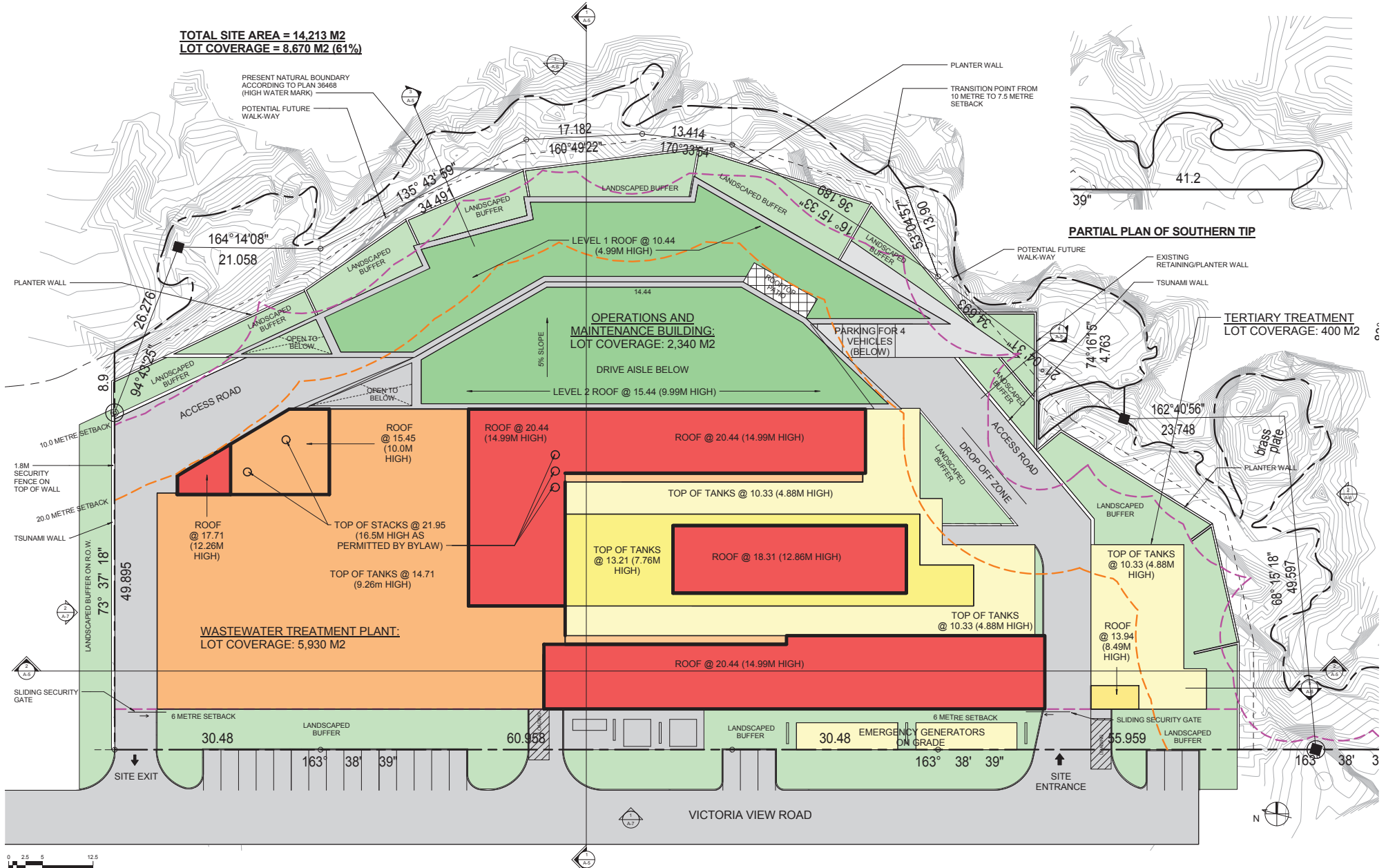
PROJECT DATA			
SITE	LEGAL DESCRIPTION	CIVIC ADDRESS	NOTES
PROJECT LOT	LOT 1, SECTION 11, AND PART OF THE BED OF VICTORIA HARBOUR, ESQUIMALT DISTRICT, PLAN 36468		SECTION 11 ESQUIMALT DISTRICT
LOT AREA	14,213 m2		

ZONING ANALYSIS - McLOUGHLIN POINT SPECIAL USE [I-3]			
BONUS DENSITY LEVEL 3	PERMITTED	PROPOSED	
FLOOR AREA	4500 m2	2157 m2	
DENSITY (FAR)	0.35	0.15	
LOT COVERAGE	65%	61%	
MAX HEIGHT	15m	15m	

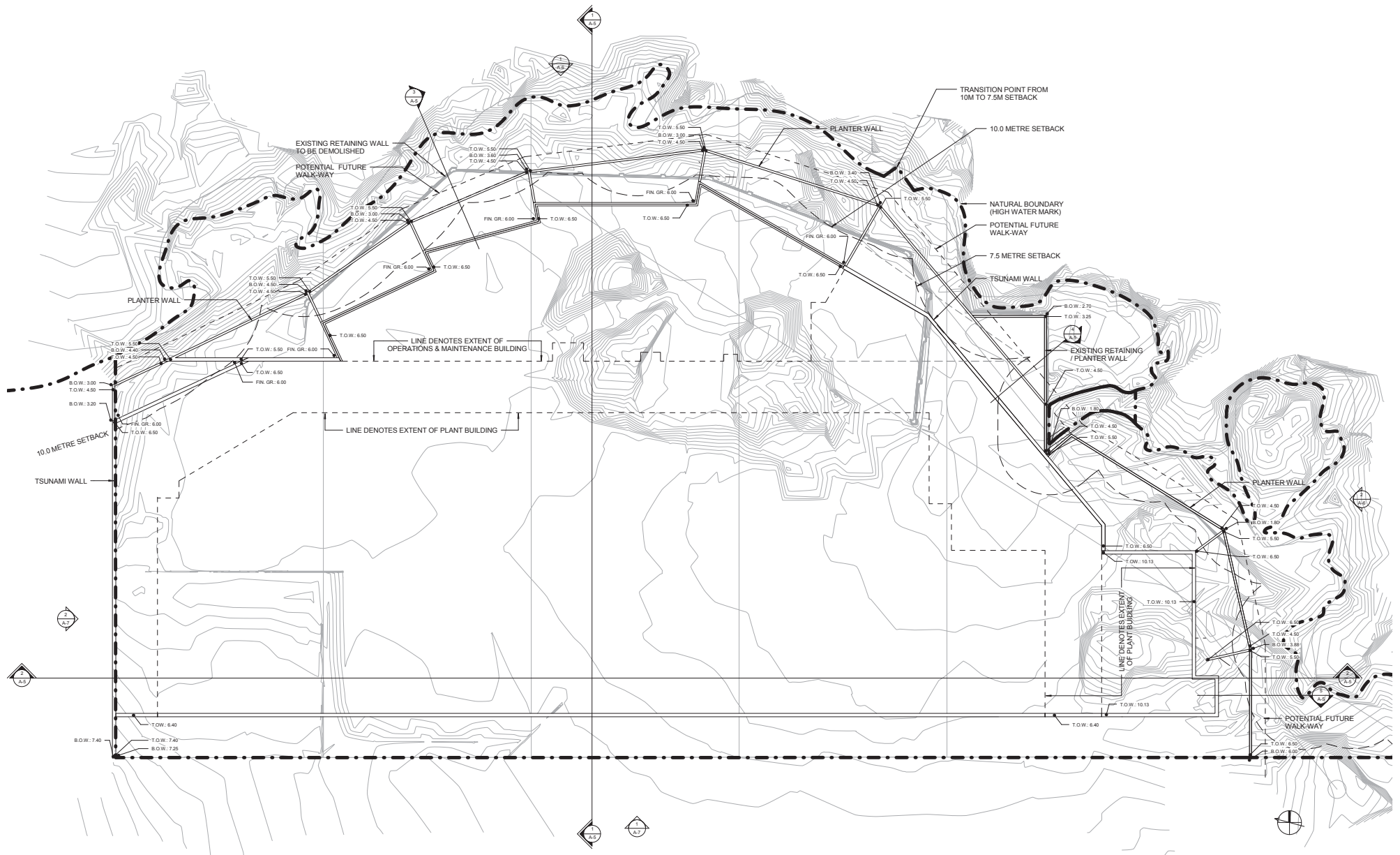
PARKING			
PARKING REQUIREMENTS INCLUDE			
SPACES AS DETERMINED BY ZONING BYLAW			
		TOTAL SPACES PROVIDED	REQUIRED SPACES
STANDARD SPACE MIN. SIZE: 2.6 X 5.5		2	1
DISABLED SPACE MIN. SIZE: 3.7X 5.5		1	1
ELECTRIC CAR CHARGING STATION STALLS MIN. SIZE:		1	0
SUBTOTAL		4	2
LOADING SPACE MIN. SIZE:	3m X 7.5m X 4.25h	2	2
NOTES:			

BUILDING DATA		
FLOOR	O & M (FLOOR AREA m²)	PROCESSING PLANT (GROSS m²)
LEVEL 1	1273 m2	PLANT NOT INCLUDED IN FLOOR AREA
LEVEL 2	884 m2	
TOTAL	2157 m2	
LOT COVERAGE	2340 m2	6330 m2 (INCLUDED IN LOT COVERAGE)

TOTAL SITE AREA = 14,213 M2
LOT COVERAGE = 8,670 M2 (61%)

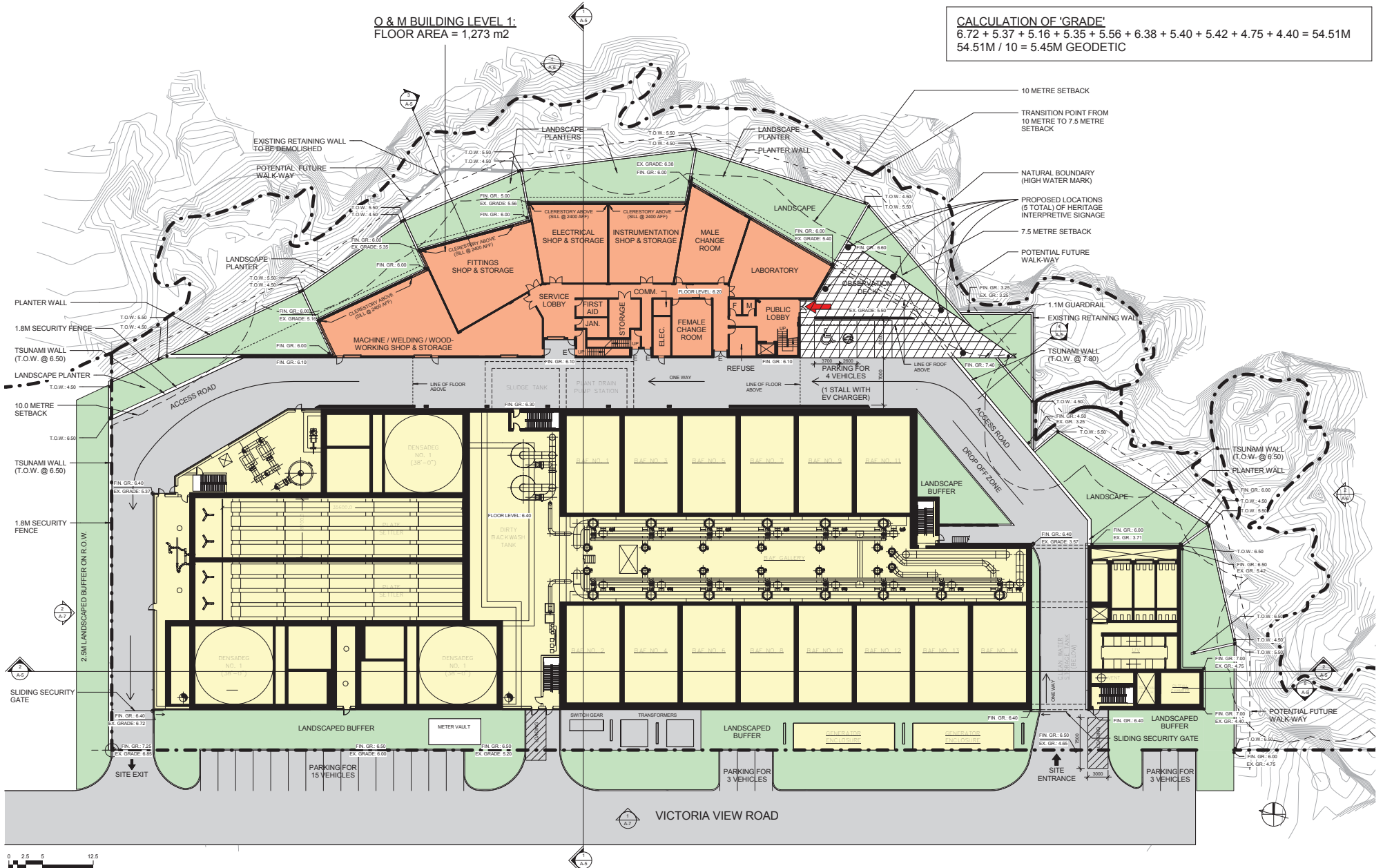


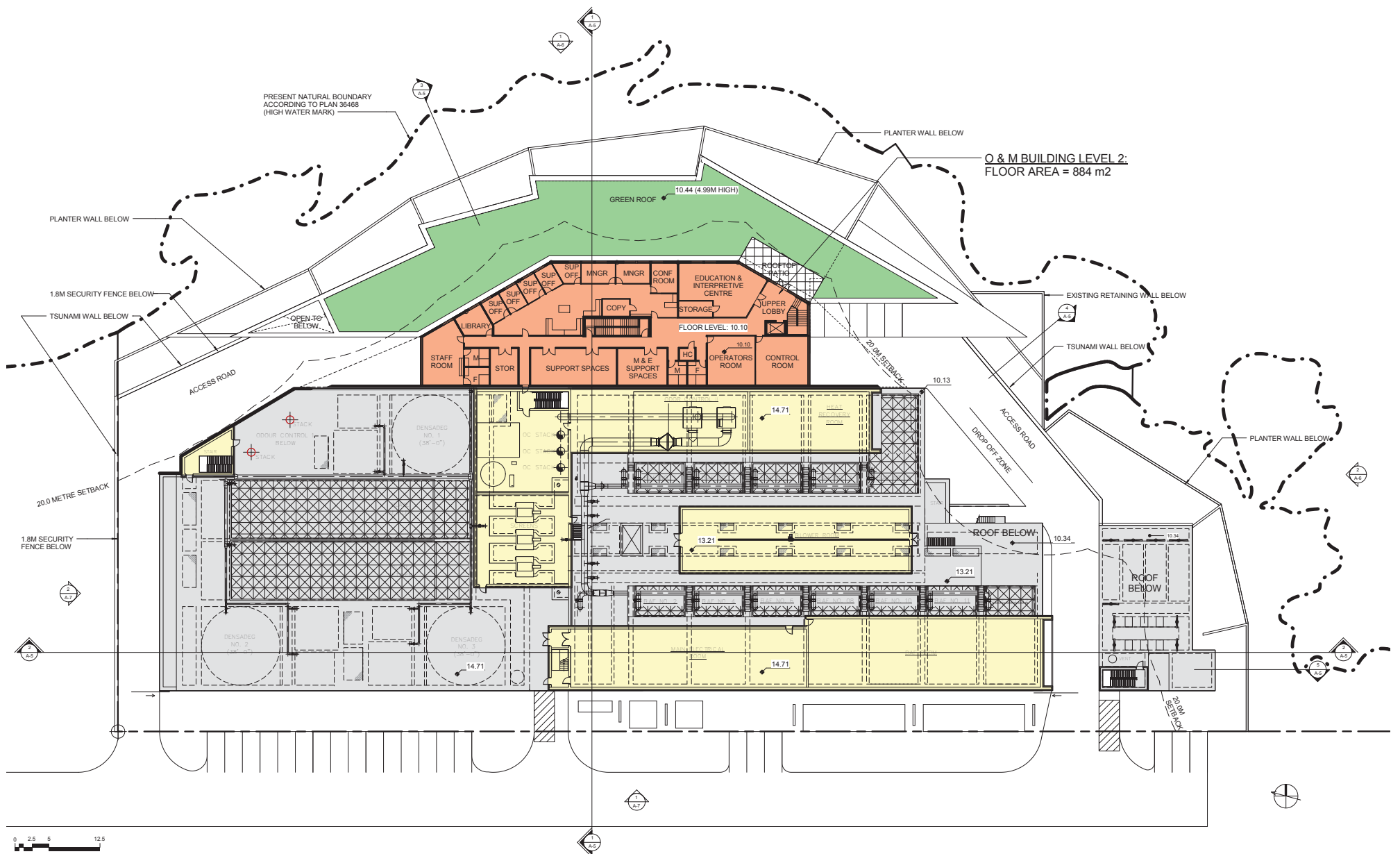
PARTIAL PLAN OF SOUTHERN TIP



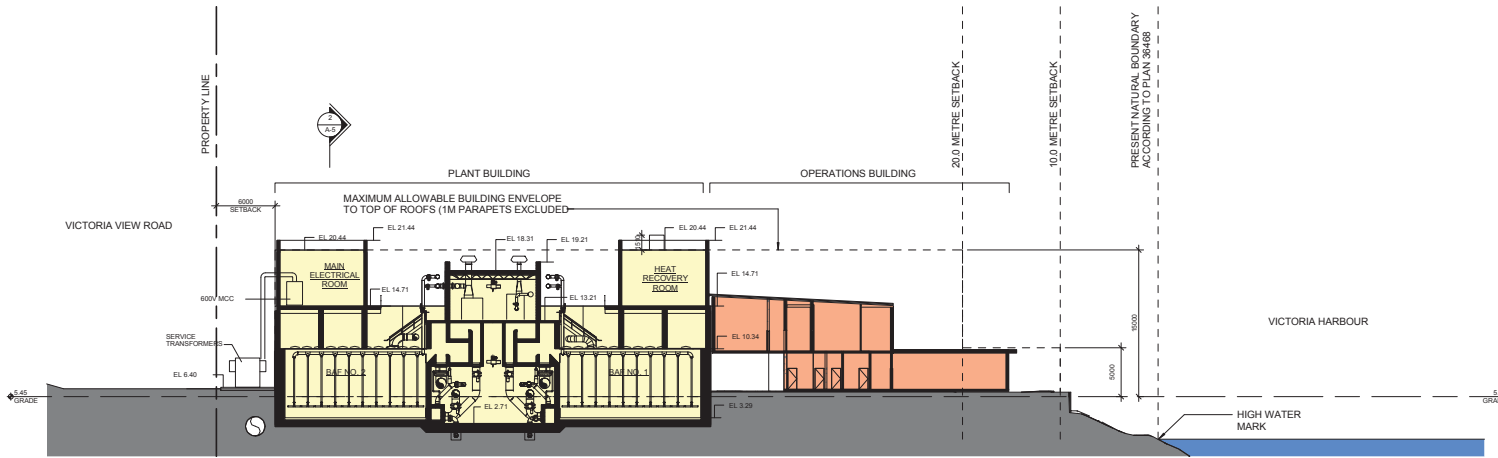
O & M BUILDING LEVEL 1:
FLOOR AREA = 1,273 m²

CALCULATION OF 'GRADE'
 $6.72 + 5.37 + 5.16 + 5.35 + 5.56 + 6.38 + 5.40 + 5.42 + 4.75 + 4.40 = 54.51\text{M}$
 $54.51\text{M} / 10 = 5.45\text{M GEODETIC}$

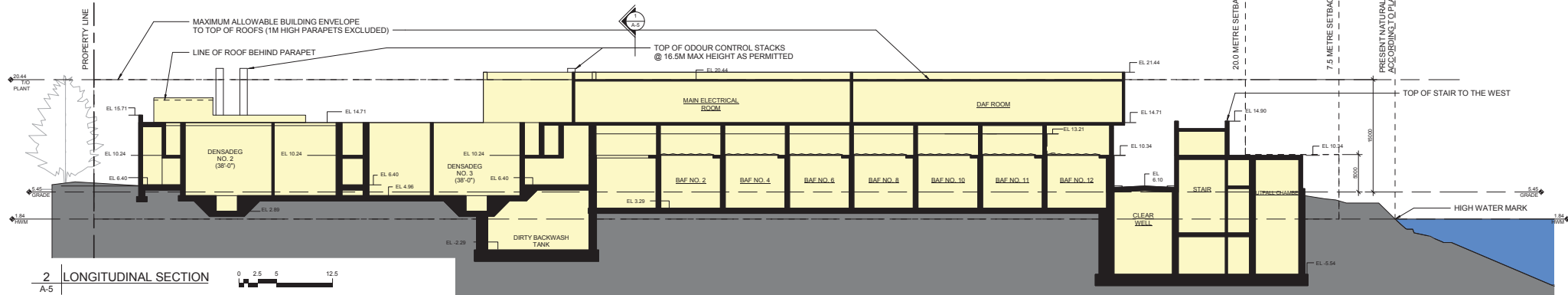




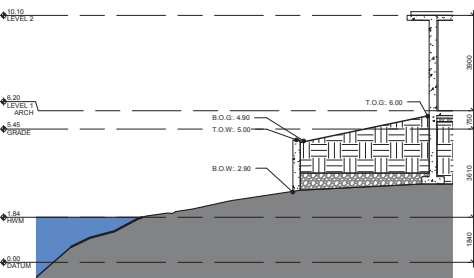
McLoughlin Point Wastewater Treatment Plant



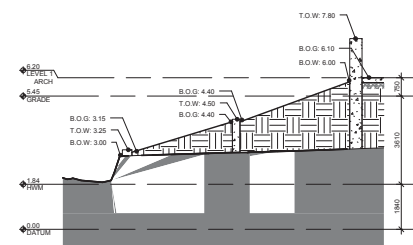
1 TRANSVERSE SECTION A-5



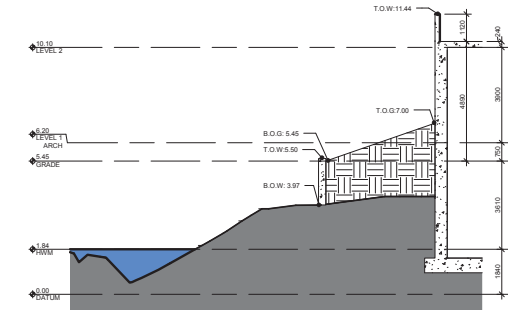
2 LONGITUDINAL SECTION A-5



3 RETAINING WALL SECTION 1 A-5



4 RETAINING WALL SECTION 2 A-5



5 RETAINING WALL SECTION 3 A-5



1 BUILDING ELEVATION - EAST
A-6



2 BUILDING ELEVATION - SOUTH
A-6



EXTERIOR ELEVATION KEY NOTES

GENERAL NOTES

- ALL RETAINING WALLS TO RECEIVE A LIGHT SANDBLAST FINISH

1	CONCRETE - ARCHITECTURAL FINISH (SMOOTH)	6	MASONRY CLADDING - LIGHT	11	SEDUM MAT GREEN ROOF	16	SECURITY FENCE	21	PRE-PAINTED GENERATOR ENCLOSURE
2	CONCRETE - BOARDFORM (MEDIUM TEXTURE)	7	METAL PANEL CLADDING - DARK	12	LANDSCAPE ELEMENTS	17	INTERPRETIVE SIGNAGE	22	TRANSLUCENT POLYCARBONATE PANEL
3	CONCRETE - BOARDFORM (ROUGH TEXTURE)	8	METAL PANEL CLADDING - LIGHT	13	CLIMBING VEGETATION ON SCREEN ELEMENT	18	ODOUR CONTROL STACKS - GREY METAL		
4	CONCRETE - PRECAST CONCRETE PANEL	9	STAINED CONCRETE PANEL	14	METAL BAR GRATE SCREEN	19	GLASS GUARD		
5	MASONRY CLADDING - DARK	10	GLAZING IN ALUMINUM FRAME	15	METAL BAR GRATE - SECURITY GATE	20	PIPE RAIL GUARD		

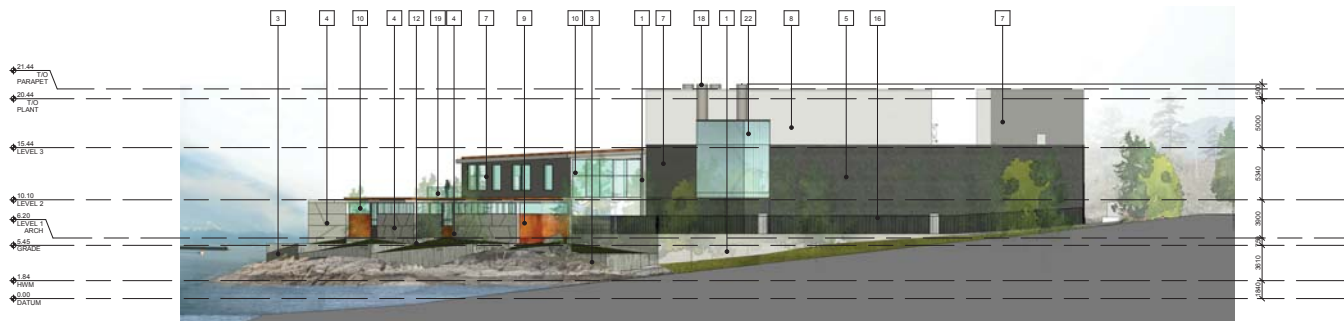
As indicated

McLoughlin Point Wastewater Treatment Plant

BUILDING ELEVATIONS A-6 **FOR** **CEI**



1 BUILDING ELEVATION - WEST
A-7



2 BUILDING ELEVATION - NORTH
A-7



EXTERIOR ELEVATION KEY NOTES

GENERAL NOTES

1. ALL RETAINING WALLS TO RECEIVE A LIGHT SANDBLAST FINISH

1	CONCRETE - ARCHITECTURAL FINISH (SMOOTH)	6	MASONRY CLADDING - LIGHT	11	SEDUM MAT GREEN ROOF	16	SECURITY FENCE	21	PRE-PAINTED GENERATOR ENCLOSURE
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5	MASONRY CLADDING - DARK	10	GLAZING IN ALUMINUM FRAME	15	METAL BAR GRATE - SECURITY GATE	20	PIPE RAIL GUARD		

As indicated

McLoughlin Point Wastewater Treatment Plant



AERIAL VIEW FROM SOUTH EAST



VIEW FROM OGDEN POINT



VIEW FROM SHOAL POINT



VIEW FROM SONGHEES WALKWAY



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
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DRC Meeting: February 9, 2017

STAFF REPORT

DATE: February 3, 2017

TO: Chair and Members of the Design Review Committee

FROM: Trevor Parkes, Senior Planner

SUBJECT: REZONING APPLICATION
“Marinaview”
460 Head Street [Lot 10, Block H, Section 11, Esquimalt District, Plan 292]
464 Head Street [Lot 9, Block H, Section 11, Esquimalt District, Plan 292]

RECOMMENDATION:

The Esquimalt Design Review Committee recommends that the application for rezoning to facilitate consolidation of two properties located between Head Street and Lyall Street to permit a new five storey, commercial/ residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units, and incorporating siting, height and massing consistent with architectural plans prepared by Hillel Architecture, stamped “Received January 18, 2017” be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application

The applicant is requesting a change in zoning from the current mix of RS-1 [Single Family] and C-7 [West Bay Commercial] zoning which currently regulates the two subject properties to a Comprehensive Development zone [CD] to facilitate the consolidation of the subject properties and authorize a new five storey, commercial/ residential mixed use building containing ground floor commercial space oriented toward Head Street and 12 residential units.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, siting, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan and the West Bay Neighbourhood Design Guidelines [attached], endorsed by Esquimalt Council on November 16, 2015 [attached].

Specific form and character issues relating to the aesthetics of the building, such as cladding materials, window materials, doorways, streetscape improvements and landscaping will be evaluated in a separate application for Development Permit should this rezoning application be approved by Council.

Context

Applicant/Owner: West Bay Investments Ltd., Inc. No. 0290580 [Mark Lindholm]

Architect: Hillel Architecture Inc. [Peter Hardcastle]

Property Size: Metric: 1207.7 m² Imperial: 13,000 ft²

Existing Land Use: Vacant Single Family Residential land and vacant Commercial land [parking lot].

Surrounding Land Uses:

North: 2-6 Storey Commercial/ Residential Mixed Use [pending final zoning approval]

South: DND Federal Land [Work Point]

West: Single Family and Two Family Residential

East: Marine Commercial

Existing Zoning: RS-1 [Single Family Residential] – Lot 9
C-7 [West Bay Commercial] – Lot 10

Proposed Zoning: CD [Comprehensive Development District]

Existing OCP Designation: Commercial Mixed-Use [No change required]

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received by the submission deadline:

Community Safety Services: Staff have completed a preliminary evaluation of the proposed new building and have no concerns at this time. Construction must satisfy BC Building Code and Esquimalt's Building Code Bylaw, 2002, No. 2538 requirements. Detailed review of plans will occur at time of Building Permit application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed commercial mixed use development including commercial space and 12 residential units located on two properties within the West Bay neighbourhood. The applicant is responsible for retaining the services of a qualified professional for the design, construction and supervision of all Works and Services required to service the proposed development as indicated in Esquimalt's Subdivision and Development Control Bylaw No. 2175. A capacity study should verify all main sizes, material, etc., and to determine if the Sewer and Drain mains are of adequate size and condition to handle the increased flows introduced as a result of the development. Existing municipal infrastructure may need to be relocated or SRWs may need to be secured. Adjacent properties across Head Street are to remain fully serviced during construction.

The Director of Engineering and Public Works notes that clarity regarding the responsibility for maintenance and liability relating to the proposed plaza located in the public realm at the Lyall Street and Head Street intersection should be provided by the applicant for review by staff. Staff also request additional clarity regarding the applicant's proposed servicing plan, specifically as it relates to the retention or alteration of the existing overhead Hydro/ Telephone/ Cable lines located on the west side of Head Street.

Additional review comments will be provided when detailed civil engineering drawings are submitted.

ISSUES:

Zoning

Density, Lot Coverage, Setbacks, Height and Parking:

The following chart details the setbacks, lot coverage and floor area ratio and parking requirements of this proposal.

	Comprehensive Development Zone	
Floor Area Ratio	1.25	Zoning Bylaw, 1992, No. 2050 does not contain a zone that could accommodate this commercial mixed-use proposal. Staff present this summary table as the basis for a site specific zone written to accommodate this proposal should it be forwarded in the development review process.
Lot Coverage	46%	
Setbacks		
• Front	2.8 m plus 1.6 m [Balcony]	
• Rear	1.2 m	
• Exterior Side [East]	2.4 m plus 1.4 m [Balcony]	
• Exterior Side [West]	2.8 m plus 1.0 m [Balcony]	
Building Height	18 m [5 storeys]	
Off Street Parking	Total spaces required for all uses = 34 Total proposed = 35	
Commercial Floor Area	Not less than 215 square metres	

Parking: The applicant proposes to provide modestly greater parking than the minimum required for this project, exceeding the minimum number of spaces by 1 space. Eleven [11] spaces accessed off Lyall Street serve as Commercial parking/ Residential Visitor Parking for both the commercial and residential units, while the remaining 24 parking spaces associated with this development are located underground and accessed via the parking entrance to the West Bay Triangle located on Gore Street.

Height: The applicant proposes to construct a building five storeys in height with a total density [Floor Area Ratio] of 1.25. Approval of this rezoning application would make Marinaview complimentary to the abutting West Bay Triangle development proposed to be six storeys in height. While this would make Marinaview one of the tallest buildings in Esquimalt, the proposed height is substantially less than initially considered for this Commercial Mixed-Use site and is consistent with the recently adopted West Bay Neighbourhood Design Guidelines.

Setbacks: Commercial development best practices encourage placing commercial retail uses in proximity to the public realm. Accordingly, the proposed siting of the portions of the building containing retail/ office commercial space is appropriate as the large balconies proposed along Head Street and at the southeastern plaza will serve to create a covered transitional space between the indoor uses and the outdoor public realm.

Permitted Uses: The West Bay Area is identified as a Commercial Node in the Official Community Plan therefore it is appropriate to consider an application that proposes a mix of

commercial retail, office, and boutique commercial spaces. Permitted Uses in the proposed Comprehensive Development District may include but are not limited to the following uses:

- a) Dwelling – Multiple Family
- b) Home Occupation
- c) Business and Professional Office
- d) Personal Service Establishment
- e) Retail Store
- f) Restaurant
- g) Liquor store
- h) Convenience Store
- i) Group Children's Daycare
- j) Boat Rental and Passenger Charter

Traffic Study: The applicant provided a detailed Traffic Impact Assessment for the original 97 unit mixed-use proposal originally envisioned for the West Bay Triangle dated February 20, 2012. Boulevard Transportation Group [BTG] determined at that time that the proposed project would have little local impact, adding an estimated 38 trips in the AM peak hour and 44 trips in the PM peak hour. BTG also determined that it was most likely that the existing traffic flow pattern, including DND traffic, would not change with the addition of this building, as additional delays would be very minor. BTG concluded that no improvements or changes would be required to the existing key intersections to accommodate the new traffic but recommended that sidewalks should be provided along the proposed development frontages of Head Street, Gore Street and Lyall Street.

Based on this assessment, staff are confident that the combination of the proposed 73 unit, commercial/residential mixed use West Bay Triangle development and the 15 unit, commercial/residential mixed use, Marinaview development would not result in any material changes to these recommendations.

Fit with Neighbourhood: The subject properties are located within a predominantly low rise neighbourhood consisting of a mix of commercial properties, single and two family residential homes, a marine community, and military uses including training facilities and housing. The exceptions to the established one to three storey built environment are the DND "Accommodations" building located at Work Point to the south of the subject properties and the West Bay Triangle located abutting the subject properties that is awaiting zoning approval. The "Accommodations" building is approximately seven storeys [22 metres] in height and commands a substantial presence in the area due to the combination of height and massing of the building. The five storey Marinaview development is proposed to be complimentary to the abutting six storey West Bay Triangle proposal. Construction of Marinaview and West Bay Triangle would create a trio of taller buildings, complimentary in height and mass, creating a sense of balance in West Bay, particularly when viewed from the water.

The current Land Use Designation of properties adjacent to the West Bay Triangle and Marinaview sites include Commercial Mixed-use for lands to the east, adjacent to the water, Park and Open Space for Captain Jacobson Park and Townhouse Residential for properties on the north side of Gore Street and the south side of Paradise Street. The West Bay Neighbourhood Design Guidelines further refine these land use designations by detailing a maximum of three storeys on lands to the east side of Head Street and the north side of Gore Street. It is staff's opinion that it is not unreasonable to expect some of these lands adjacent to the subject properties to redevelop in the future thereby integrating the proposed five storey height into the urban fabric of West Bay.

Official Community Plan

The current Esquimalt Official Community Plan contains policies and statements relevant to the West Bay Triangle proposal under the following broad categories:

General Land Use and Development Objectives: The Township encourages a mix of land uses that facilitate multiple modes of transportation and reduce non-essential trips by private motor vehicles.

Commercial Mixed Land Use: The Township encourages mixed-use projects in areas designated Commercial Mixed-Use, generally up to 12 storeys in height with a Floor Area Ratio of 3.0. This policy has been refined by the West bay Neighbourhood Design Guidelines to limit height to 6 storeys in the West Bay Neighbourhood. The Township encourages the provision of local amenities including street furniture, artworks and decorative lighting in commercial areas as well as the provision of cycling amenities such as end of trip facilities for employees, lockers, and secure bicycle storage in addition to general bicycle parking integrated into the development.

Public Art: The Township encourages the private sector to include artworks in new and existing developments.

Smart Design and Construction: The Township encourages the use of sustainable technology in the design of all new buildings, encourages design teams to achieve LEED or equivalent rating and encourages the incorporation of Crime Prevention through Environmental Design [CPTED] principles and measures in new projects.

Parking: The Township will explore further opportunities for on and off street public parking in the vicinity of West Bay waterfront so as to ensure that local residential streets are not overburdened by West Bay's commercial uses

Utilities: The Township will require underground wiring for hydro, street-lighting, telecoms and cable for all new subdivisions and infill development in Esquimalt.

Stormwater, Sewer Systems and Source Pollution: The Township will require intervening /mitigating measures such as oil traps for development proposal adjacent to or near the water to preserve or rehabilitate shoreline habitat.

Economic Development: The Township supports expansion and commercial waterfront development around West Bay Harbour, provided it is planned to be compatible with adjacent residential areas.

Keep Urban Settlement Compact – Regional Growth Strategy [RGS]: The RGS encourages densification through a combination of infill and redevelopment to higher densities particularly for areas near transit corridors.

West Bay Neighbourhood Design Guidelines

As a direct result of the controversy surrounding previous development proposals in West Bay, Esquimalt Council directed staff to coordinate the creation of the West Bay Neighbourhood Design Guidelines. This document was to be created including input from local residents, business owners, staff, planners, developers and architects to create a vision for West Bay as well as guidelines for achieving this vision to ensure new development enhances West Bay's unique character.

Staff engaged a consulting group and the guidelines were crafted via a collaborative and iterative process through the spring and summer of 2015. Esquimalt Council received and endorsed the West Bay Neighbourhood Design Guidelines on November 16, 2015.

The Marinaview development proposal has been tailored to be consistent with the West Bay Neighbourhood Design Guidelines. The applicant has included in the drawing package a detailed shadow analysis that indicates the proposed design effectively mitigates shadowing impacts on adjacent sites which was identified as a major concern for local residents during the public consultation process of the West Bay Neighbourhood Design Guidelines.

Development Services staff completed a review of the proposed design and note the following apparent inconsistencies to the West Bay Neighbourhood Design Guidelines:

Page 10, Bullet 4 – “Avoid locating off-street surface parking adjacent to active public streets and open spaces.....”. The proposed design partially achieves this policy when considered from Head Street and the southern public plaza, however, surface parking is provided with limited screening adjacent to Lyall Street.

Page 17, Bullet 2 – “Locating off-street surface parking in front of buildings,.....immediately adjacent to the public sidewalk or open spaces.....is strongly discouraged and should be avoided.”. Once again, the proposed design partially achieves this policy when considered from Head Street and the southern public plaza, however, surface parking is provided with limited screening adjacent to Lyall Street.

Development Services staff also request the DRC give consideration to the merits of providing vehicular access off Head Street through the proposed “porte cochere”. Specifically, staff request feedback on an alternative possibility of adding another commercial unit, in lieu of this architectural element. Staff recognize the potential impact on parking ratios and are open minded to favorably recommending a reduction to the parking requirement should the DRC deem it appropriate.

Green Building Features

The applicant has completed the Esquimalt Green Building Checklist [attached].

Public Notification

As this is a rezoning application, should it proceed to a Public Hearing, notice would be mailed to tenants and owners of properties within 100m (328 ft) of the subject property. Signs indicating that the properties are under consideration for a change in zoning have been placed on the Head Street and Lyall Street frontages the subject properties and would be updated to reflect the date, time and location of the Public Hearing. Additionally, notice of the Public Hearing would be placed in two editions of the Victoria News.

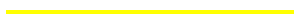
ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial**

including reasons for the recommendation.

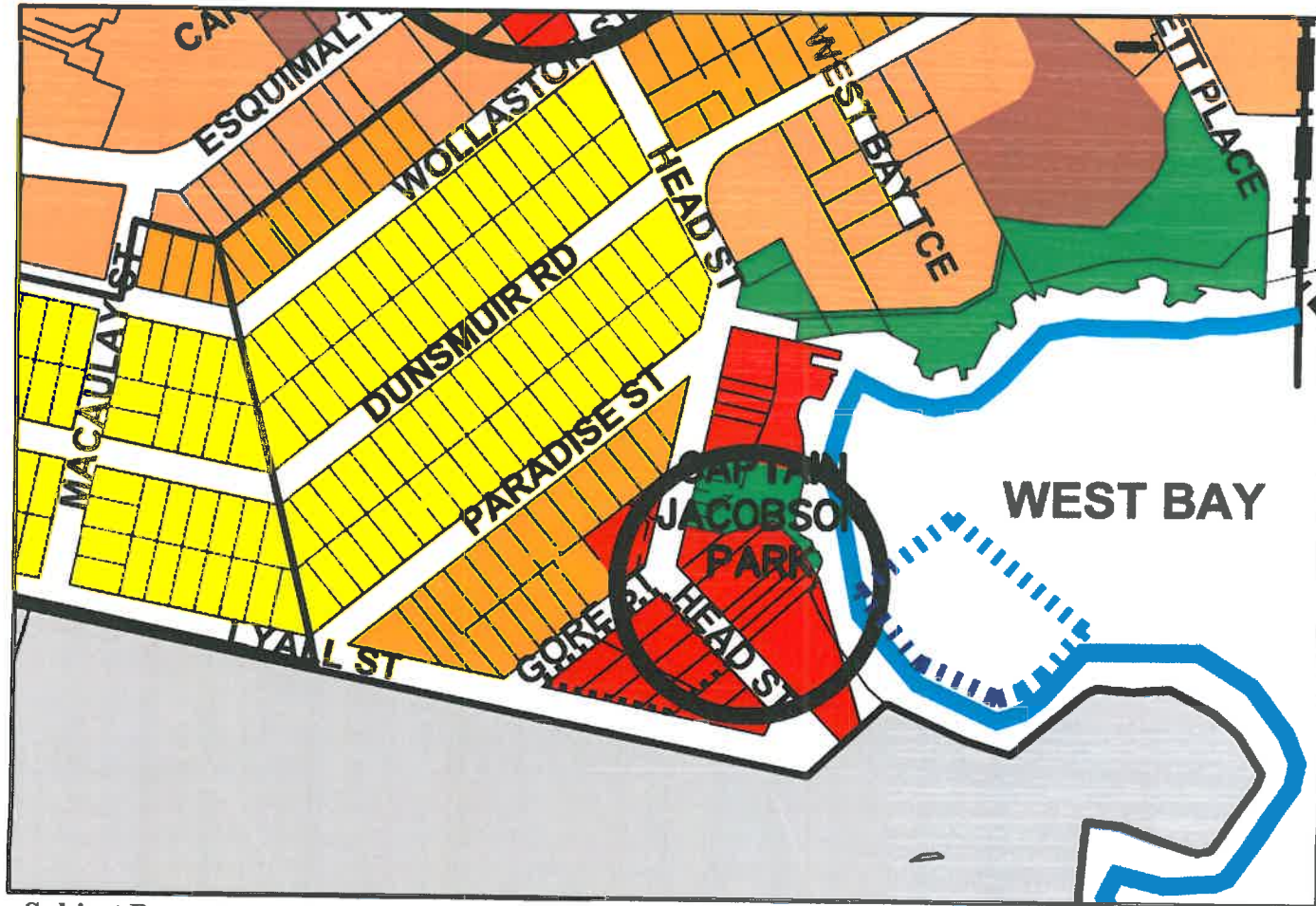
460 Head Street – ‘Marina View’



Subject Property Boundary: 



Extract from Official Community Plan Bylaw No. 2646 Schedule 'A'
Land Use Designations



Subject Property:

LEGEND	
	Single and Two-Unit Residential
	Townhouse Residential
	Multi-Unit, Low-Rise Residential (2 to 4 storeys)
	Multi-Unit, High-Rise Residential (5 to 6 storeys)
	Floating Homes Residential
	Parks and Open Space
	Trails existing and proposed
	Transportation Corridor
	Industrial
	Commercial Mixed-Use
	Commercial Node
	Institutional
	DND Federal Land
	Water Area
	Municipal Boundary

2.3 Commercial—Mixed Land Use

Commercial activity in Esquimalt is clustered in four main groupings:

- Esquimalt Village;
- Esquimalt Road/Head Street;
- Craigflower Road/Tillicum Road; and
- West Bay Harbour.

2.3.1 General Commercial - Mixed Use Objectives

- a) To create a diversified commercial and employment sector that provides a wide range of goods and services to residents of Esquimalt.
- b) To foster the creation of an identifiable and vibrant Esquimalt Village that successfully integrates commercial, public and residential activity.
- c) To encourage growth through revitalization and redevelopment of commercial areas.
- d) To encourage a mix of ground-level commercial and upper-level(s) residential.

2.3.2 General Commercial - Mixed Use Policies

- a) The Township encourages a mix of commercial and multi-unit residential developments in all commercial-mixed use areas denoted on "Schedule A". These will have commercial uses on the ground floor and residential uses above.
- b) All commercial-mixed use areas are designated Development Permit Areas, as shown on "Schedule C" in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.
- c) The Township encourages public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.
- d) The Township will develop signage guidelines for each commercial area, as part of design guidelines for these areas. The Township, in partnership with interested members of the community, will take the initiative to design and install entrance signage at key street locations, including entrances to Esquimalt.
- e) The Township encourages the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas. The Township is amenable to using density bonusing, or providing variances to zoning or parking regulations for redevelopment proposals.
- f) To encourage the use of bicycles, provision should be made in new commercial buildings for bicycle parking for employees and visitors. Secure bicycle parking for employees should be provided in the ratio of one (1) parking space per ten (10) full-time employees with a minimum of one (1) space for each new building. In all new commercial buildings, six (6) bicycle parking spaces should be available for the use of temporary visitors.
- g) End of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

- h) Where all of the following criteria are met in a commercial building, Council may reduce the off-street parking requirement through the Development Permit:
 - i) Two (2) or more secure bicycle storage spaces are provided;
 - ii) Shower and change rooms are provided;
 - iii) Six (6) visitor bicycle parking spaces are provided; and
 - iv) The building is located within 200 metres of a regional bus route.
- i) Lands outside the Commercial-Mixed Use designation on "Schedule A" will not be considered for commercial zoning unless the following criteria are met:
 - i) The project is needed to serve tourists or local residents and cannot be appropriately located within established commercial areas;
 - ii) The density and scale of the project is sensitive to the prevailing character of surrounding lands;
 - iii) The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
 - iv) The project's parking requirements can be satisfied on-site and will not unduly affect neighbouring residences; and
 - v) The proponent demonstrates that the neighbourhood has been consulted and residents have had an opportunity to express their concerns.

[Amendment Bylaw [No. 6], 2010, No. 2730 - Adopted May 25, 2010]

- j) In mixed commercial and multi-unit residential developments, buildings up to 12 storeys in height and with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.
- k) Development proposals with heights and/or densities greater than those set out in Section 2.3.2 (j) may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor space where new commercial buildings provide affordable, accessible, or special needs housing units or amenities for the benefit of the community.

9.4 Development Permit Area No. 2 – Commercial

9.4.1 Scope

All lands designated Commercial on Schedule “C” are part of DPA No. 2.

9.4.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, commercial.

9.4.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

Where new development is to occur within Esquimalt’s commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials, and by their orientation to the street rather than to a parking lot or internal square.

The goals for Development Permit Area No. 2 are:

- a) to enhance the aesthetic image of Esquimalt’s commercial district, particularly those areas that are considered community focal points, such as the Village, the Head Street/Esquimalt Road intersection and major entrance points to the municipality;
- b) to revitalize existing commercial areas by encouraging a variety of businesses;
- c) to encourage growth in the tax base through diversified commercial development and redevelopment of existing commercial areas; and
- d) to encourage integrated residential/institutional/commercial uses in commercial areas.

9.4.4 Requirements of Owners of Land within the Development Permit Area

- a) Owners of land within Development Permit Area No. 2 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:
 - i) subdivide lands; or
 - ii) construct or alter a building or structure;without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.
- b) Exemptions:

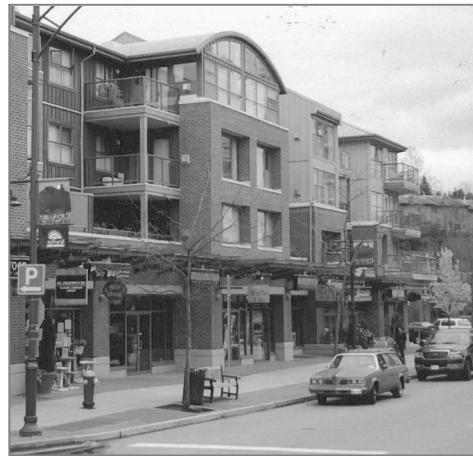
The following do not require a development permit:

 - i) construction of buildings or structures less than 10 square metres in area;

- ii) minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted by the municipal tree protection bylaw; and
- vi) replacement or changing of existing signs, provided the sign area is not to be increased.

9.4.5 Guidelines for Owners of Land within the Development Permit Area

- a) Commercial building facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
- b) Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, commercial lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
- c) Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- d) Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
- e) Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- f) The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
- g) Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
- h) Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.





West Bay Neighbourhood Design Guidelines



We would like to thank the following members of the West Bay Design Guidelines Stakeholders Group whose local insight and involvement in the process was of great benefit to the development of these design guidelines.

West Bay Design Guidelines Stakeholders Group:

Brian Emmett
Kris Samuels
Lieutenant-Colonel Ulpiano Honorio
Carolyn Gisborne
Christina Clarke
Laurie Hurst
Jeff Miller
Janice Rose
Scott Hartman
Liz Dill
Jim Witter
Peter Hardcastle
Mark Lindholm
Sally Reid
Katrina Dwulit
Julie Flatt
Carole Witter

Submitted: 10 September, 2015.

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1. Purpose & Overview

Purpose and Overview

These design guidelines were developed through a collaborative, community-based process to ensure new development enhances rather than detracts from West Bay's unique and rich character and identity. These design guidelines focus on the interface between and integration of public and private lands towards the preservation and enhancement of the identity, social vitality and overall liveability that characterizes the West Bay neighbourhood. This includes integration with federal Department of National Defense (DND) lands, integration of local ecosystems, and connection to the harbourfront and the range of recreational and other activities associated with the working harbour.

This document includes:

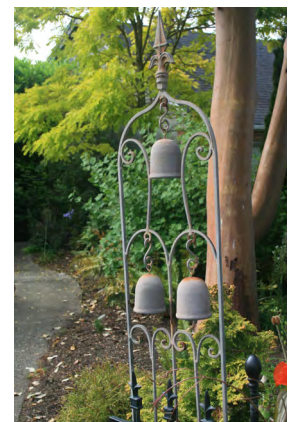
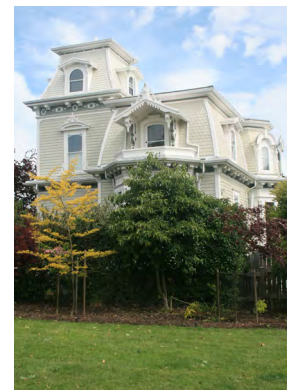
- An inventory and analysis of neighbourhood design characteristics;
- A vision statement and set of design principles as the rationale for the design guidelines;
- Development Permit Area design guidelines for Form and Character;
- A set of general design guidelines and recommendations for public realm design and improvements; and,
- Specific guidance on site planning and design for development opportunity sites along Head Street south adjacent to the harbourfront and including the "Triangle Lands", are also included.

The Spirit of West Bay

During the West Bay Design Guidelines Stakeholder Workshop, we asked: "What words capture the spirit of West Bay?" The following is a summary of responses:



Neighbourhood Characteristics: Photo Essay



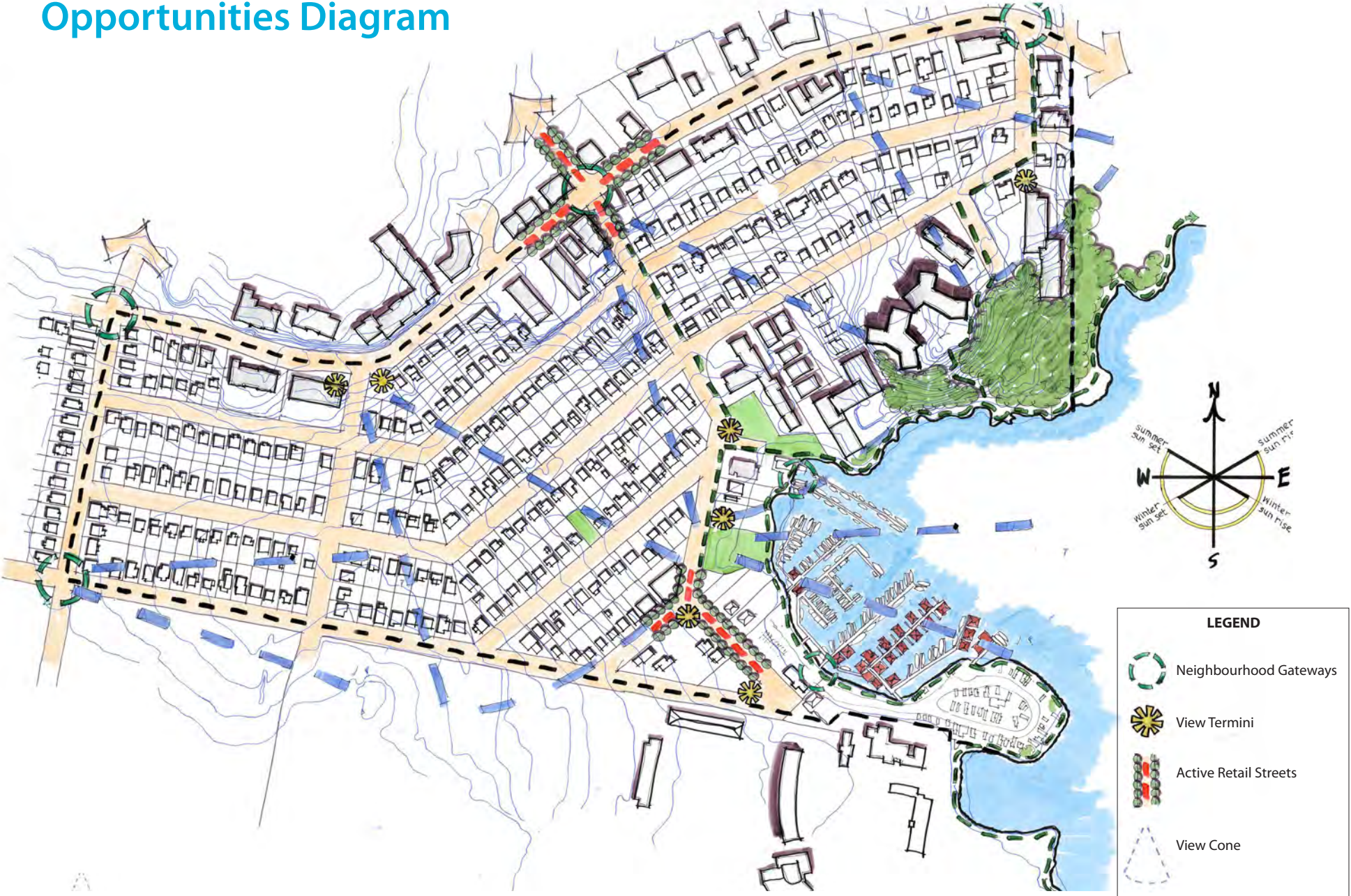
West Bay Ortho Photo and Study Area



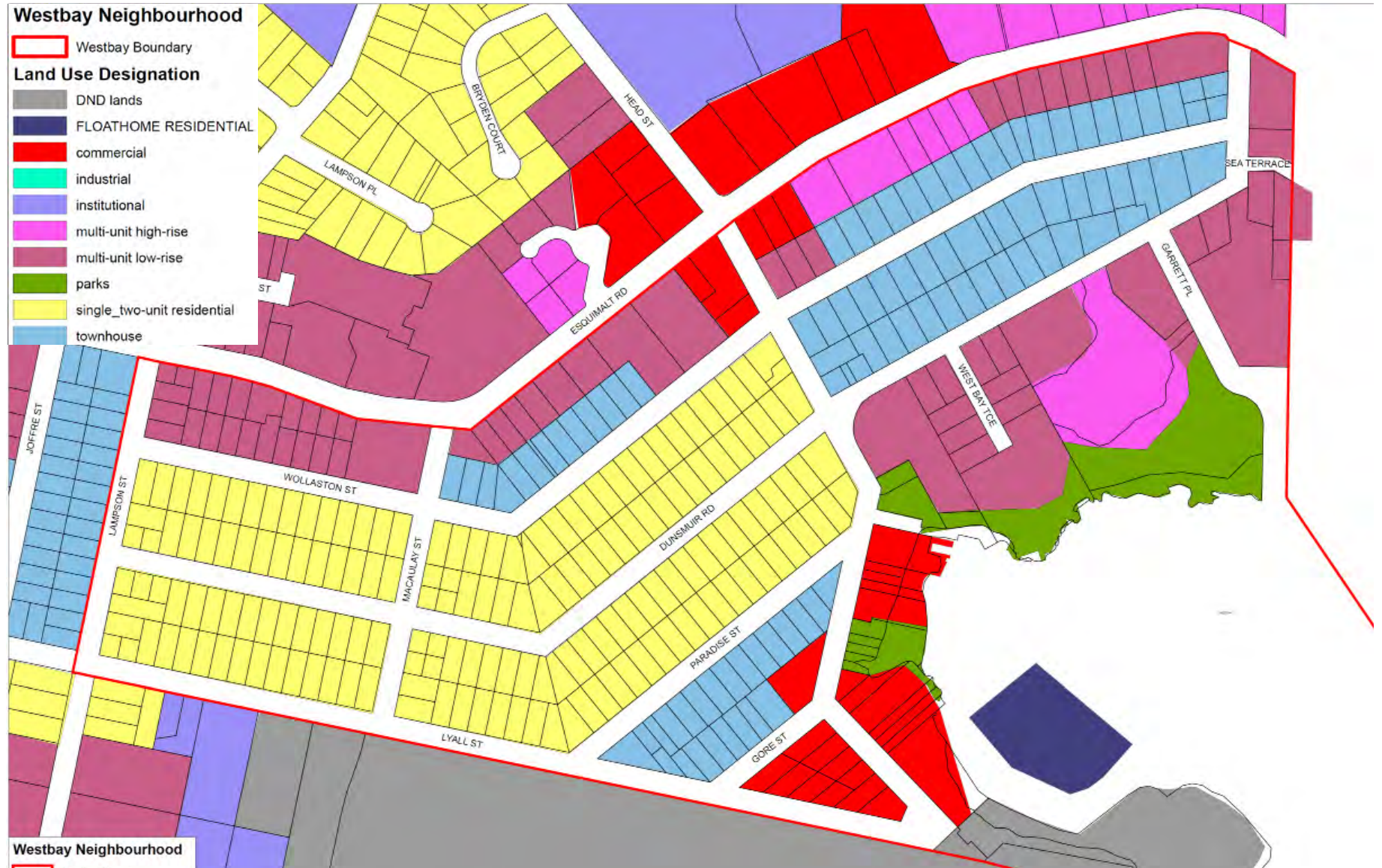
Neighbourhood Characteristics: Topography and Building Massing (Existing)



Neighbourhood Characteristics: Opportunities Diagram



West Bay Neighbourhood - OCP Land Use



2. Design Vision & Principles

Vision

West Bay is a thriving, attractive and walkable harbourfront neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbourfront recreation, and sunny open spaces with strong connections to the harbour, make this a truly liveable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.



Photo courtesy: Josh McCulloch, PictureBC

Design Principles

The following design directions for the public realm were identified through the workshop to help maintain and enhance the unique identity of West Bay:

- **Strong connection to the harbourfront:** Maintain and enhance visual and physical connections to the harbour and harbourfront trail network.
- **A walkable neighbourhood:** Publicly accessible buildings present a friendly face to streets and open spaces to make them active, attractive and inviting to pedestrians. Make connections within and to adjacent neighbourhoods to make walking easy.
- **Human scale:** Use architectural features, details, and site design elements that are of human proportion and clearly oriented for public, pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it on foot.
- **Marine ecology:** Enhance, restore and celebrate the unique marine and upland ecology. Layer and integrate mobility, recreation, stormwater, habitat and ecological systems in the context of the working harbour.
- **Harbourfront community & identity:** Ensure building and open space design that supports and enhances the unique sense of community and identity, including its architectural and marine heritage. More broadly, this includes the rich natural and human history of the area and associated expression of local culture, festivals, and gathering at and near the harbour front and water's edge.
- **Neighbourliness:** Ensure new development responds positively to the existing context by ensuring a sensitive transition in scale, by minimizing view and shadow impacts, and by responding to the positive design characteristics, if only subtly, of adjacent development.

3. Design Guidelines

Overview

Urban design is the comprehensive and cohesive combination of buildings, streets, open spaces and the natural environment and has, as its objective, the creation of memorable public spaces.

The essence of good urbanism is determined by the relationship between:

- the built and natural environments, and
- the public and private realm - the areas between buildings and public open spaces - at street level.

Buildings, streets, and other public open spaces scaled for human comfort and use are essential to the creation of a functional, aesthetically rich and vibrant neighbourhood. Building fronts and their orientation to streets and public open spaces are therefore the strongest determinants of the character and quality of neighbourhoods.

The design elements and approaches on the following pages provide a starting point and preliminary framework in maintaining and enhancing what is special, unique and wonderful about West Bay. They also provide a foundation to ensure new development and public realm improvements contribute to a vibrant, safe and accessible neighbourhood environment that encourages walking and street life while creating a unique social and physical environment that celebrates the beautiful natural setting of West Bay.



Photo: Joaquin Karakas



Photo: Joaquin Karakas

A Friendly Face to the Street

INTENT

- To define residential and commercial streets with active and attractive building fronts that have a positive orientation to streets and other public open spaces.
- To ensure buildings are sited and designed to be welcoming, and to encourage street vitality, visual interest, and safety.

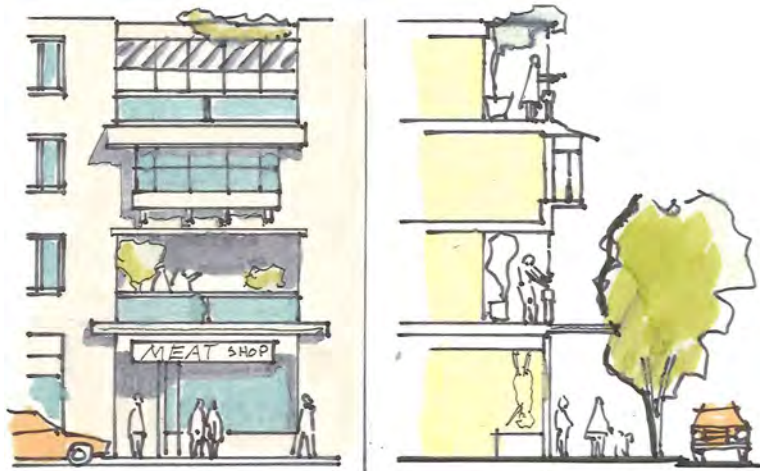
GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

- Locate publicly oriented active uses at grade and at or near the sidewalk edge.
- Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;
- A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.

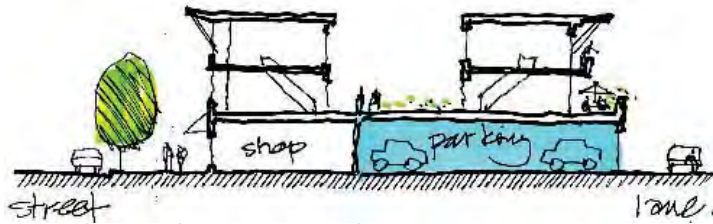


Active uses oriented to public streets and openspaces creates vibrant and attractive neighbourhoods (above, below and below left)





Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.

- Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.
- On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Locate on-street parking at the curb in front of shops.
- Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
- Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



Photo: Joaquin Karakas

On corner sites, develop street-facing façades for both streets. .

- Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.
- Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
- Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above and right)



Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

Photo: Dan Burden



Incorporate plantings and other landscape details such as banners.

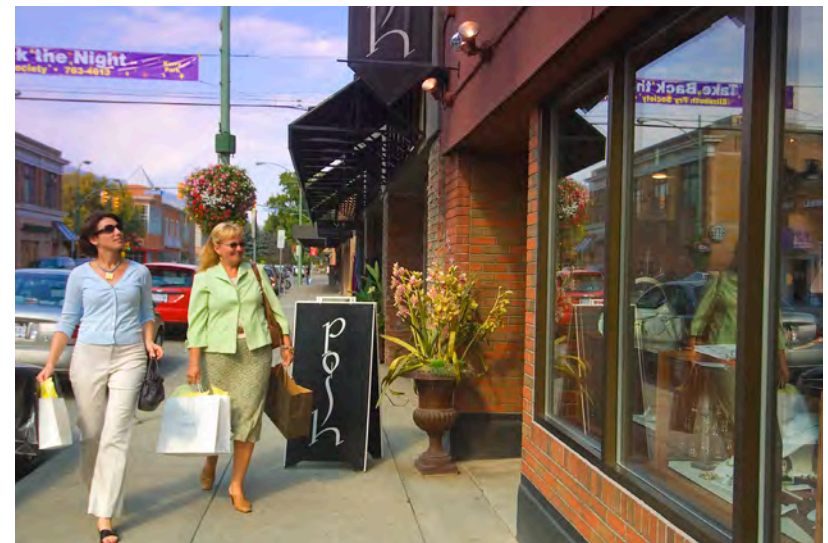


Screen



When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.

- Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:
 - » Install a vertical trellis in front of the wall with climbing vines or other plant material
 - » Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
 - » Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
 - » Employ quality materials of different textures and colours to make the wall more interesting visually
 - » Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
 - » Incorporate walls into a patio or sidewalk café space
 - » Terrace (step down) retaining walls



GUIDELINES: RESIDENTIAL BUILDINGS

- Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
- incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
- Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
- Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
- Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.

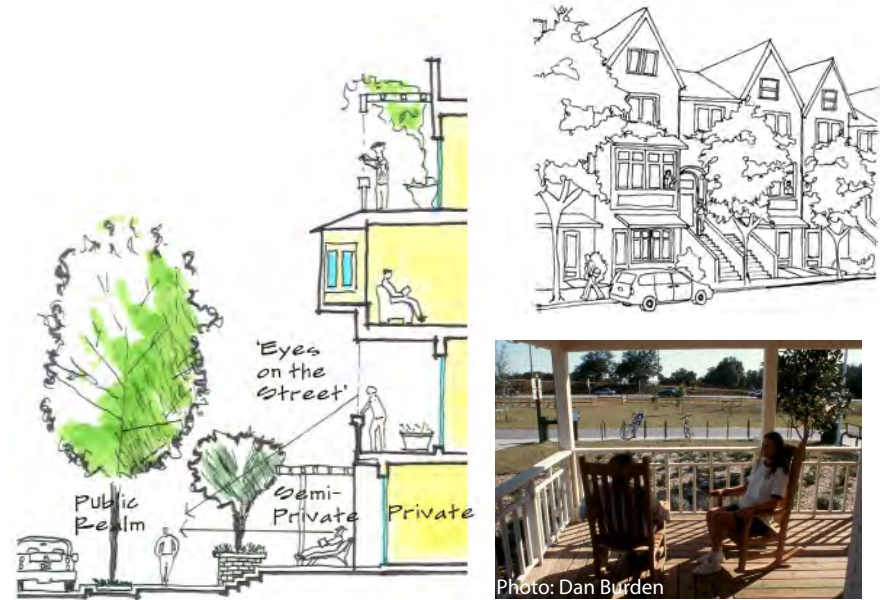


Photo: Dan Burden

Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



Photo: Joaquin Karakas



Photo: Joaquin Karakas



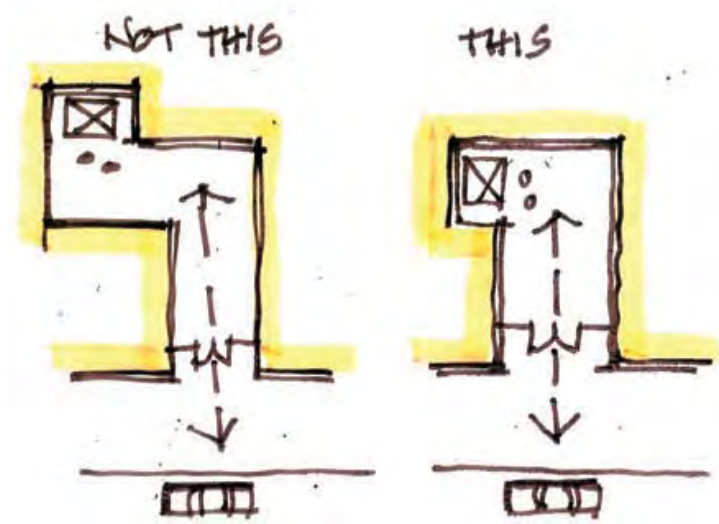
Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street. .



Photo: Joaquin Karakas

Consider using a landscape transition zone between entryways and public sidewalks.

- Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
- A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
- Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Direct sight lines into elevator lobbies are safer

VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
- New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.
- Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour
- Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's
- Mark/celebrate corners and street-end views through building and open space design.
- Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

- New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. "Flood Hazard Area Land Use Management Guidelines" (2004) (http://www.env.gov.bc.ca/wsd/public_safety/flood/pdfs_word/guidelines-2011.pdf).



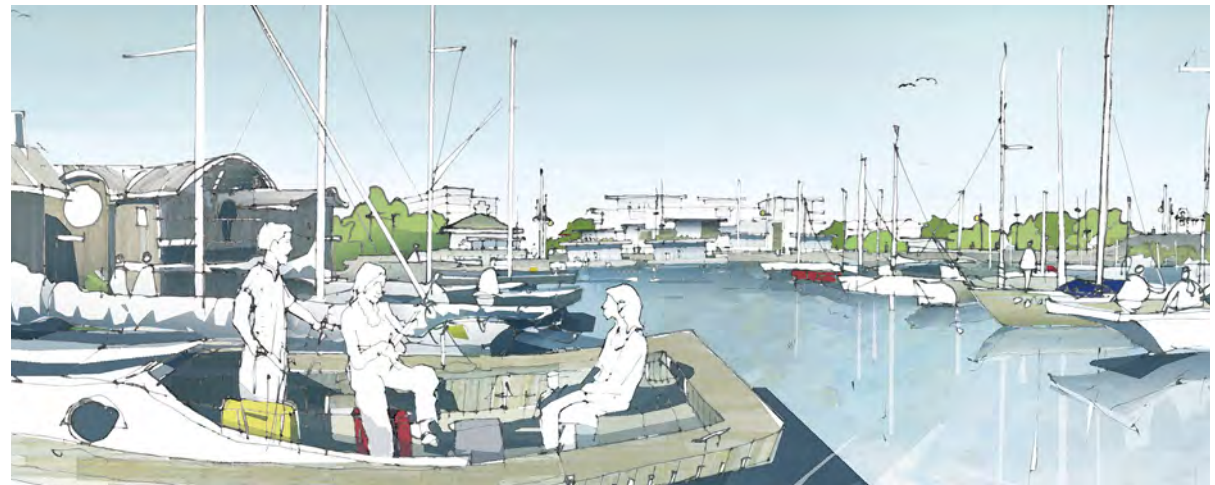
Photo: Bill Brown

Maintain street-end views to the harbour-front



Photo: Franc D'Ambrosio Architecture and Urbanism

Create new public connections to harbourfront uses and activities.



Neighbourliness

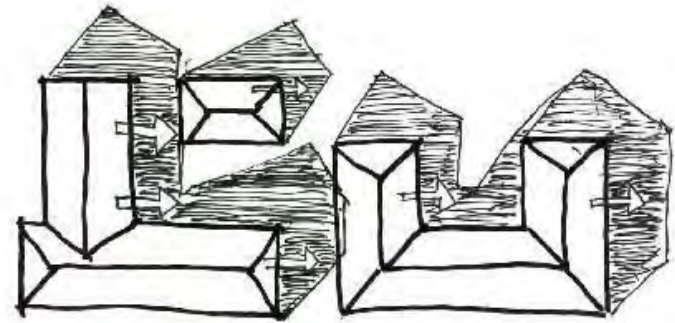
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

GUIDELINES

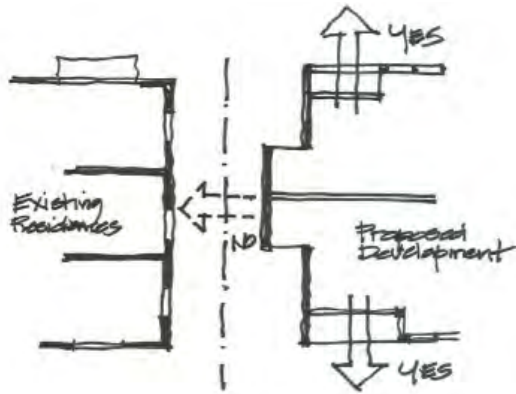
- New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.
- Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
- In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.
- Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
- Locate development to minimize view impacts on existing and planned future development.
- Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.
- Sun shade diagrams for the Winter Solstice will be required only when significant impacts are expected such as shading solar panels
 - » Winter Solstice: 9 a.m., 12 noon, 3 p.m.



Transition in scale to existing context



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and buildings

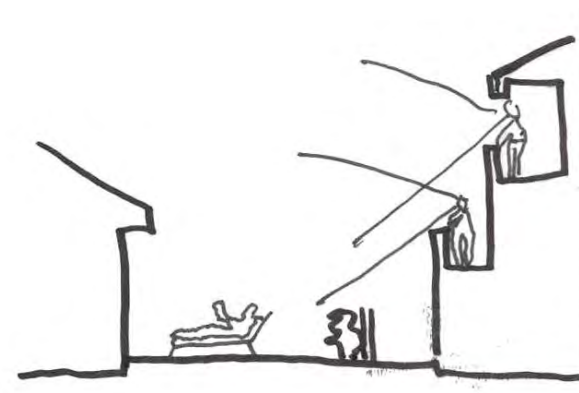


Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.



This building steps down to the slope to reduce the visual impact from it's massing

- Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
- Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
- Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
- Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 - » Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 - » Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 - » Stagger windows to not align with adjacent, facing windows.
 - » Primary windows into habitable spaces should not face interior side-yards



Minimize overlook into adjacent private yards

Architectural Concept: Achieving a Human Scale

OVERVIEW AND INTENT

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

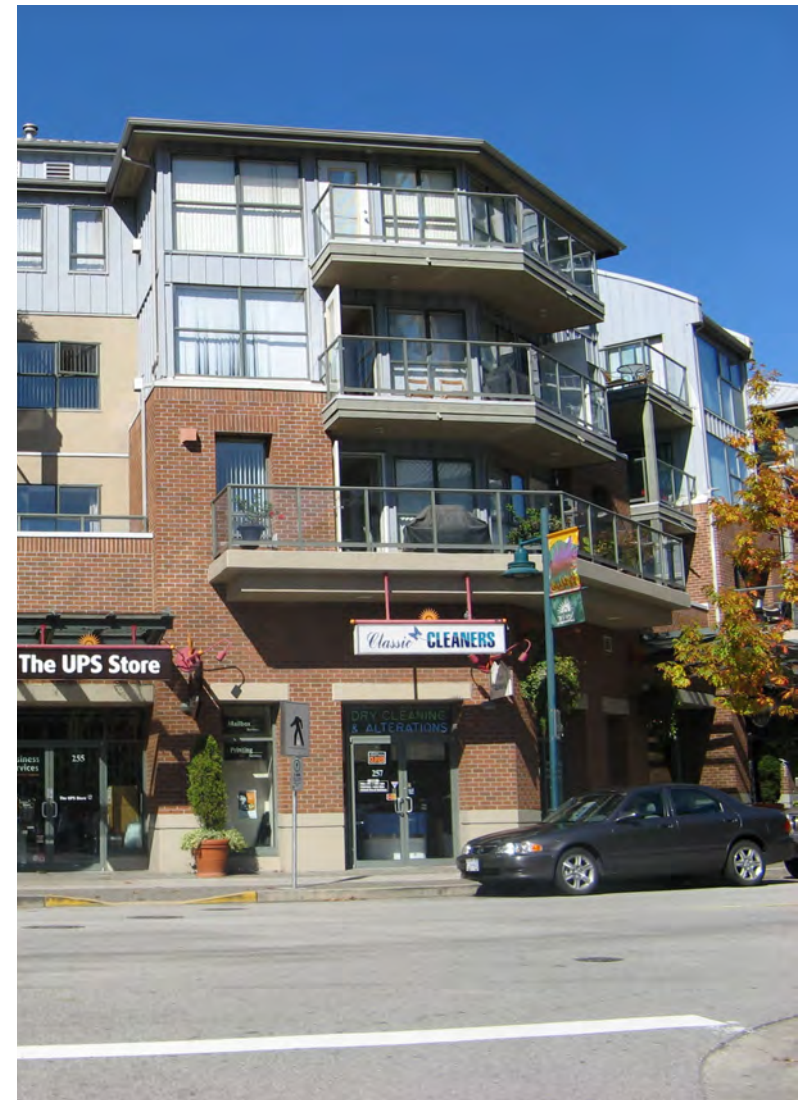
Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in a modern composition.

GENERAL GUIDELINES

- The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- Design buildings to express their internal function and use.
- Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.
- Examples of architectural features include:
 - » Building height, massing, articulation and modulation
 - » Bay windows and balconies
 - » Corner features accent, such as turrets or cupolas
 - » Decorative rooflines and cornices
 - » Building entries
 - » Canopies and overhangs
- Examples of architectural details include:
 - » Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
 - » Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
 - » Articulation of columns and pilasters
 - » Ornament or integrated artwork
 - » Integrated architectural lighting
 - » Detailed grilles and railings
 - » Substantial trim details and moldings
 - » Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians



Photo: Franc D'Ambrosio Architecture and Urbanism

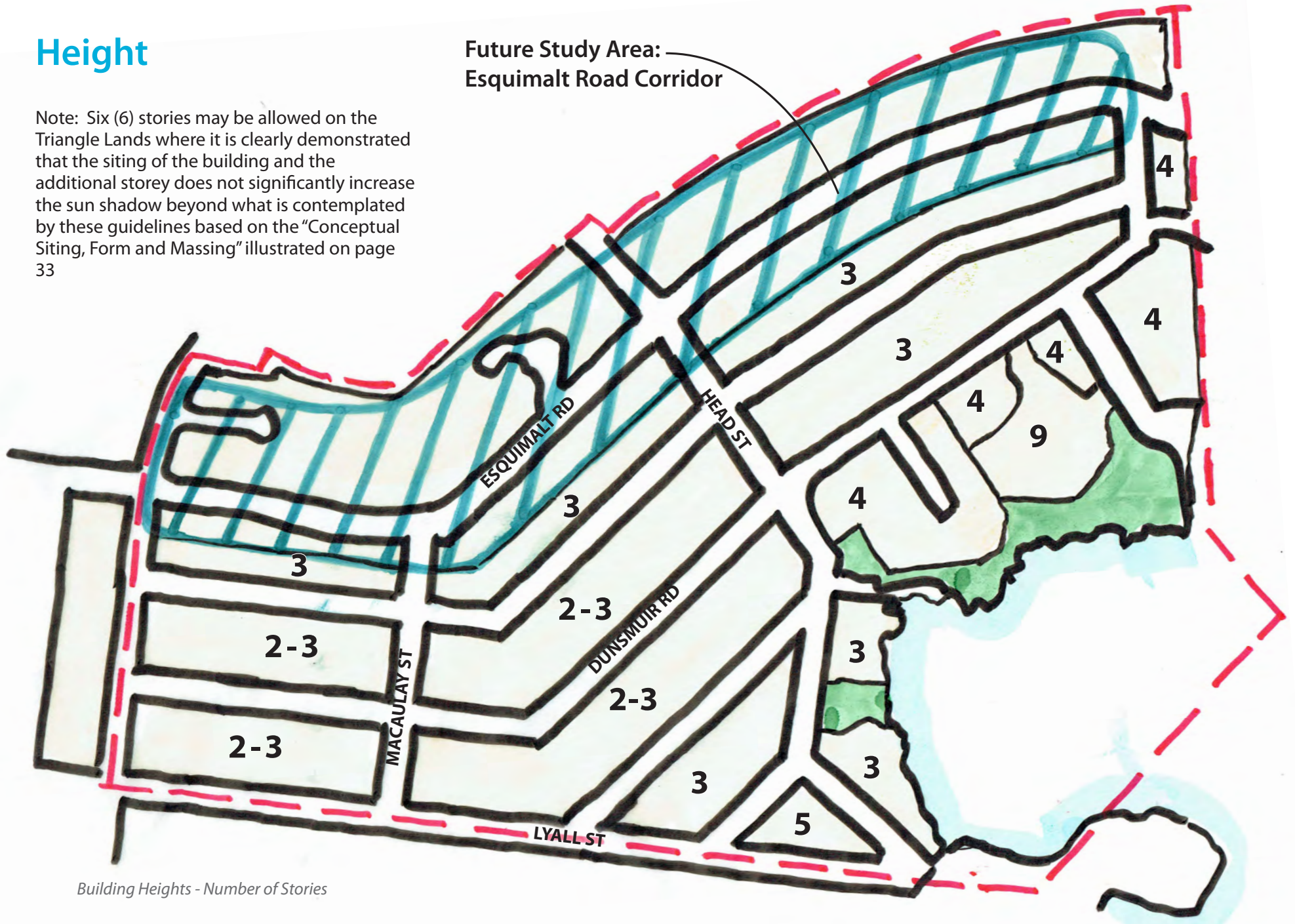
Design entrances to distinguish between individual commercial and residential.

- Locate and design entrances to create building identity and to distinguish between individual commercial and/or residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
- Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
- Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
- Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - » Are vertically proportioned and approximately the size and proportion of a traditional window
 - » Include substantial trim or molding
 - » Are separated from adjacent windows by a vertical element
 - » Are made up of small panes of glass
 - » Are separated with moldings or jambs but grouped together to form larger areas of glazing
- The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
- In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
- Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.

Height

Note: Six (6) stories may be allowed on the Triangle Lands where it is clearly demonstrated that the siting of the building and the additional storey does not significantly increase the sun shadow beyond what is contemplated by these guidelines based on the "Conceptual Siting, Form and Massing" illustrated on page 33

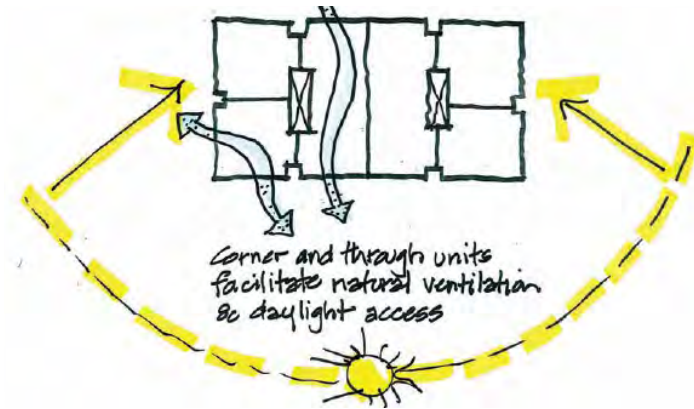
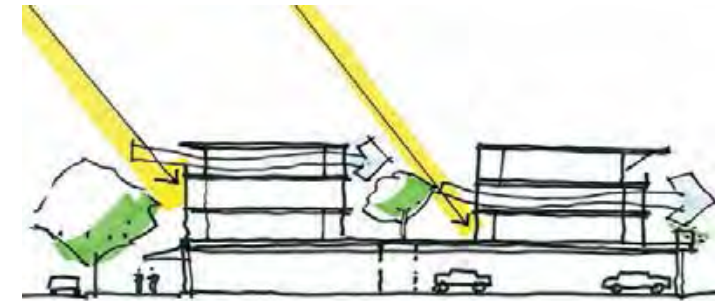
Future Study Area:
Esquimalt Road Corridor



Building Heights - Number of Stories

Green Healthy Buildings & Open Spaces

- Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.
- Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
- Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.

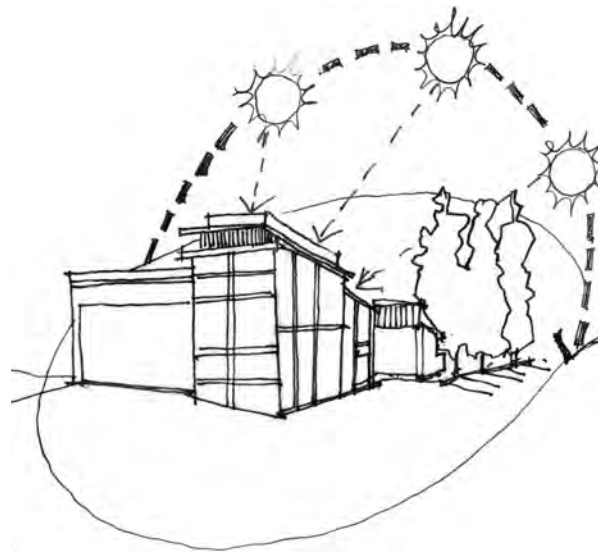
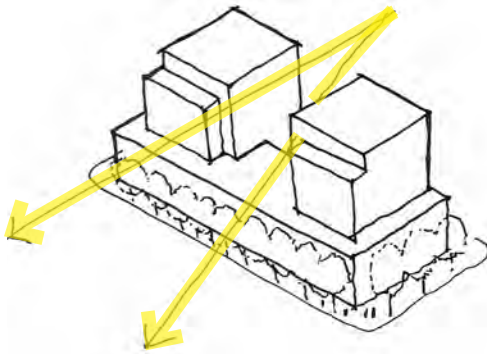


Incorporate common areas as defining elements of projects.



Photo: Joaquin Karakas

A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.



- New buildings should not block significant views or solar access to adjacent buildings and open spaces.
- Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
- Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
- Retention and infiltration best management practices for rainwater should be used as appropriate.
- Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.

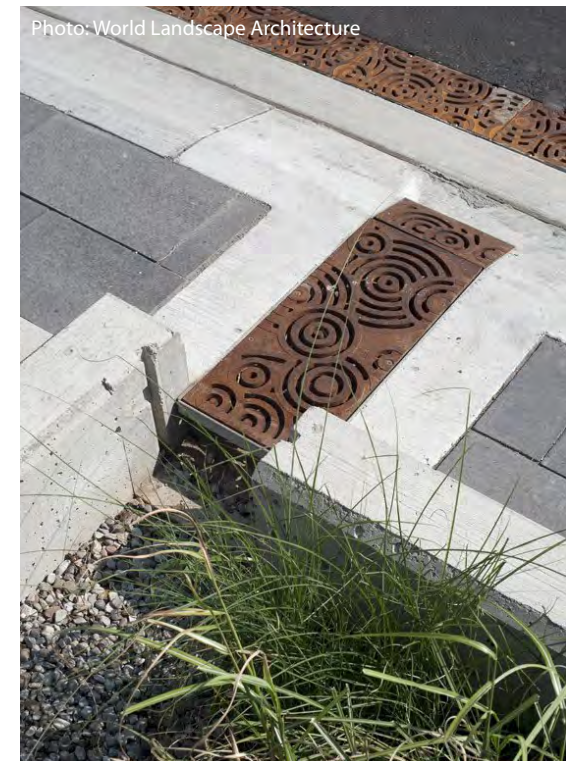


Photo: World Landscape Architecture

Use best management practices for rainwater where appropriate.

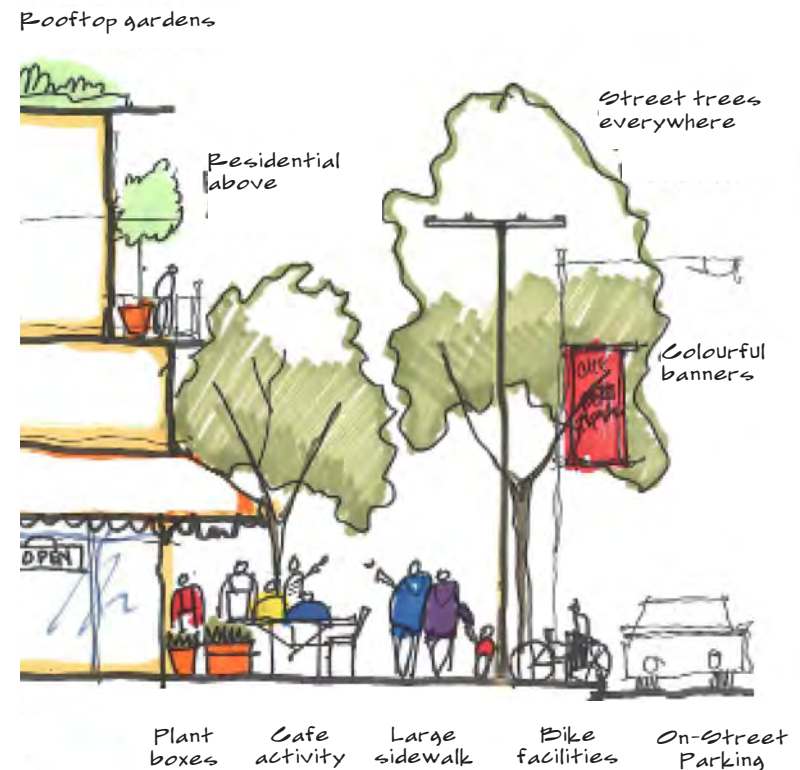
Public Realm Design: Pedestrian Comfort & Safety

INTENT

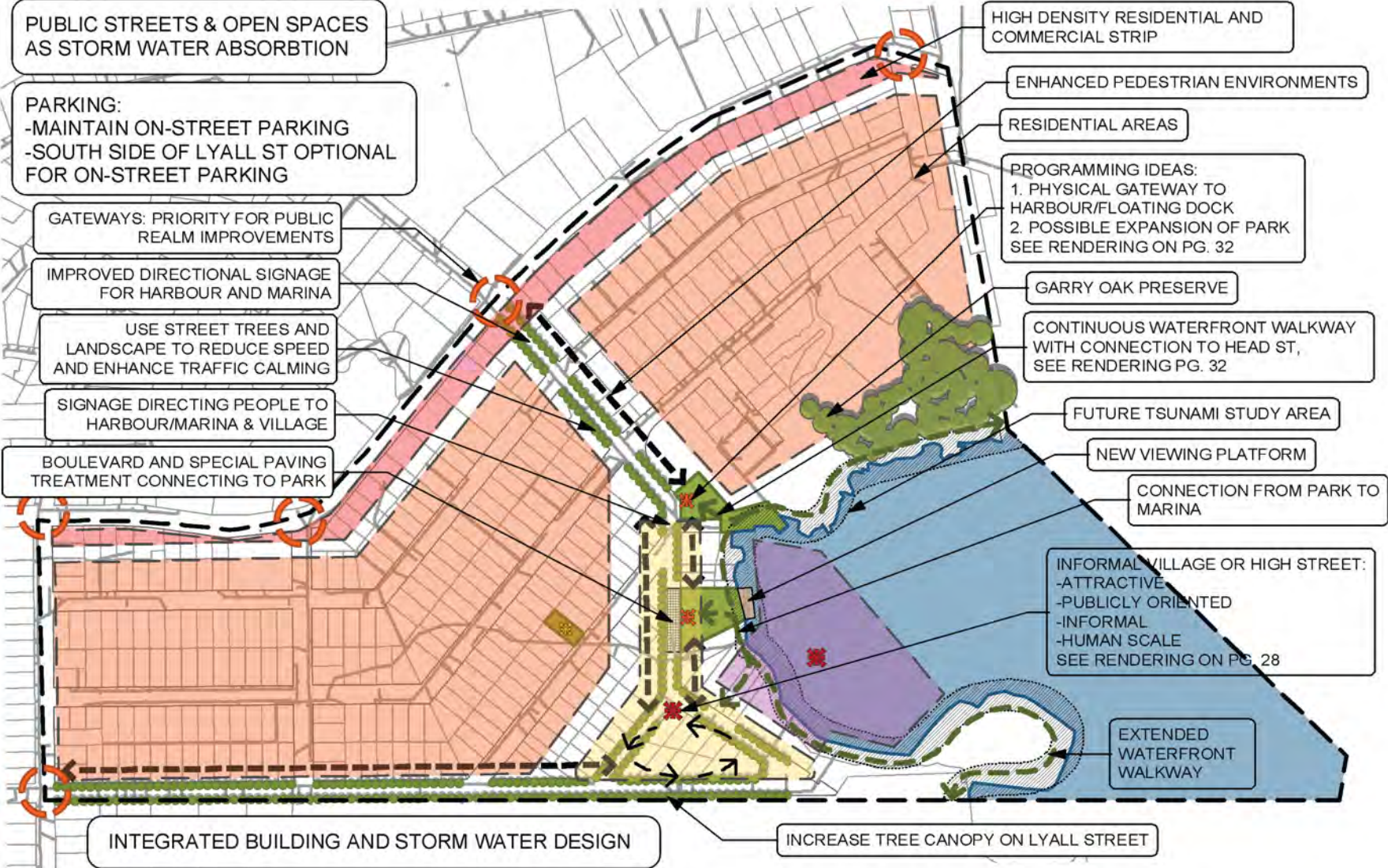
To ensure that the design of streets and open spaces creates visual interest, comfort, and safety for pedestrians and contributes to a unique local identity and sense of place for West Bay.

GUIDELINES

- Provide a continuous planting of street trees along both sides of neighbourhood streets, with priority for the Head Street South Village High Street and Esquimalt Road as indicated in the Recommended Public Realm Improvements map on page 25.
- Provide zebra- or ladder-painted crosswalks, or crosswalks made of special paving materials, at all key pedestrian crossings to increase driver awareness.
- Incorporate corner bulges into streetscape design in key pedestrian areas to enhance pedestrian crossings and provide space for landscaping, seating, rain gardens and public art.
- Provide adequate public streetscape amenities including benches, planters, garbage receptacles, bike racks, and distinctive bus shelters in areas with high pedestrian activity.
- Café tables are permitted and encouraged on public sidewalks in retail areas
- Pedestrian-oriented lighting should be provided with priority for high-activity pedestrian areas such as along Head Street and Esquimalt Road.
- Small plazas and squares should be incorporated in prominent locations with good solar gain, such as at Lyall and Head Street.
- Public art should be incorporated into parks, plazas and other key activity areas, especially at gateways and entry points, to enhance the sense of identity and entry into the neighbourhood.



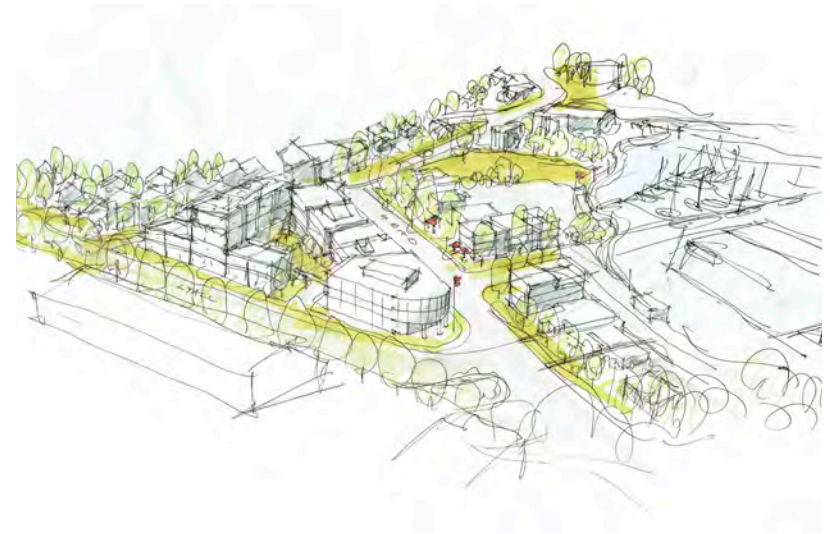
Recommended Public Realm Improvements



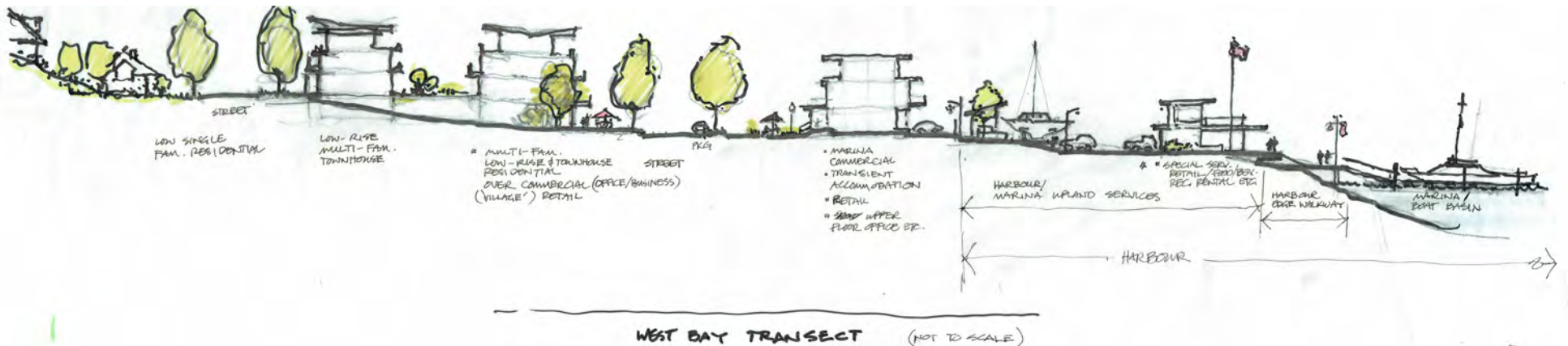
4. Site Specific Guidelines: Harbourfront Urban Village High Street

An important objective in the development of the design guidelines to address local concerns about density, height and massing, shadowing and view impacts, as well as the interface between federal lands and local ecosystems, specifically, in the harbourfront urban village area centred on Head Street South between Lyall and Paradise Streets. To this end, these guidelines were developed with input and feedback from neighbourhood stakeholders. This included an integrated design workshop (charrette) where design concepts and approaches for both public and private realms were developed. The sketch examples on this page were produced during this charrette.

This section is intended to provide site planning and design guidance and recommendations for this part of the West Bay Neighbourhood, based on stakeholder input. It is important to note that the concepts and illustrations on the following pages do not represent a development proposal, but rather a conceptual implementation of the preceding West Bay Neighbourhood Design Vision, Principles and Guidelines.



Sketches from the West Bay Design Workshop (above and below)



URBAN VILLAGE HIGH STREET LOOKING NORTH-WEST AT CORNER OF HEAD AND LYALL STREET

Maximum 5 storey building height



Urban Village high street defined by low scale buildings with active fronts and lush street tree canopy

Harbourfront retail, food and beverage uses

New public connection to harbourfront and Hidden Harbour Marina

Landmark corner plaza located at south gateway to urban village with views to harbourfront

URBAN VILLAGE HIGH STREET (HEAD STREET SOUTH) LOOKING SOUTH

Street trees, pedestrian oriented lighting and banners line either side of the street.

Continuous planting of street trees along south-west side of Head Street

Maximum 3 storey street wall along Head Street south

A rhythm of pedestrian oriented shop fronts and entryways line the street with sidewalk cafes and other uses spilling out onto the sidewalk

Landscaped bulb-outs and parking bays on north-east side of Head Street



Marked (zebra striped) pedestrian crossings at Core and Lyall Streets

LOOKING NORTH TO HARBOUR AND URBAN VILLAGE HIGH STREET

*Publicly oriented active uses
activate the harbourfront*



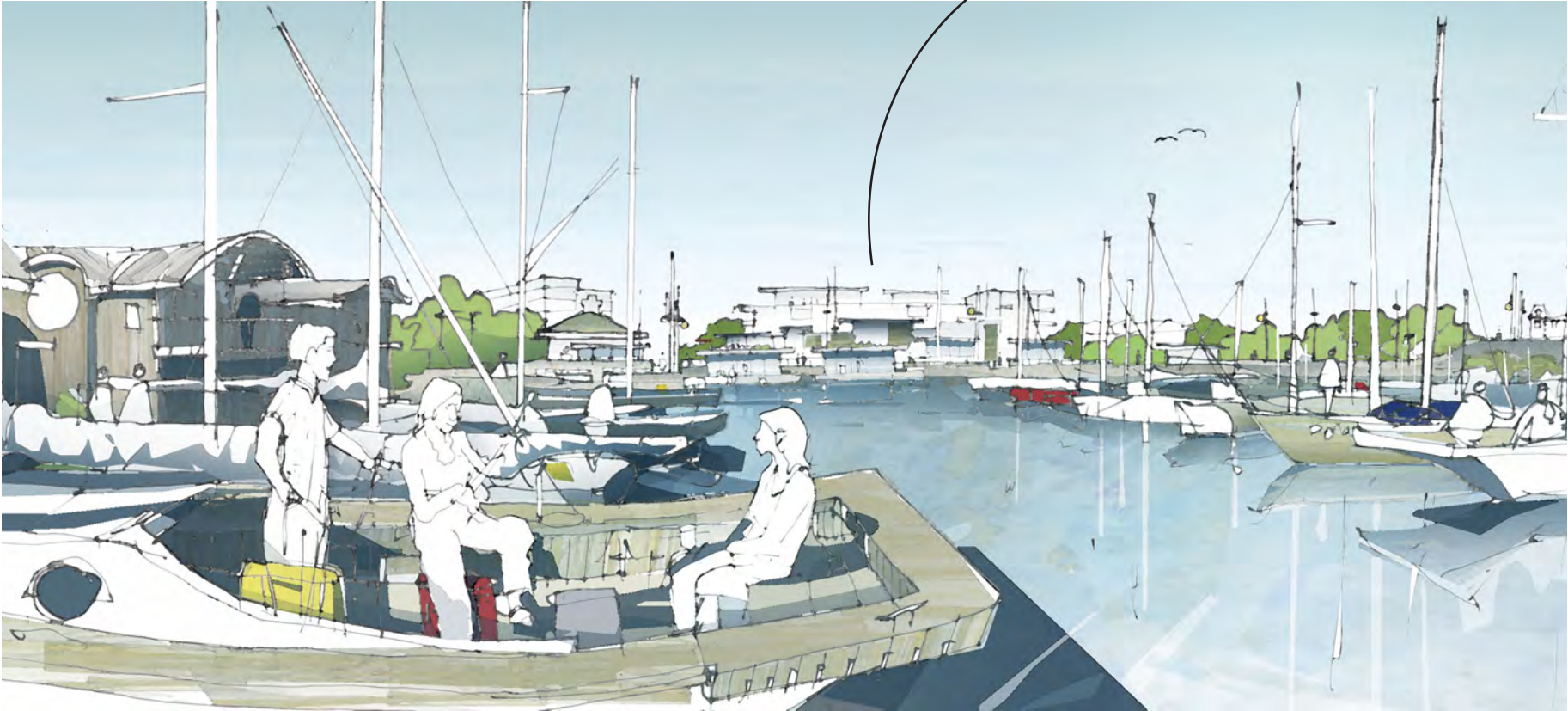
*Parking areas along the
harbourfront represent great
redevelopment opportunities where
appropriate*

*Continuous walking connecting
Songhees to West Bay Urban
village/marina and beyond*

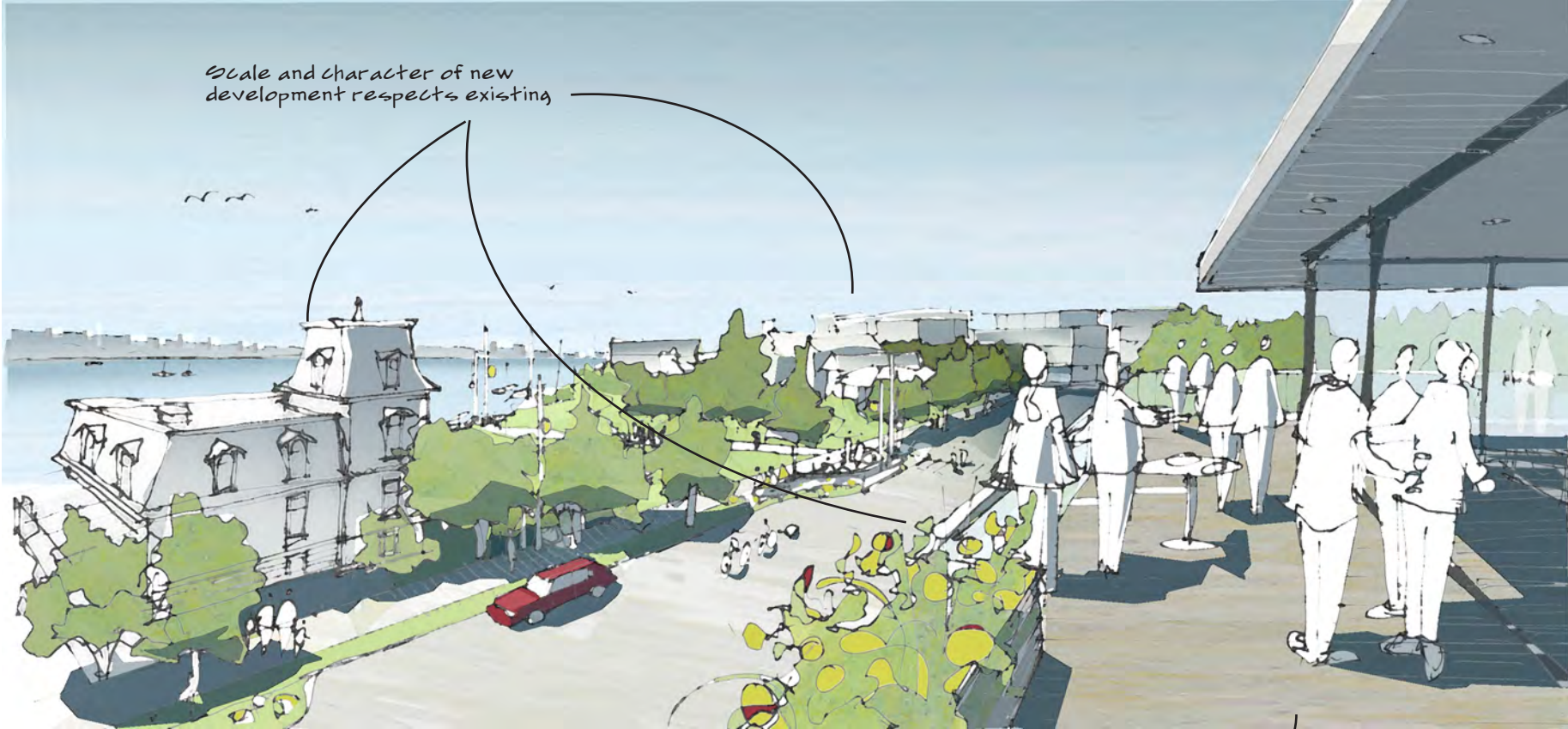
VIEW TO TOWARDS URBAN VILLAGE HIGH STREET FROM THE HARBOUR LOOKING WEST

Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity.

New development scaled to be compatible with, and not overwhelm, harbourfront character and identity. Also exhibits the transition massing from existing 7 storey on DND land.



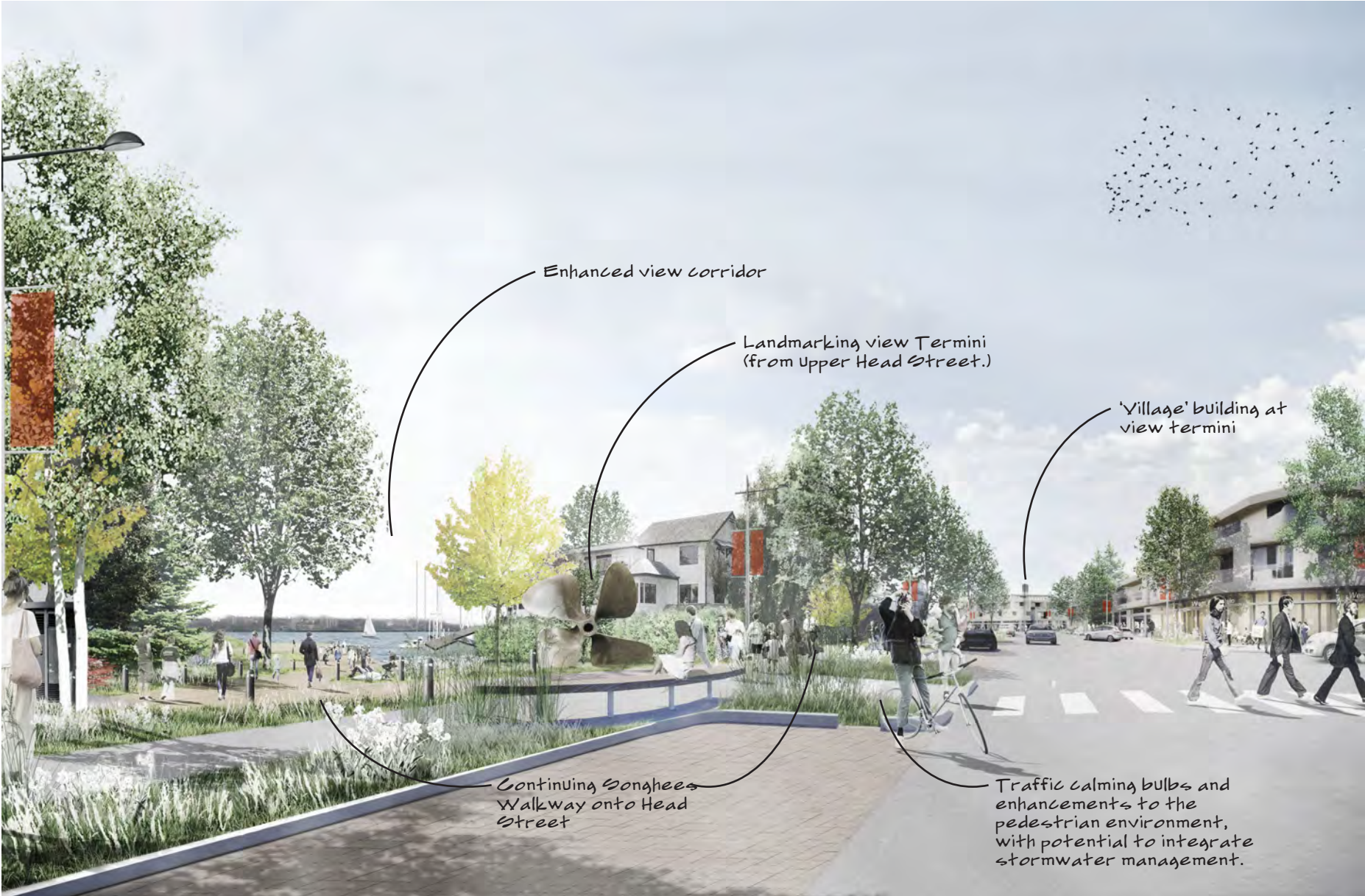
VIEW FROM FUTURE BUILDING LOOKING SOUTH-EAST TO HARBOUR ACROSS URBAN VILLAGE HIGH STREET AND CAPTAIN JACOBSON PARK



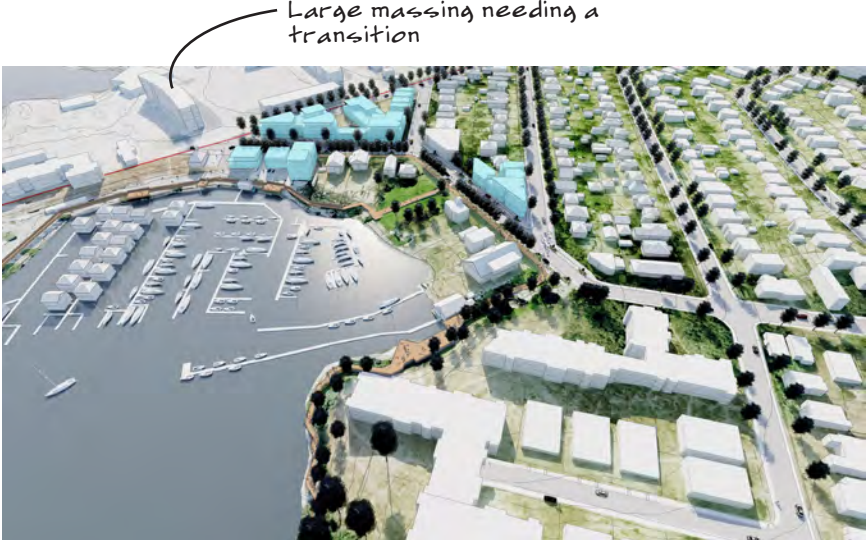
Scale and character of new development respects existing

Upper storeys step back to let sun onto urban village high street

HEAD STREET LOOKING SOUTH TOWARDS GORE AND LYALL

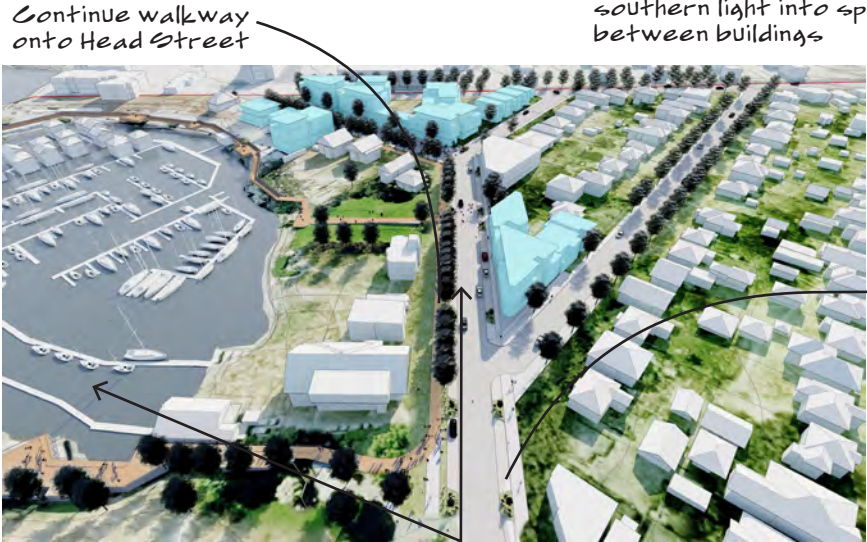


URBAN VILLAGE HIGH STREET: CONCEPTUAL SITING, FORM AND MASSING



Large massing needing a transition

- Legend**
- Conceptual massing
 - Continuous Boardwalk/Walkway



Continue walkway onto Head Street

Masses oriented to let southern light into spaces between buildings



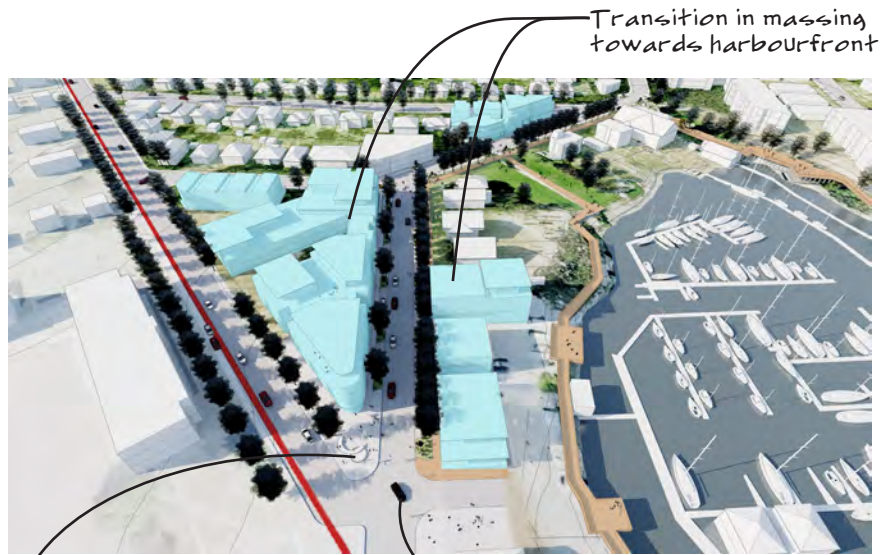
Continuous walkway

Public realm enhancements opportunity areas

New intersection

Improved view corridor

View on page 32.



Transition in massing towards harbourfront

Landmark corner plaza located at South gateway to urban village, with views to harbourfront. See sketch on page 27.

New intersection



View on page 28

Preserve and highlight heritage building

View termini



Streetscape Guidelines

OVERVIEW

The design, construction and maintenance of streetscapes in the West Bay Neighbourhood should serve to reinforce its unique identity by incorporating:

1. Strong connection to the harbourfront;
2. A walkable neighbourhood;
3. Human scale;
4. Harbourfront ecology;
5. Harbourfront community & identity, and
6. Neighbourliness.

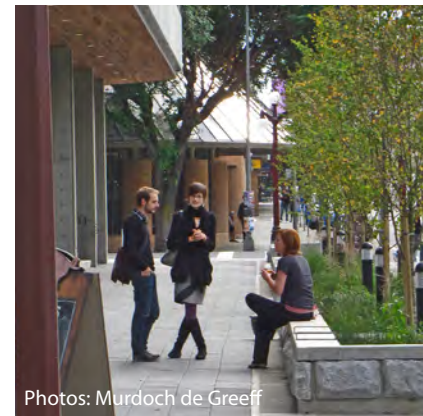
To illustrate the integration of these principles four typical streetscape sections and plans are presented below. These include Head Street North, Head Street South, Lyall Street West, and Lyall Street East. The sections focus on Public Realm improvements within two distinct areas of the West Bay Neighbourhood; West Bay Urban Village Gateways and West Bay Urban Village.

WEST BAY URBAN VILLAGE GATEWAY

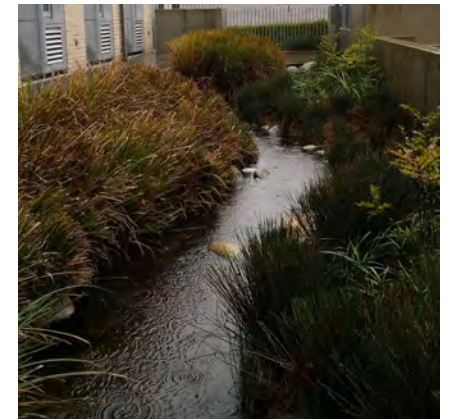
West Bay Urban Village is accessible by Lyall Street and Head Street. Both streets currently have 18m wide Right of Ways (R.O.W) and accommodate two way traffic in standard 4.25m wide lanes. Objectives for the Lyall Street and Head Street Gateways include: improving the pedestrian environment, maintaining on-street parking, improved wayfinding, increased tree canopy, and integrated stormwater management. The Lyall Street West section shows a typical treatment through an existing residential lot on Lyall across from the Department of Defense (DND) lands. The Head Street North section shows a typical treatment near the intersection of Head and Esquimalt.

WEST BAY URBAN VILLAGE

West Bay Urban Village is located on the harbour front adjacent to the Marina. The area is identified for potential future development with a primary focus on the 'Triangle Property'. Some of the objectives for the Urban Village include: increased density, creation of places to gather, an inviting and active public realm, and reduced speed/traffic calming. Both the Lyall Street East section and Head Street South section show typical treatments at the triangle property and suggest a 'Skinny Streets' approach with reduce travel lane widths (3.0m)



Integrated Storm Water Management, The Atrium building, Victoria BC



Integrated Storm Water Management, Reliable Controls building, Victoria BC

Integrated Storm Water Management

Integrate rain gardens, swales, tree planting, and other suitable BMP's to capture, slow, and treat storm water before in enters the harbour.



Photos: Murdoch de Greeff

Storm Water Plaza,
Cecelia Road, Victoria BC



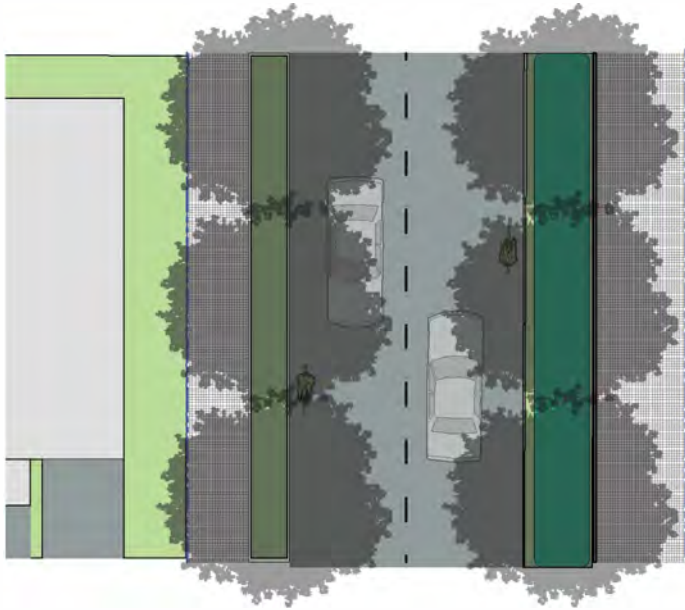
Photo: Paul de Greeff

Storm Water Streetscape,
Portland, Oregon



Photo: Murdoch de Greeff

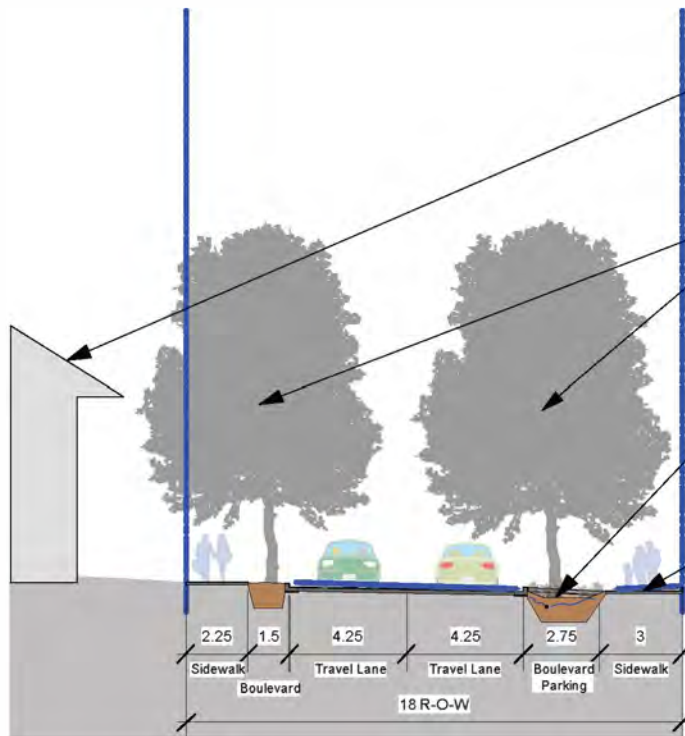
Storm Water Traffic Bulge,
Trent St., Victoria BC



Head Street North

Head street North, near the intersection of Head and Esquimalt Road, is intended to act as a Gateway to the Village, Harbour, and Marina. The intent on this section of Head is to increase the appeal of the pedestrian environment, employ clear wayfinding strategies to direct traffic towards the Village and/or Harbour/Marina.

Strategies to achieve this include: Increased sidewalk width to encourage walking, a double row of trees to accentuate Head as a linear corridor to the harbour/village, source controls such as onstreet rain gardens to treat stormwater, and similar building scales to reinforce the street end/harbour as a focal point. Development fronting onto the east side of this section of Head street should maintain physical and visual permeability to the waterfront by integrating paseos and achieving a minimal building separation of 6 metres



Neighbourliness-

Transition building heights from shoreline to upland areas to minimize shadow and view impacts.

Human Scale-

Utilize a double row of trees to accentuate the linearity of Head Street as a Gateway and the Village as a destination at the terminus.

Upland Ecology-

Install onstreet rain gardens to capture, treat and slow street run off before it enters the harbour downstream.

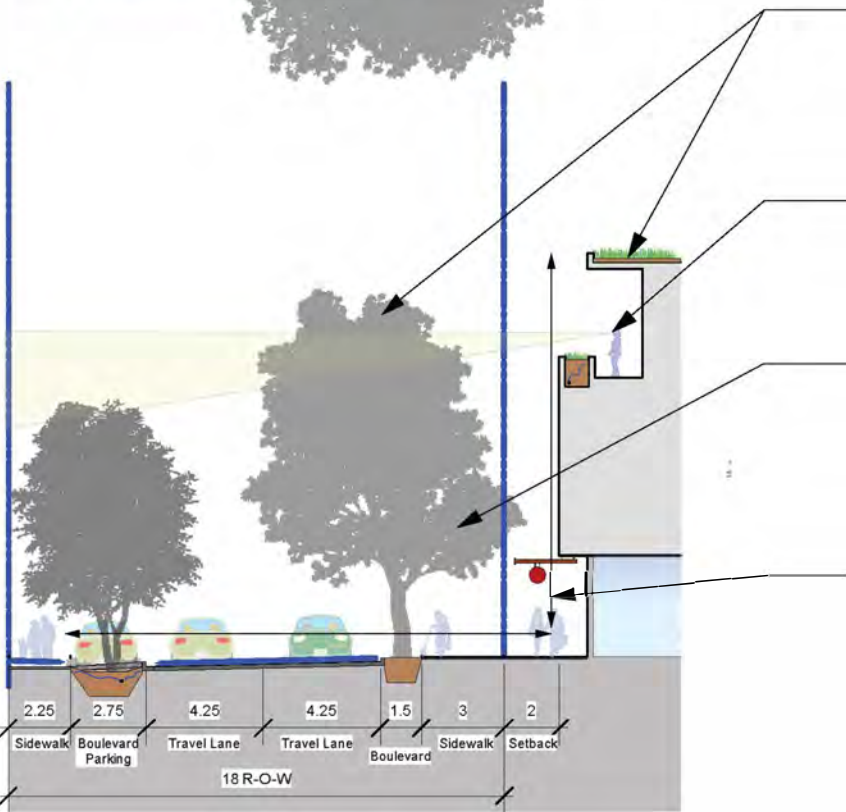
Walkable Neighbourhood-

Utilise vegetated boulevard strips to provide separation from vehicular traffic and increase sidewalk widths on gateway streets to promote wayfinding while contributing to attractive and inviting pedestrian environments.

Head Street South

Head street South, is intended to act as the communities Village or High Street. The intent on this section of Head is to provide public open spaces that contribute to a sense of being on the waterfront while supporting the commercial viability of the area. The *West Bay Village* public space should include but is not limited to waterfront access, space for gathering and festivals, and an attractive pedestrian oriented environment.

Strategies to achieve this include: *Distance to Building Height Ratios* of between 1:1 and 3:1, use of green building techniques to preserve the health and visual aesthetic of the shoreline, and transitions in scale to perserve upslope and adjacent views.



Waterfront Community and Identity-

Green Roofs, street trees and building heights support each other to reinforce the identity of West Bay as a Green Waterfront Community. Designs acknowledge views both to and from the inner harbour.

Strong Connection to the Waterfront/Neighbourliness-

Buildings step back and transition in scale to maximize potential for harbour views while minimizing shadow impacts on adjacent properties.

Waterfront Ecology-

Tree plantings intercept rainwater, reduce storm water flows through evapotranspiration, and provide valuable bird habitat contributing significantly to the shoreline ecology.

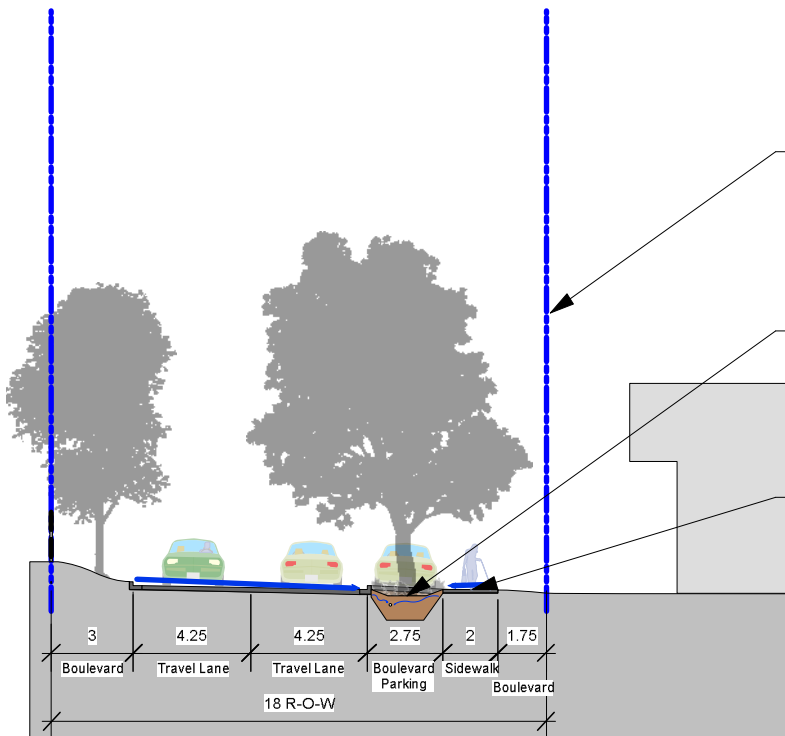
Human Scale-

Buildings should be between 1:1 and 3:1 *Distance to Building Height Ratio* to create intimate and inviting public open spaces.



Lyall Street West- Existing Residential

West Lyall Street acts as an important connection from the harbour to the surrounding residences, Esquimalt village, and the Esquimalt Rec. Center. Future developments along this corridor should focus on improving the pedestrian environment to increase the walkability of the neighbourhood. Strategies to achieve this include: Tree planting to reduce the visual scale of the street and provide shade, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour downstream, and separated boulevards to increase the attractiveness of the pedestrian environment.



Human Scale-

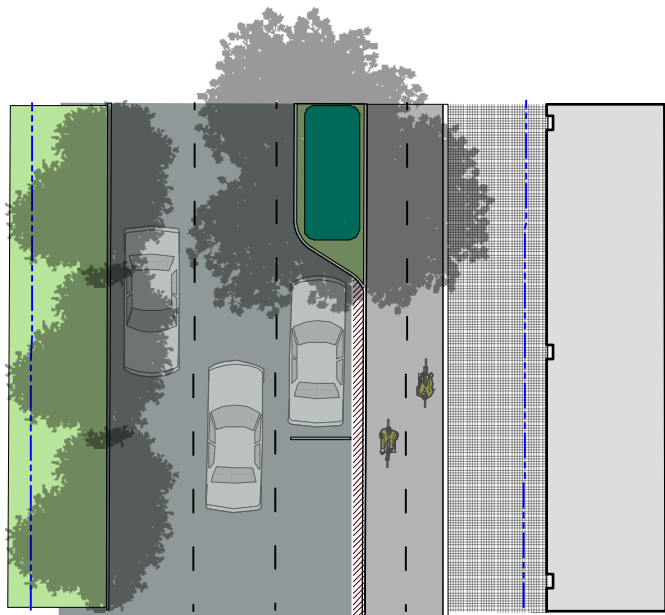
Tree planting can reduce the visual scale of the street while offering summer shade for the street and sidewalk.

Waterfront Ecology-

Integrated rain gardens to treat stormwater and provide habitat for birds and pollinators.

A Walkable Neighbourhood-

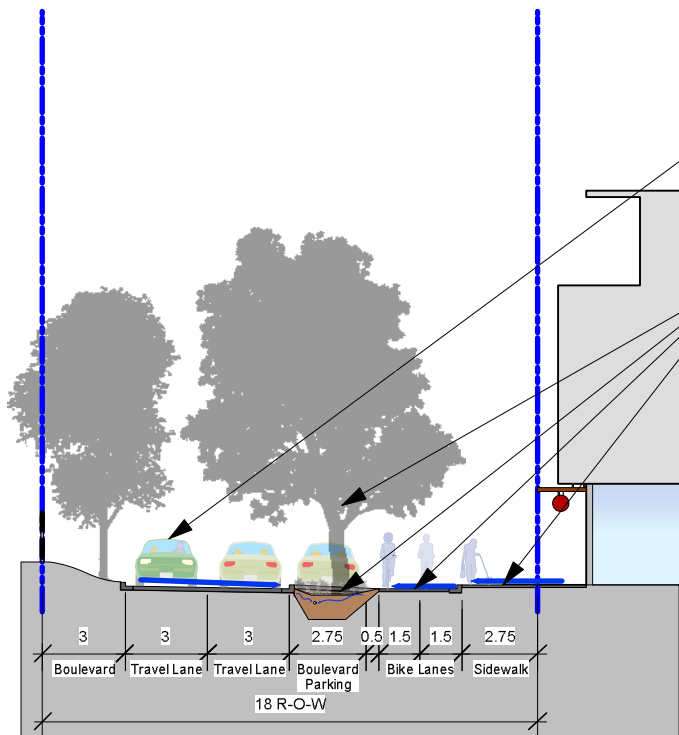
Use separated boulevards to remove sidewalks from vehicular traffic and increase the attractiveness of the pedestrian environment.



Lyall Street East- West Bay Village

East Lyall Street at the harbour interface is meant to act as part of the *West Bay Village*. The intent here is to contribute to the sense of a waterfront village while providing an attractive pedestrian environment with places for festivals and gatherings. Located adjacent to the DND lands Lyall street, in general, should focus on pedestrian improvements on the North side which is more heavily used by the public.

Strategies to achieve this include: Tree planting to reduce the visual scale of the street, vehicular travel lane reductions to provide space for bike lanes and encourage alternative modes of transportation, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour, and seperated boulevards to increase the attractiveness of the pedestrian environment.



Walkable Neighbourhood

Skinny Streets help to maximize pedestrian oriented space and allow for increased sidewalk widths as well as seperated bike lanes.

Waterfront Ecology-

Significant tree planting, green infrastructure, bike lanes, access corridors to waterfront recreation, clear wayfinding, and attractive pedestrian spaces should be layered and integrated to create a vibrant community, ecologically healthy shoreline, and healthy upland ecology.



18 January 2017

Mayor and Council
c/o Trevor Parkes, Senior Planner
Development Services
Township of Esquimalt



101 1851 Oak Bay Avenue
Victoria BC V8R - 1C3
phone 250 . 592 . 9198
fax 250 . 592 . 9178

RE: Westbay Marinaview
Redevelopment of 460 and 464 Head Street, Westbay, Esquimalt, BC

Rezoning Application: Record of Neighbourhood Consultation

Mayor and Council, c/o Trevor Parkes, Senior Planner

Please find enclosed a redevelopment proposal for phase 2, of the Westbay Triangle lands, known here as Westbay Marinaview, a proposal for a comprehensive redevelopment of the following two single family home lots being amalgamated into one land holding:

PID 009-174-851 Lot 10, Block H, Section 11, Esquimalt District, Plan 292 [460 Head Street];
PID 009-174-869, Lot 9, Block H, Section 11, Esquimalt District, Plan 292 [464 Head Street];

Hillel Architecture was retained in August of 2014 to chart a new course for the Development of the Westbay Triangle. Hillel Architecture engaged in a comprehensive community consultation process, involving neighbouring residents, Esquimalt Planning and Development staff, Esquimalt Engineering staff, the Esquimalt Chamber of Commerce, and numerous one on one interviews which defined an outcome for the Westbay Triangle as a whole. From this body of work, and within that consultation process, a rezoning and development proposal was illustrated for one portion of the land holdings known as the Westbay Triangle, and partially developed for the remaining half to be submitted at a later date, now known as Marinaview.

The first half of this land area development received broad based support from residents and Esquimalt Staff. At that same time Esquimalt engaged in a separate and independent neighbourhood consultation to create the Westbay Community Development Guidelines. As a new document, it was expected that the Westbay Triangle Development Proposal would conform to that independently developed guideline. It has been stated that it clearly did so.

Through its own consultation with Neighbours, Esquimalt Staff, and later with the members of APC, DRC, the proposal evolved subtly, each time improving, and each time not waivering from its original form as developed with, and supported by those earlier participants, the neighbours. The Westbay Triangle went on to receive its Public Hearing in the summer of 2016, without one voice of objection.

In anticipation of that Rezoning successfully being concluded, the second half of that same land holding, known herein as Westbay Marinaview, was developed further to meet the requirements of its own rezoning application. This submission is in keeping with - and conforming with - those same principals established in the Westbay Community Development Guidelines, and through Hillel Architecture's own consultation process.

Westbay Marinaview is different

The Marinaview design objectives differ from the Westbay Triangle. The Westbay Triangle establishes a dynamic village shopping district, respects the location of, and architectural style of, the neighbouring heritage homes and businesses. The architectural style, its materiality and detailing, respects not only the local heritage buildings but also the practical hard working nature of buildings serving our marine industries along the coastal edge. The Triangle also at this street level has a series of design instruments which define the perceived boundaries of this village.

The Triangle's deep front yard setbacks are designed to provide a leisurely outdoor shopping area where one can be surrounded by, and walk through a display space enlivened with the wares and services of those retailers. This outdoor village concludes at one end facing Gore by its purposeful interruption by a glazed staircase enclosure (to interrupt sound) and parking entry (interrupting pedestrians) and protecting the quiet residential nature of Gore Street.

In the direction of Marinaview, the wide front yard setback concludes with planters which feed pedestrians into a narrower pedestrian walkway. Marinaview steps forward subtly indicating the end of the dynamic village, provides a generous walking path in front of its own commercial tenants, each noticeably smaller than those of the Triangle shopping district. This provides a narrower pedestrian path across a building / vehicle entry increasing safety where it is required most, and then again is enlarged to a generous outdoor social space at the conclusion of the block – as the view opens towards the water.

Marinaview has a different focus.

Marinaview has a different focus. Marinaview has a different perspective. Quite literally Marinaview, has a differing view. Where the Triangle focuses its attention internal to the village scene, and benefitting all local businesses, Marinaview's sightlines are outward, far in nature, and wide open. Out from under the canopy of mature trees and substantive canopies of the Triangle, one step removed from the heritage setting of the village, towards the more modern forms of the Westbay Marina, the Westbay Marina contemporary float homes, and the modern lines of power and sail boats. Here in the open, in a location where orientation towards the sun is all day long, is both an area of substantive public seating, and a commercial tenant's outdoor seating surrounding this location reserved for a full service restaurateur.

This context is clean and bright, open, its sightlines far reaching and its social spaces bathed in sun all day long, and conducive to longer leisurely stays. Marinaview is a design extension of the Westbay Triangle, conceived together, and has several design cues joining the two buildings together. Yet Marinaview is the younger sibling. Its materials brighter and more contemporary. Its lines and form decidedly more modern. Where its sighting offers panoramic views the building responds and offers up panoramic vistas for its patrons or residents alike.

Where the triangle offers a coffee shop, perhaps a bakery, and retail spaces for shopping and a much needed neighbourhood service, Marinaview can offer something else. Marinaview has social spaces not looking into a dynamic village, but looking out over a dynamic waterfront. At this "Triangle" corner, that of Head and Lyall Streets, a restaurateur could offer a fuller menu, a longer more leisurely meal. This street corner of Head and Lyall differs from the dynamics of the village at the corner of head and Gore, and the quiet green spaces of the corner at Gore and Lyall Streets.

Westbay Community Development Guidelines

The Westbay Community Development Guidelines were independently prepared, by independent consultants, in conference with, and as directed, by those same neighbourhood members. The similarity of form and character, volume and public open spaces is exceptionally similar to those proposed herein. Close enough in fact that in this proposal, diagrams could have been used, side by side with our own, to aid drawing that opinion, and reassuring reviewers of this proposal, that independently, others have "drawn" the same conclusions.

Enclosures

- 3 sets 24x36 full size submission drawings
- 1 set 11x17 record set of submission drawings
- Certificates of Title, of less than 30 days.
- Application form
- Green building check list
- UBS digital record of same + video flyover of building massing in neighbourhood setting, with neighbouring buildings

Project Data Summary

Proposal Data	Current Zoning
Permitted uses of C-7 respected	Proposal conforms to permitted and prohibited uses.
Site Area 1207.7m ²	
Lot Coverage 38%	No ground floor maximum specified, second floor and higher deemed not to exceed 80%
Floor Space Ratio 1 : 1.225	No FSR stated
Building Footprint 456.2 m ²	No ground floor maximum specified, second floor and higher deemed not to exceed 80% or ±966.16 m ²
12 ocean view residences	
3 ground floor commercial tenancies	
Building ht. 5 stories / 17.7m	13 m by current zoning (±4 stories), 5 stories permitted by WBCDG
Building setback front yard <ul style="list-style-type: none"> • Head Street: 2.4m bldg. face / 1m balcony • Head at Lyall St: 2.8m bldg face / 1.2m Balcony • Lyall Street: 2.8m bldg. face / 1.8 balcony • (note: balcony overhangs do not exceed permitted roof overhangs) 	0m by current zone, 2m implied by WBCDG 0m, no mention in WBCDG 0m, no mention in WBCDG
Building setback exterior side yard	0m, no mention in WBCDG
Building setback rear yard to residential uses <ul style="list-style-type: none"> • over 21m shown, unit to unit along Head Street • respects 12m no build setback along Lyall Street 	3m by current zoning 3m by current zoning
Independent entry provided	In mixed use buildings, residential suites are to be serviced by an independent entry

Marinaview is a five storey mixed use, non-combustible building hosting 12 ocean view residences and 3 ground floor commercial tenancies, one of which is specifically designed to attract a full service restaurateur. The project meets the Schedule C Parking Bylaw without seeking a variance.

The Schedule C Parking Bylaw, however, does not differentiate between commercial parking for patrons or commercial parking for employees & leaseholders which are both critical to leasing, and in many ways to the public use of parking facilities. Of the 18 calculated commercial parking stalls, which by definition are available to commercial patrons at all times, 11 are demonstrated above grade and available for use at all times as per Sch C Bylaw, and 8 are demonstrated below grade for employee & leaseholder designated use. These commercial stalls are not available for residential use, or available for sale to others. Serving the needs of the employees & leaseholders below grade also ensures that surface parking is: *always available for, and used by patrons in the high turnover manor the Bylaw had intended* - without a series of employee vehicles that become more permanent daily parked vehicles occupying prime spaces.

Street side parking

The original Westbay Triangle contributed 14 street side parallel parking stalls safely recessed from traffic flow to three sides of the Triangle, outside of Sch C parking requirements. Marinaview similarly contributes 5 additional Street side parallel parking stalls outside of its Sch C requirements. These street side parking stalls, 19 in number, contribute a significant number of high turnover public stalls benefitting the new and existing businesses of the Westbay and Westbay marina area. In addition it is important to note that the Westbay Village area is well served by bus routes, community linking pedestrian walkways and ocean side trails – The Songhees Walkway - leading to the downtown core. It must also be remembered that here, in Westbay, a large portion of the neighbourhood resides off shore. Their parking has already been provided. To this village setting they will gladly walk.

Summary

Most importantly, the design of both Westbay Marinaview enclosed here, and the formerly submitted Westbay Triangle grew out of the same consultation process. The design solution evolved after many conversations and multiple contributions from a neighbourhood that deeply cares about its character today. Members of the neighbourhood clearly stated that they were ready for a new village center in Westbay, but that they clearly needed it to be designed carefully by those prepared to listen.

We hope that in the enclosed application package, and in each of the coming presentations, that everyone involved, from neighbours, Esquimalt staff, local businesses, Mayor and Council members, can see a piece of their personal contributions in the now fully realized composition of the Westbay Triangle.

Regards

Peter Hardcastle

Hillel Architecture Inc.

WESTBAY MARINA VIEW, JAN 2017

Township of
ESQUIMALT



GREEN BUILDING CHECKLIST

The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of **Green Technologies** in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project .

As you review this checklist, if you have any questions please contact **Development Services** at 250.414.7108 for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt

Adopted on January 10th, 2011



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level?	Yes	<input checked="" type="radio"/> No
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features?	Yes	<input checked="" type="radio"/> No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them.	<input checked="" type="radio"/> Yes	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building? _____ %	NO POTENTIAL EXISTS FROM EXISTING OLD STOCK SINGLE FAMILY HOMES	
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project.		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings?	<input checked="" type="radio"/> Yes	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? For which parts of the building (e.g. framing, roof, sheathing etc.)?	WOOD PRODUCTS MAY BE ECO-CERTIFIED INTERIOR WOOD PRODUCTS	
8	Can alternatives to Chlorofluorocarbons and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents be used in this project? If so, please describe these.	<input checked="" type="radio"/> Yes	No
9	List any products you are proposing that are produced using lower energy levels in manufacturing.	STONE VENEER, NATURAL PRODUCTS, RECYCLED ALUMINUM	
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]?	<input checked="" type="radio"/> Yes	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde?	Yes	<input checked="" type="radio"/> No

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

12	Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs? <i>COMMERCIAL TENANTS CAN BE ASKED TO DO SO IN THEIR CONST. DOCUMENTS</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No
13	For commercial buildings, do flushes for urinals exceed BC Building Code requirements? <i>COMMERCIAL TENANT ARE NOT TO EXCEED WATER FLOWS PERMITTED BY CODE</i>	<input type="radio"/> Yes	<input checked="" type="radio"/> No
14	Does your project use dual flush toilets and do these exceed the BC Building Code requirements? <i>DUAL FLUSH TOILETS USED THROUGHOUT</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No
15	Does your project exceed the BC Building Code requirements for maximum flow rates for private showers? <i>WATER FLOW REDUCING SHOWERHEADS THROUGHOUT</i>	<input type="radio"/> Yes	<input checked="" type="radio"/> No
16	Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets? <i>WATER FLOW REDUCING FAUCETS THROUGHOUT</i>	<input type="radio"/> Yes	<input checked="" type="radio"/> No

Storm Water

17	If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.]	<input type="radio"/> Yes	<input type="radio"/> No	<input checked="" type="radio"/> N/A
18	Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property? <i>NEW CIVIL ENGINEERING WILL PREVENT THESE ISSUES</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
19	Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. <i>PORTIONS OF RAIN FLOW CAN BE DELAY FED TO STORM SEWER, PORTIONS CAN BE USED FOR IRRIGATION</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
20	Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses? <i>BUILDING HAS BEEN DESIGNED WITH SIMILAR CONSIDERATIONS</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
21	Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. <i>NEW CIVIL ENGINEERING INSTALLATIONS DO INCLUDE THESE NECESSARY ITEMS</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
22	Will this project have an engineered green roof system or has the structure been designed for a future green roof installation? <i>LARGE PORTIONS ARE PLANTED.</i>	<input type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
23	What percentage of the site will be maintained as naturally permeable surfaces? <i>LARGE PORTIONS ARE PERMEABLE GREEN ROOFS & TERRACES</i>			_____ %

Waste water

24	For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these.	<input type="radio"/> Yes	<input checked="" type="radio"/> No	<input type="radio"/> N/A
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Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

25	Are any healthy trees being removed? If so, how many and what species? <i>SOME MATURE TREES BEING REMOVED, EX GERRY OAK BEING SAVED</i> Could your site design be altered to save these trees? Have you consulted with our Parks Department regarding their removal? <i>COMPLETE SITE EXCAVATION PREVENTS SAVING EX. TREES HOWEVER NEW PLANTINGS REPLACE EX TREE CANOPY</i>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
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26	Will this project add new trees to the site and increase our urban forest? If so, how many and what species? <u>EXISTING SITE IS SURFACE PARKING.</u>	Yes	No	N/A
27	Are trees [existing or new] being used to provide shade in summer or to buffer winds? <u>TREES ARE USED TO AID PUBLIC OUTDOOR SPACES</u>	Yes	No	N/A
28	Will any existing native vegetation on this site be protected? If so, please describe where and how. <u>EXISTING SITE IS BOTH SURFACE PARKING FACILITY & TEMPORARY LAWN AWAITING THIS PROPOSAL</u>	Yes	No	N/A
29	Will new landscaped areas incorporate any plant species native to southern Vancouver Island?	Yes	No	N/A
30	Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas?	Yes	No	N/A
31	Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)?	Yes	No	N/A
32	Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property?	Yes	No	N/A
33	Will topsoil will be protected and reused on the site? <u>TOP SOILS MAY PROVE USEFUL FOR REUSE OR EXCHANGE</u>	Yes	No	N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

34	Will the building design be certified by an independent energy auditor/analyst? If so, what will the rating be? <u>NOT KNOWN AT THIS STAGE</u>	Yes	No	N/A
35	Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting?	Yes	No	N/A
36	Does the design and siting of buildings maximize exposure to natural light? What percentage of interior spaces will be illuminated by sunlight? _____%	Yes	No	N/A
37	Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.). If so, please describe. <u>PRE-CONFIGURED FOR SOLAR HOT WATER PREHEAT.</u> If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? <u>ROOF TOP UNITS REMOVE CONCERN</u>	Yes	No	N/A
38	Has the building been designed to be solar ready? <u>ROOF AREAS SET ASIDE FOR SOLAR DEVICES</u>	Yes	No	N/A
39	Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity? <u>ROOF AREAS RESERVED FOR SOLAR DEVICES</u>	Yes	No	N/A
40	Do windows exceed the BC Building Code heat transfer coefficient standards? <u>WINDOW UNITS MEET OR EXCEED CODE REQUIREMENTS</u>	Yes	No	N/A
41	Are energy efficient appliances being installed in this project? If so, please describe. <u>YES, LED FIXTURES IN PUBLIC SPACES</u>			
42	Will high efficiency light fixtures be used in this project? If so, please describe.	Yes	No	N/A
43	Will building occupants have control over thermal, ventilation and light levels?	Yes	No	N/A
44	Will outdoor areas have automatic lighting [i.e. motion sensors or time set]? <u>YES, MANY OUTDOOR LOCATIONS WILL HAVE SENSORS</u>	Yes	No	N/A
45	Will underground parking areas have automatic lighting? <u>YES, PORTIONS OF UNDERGROUND PARKING WILL HAVE SENSORS</u>	Yes	No	N/A

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. <u>PRODUCTS DETERMINED AT A LATER STAGE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
48	Will the building have windows that occupants can open?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
50	Will fresh air intakes be located away from air pollution sources?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. <u>LUMBER MATERIALS CAN BE MADE AVAILABLE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. <u>A LIMITED LIST OF MATERIALS & PACKAGING WILL BE RECYCLED DURING CONSTRUCTION PHASE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
57	Is access provided for those with assisted mobility devices?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
58	Are accessible bike racks provided for visitors?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input type="checkbox"/> transit passes <input type="checkbox"/> car share memberships <input type="checkbox"/> shared bicycles for short term use <input checked="" type="checkbox"/> weather protected bus shelters <u>OFF SITE, PROVIDED BY OTHERS</u> <input checked="" type="checkbox"/> plug-ins for electric vehicles			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.



WESTBAY MARINAVIEW RESIDENCES

PROPERTY OWNER

WESTBAY INVESTMENTS LTD.

7081 Central Saanich Road, Victoria, BC V8M 1Y3
 Contact: Mark Lindholm
 Phone: 778.351.1549
 Email: mark.lindholm@lindholm.ca

PRIME CONSULTANT & ARCHITECT ON RECORD

HILLEL ARCHITECTURE INC.

Unit 101 - 1831 Oak Bay Avenue, Victoria, BC V8R 1C3
 Contact: Peter Hardcastle
 Phone: 250.592.9198
 Email: peter@hillelarch.ca

DRAWING LIST

ARCHITECTURAL

- A0.0 Cover Sheet
- A1.1 Existing Site & Project Data
- A1.2 Site Development
- A1.3 Site Finishes
- A1.4 Shadow Studies
- A2.1 Parkade Level Floor Plan
- A2.2 Ground Floor Level Floor Plan
- A2.3 2nd, 3rd, & 4th Floor Plans
- A2.4 5th Floor Plan & Roof Plan
- A3.1 Exterior Elevations - Head Street View
- A3.2 Exterior Elevations - Lyall Street View
- A3.3 Exterior Elevations - North & South Views
- A4.1 Building Sections



WEST BAY INVESTMENTS LTD.



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Hillel architecture

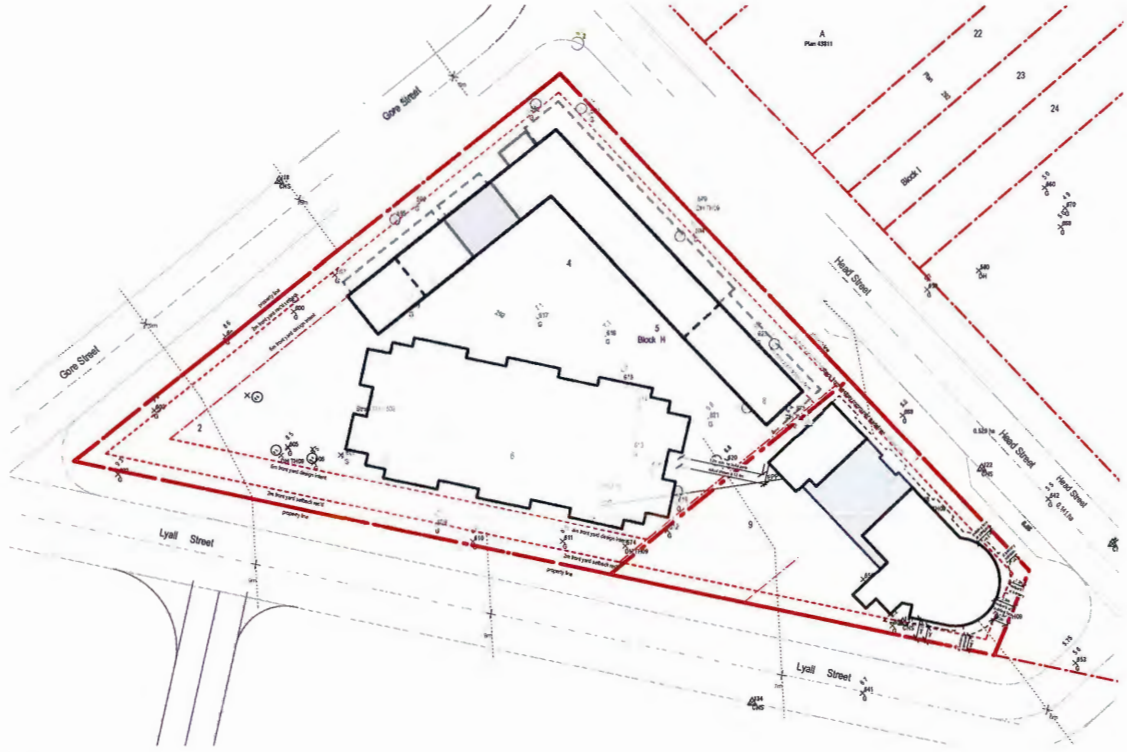


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 phone 250.592.9198
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Permission is granted for these drawings to be reprinted by West Bay Investments Ltd.



1 Site Context Plan
metric scale: NTS



2 Existing Site Survey
metric scale: 1 : 500

CIVIC ADDRESS

CIVIC ADDRESS
460 & 464 Head Street, Victoria, BC

PROJECT DESCRIPTION

A 5 storey mixed use proposal combining a main public floor with marina view restaurant with outdoor social spaces, combined with 4 floors of residential ocean view units above.

All commercial tenants add to Westbay Village scene.
All residences are ocean view suites.

PROJECT DATA

PROPOSED PROJECT

ZONING

460 Head Street - Current Zone is C-7
464 Head Street - Current Zone is RS-1

Combined properties are designed in conformance with Westbay Community Development Guidelines produced by Esquimalt, August 2015.

DENSITY

Site Area (m2) 1,207.7 m2 (13,000 ft2)
Lot Coverage % 38% (456.2 m2 building horizontal footprint)
Floor Space Ratio 1.225 : 1
Building Footprint 456.2 m2 (4,910 ft2)
Total Floor Area

Esquimalt definition for floor space ratio calculations
Ground Floor: 270 m2 (2,910 ft2)
2nd Floor: 366 m2 (3,940 ft2)
3rd Floor: 366 m2 (3,940 ft2)
4th Floor: 301 m2 (3,240 ft2)
5th Floor: 176 m2 (1,895 ft2)
Total Floor Area: 1,479 m2 (15,925 ft2)

Building 1 Ground Floor breakdown:
CRU 101: 47.8 m2 (515 ft2)
Group D, Group E
CRU 102: 41.8 m2 (450 ft2)
Group D, Group E
CRU 103: 126 m2 (1,360 ft2)
Group A

Maximum Height 17.7m ± above average grade 6.05m (23.71m geodetic)
Maximum Storeys 5 storeys

SITE SETBACKS

Head Street •2.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 38
Drawings demonstrate 2.4m to building face
Junction at Head & Lyall Streets •illustrated similar to 2.0m page 38 of development guidelines
Drawings demonstrate 2.8m to building face
Lyall Street •1.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 40
Drawings demonstrate 2.8m to building face
Interior Side Yard •1.2m demonstrated to internal sideyard
No build Zone Minimum 12m. no build zone to neighbouring property at West Bay 1
Drawings demonstrate 12.4m setback to building face

RESIDENTIAL UNITS

No. of Residential Units **12 total residential units plus 3 commercial units**

Ground Floor: 3 commercial units
2nd Floor: 4 residential units
3rd Floor: 4 residential units
4th Floor: 3 residential units
5th Floor: 1 residential unit
15 total

2nd & 3rd Floor Residential Units

1 One Bedroom Unit
3 Two Bedroom Units

4th Floor Residential Units

3 Two Bedroom Units

5th Floor Residential Unit

1 Two Bedroom plus den Unit

LEGAL ADDRESS

PID 009-174-869, Lot 9, Block H, Section 11, Esquimalt District, Plan 292 (464 Head Street)
PID 009-174-851, Lot 10, Block H, Section 11, Esquimalt District, Plan 292 (460 Head Street)

PARKING

Parking stalls on site

Parking Bylaw Standards referenced:
Cafe @ 14m2 or 5 seats per stall
Grocer @ 35m2
Retail @ 25m2
Office @ 30m2

15% of required commercial parking can be met by use of residential guest parking stalls during business hours
Residential 1.3 each
25% to be assigned as visitors stalls

CRU 101 @ 47.8 m2 / 30
CRU 102 @ 41.8 m2 / 25
CRU 103 @ 126 m2 / 14
1 stall / 5 seats x 68
Total Commercial: 18 parking stalls

Total Residential 12 x 1.3 each: 15.8 stalls required
Residential Guest Parking **16 stalls provided**
(25% assigned as visitors - 3 stalls)
Total Project provided: **34 stalls req'd / 35 provided**
(24 underground stalls / 11 surface stalls)

Commercial Loading Stall

Not required / less than 700 m2 commercial space

parking stalls off site

5 parallel street parking stalls provided on municipal property and not counted

BIKE PARKING

Bicycle parking Class I (enclosed in bldg)

18 provided (1.5 per residential unit) As per: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1.j, page 16

Commercial bicycle parking Class II (outdoor racks)

2 provided (1 per 10 full-time employees): Esquimalt OCP, Section 2. Managed growth, 2.3.1.1, page 18

Guest bicycle parking: Residential Class II (outdoor racks)

6 guest bikes as per: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1.j, page 16

Guest bicycle parking: Commercial Class II (outdoor racks)

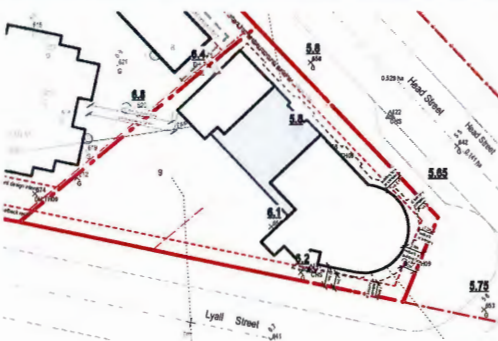
6 guest bikes as per: Esquimalt OCP, Section 2. Managed growth, 2.3.1.1, page 18

BUILDING STORAGE

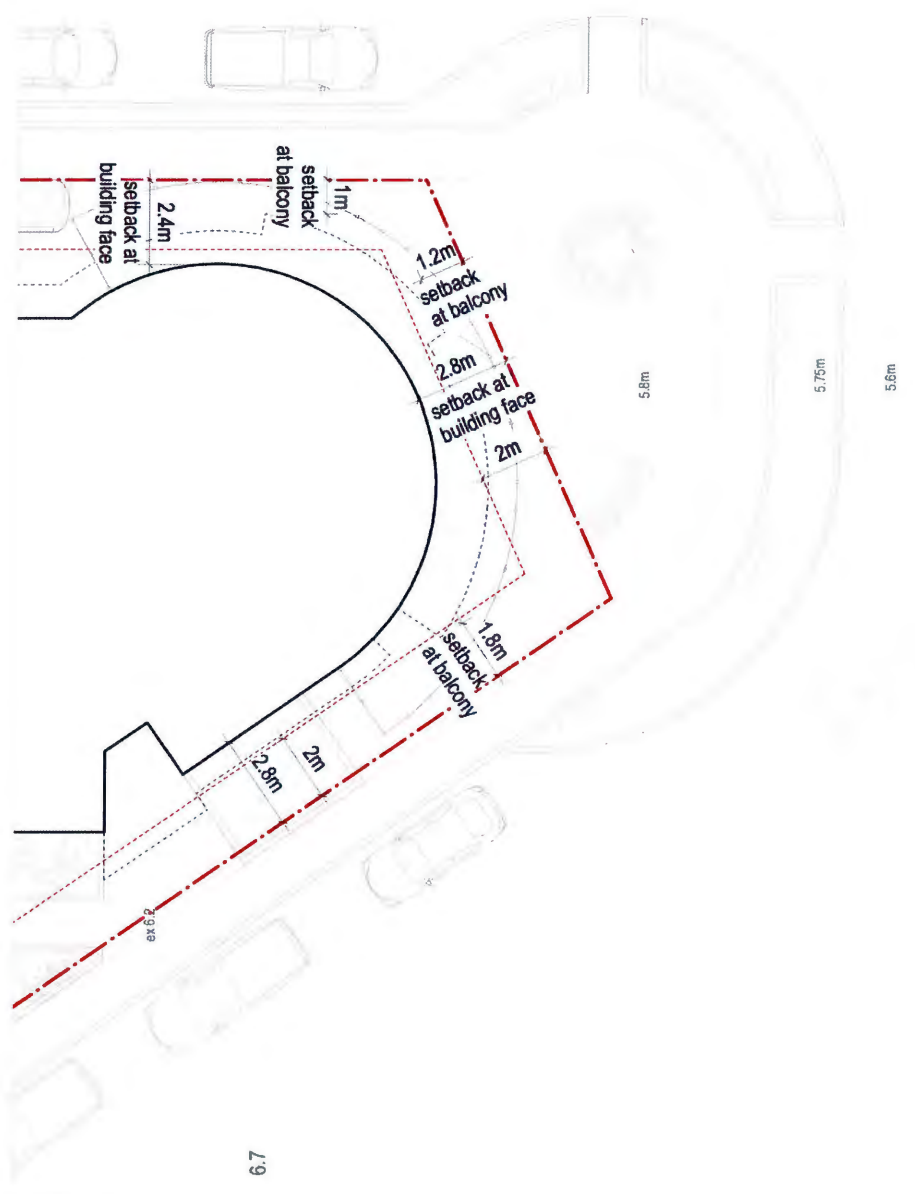
Residential Storage

12 residential storage lockers provided (min 1 per residential unit)

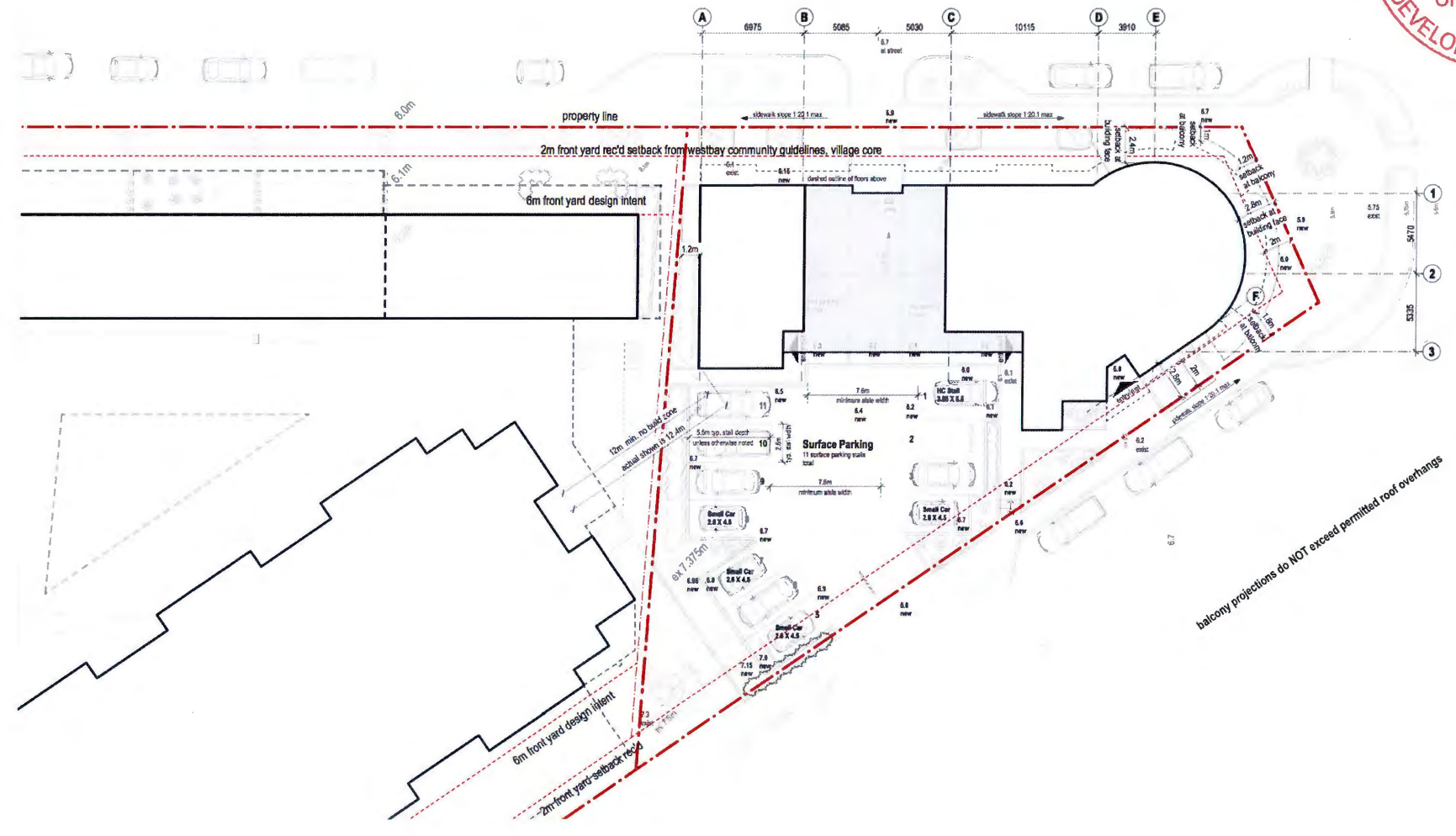
AVERAGE GRADE POINTS



no.	date	revision
1	19 January 2017	Issue for Planning Application
2	15 July 2018	Preliminary
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2 Detailed Site Plan
A1.2 metric scale: 1:100



1 Site Plan
A1.2 metric scale: 1:200

rev	date	description
1	14 January 2017	Issue for Planning Application
2	15 July 2016	Preliminary
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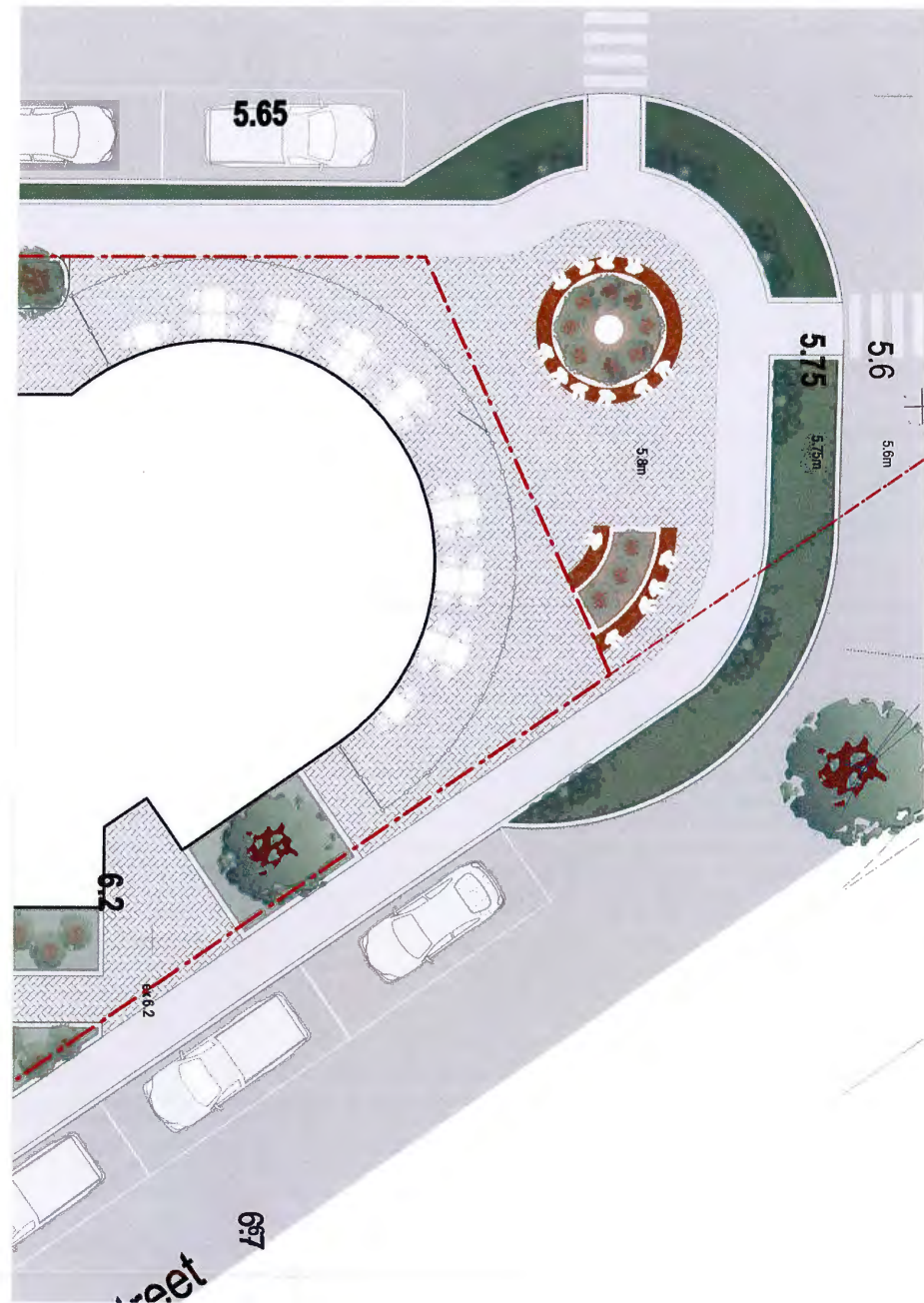
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WESTBAY MARINAVIEW

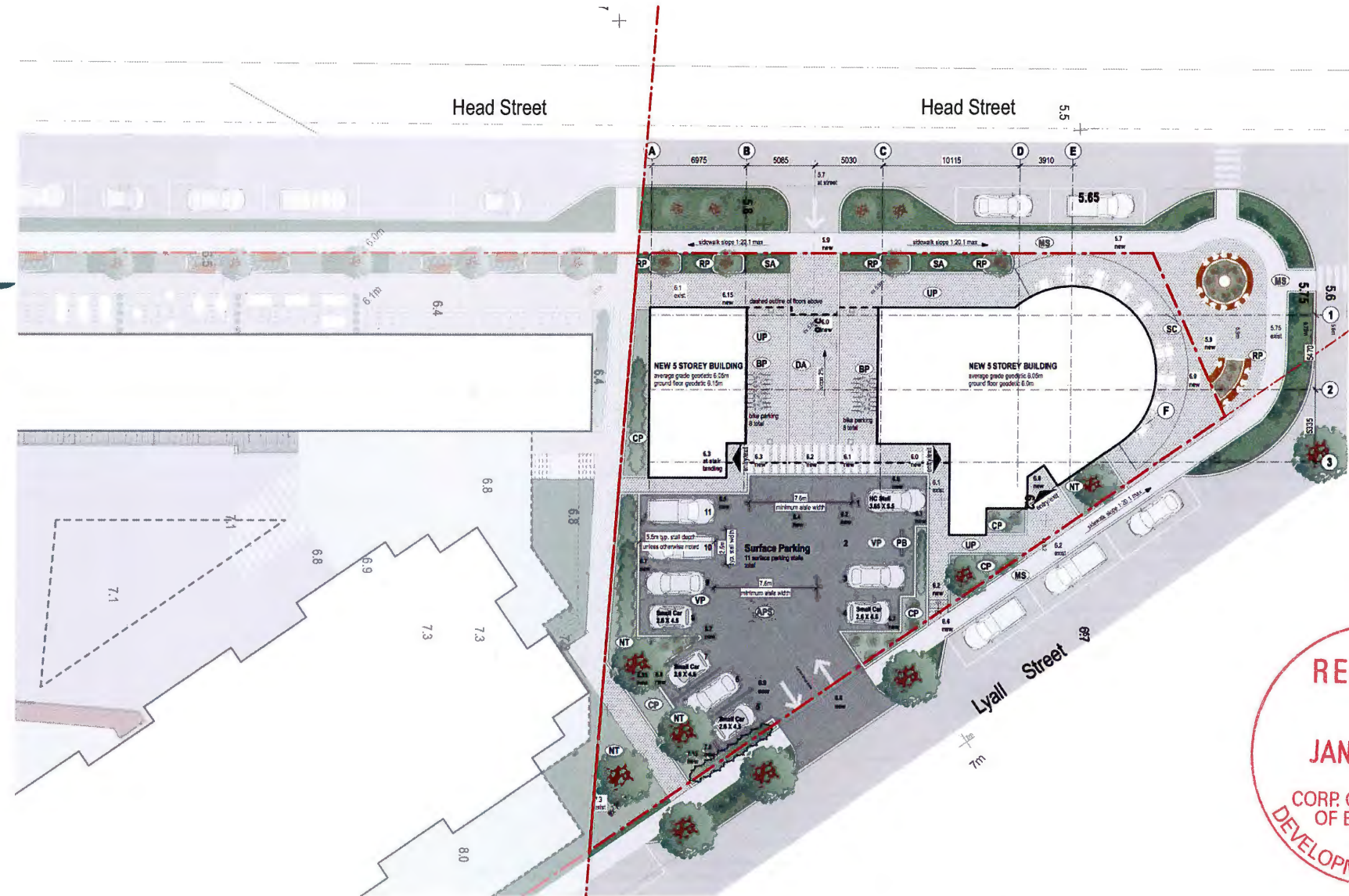
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

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project: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC
drawing title: Site Development
drawing no: 172-001-00-00
drawing date: 02/09/17
drawing scale: A1.2



2 Detailed Site Plan
A1.3 metric scale: 1 : 100



1 Site Finishes
A1.3 metric scale: 1 : 200



SITE FINISHES LEGEND

List of Finishes typical of all elevations

- CP common planting area - see landscape plan for details
- SA seeded or sodded grass area - see landscape plan for details
- NT approximate location of new tree planting area - see landscape plan for details
- RP raised concrete planter - see landscape plan for details
- UP unit paver area - pathways and patios
- AP new asphalt surface
- MS municipal sidewalk - poured concrete finish
- DA drive aisle - coloured poured concrete finish to match unit pavers
- SC glazed screening at outdoor patio for commercial unit 103
- NS natural stone veneer on concrete retaining wall with a concrete cap
- CW concrete retaining wall with a concrete cap
- BP eight bike class 2 visitors bike parking / lockable
- VP asphalt surface vehicle parking stalls
- PB precast concrete parking blocks, typical all parking stalls
- PL parking / site lighting

no.	date	description
1	16 January 2017	issues for Rezoning Application
2	15 July 2016	Preliminary

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WESTBAY MARINAVIEW

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

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project
WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title
Site Finishes

drawing no.
A1.3



1 9AM Equinox Shadow Studies
A1.4 4th and 5th floor shadows shown. metric scale: 1 : 800



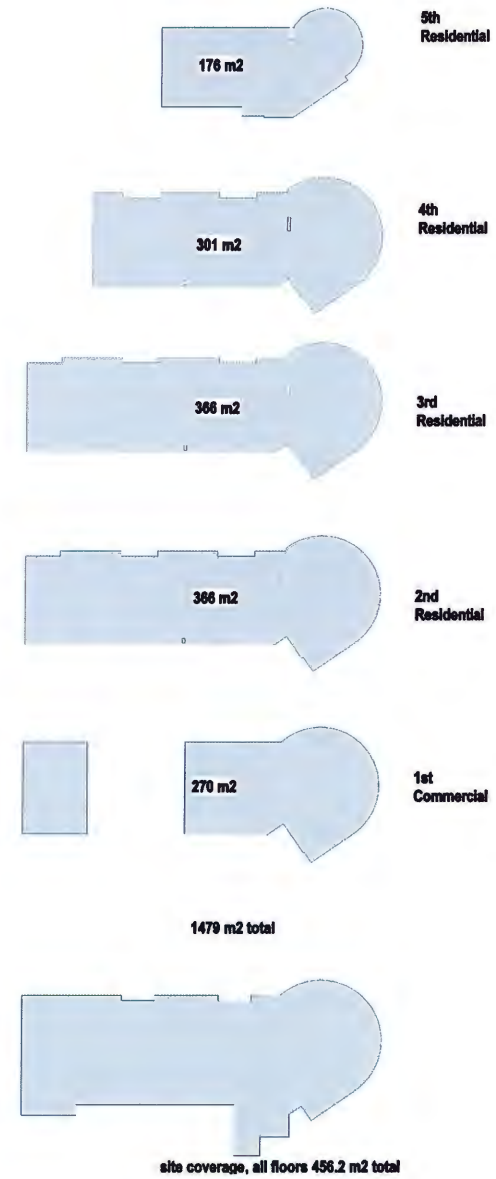
2 3PM Equinox Shadow Studies
A1.4 4th and 5th floor shadows shown. metric scale: 1 : 800



3 8AM to 4PM Shadow Studies as per WBCD Guidelines
A1.4 4th and 5th floor shadows shown. metric scale: 1 : 800



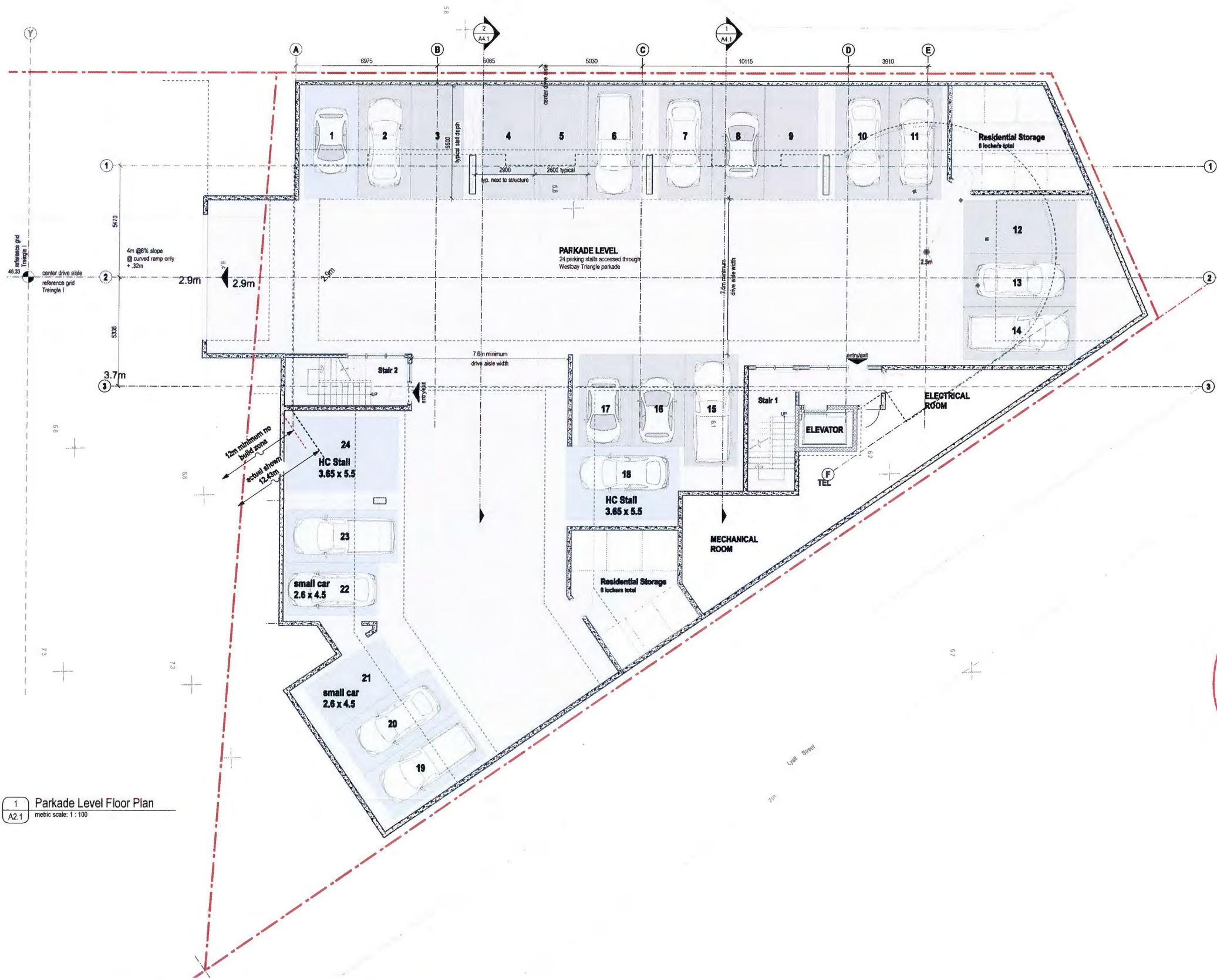
4 6AM to 6PM Shadow Studies Summer Solstice
A1.4 4th and 5th floor shadows shown. metric scale: 1 : 800



5 Graphic Area summary
A1.4 metric scale: 1 : 800



rev	date	description
1	10 January 2017	Issues for Planning Application
2	15 July 2018	Preliminary



1 Parkade Level Floor Plan
A2.1 metric scale: 1:100

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OF ESQUIMALT
DEVELOPMENT SERVICES

NO.	DATE	DESCRIPTION
1	16 January 2017	Issue for Planning Application
2	15 July 2015	Preliminary

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WESTBAY MARINAVIEW

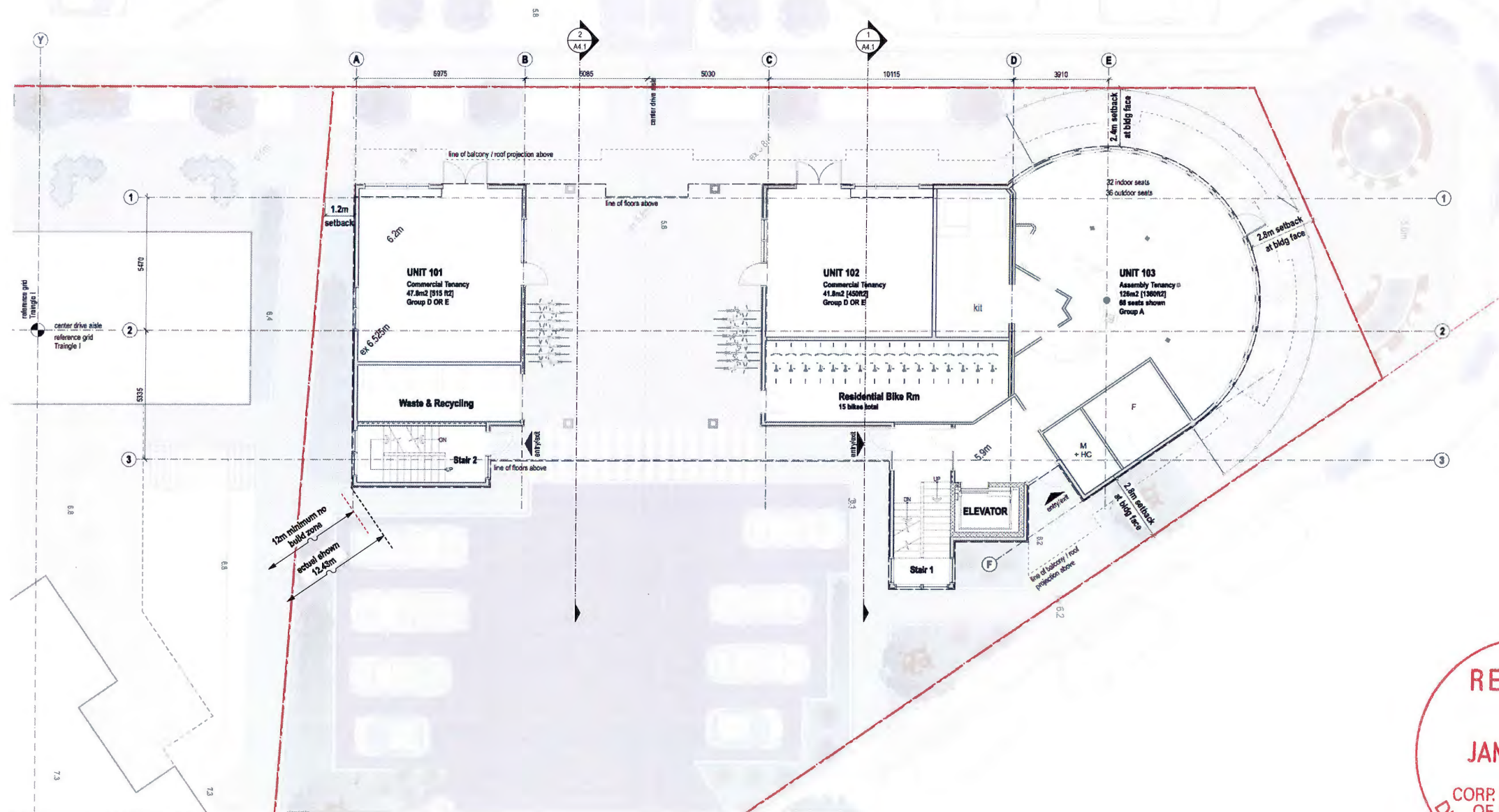
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

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fax 250.592.3998

project: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title: Parkade Level Floor Plan

revision no. drawing no. **A2.1**



1 Ground Floor Plan
 A2.2 metric scale: 1:100

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1	16 January 2017	Issue for Planning Application
2	15 July 2016	Preliminary

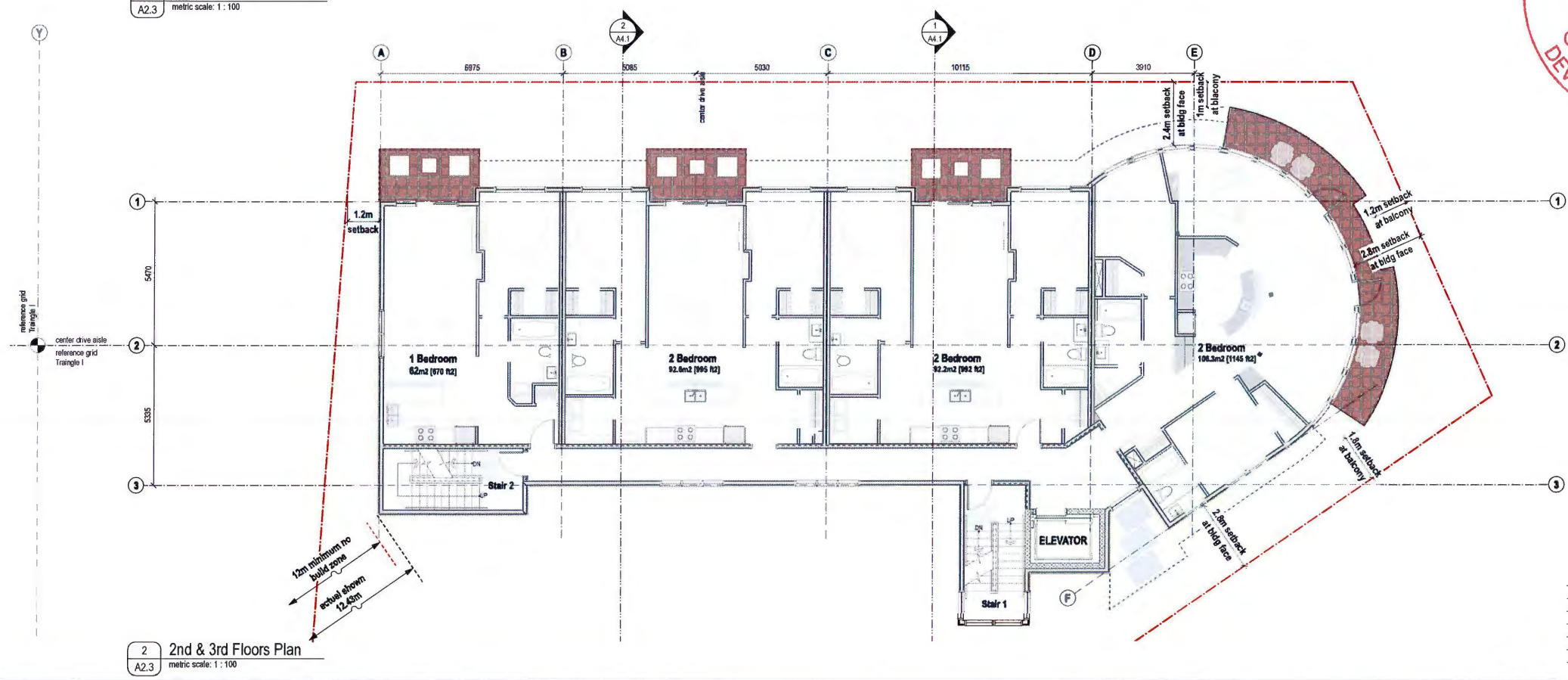
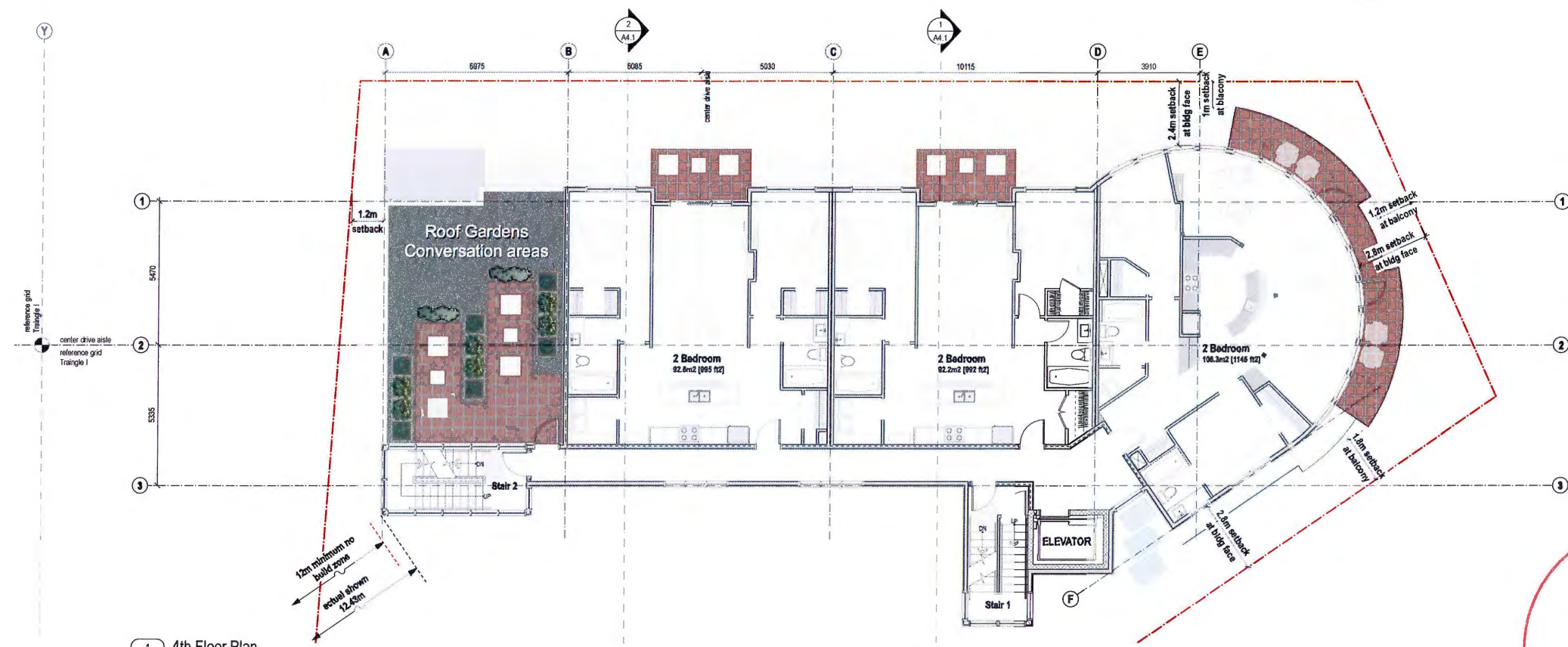
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WESTBAY MARINAVIEW

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project: WESTBAY MARINAVIEW
 460 & 464 HEAD STREET ESQUIMALT BC
 drawing title: Ground Floor Level Floor Plan
 drawing no: A2.2



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no.	date	description	author
1	16 January 2017	Issued - Items for Planning Application	
2	15 July 2016	Revision	Preliminary
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WESTBAY MARINAVIEW

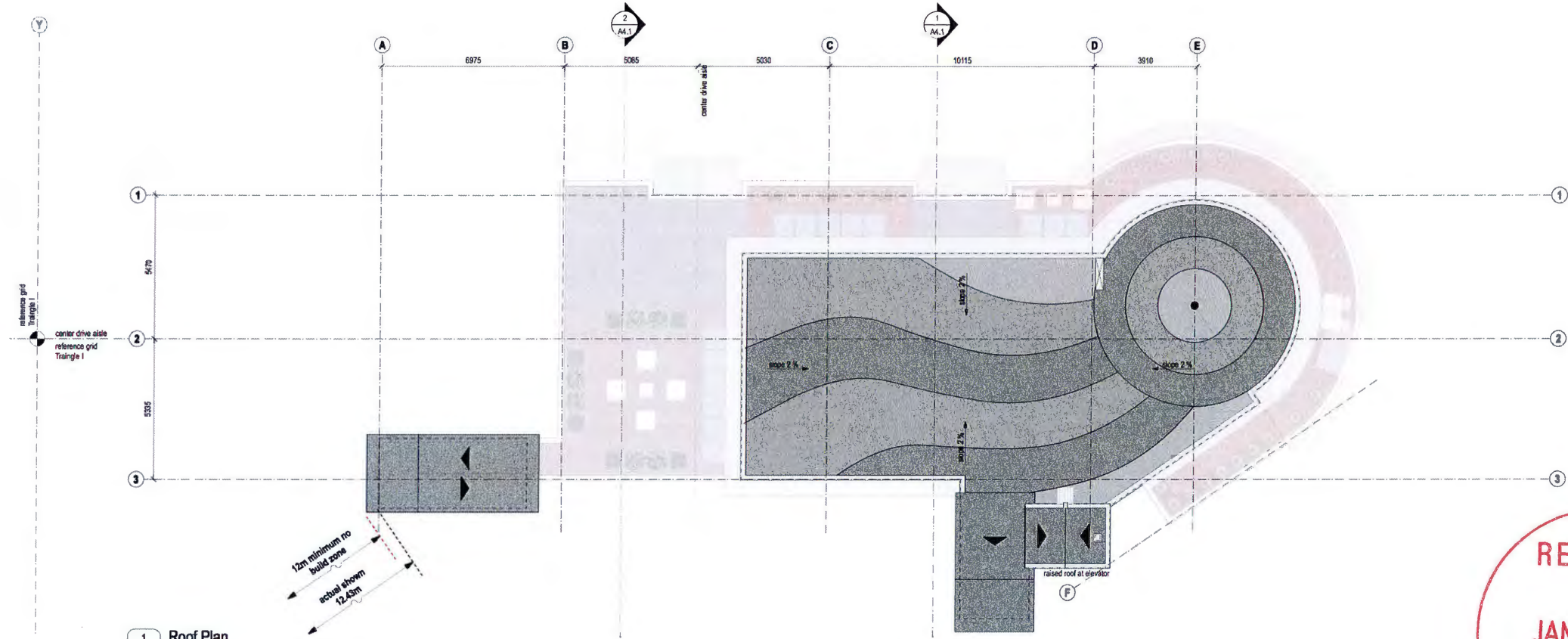
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

Hillel architecture
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fax 250-592-9778

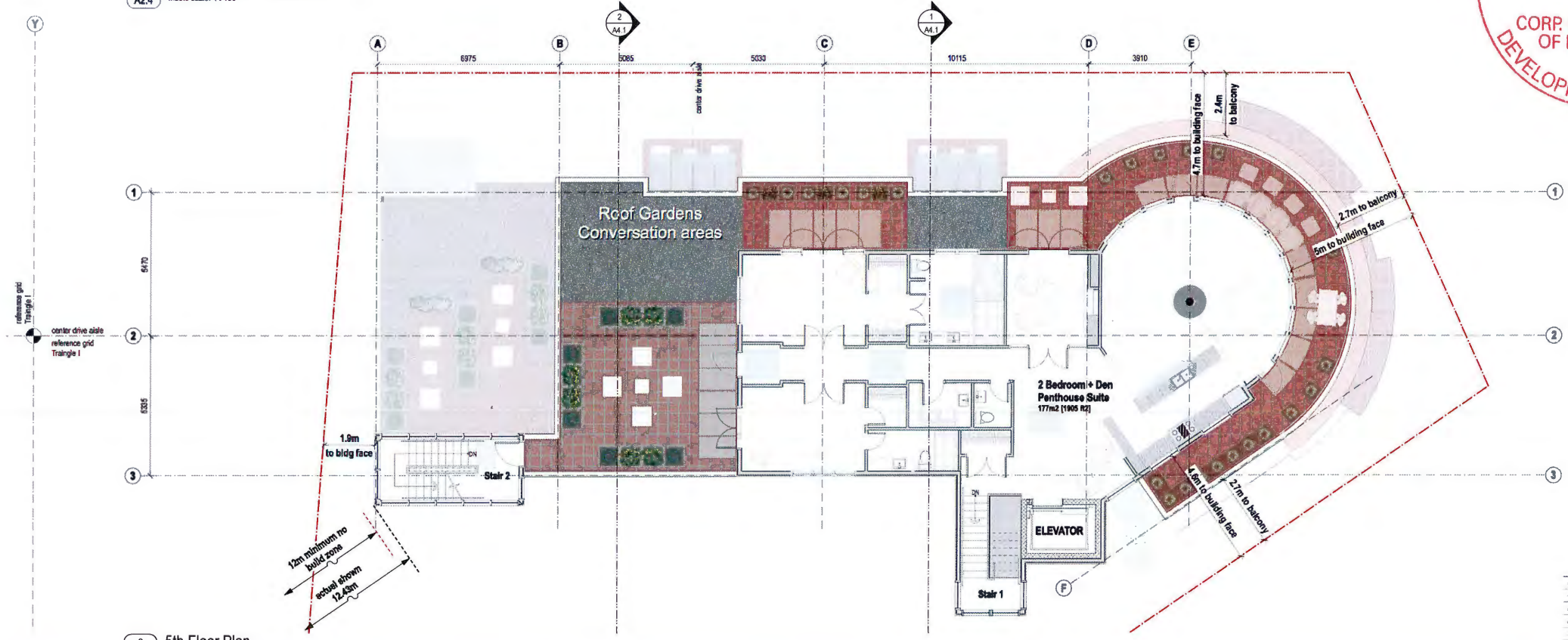
project: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT, BC

drawings: 2nd, 3rd, & 4th Floor Plans

drawing no: A2.3



1 Roof Plan
A2.4
metric scale: 1:100



2 5th Floor Plan
A2.4
metric scale: 1:100

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OF ESQUIMALT
DEVELOPMENT SERVICES

no.	date	description
1	19 January 2017	Issues for Planning Application
2	15 July 2018	Preliminary

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WESTBAY MARINAVIEW

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

Hillel architecture

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phone 250-592-2998
fax 250-592-5978

project: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title: 5th Floor Plan & Roof Plan

revision no. 02/18/17 no. A2.4



1 East Exterior Elevation - Head Street View
metric scale: 1 : 100



2 East Exterior Elevation - Head Street View
metric scale: 1 : 200

Exterior Finishes Legend

List of finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel flashing
- 02 Cementitious wood fascia board, smooth face - arch spec colour
Item 02a: bright white
Item 02b: custom colour shown, sea foam green.
- 03 Non combustible wood lined cementitious fascia board
Colour to match to the Westbay Triangle.
- 04 Non combustible wood lined cementitious board soffit
Colour to match to the Westbay Triangle.
- 05 Structural framed roof canopy, and fabricated metal support brackets. These items to reflect an increased level of finish over those executed in the Westbay Triangle, while being a clear extension of, and drawing reference from those items.

Exterior Finishes & Components

- 10 Panelized, smooth stucco c/w prefinished metallic gray metal reveals
Item 10a: oxford white
Item 10b: custom colour shown, sea foam green.
- 11 Aluminum plank 1x4 T & Groove Cedar image siding coloured matched to finishes executed in the Westbay Triangle.
- 12 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray. Stone veneer product colour matched, joint treatment matched, to the Westbay Triangle.
- 13 Dark gray elastomeric paint finish to exposed concrete components
- 14 Clear finished solid wood, glazed storefront entry doors in anodized alum storefront framing system.
- 15 Clear anodized aluminum glazed storefront window / door system
- 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
- 17 Graphite gray painted hollow core steel door and frame.
- 18 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 19 Upstand balcony wall; panelized, smooth gray stucco (Arch spec colour) finish c/w metallic gray steel reveals and metallic gray steel flashing cap

Exterior Features

- 20 Cementitious wood accent trim band painted
Item 20a: oxford white
Item 20b: custom colour shown, sea foam green
- 21 Aluminum plank 1x4 T & Groove Cedar image siding coloured matched to finishes executed in the Westbay Triangle.
- 22 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray. Stone veneer product colour matched, joint treatment matched, to the Westbay Triangle.
- 23 Exterior light fixture
- 24 Exterior building signage
- 25 Aluminum framed skylight

no.	date	description	revision
1	10 January 2017	Issue for Planning Application	
2	15 July 2018	Revised	Preliminary
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WESTBAY MARINAVIEW

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

Hillel architecture

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phone 250-592-9984
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PROJECT: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC

DRAWING NO: Exterior Elevation - Head Street View

DATE: 18.10

SCALE: A3.1



1 West Exterior Elevation - Lyall Street View
metric scale: 1 : 100



2 West Exterior Elevation - Lyall Street View
metric scale: 1 : 200

Exterior Finishes Legend

List of Finishes typical of all elevations

- | | | |
|--|---|--|
| <p>Roofing</p> <ul style="list-style-type: none"> 01 Pre-finished metallic gray steel flashing 02 Cementitious wood fascia board, smooth face - arch spec colour
Item 02a: bright white
Item 02b: custom colour shown, sea foam green. 03 Non combustible wood toned cementitious fascia board
Colour to match to the Westbay Triangle. 04 Non combustible wood toned cementitious board soffit.
Colour to match to the Westbay Triangle. 05 Structural framed roof canopy, and fabricated metal support brackets. These items to reflect an increased level of finish over those executed in the Westbay Triangle, while being a clear extension of, and drawing reference from those items | <p>Exterior Finishes & Components</p> <ul style="list-style-type: none"> 06 Panelized, smooth stucco c/w prefinished metallic gray metal reveals
Item 10a: custom colour shown, sea foam green. 11 Aluminum plank 1x4 T & Groove Cedar image siding coloured matched to finishes executed in the Westbay Triangle. 12 Natural stone veneer: Eldorado Natural Stone, Chestnut Pebble Gray. Stone veneer product colour matched, joint treatment matched, to the Westbay Triangle. 13 Dark gray elastomeric paint finish to exposed concrete components 14 Clear finished solid wood, glazed storefront entry doors in anodized alum storefront framing system. 15 Clear anodized aluminum glazed storefront window / door system 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame 17 Graphite gray painted hollow core steel door and frame. 18 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels 19 Upstand balcony wall: panelized, smooth gray stucco (Arch spec colour) finish c/w metallic gray steel reveals and metallic gray steel flashing cap | <p>Exterior Features</p> <ul style="list-style-type: none"> 20 Cementitious wood accent trim band painted
Item 20a: custom colour shown, sea foam green
Item 20b: custom colour shown, sea foam green 21 Aluminum plank 1x4 T & Groove Cedar image siding coloured matched to finishes executed in the Westbay Triangle. 22 Natural stone veneer: Eldorado Natural Stone, Chestnut Pebble Gray. Stone veneer product colour matched, joint treatment matched, to the Westbay Triangle. 23 Exterior light fixture 24 Exterior building signage 25 Aluminum framed skylight |
|--|---|--|

Rev	Date	Description
1	18 January 2017	Issued for Permitting Application
2	15 July 2016	Preliminary



WESTBAY MARINAVIEW

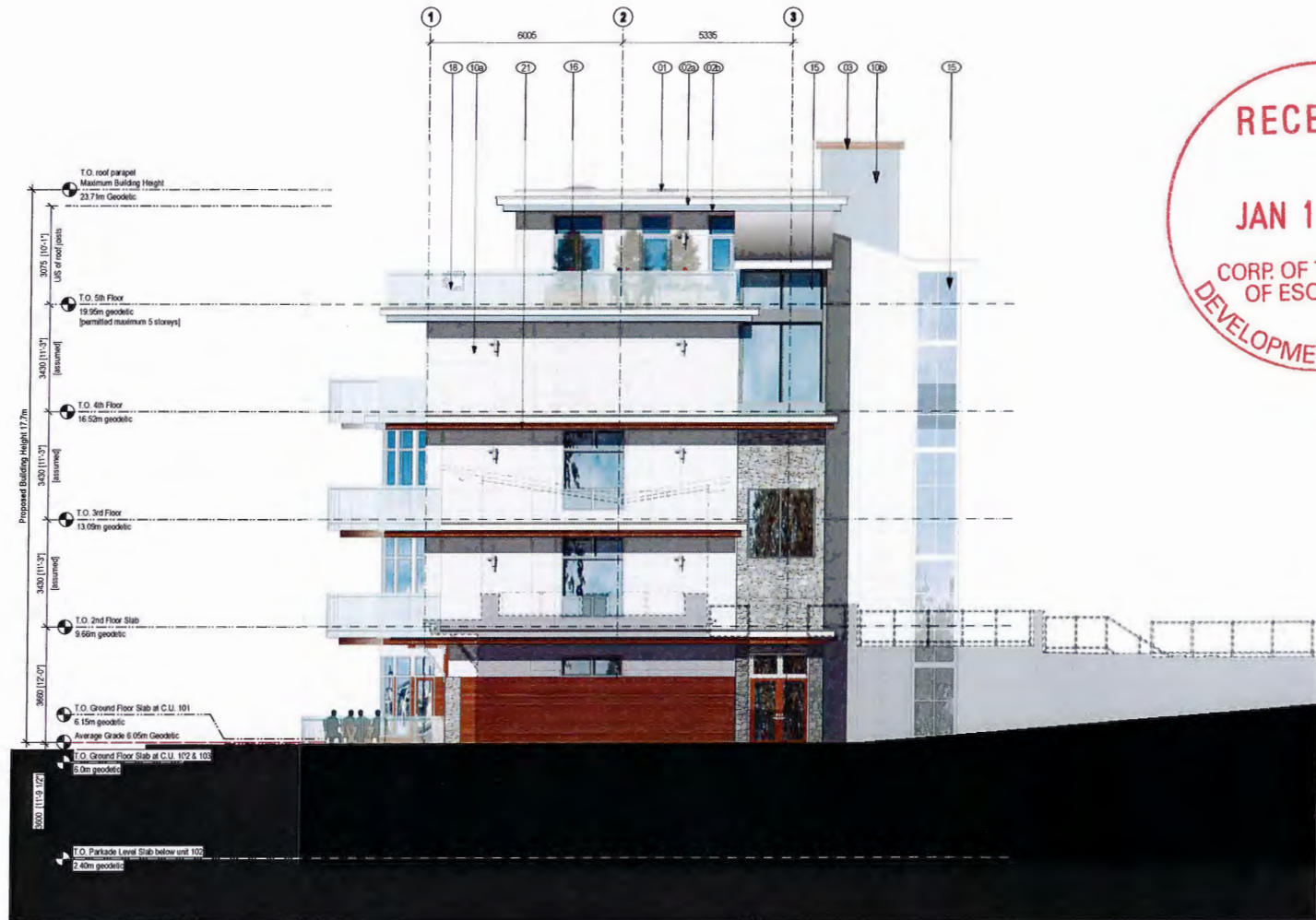
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



project: WESTBAY MARINAVIEW
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title: Exterior Elevation - Lyall Street View

drawing no: A3.2



1 South Exterior Elevation - Facing Marina View Point
metric scale: 1:100

2 North Exterior Elevation - Facing Triangle 1
metric scale: 1:100



3 South Exterior Elevation - Facing Marina View Point
metric scale: 1:200



4 North Exterior Elevation - Facing Triangle 1
metric scale: 1:200

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel flashing
- 02 Cementitious wood fascia board, smooth face - arch spec colour
Item 02a: bright white
Item 02b: custom colour shown, sea foam green.
- 03 Non combustible wood lined cementitious fascia board.
Colour to match to the Westbay Triangle.
- 04 Non combustible wood lined cementitious board soffit.
Colour to match to the Westbay Triangle.
- 05 Structural framed roof canopy, and fabricated metal support brackets. These items to reflect an increased level of finish over those executed in the Westbay Triangle, while being a clear extension of, and drawing reference from those items.

Exterior Finishes & Components

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Item 10a: oxford white
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- 13 Dark gray elastomeric paint finish to exposed concrete components
- 14 Clear finished solid wood, glazed storefront entry doors in anodized aluminum storefront framing system.
- 15 Clear anodized aluminum glazed storefront window / door system
- 16 Clear anodized window / door assembly c/w matching clear anodized aluminum frame
- 17 Graphite gray painted hollow core steel door and frame.
- 18 Side mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 19 Upstand balcony wall: panelized, smooth gray stucco (Arch spec colour) finish c/w metallic gray steel reveals and metallic gray steel flashing cap

Exterior Features

- 20 Cementitious wood accent trim band painted
Item 20a: oxford white
Item 20b: custom colour shown, sea foam green
- 21 Aluminum plank 1x4 T & Groove Cedar image siding coloured matched to finishes executed in the Westbay Triangle.
- 22 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray. Stone veneer product colour matched, joint treatment matched, to the Westbay Triangle.
- 23 Exterior light fixture
- 24 Exterior building signage
- 25 Aluminum framed skylight



WESTBAY MARINAVIEW
460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT

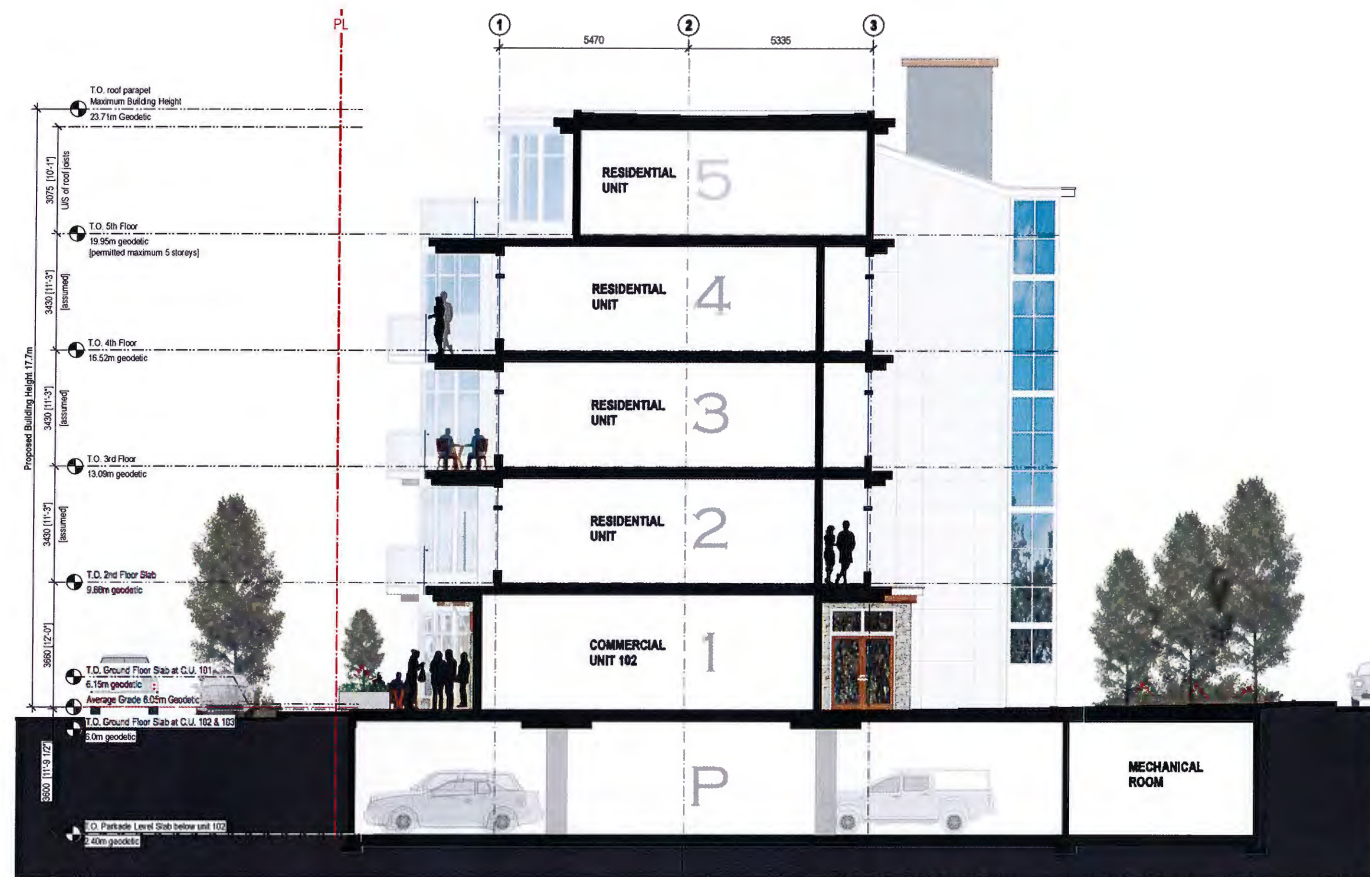


Rev	Date	Description
1	15 January 2017	Issue for Planning Application
2	15 July 2015	Preliminary

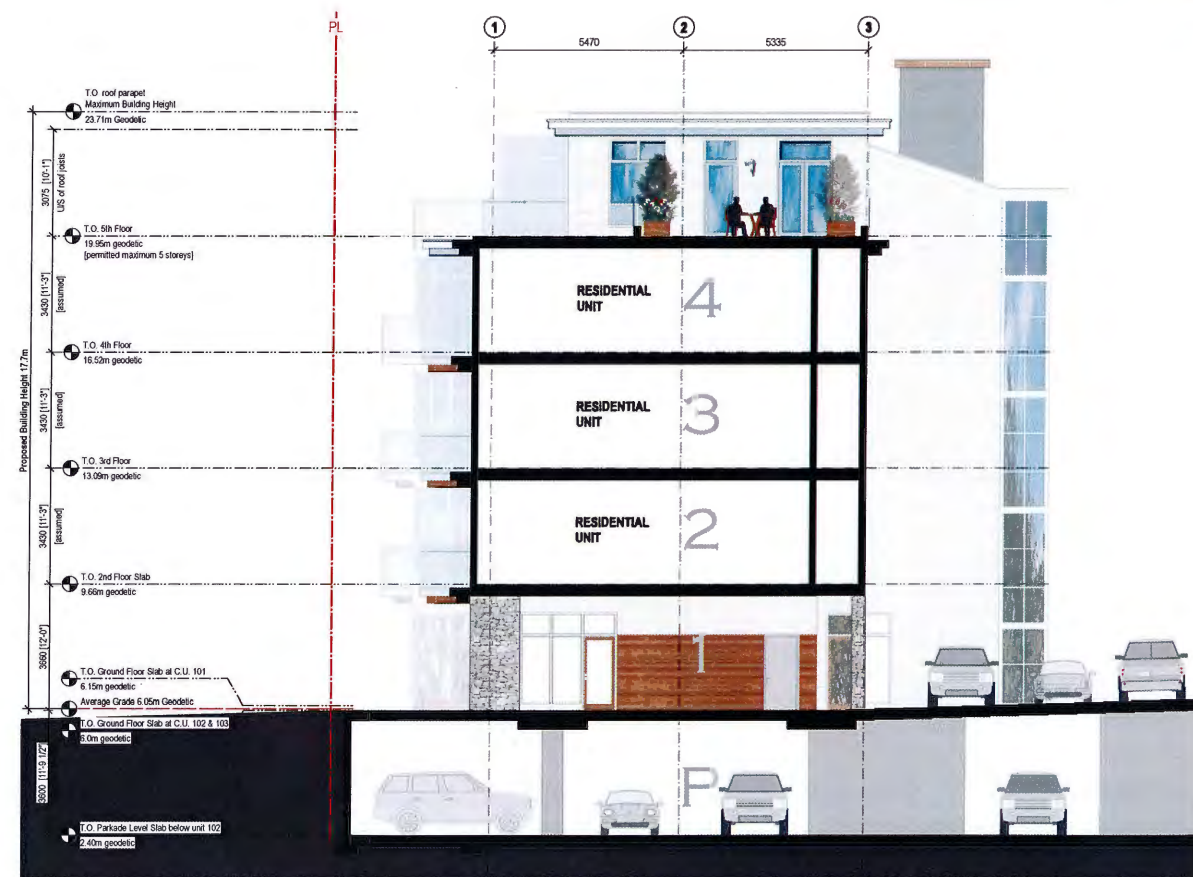
project: **WESTBAY MARINAVIEW**
460 & 464 HEAD STREET, ESQUIMALT BC

drawing title: **Exterior Elevations - North & South**

drawing no: **A3.3**



1 Building Section at CRU 102
metric scale: 1 : 100



2 Building Section at Porte Cochère
metric scale: 1 : 100

no.	date	description	revision
1	14 January 2017	Issues for Planning Application	
2	15 July 2016	Submission	Preliminary



WESTBAY MARINAVIEW

460 & 464 HEAD STREET REDEVELOPMENT, ESQUIMALT



project	WESTBAY MARINAVIEW 460 & 464 HEAD STREET, ESQUIMALT BC
drawing title	Building Sections
drawing no.	A4.1



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

DRC Meeting: February 9, 2017

STAFF REPORT

DATE: February 6, 2017

TO: Chair and Members of the Design Review Committee

FROM: Trevor Parkes, Senior Planner

SUBJECT: DEVELOPMENT PERMIT APPLICATION
“West Bay Triangle”
468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]
470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]
472 Head Street [Lot 4, Block H, Section 11, Esquimalt District, Plan 292]
515 Gore Street [Lot 1, Block H, Section 11, Esquimalt District, Plan 292]
509 Gore Street [Lot 2, Block H, Section 11, Esquimalt District, Plan 292]
922 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]
912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]

RECOMMENDATION:

That the Esquimalt Design Review Committee [DRC] recommends to Council that the application for a Development Permit authorizing the form and character of the proposed development to that shown on architectural plans provided by Hillel Architecture, stamped “Received January 20, 2017”, and the landscape plan prepared by Small and Rossell Landscape Architects, stamped “Received January 20, 2017”, and sited as detailed on the survey plan prepared by prepared by McIlvaney Riley Land Surveying Inc., stamped “Received February 3, 2017” for the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, proposed to be located on the “West Bay Triangle” lands, be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application

The applicant is proposing to construct a the six storey, commercial and residential mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units on a consolidation of nine subject properties to be regulated by Comprehensive Development district No. 98 [CD-98].

This site is located within Development Permit Area No. 2 – Commercial; therefore a development permit is required to ensure the application meets the intent of the design

guidelines contained in Section 9.4 of the Township's Official Community Plan. This application must also be evaluated for consistency with the West Bay Neighbourhood Design Guidelines [attached] as Esquimalt Council endorsed this policy direction for West Bay on November 16, 2015.

The applicant is requesting approval of a Development Permit for the form and character, and landscaping proposed on the attached drawings as one is required prior to submitting for a Building Permit. Evaluation of this application should focus on issues respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of buildings and other structures.

Context

Applicant/Owner: Triangle Estates Ltd. [Mark Lindholm]

Architect: Hillel Architecture Inc. [Peter Hardcastle]

Property Size: Metric: 4080 m² Imperial: 43,917 ft²

Existing Land Use: Single Family and Two Family residences and vacant Commercial land

Surrounding Land Uses:

North: Neighbourhood Commercial/ West Bay Commercial
South: DND Federal Land [Work Point]
West: Single Family and Two Family Residential
East: Marine Commercial

Existing Zoning: RS-1 [Single Family Residential] – 5 Lots
RD-2 [Two Family Small Lot Residential] – 1 Lot
C-7 [West Bay Commercial] – 2 Lots

Proposed Zoning: CD-98 [Comprehensive Development District No. 98 – Pending Adoption]

Existing OCP Designation: Commercial Mixed-Use [No change required]

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received by the submission deadline:

Building Inspection: Construction must satisfy BC Building Code and Esquimalt's Building Code Bylaw, 2002, No. 2538 requirements. Detailed review of plans will occur when at time of Building Permit application.

Fire Services: Fire Services staff have completed a preliminary evaluation of the proposed new building and offer no objections at this time. Detailed additional comments will be provided as part of the consideration of the Building Permit Application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed commercial mixed use development including commercial space and 73 residential units located on 9 properties within West Bay. The applicant is responsible for retaining the services of a qualified professional for the design, construction and supervision of all Works and Services required to service the proposed development as indicated in Esquimalt's Subdivision and Development Control Bylaw No. 2175.

A capacity study should verify all main sizes, material, etc., and to determine if the Sewer and Drain mains are of adequate size and condition to handle the increased flows introduced as a result of the development. Existing municipal infrastructure may need to be relocated or SRWs may need to be secured. Existing properties are to remain fully serviced during construction. Additional review comments will be provided when detailed engineering drawings are submitted.

ISSUES:

Zoning

CD-98 zoning was specifically tailored to accommodate this proposal. The proposed design is consistent with the form and character presented supporting the rezoning application and the proposed building height, massing, density, siting and parking requirements satisfy all of the CD-98 zone regulations outlined in Amendment Bylaw No. 2873 [attached] which is pending adoption by Council.

Density, Lot Coverage, Setbacks, Height and Parking:

The following chart details the setbacks, lot coverage and floor area ratio and parking requirements of this proposal.

	Comprehensive Development Zone	
Floor Area Ratio	1.40	Zoning Bylaw, 1992, No. 2050 does not contain a zone that could accommodate this commercial mixed-use proposal. Staff present this summary table as the basis for a site specific zone written to accommodate this proposal should it be forwarded in the development review process.
Lot Coverage		
• 1 st Storey	55%	
• 2 nd Storey	63%	
• Above 2 nd Storey	23%	
Setbacks		
• Front	5.8 m plus 3.0 m [Canopy]	
• Rear	6.7 m plus 1.8 m [Canopy]	
• Interior Side	0.9 m plus 0.9 m [Canopy]	
• Exterior Side	3.2 m plus 0.5 m [Canopy]	
Building Height	7.6m [Head Street and Gore Street] 22 m (6 Storeys) [Lyllall Street]	
Off Street Parking	Total spaces required for all uses = 114 Total proposed = 119	
Useable Open Space	15% [Including 2 nd Storey Terrace]	
Commercial Floor Area	Not less than 395 square metres	

Official Community Plan

The West Bay Triangle lands are located within a predominantly low rise neighbourhood consisting of a mix of commercial properties, single and two family residential homes, a marine community, and military uses including training facilities and housing. The exception to the established one to three storey built environment is the DND “Accommodations” building located at Work Point to the south of the subject properties. This building is approximately 7 storeys [22 metres] in height and commands a substantial presence in the area due to the combination of height and massing of the building.

The West Bay Triangle would integrate into the existing urban fabric by complimenting the DND Accommodations building while presenting a 2 storey built form sympathetic to the existing residential and commercial buildings along Gore Street and Head Street.

The current Land Use Designation of properties adjacent to the West Bay Triangle site include Commercial Mixed-use for lands to the east, adjacent to the water, Park and Open Space for Captain Jacobson Park and Townhouse Residential for properties on the north side of Gore Street and the south side of Paradise Street. Based on these land use designations, it is not unreasonable to expect some of these properties adjacent to the proposed West Bay Triangle building to redevelop to a modestly higher density than is present today.

Policy direction contained in the Esquimalt Official Community Plan was reviewed by staff and presented to Council as part of the consideration of the rezoning application. Official Community Plan, Section 9.4.5, Guidelines for Owners of Land within the Development Permit Area No. 2 – Commercial is specifically relevant to consideration of this Development Permit Application [attached].

OCP Section 9.4.5(a) supports windows facing the street and doors opening onto the street. This is consistent with the proposed design.

OCP Section 9.4.5(b) supports ornamental lighting highlighting the building and lighting pedestrian area while avoiding the casting of glare or direct light onto adjacent sites. The drawing package does not specifically detail the lighting plan however the proposed design could accommodate glass and lighting consistent with this policy.

OCP Section 9.4.5(c) supports building design that limits the casting of shadows on public space. At six storeys in height, the proposed building casts shadows, however, the majority of these shadows are restricted to the subject lands and Gore Street and Head Street and do not rest in one area for prolonged periods.

OCP Section 9.4.5(d) supports canopies covering pedestrian walkways. The proposed design includes generous 3 metre deep canopies over the sidewalk on Head Street to create a covered transitional space between the indoor uses and the outdoor public realm.

OCP Section 9.4.5(e) supports screened or underground parking be provided. If surface parking is proposed it should be screened with landscaping. The proposed design addresses this guideline as all parking is screened within the building with the exception of 12 spaces near the intersection of Lyall Street and Gore Street which are wrapped in landscape screening.

OCP Section 9.4.5(f) supports the inclusion of Crime Prevention through Environmental Design principles in any project. The proposed design is well lit with good connectivity to the public street and resident overlook onto internal courtyard areas.

OCP Section 9.4.5(g) supports decreased setbacks where appropriate. This proposal maintains generous setbacks in order to create ample space in front of businesses for walking and the display of goods.

OCP Section 9.4.5(h) supports screening of garbage and recycling facilities. The proposal has these facilities contained within the building, accessed from the interior parking courtyard.

OCP Section 9.4.5(i) supports the retention of trees and habitat wherever possible. This proposal is a complete revitalization of the subject lands including a significant excavation for underground parking facilities. Accordingly the proposal has included the retention of only two trees on or abutting the site, both mature and both located outside the excavation area. It is noteworthy that the proposed landscaping and public realm plantings will increase the number of trees on the lands.

West Bay Neighbourhood Design Guidelines

The West Bay Triangle development proposal has been tailored to address the West Bay Neighbourhood Design Guidelines resulting in an application that is, for the most part, consistent with these policies with the notable exception of orienting the tallest portion of the building east to west as opposed to the recommend orientation of north to south. The applicant has provided a detailed shadow analysis that indicates the proposed design is superior in mitigating shadowing than any alternatives. As shadow casting was identified as a major concern for local residents during the public consultation process of the West Bay Neighbourhood Design Guidelines, the applicant has chosen to proceed with the design as presented.

Development Services staff completed a review of the proposed design and note the following apparent inconsistencies to the West Bay Neighbourhood Design Guidelines:

Page 10, Bullet 4 – "Avoid locating off-street surface parking adjacent to active public streets and open spaces.....". The proposed design is consistent with this policy with the exception of the surface parking provided with limited screening adjacent to Lyall Street near the Gore Street intersection. Staff note this parking area provides desirable visitor parking. Staff also note that during consideration of the rezoning application, the applicant was encouraged to site large conifers in the unexcavated planting areas to screen both this parking lot and interrupt the mass of the east face of the six storey portion of the building. The applicant has detailed deciduous trees abutting the parking area.

Page 17, Bullet 2 – "Locating off-street surface parking in front of buildings,...immediately adjacent to the public sidewalk or open spaces.....is strongly discouraged and should be avoided.". Once again, the proposed design partially achieves this policy when considered from Head Street and Gore Street; however, surface parking is provided with limited screening adjacent to Lyall Street. It is noteworthy that while surface parking is inconsistent with this policy, the surface parking provides an intuitive destination for both commercial and residential visitors to the site.

Page 38 – The proposed streetscape and private realm pattern is inconsistent with that envisioned in the guidelines. Specifically, the public sidewalk is limited to 1.5 metres in width across the Head Street frontage as opposed to the recommended 3.0 metres. Staff acknowledge that West Bay Triangle incorporates a larger setback than proposed in the guidelines and accommodates a separated, private realm, sidewalk 4.0 metres in width. Staff also note that trees proposed from installation in the public realm are notably smaller than many of those proposed for the private realm and along Gore Street the proposed tree placement is inconsistent with the guidelines as trees are placed at back of sidewalk, in the private realm. The guidelines detail street trees placed in a boulevard between the curb and the sidewalk serving to separate pedestrian and vehicular activities.

Page 40 – Streetscape design envisioned for Lyall Street East is inconsistent with the established guidelines. Once again street trees are detailed at the back of sidewalk in the private realm as opposed to being placed at the back of curb to separate the sidewalk from the vehicle lane and intercept stormwater landing on the public sidewalk. Staff also note the proposed design does not accommodate the conceptual two way separated bike lanes. Staff acknowledge that inclusion of this design feature in the public realm is premature at this time as further consultation with the community on this concept is required.

Public Notification

As this is a Development Permit application without any requested variances, public notification


is not required.

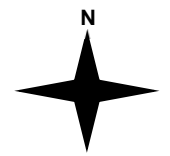
ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**

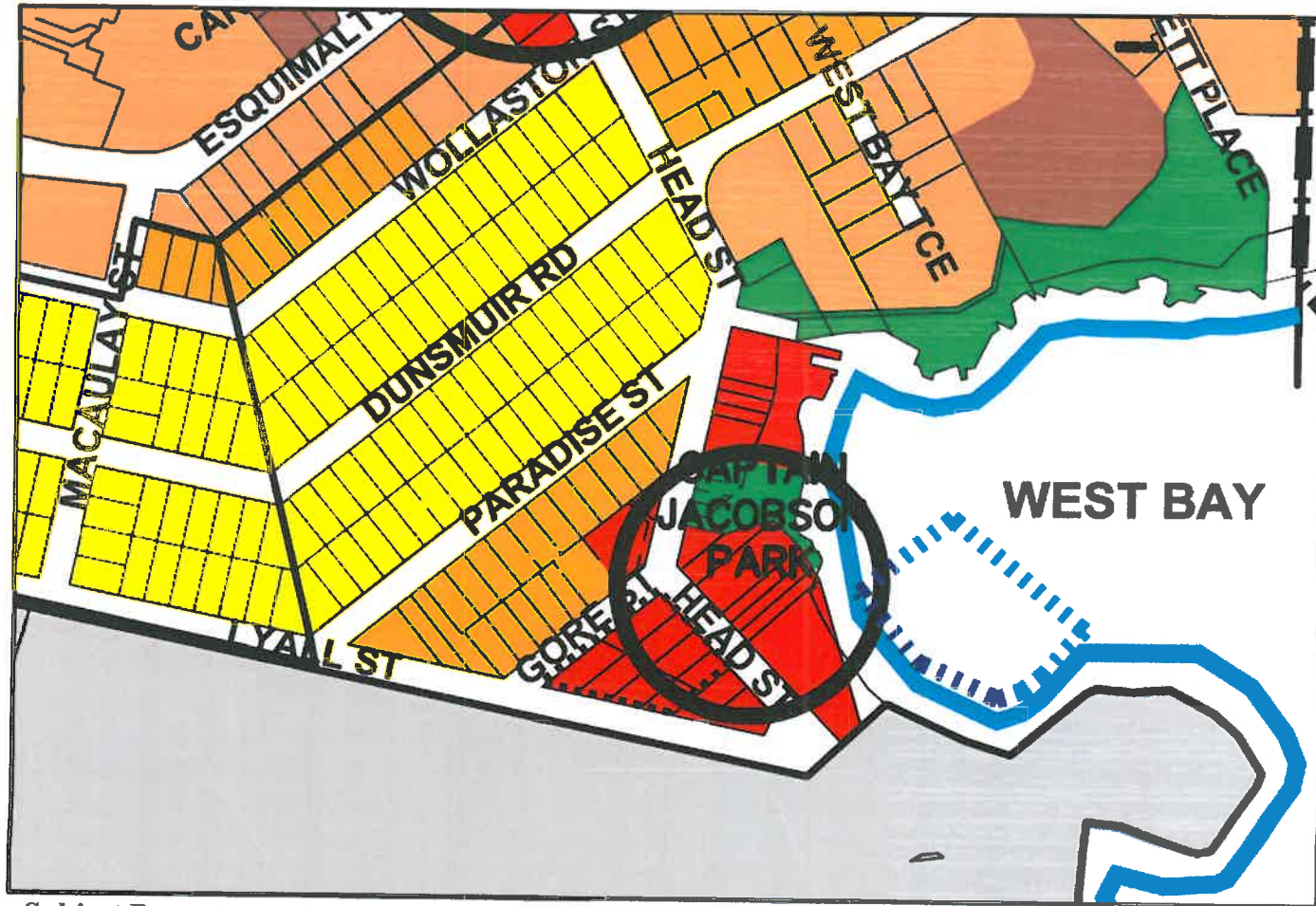
468 Head Street – ‘West Bay Triangle’



Subject Property Boundary: 



Extract from Official Community Plan Bylaw No. 2646 Schedule 'A'
Land Use Designations



Subject Property:

LEGEND	
	Single and Two-Unit Residential
	Townhouse Residential
	Multi-Unit, Low-Rise Residential (2 to 4 storeys)
	Multi-Unit, High-Rise Residential (5 to 6 storeys)
	Floathome Residential
	Parks and Open Space
	Trails existing and proposed
	Transportation Corridor
	Industrial
	Commercial Mixed-Use
	Commercial Node
	Institutional
	DND Federal Land
	Water Area
	Municipal Boundary

2.3 Commercial—Mixed Land Use

Commercial activity in Esquimalt is clustered in four main groupings:

- Esquimalt Village;
- Esquimalt Road/Head Street;
- Craigflower Road/Tillicum Road; and
- West Bay Harbour.

2.3.1 General Commercial - Mixed Use Objectives

- a) To create a diversified commercial and employment sector that provides a wide range of goods and services to residents of Esquimalt.
- b) To foster the creation of an identifiable and vibrant Esquimalt Village that successfully integrates commercial, public and residential activity.
- c) To encourage growth through revitalization and redevelopment of commercial areas.
- d) To encourage a mix of ground-level commercial and upper-level(s) residential.

2.3.2 General Commercial - Mixed Use Policies

- a) The Township encourages a mix of commercial and multi-unit residential developments in all commercial-mixed use areas denoted on "Schedule A". These will have commercial uses on the ground floor and residential uses above.
- b) All commercial-mixed use areas are designated Development Permit Areas, as shown on "Schedule C" in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.
- c) The Township encourages public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.
- d) The Township will develop signage guidelines for each commercial area, as part of design guidelines for these areas. The Township, in partnership with interested members of the community, will take the initiative to design and install entrance signage at key street locations, including entrances to Esquimalt.
- e) The Township encourages the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas. The Township is amenable to using density bonusing, or providing variances to zoning or parking regulations for redevelopment proposals.
- f) To encourage the use of bicycles, provision should be made in new commercial buildings for bicycle parking for employees and visitors. Secure bicycle parking for employees should be provided in the ratio of one (1) parking space per ten (10) full-time employees with a minimum of one (1) space for each new building. In all new commercial buildings, six (6) bicycle parking spaces should be available for the use of temporary visitors.
- g) End of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

- h) Where all of the following criteria are met in a commercial building, Council may reduce the off-street parking requirement through the Development Permit:
 - i) Two (2) or more secure bicycle storage spaces are provided;
 - ii) Shower and change rooms are provided;
 - iii) Six (6) visitor bicycle parking spaces are provided; and
 - iv) The building is located within 200 metres of a regional bus route.
- i) Lands outside the Commercial-Mixed Use designation on "Schedule A" will not be considered for commercial zoning unless the following criteria are met:
 - i) The project is needed to serve tourists or local residents and cannot be appropriately located within established commercial areas;
 - ii) The density and scale of the project is sensitive to the prevailing character of surrounding lands;
 - iii) The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
 - iv) The project's parking requirements can be satisfied on-site and will not unduly affect neighbouring residences; and
 - v) The proponent demonstrates that the neighbourhood has been consulted and residents have had an opportunity to express their concerns.

[Amendment Bylaw [No. 6], 2010, No. 2730 - Adopted May 25, 2010]

- j) In mixed commercial and multi-unit residential developments, buildings up to 12 storeys in height and with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.
- k) Development proposals with heights and/or densities greater than those set out in Section 2.3.2 (j) may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor space where new commercial buildings provide affordable, accessible, or special needs housing units or amenities for the benefit of the community.

9.4 Development Permit Area No. 2 – Commercial

9.4.1 Scope

All lands designated Commercial on Schedule “C” are part of DPA No. 2.

9.4.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, commercial.

9.4.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

Where new development is to occur within Esquimalt’s commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials, and by their orientation to the street rather than to a parking lot or internal square.

The goals for Development Permit Area No. 2 are:

- a) to enhance the aesthetic image of Esquimalt’s commercial district, particularly those areas that are considered community focal points, such as the Village, the Head Street/Esquimalt Road intersection and major entrance points to the municipality;
- b) to revitalize existing commercial areas by encouraging a variety of businesses;
- c) to encourage growth in the tax base through diversified commercial development and redevelopment of existing commercial areas; and
- d) to encourage integrated residential/institutional/commercial uses in commercial areas.

9.4.4 Requirements of Owners of Land within the Development Permit Area

- a) Owners of land within Development Permit Area No. 2 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:
 - i) subdivide lands; or
 - ii) construct or alter a building or structure;without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.
- b) Exemptions:

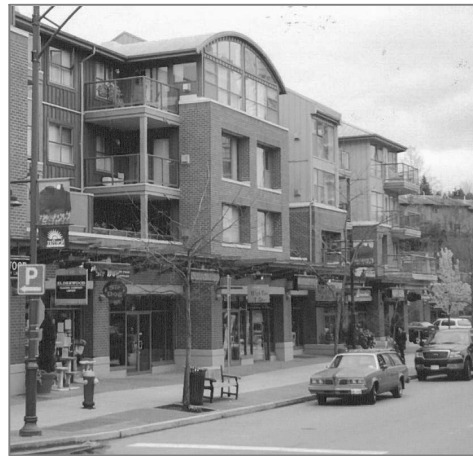
The following do not require a development permit:

 - i) construction of buildings or structures less than 10 square metres in area;

- ii) minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted by the municipal tree protection bylaw; and
- vi) replacement or changing of existing signs, provided the sign area is not to be increased.

9.4.5 Guidelines for Owners of Land within the Development Permit Area

- a) Commercial building facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
- b) Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, commercial lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
- c) Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- d) Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
- e) Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- f) The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
- g) Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
- h) Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

BYLAW NO. 2873

A Bylaw to amend Bylaw No. 2050, cited as the
“Zoning Bylaw, 1992, No. 2050”

THE MUNICIPAL COUNCIL OF THE CORPORATION OF THE TOWNSHIP OF
ESQUIMALT, in open meeting assembled, enacts as follows:

1. This bylaw may be cited as the “*ZONING BYLAW, 1992, NO. 2050, AMENDMENT BYLAW NO. 2873*”.
2. That Bylaw No. 2050, cited as the “Zoning Bylaw, 1992, No. 2050” be amended as follows:

- (1) by adding the following words and figures in Part 31, Zone Designations, in the appropriate alpha-numeric sequence:

“Comprehensive Development No. 98 (468 Head Street) CD No. 98”

- (2) by adding the following words and figures in Section 2 - Definitions of PART 1 - Interpretation:

“**Boat Rental and Passenger Charter**” means the use of land or a building or structure for the booking, registration, outfitting, and commercial transactions for boat rental, marine tours, diving expeditions, kayaking, sailing, fishing and other boat excursions.

- (3) by adding the following text as Section 67.85 (or as other appropriately numbered subsection within Section 67):

67.85 COMPREHENSIVE DEVELOPMENT DISTRICT NO. 98 [CD NO. 98]

In that Zone designated as CD No. 98 [Comprehensive Development District No. 98] no Building or Structure or part thereof shall be erected, constructed, placed, maintained or used and no land shall be used except in accordance with and subject to the regulations contained in or incorporated by reference into this Part.

- (1) **Permitted Uses**

Only Mixed Commercial / Residential use is permitted and the following Uses and no others shall be permitted as part of that development:

- a) Dwelling – Multiple Family
- b) Home Occupation
- c) Business and Professional Office
- d) Personal Service Establishment

- e) Retail Store
- f) Restaurant
- g) Liquor Store
- h) Convenience Store
- i) Group Children's Day Care Centre
- j) Boat Rental and Passenger Charter

(2) **Floor Area Ratio**

(a) **Residential Uses**

The Floor Area Ratio shall not exceed 1.30.

(b) **Combined Mixed Use**

The combined Floor Area Ratio for all uses shall not exceed 1.40.

(3) **Unit Size**

Dwelling Units shall not be less than 45 square metres.

(4) **Number of Dwelling Units**

(a) Not less than sixty five (65) Dwelling Units shall be located in this Zone.

(b) Not more than seventy three (73) Dwelling Units shall be located in this Zone.

(5) **Size and Location of Commercial Space**

(a) The minimum Floor Area dedicated to Commercial Uses shall not be less than 395 square metres located on the First Storey.

(b) The maximum Floor Area dedicated to Commercial Uses shall not be greater than 425 square metres.

(c) Commercial Uses are not permitted on any Storey located above the First Storey.

(d) Commercial Uses are not permitted within 30 metres of the southwestern Lot Line.

(6) **Parcel Size**

The minimum Parcel Size of fee simple Parcels created by subdivision shall be 4075 square metres.

(7) **Building Height**

(a) No Principal Building shall exceed a Height of 23 metres.

(b) No Accessory Building shall exceed a Height of 3.3 metres.

(8) **Lot Coverage**

(a) Principal Buildings, Accessory Buildings and Structures combined shall not cover more than 64% of the Area of the Parcel.

(b) Notwithstanding Section 7(a), Principal Buildings shall not cover more than 56% of the Area of the Parcel on the first storey.

(c) Principal Buildings shall not cover more than 24% of the Area of the Parcel for each storey of a Building that is located above the second storey.

(9) **Siting Requirements (As illustrated on Figure 1.)**

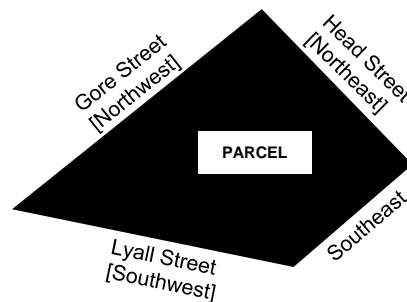


Figure 1.

(a) **Principal Buildings:**

(i) No Principal Building shall be located within 5.8 metres of the northeastern Lot Line.

(ii) No part of a Principal Building located above the second storey shall be located within 18 metres of the northeastern Lot Line.

(iii) No Principal Building shall be located within 3.2 metres of the northwestern Lot Line.

(iv) No part of a Principal Building located above the second storey shall be located within 12 metres of the northwestern Lot Line.

(v) No Principal Building shall be located within 0.9 metres of the southeastern Lot Line.

(vi) No Principal Building shall be located within 5.9 metres of the southwestern Lot Line.

(vii) That part of a Principal Building located above the second storey shall be located within 30 metres of the southwestern

Lot Line.

(b) **Accessory Buildings:**

- (i) No Accessory Building shall be located within 52 metres of the northeastern Lot Line.
- (ii) No Accessory Building shall be located within 9 metres of the northwestern Lot Line.
- (iii) No Accessory Building shall be located within 16 metres of the southwestern Lot Line.
- (iv) No Accessory Building shall be located within 0.1 metres of a Principal Building.

(10) **Siting Exceptions**

Within the CD-98 zone, the minimum distance to a Lot Line may be reduced by not more than the following distances to accommodate exterior canopies, attached to and forming part of a Principal Building:

- (a) Northeastern Lot Line: 3.0 metres
- (b) Northwestern Lot Line: 0.5 metres
- (c) Southwestern Lot Line: 0.9 metres
- (d) Southeastern Lot Line: 0.9 metres

(11) **Useable Open Space**

Useable Open Space shall be provided in an amount not less than 7.5% of the Area of the Parcel, including open space located over a parking structure.

(12) **Fencing**

Subject to Part 4, Section 22, no fence shall exceed a Height of 1.2 metres nor be located within 5.8 metres of a Lot Line abutting a Highway.

(13) **Off-Street Parking**

Off-street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011(as amended).

- (4) by changing the zoning designation of each of the following parcels, shown cross-hatched on Schedule 'A', attached hereto, from their current zoning designation, as detailed on Schedule 'B', attached hereto, to CD No. 98 [Comprehensive Development District No. 98]:

- a) PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];
 - b) PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];
 - c) PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];
 - d) PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];
 - e) PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];
 - f) PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];
 - g) PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];
 - h) PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street]; and
 - i) PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].
- (5) by changing Schedule 'A' Zoning Map, attached to and forming part of "Zoning Bylaw, 1992, No. 2050" to show the changes in zoning classification effected by this bylaw.

READ a first time by the Municipal Council on the 16th day of May, 2016.

READ a second time by the Municipal Council on the 16th day of May, 2016.

A Public Hearing was held pursuant to Sections 464, 465, 466, and 468 of the *Local Government Act* on the 20th day of June, 2016.

READ a third time by the Municipal Council on the 20th day of June, 2016.

RESCIND third reading by the Municipal Council on the 12th day of September, 2016.

AMEND AND READ ANEW at second reading by the Municipal Council on the 12th day of September, 2016.

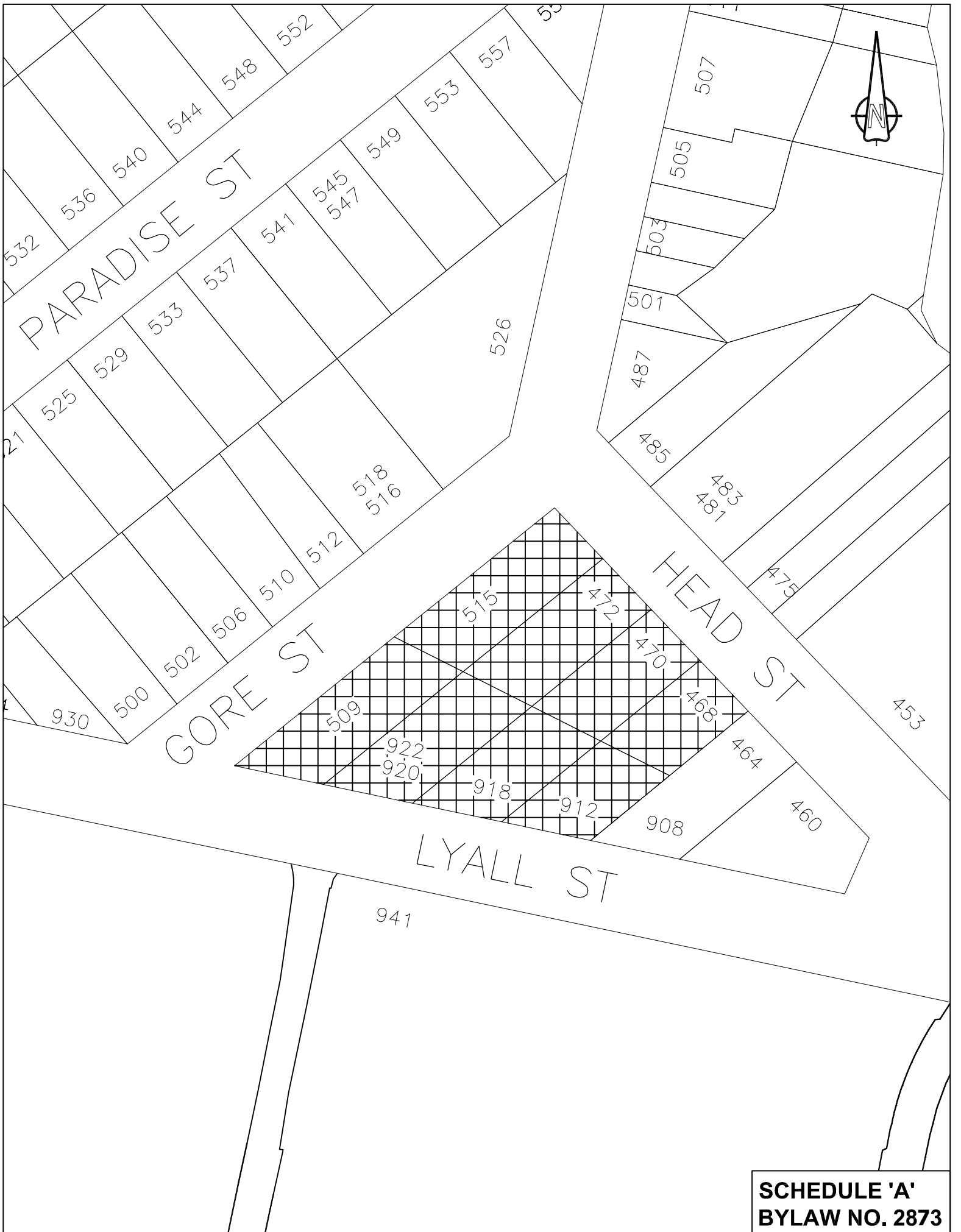
A Public Hearing was held pursuant to Sections 464, 465, 466, and 468 of the *Local Government Act* on the 3rd day of October, 2016.

READ a third time by the Municipal Council on the 3rd day of October, 2016.

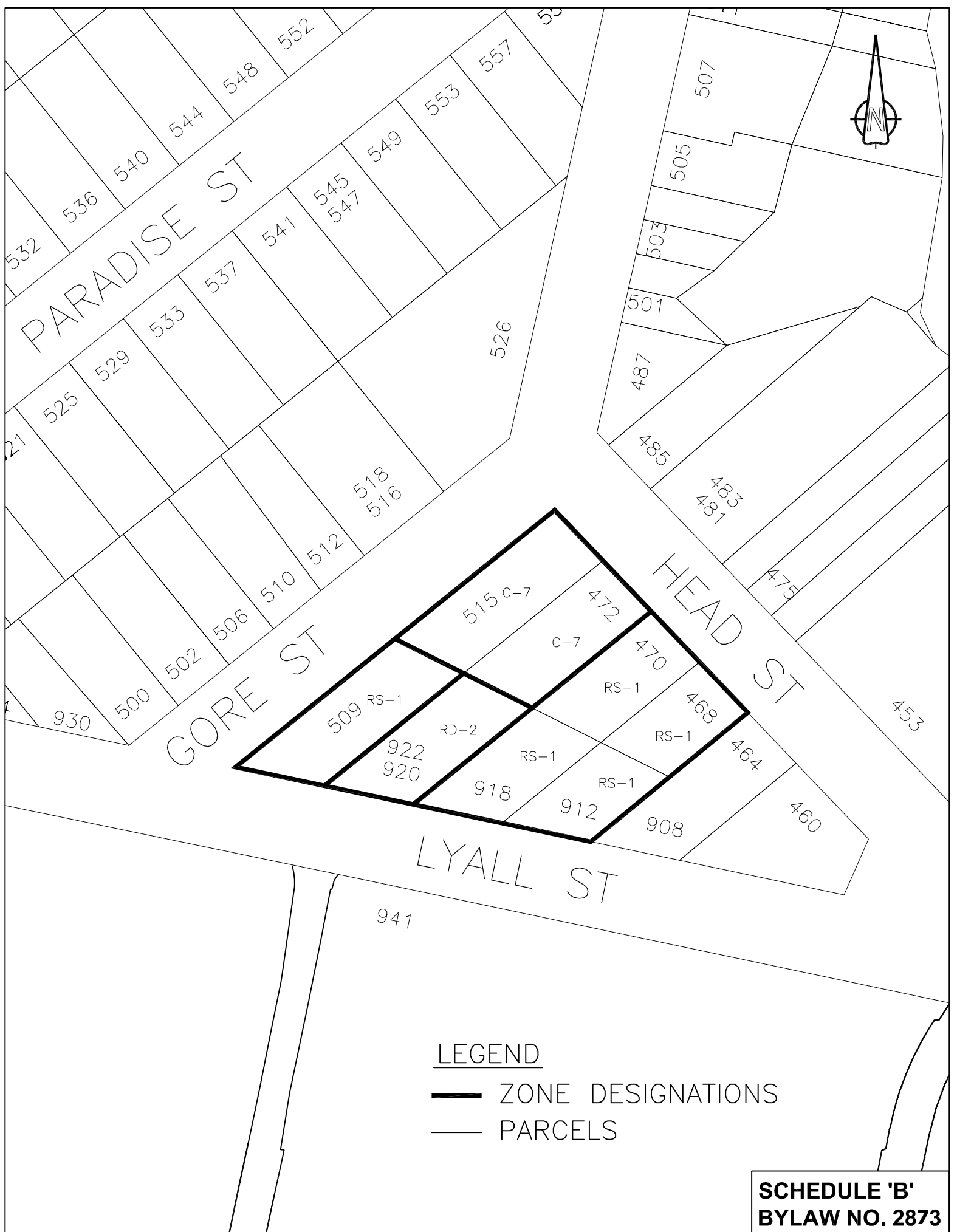
ADOPTED by the Municipal Council on the ---- day of ----, 2016.

BARBARA DESJARDINS
MAYOR

ANJA NURVO
CORPORATE OFFICER



SCHEDULE 'A'
BYLAW NO. 2873



LEGEND

-  ZONE DESIGNATIONS
-  PARCELS

**SCHEDULE 'B'
BYLAW NO. 2873**



West Bay Neighbourhood Design Guidelines



We would like to thank the following members of the West Bay Design Guidelines Stakeholders Group whose local insight and involvement in the process was of great benefit to the development of these design guidelines.

West Bay Design Guidelines Stakeholders Group:

Brian Emmett
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Lieutenant-Colonel Ulpiano Honorio
Carolyn Gisborne
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Submitted: 10 September, 2015.

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1. Purpose & Overview

Purpose and Overview

These design guidelines were developed through a collaborative, community-based process to ensure new development enhances rather than detracts from West Bay's unique and rich character and identity. These design guidelines focus on the interface between and integration of public and private lands towards the preservation and enhancement of the identity, social vitality and overall liveability that characterizes the West Bay neighbourhood. This includes integration with federal Department of National Defense (DND) lands, integration of local ecosystems, and connection to the harbourfront and the range of recreational and other activities associated with the working harbour.

This document includes:

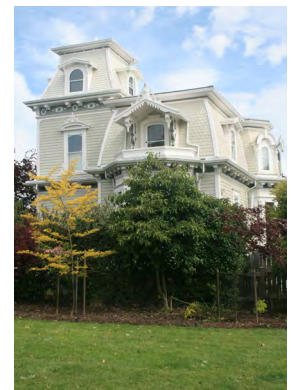
- An inventory and analysis of neighbourhood design characteristics;
- A vision statement and set of design principles as the rationale for the design guidelines;
- Development Permit Area design guidelines for Form and Character;
- A set of general design guidelines and recommendations for public realm design and improvements; and,
- Specific guidance on site planning and design for development opportunity sites along Head Street south adjacent to the harbourfront and including the "Triangle Lands", are also included.

The Spirit of West Bay

During the West Bay Design Guidelines Stakeholder Workshop, we asked: "What words capture the spirit of West Bay?" The following is a summary of responses:



Neighbourhood Characteristics: Photo Essay



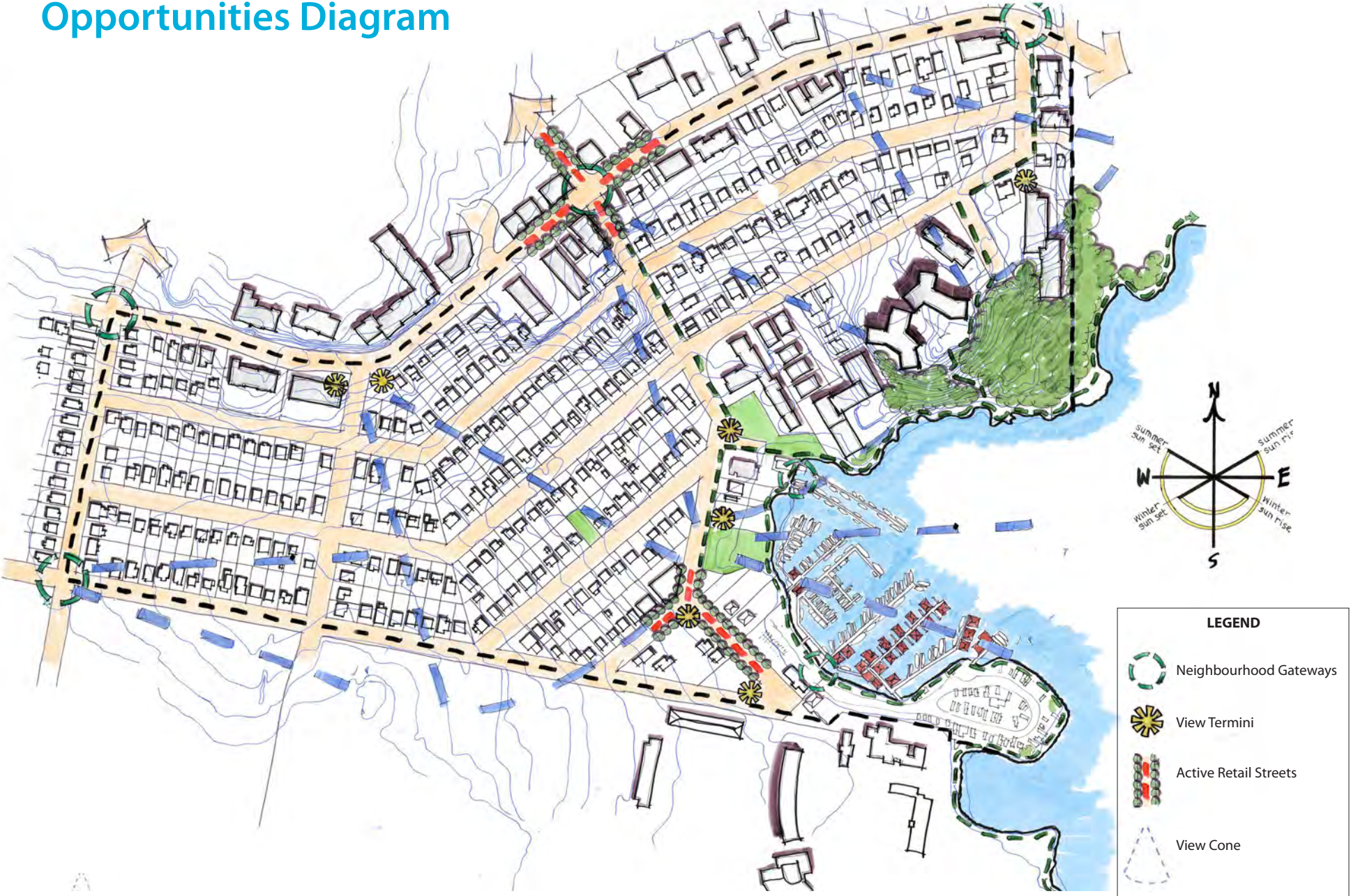
West Bay Ortho Photo and Study Area



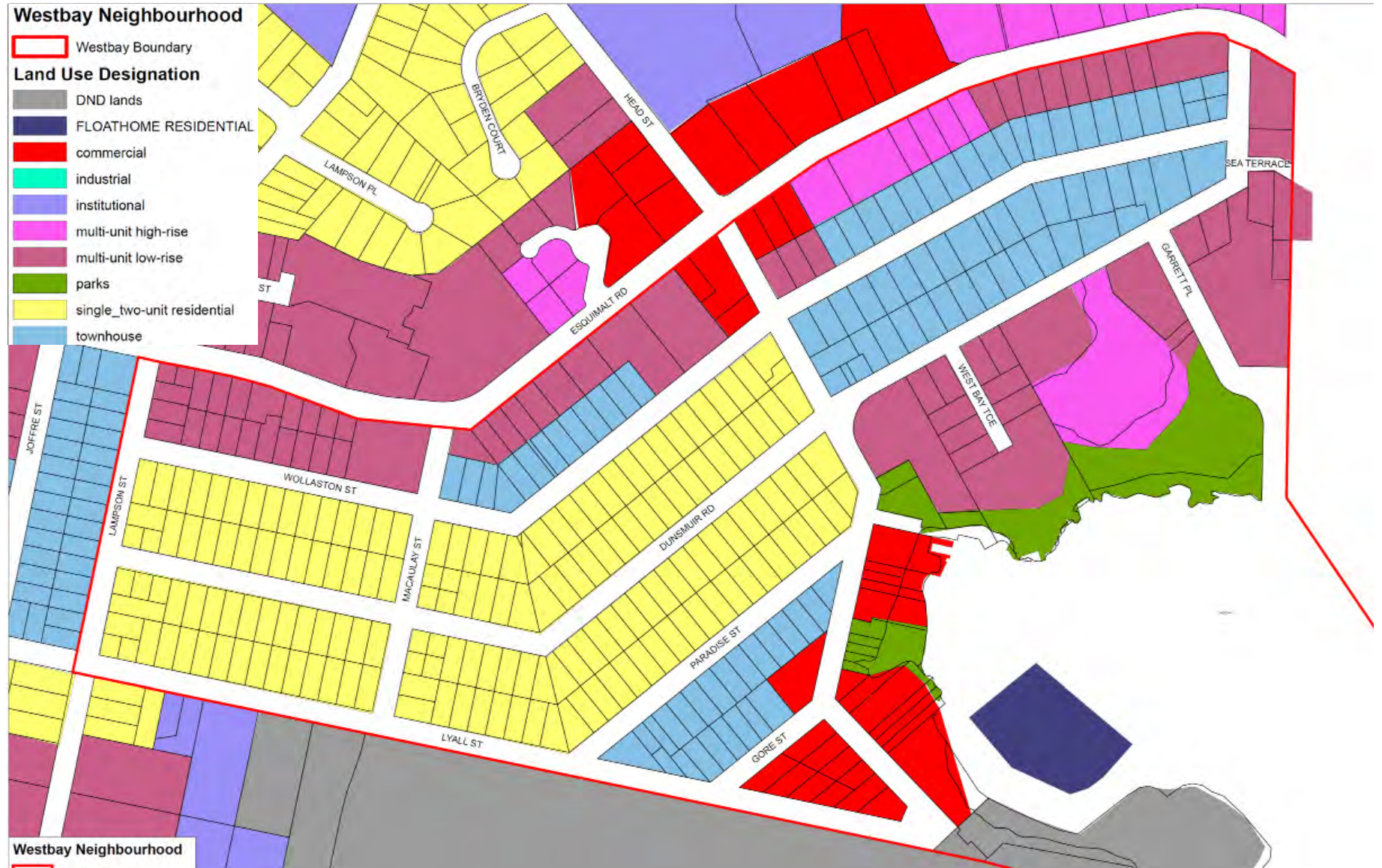
Neighbourhood Characteristics: Topography and Building Massing (Existing)



Neighbourhood Characteristics: Opportunities Diagram



West Bay Neighbourhood - OCP Land Use



2. Design Vision & Principles

Vision

West Bay is a thriving, attractive and walkable harbourfront neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbourfront recreation, and sunny open spaces with strong connections to the harbour, make this a truly liveable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.



Photo courtesy: Josh McCulloch, PictureBC

Design Principles

The following design directions for the public realm were identified through the workshop to help maintain and enhance the unique identity of West Bay:

- **Strong connection to the harbourfront:** Maintain and enhance visual and physical connections to the harbour and harbourfront trail network.
- **A walkable neighbourhood:** Publicly accessible buildings present a friendly face to streets and open spaces to make them active, attractive and inviting to pedestrians. Make connections within and to adjacent neighbourhoods to make walking easy.
- **Human scale:** Use architectural features, details, and site design elements that are of human proportion and clearly oriented for public, pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it on foot.
- **Marine ecology:** Enhance, restore and celebrate the unique marine and upland ecology. Layer and integrate mobility, recreation, stormwater, habitat and ecological systems in the context of the working harbour.
- **Harbourfront community & identity:** Ensure building and open space design that supports and enhances the unique sense of community and identity, including its architectural and marine heritage. More broadly, this includes the rich natural and human history of the area and associated expression of local culture, festivals, and gathering at and near the harbour front and water's edge.
- **Neighbourliness:** Ensure new development responds positively to the existing context by ensuring a sensitive transition in scale, by minimizing view and shadow impacts, and by responding to the positive design characteristics, if only subtly, of adjacent development.

3. Design Guidelines

Overview

Urban design is the comprehensive and cohesive combination of buildings, streets, open spaces and the natural environment and has, as its objective, the creation of memorable public spaces.

The essence of good urbanism is determined by the relationship between:

- the built and natural environments, and
- the public and private realm - the areas between buildings and public open spaces - at street level.

Buildings, streets, and other public open spaces scaled for human comfort and use are essential to the creation of a functional, aesthetically rich and vibrant neighbourhood. Building fronts and their orientation to streets and public open spaces are therefore the strongest determinants of the character and quality of neighbourhoods.

The design elements and approaches on the following pages provide a starting point and preliminary framework in maintaining and enhancing what is special, unique and wonderful about West Bay. They also provide a foundation to ensure new development and public realm improvements contribute to a vibrant, safe and accessible neighbourhood environment that encourages walking and street life while creating a unique social and physical environment that celebrates the beautiful natural setting of West Bay.



Photo: Joaquin Karakas



Photo: Joaquin Karakas

A Friendly Face to the Street

INTENT

- To define residential and commercial streets with active and attractive building fronts that have a positive orientation to streets and other public open spaces.
- To ensure buildings are sited and designed to be welcoming, and to encourage street vitality, visual interest, and safety.

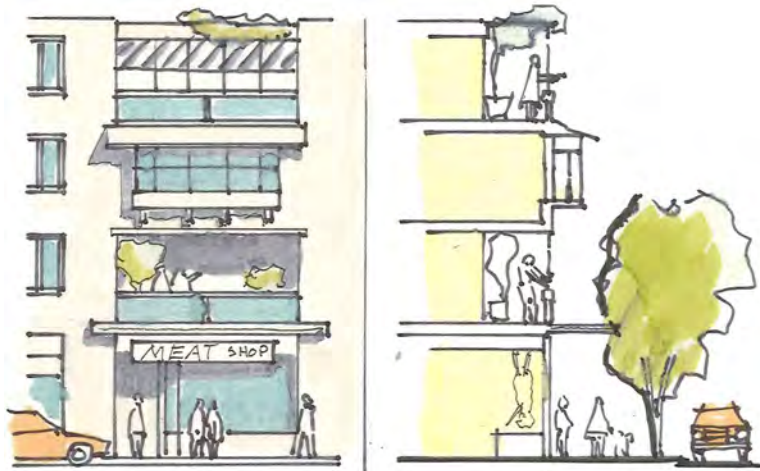
GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

- Locate publicly oriented active uses at grade and at or near the sidewalk edge.
- Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;
- A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.

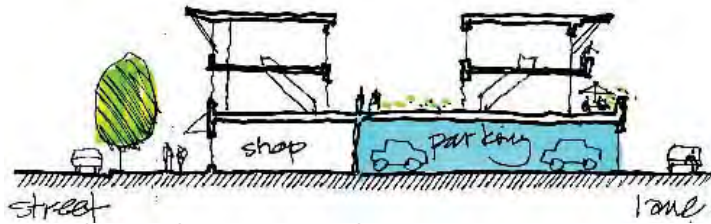


Active uses oriented to public streets and openspaces creates vibrant and attractive neighbourhoods (above, below and below left)





Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.

- Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.
- On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Locate on-street parking at the curb in front of shops.
- Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
- Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



Photo: Joaquin Karakas

On corner sites, develop street-facing façades for both streets. .

- Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.
- Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
- Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Photo: Joaquin Karakas

Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above and right)



Photo: Joaquin Karakas

Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

Photo: Dan Burden



Incorporate plantings and other landscape details such as banners.

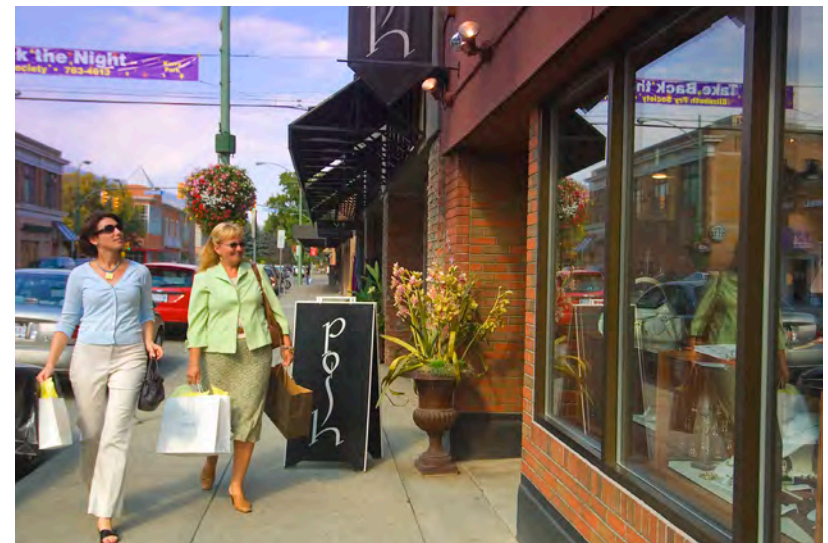


Screen



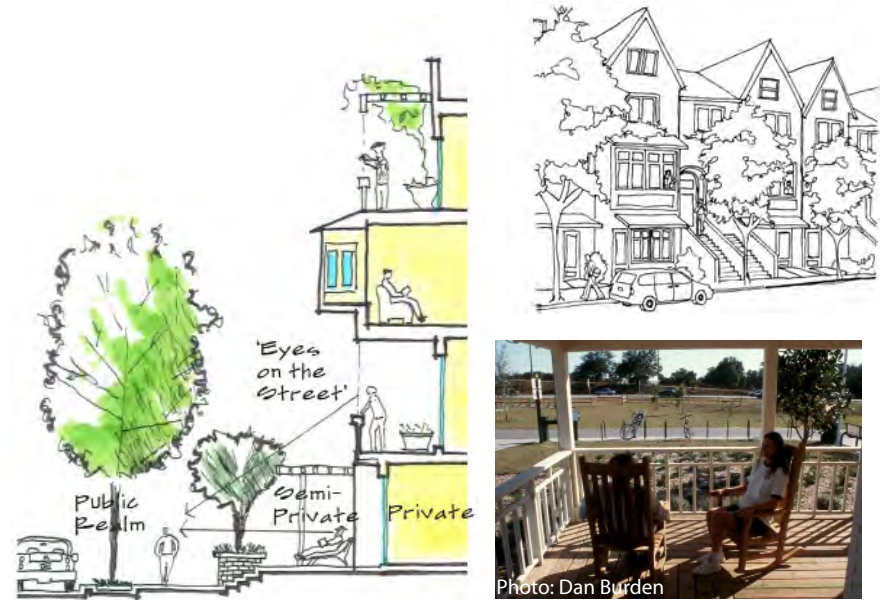
When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.

- Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:
 - » Install a vertical trellis in front of the wall with climbing vines or other plant material
 - » Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
 - » Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
 - » Employ quality materials of different textures and colours to make the wall more interesting visually
 - » Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
 - » Incorporate walls into a patio or sidewalk café space
 - » Terrace (step down) retaining walls



GUIDELINES: RESIDENTIAL BUILDINGS

- Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
- incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
- Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
- Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
- Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.



Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



Photo: Dan Burden



Photo: Joaquin Karakas



Photo: Joaquin Karakas



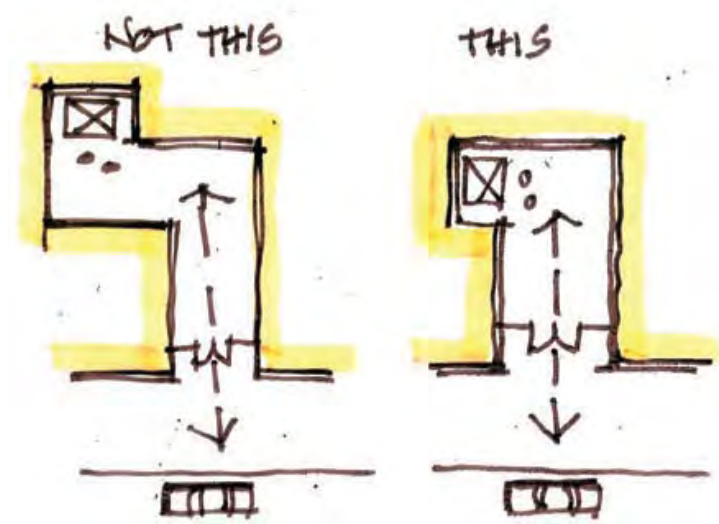
Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street. .



Photo: Joaquin Karakas

Consider using a landscape transition zone between entryways and public sidewalks.

- Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
- A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
- Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Direct sight lines into elevator lobbies are safer

VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
- New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.
- Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour
- Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's
- Mark/celebrate corners and street-end views through building and open space design.
- Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

- New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. "Flood Hazard Area Land Use Management Guidelines" (2004) (http://www.env.gov.bc.ca/wsd/public_safety/flood/pdfs_word/guidelines-2011.pdf).



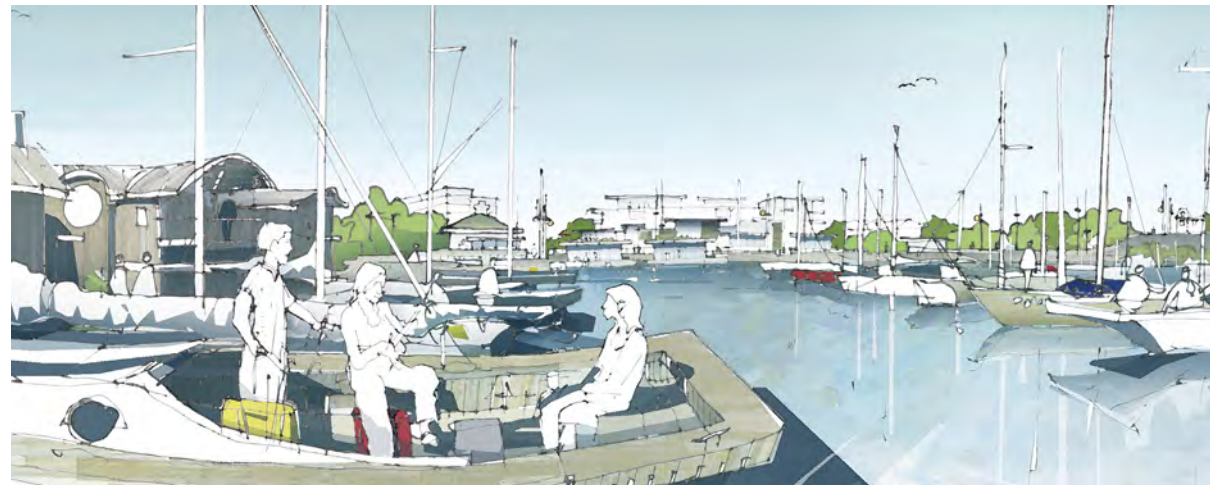
Photo: Bill Brown

Maintain street-end views to the harbour-front



Photo: Franc D'Ambrosio Architecture and Urbanism

Create new public connections to harbourfront uses and activities.



Neighbourliness

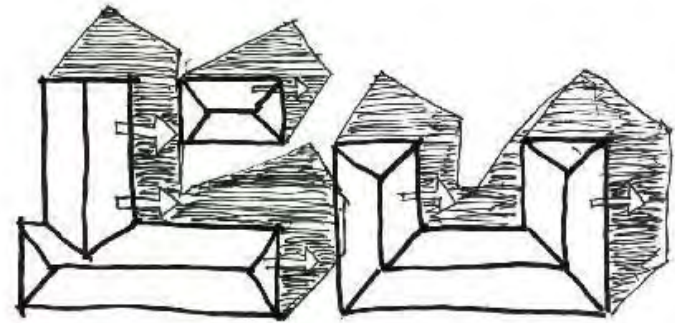
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

GUIDELINES

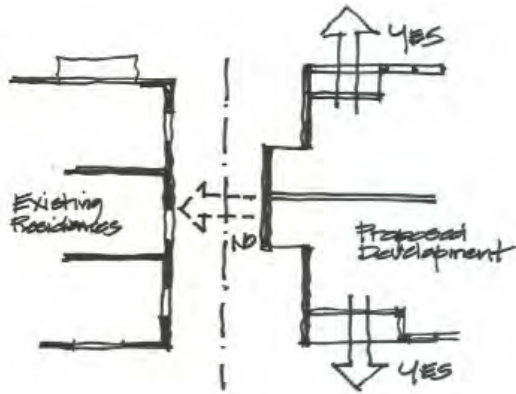
- New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.
- Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
- In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.
- Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
- Locate development to minimize view impacts on existing and planned future development.
- Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.
- Sun shade diagrams for the Winter Solstice will be required only when significant impacts are expected such as shading solar panels
 - » Winter Solstice: 9 a.m., 12 noon, 3 p.m.



Transition in scale to existing context



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and buildings

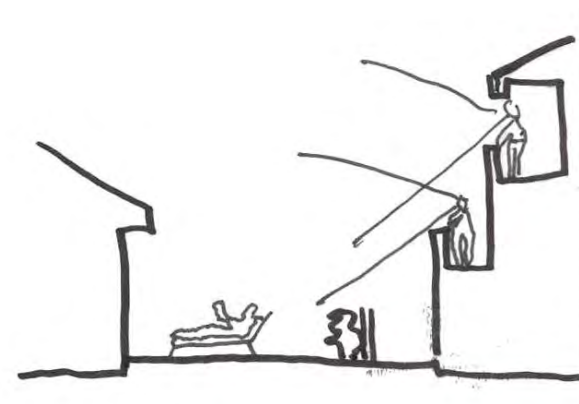


Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.



This building steps down to the slope to reduce the visual impact from it's massing

- Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
- Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
- Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
- Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 - » Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 - » Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 - » Stagger windows to not align with adjacent, facing windows.
 - » Primary windows into habitable spaces should not face interior side-yards



Minimize overlook into adjacent private yards

Architectural Concept: Achieving a Human Scale

OVERVIEW AND INTENT

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

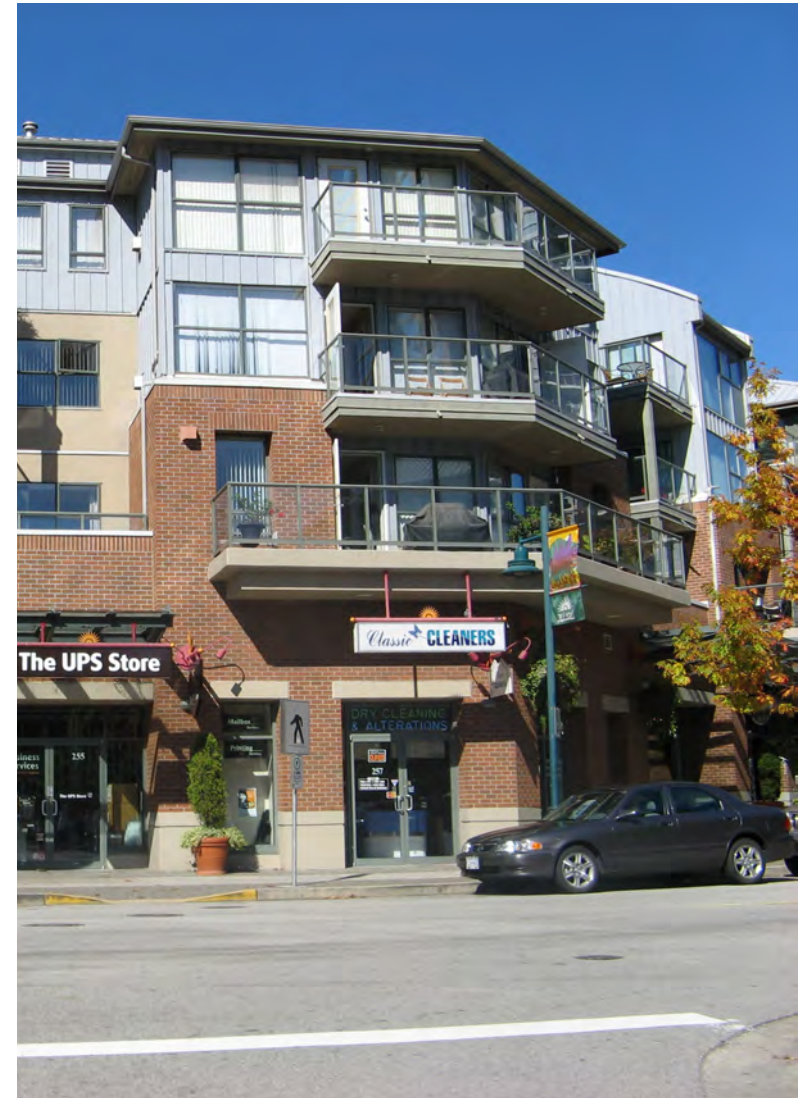
Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in a modern composition.

GENERAL GUIDELINES

- The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- Design buildings to express their internal function and use.
- Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.
- Examples of architectural features include:
 - » Building height, massing, articulation and modulation
 - » Bay windows and balconies
 - » Corner features accent, such as turrets or cupolas
 - » Decorative rooflines and cornices
 - » Building entries
 - » Canopies and overhangs
- Examples of architectural details include:
 - » Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
 - » Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
 - » Articulation of columns and pilasters
 - » Ornament or integrated artwork
 - » Integrated architectural lighting
 - » Detailed grilles and railings
 - » Substantial trim details and moldings
 - » Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians



Photo: Franc D'Ambrosio Architecture and Urbanism

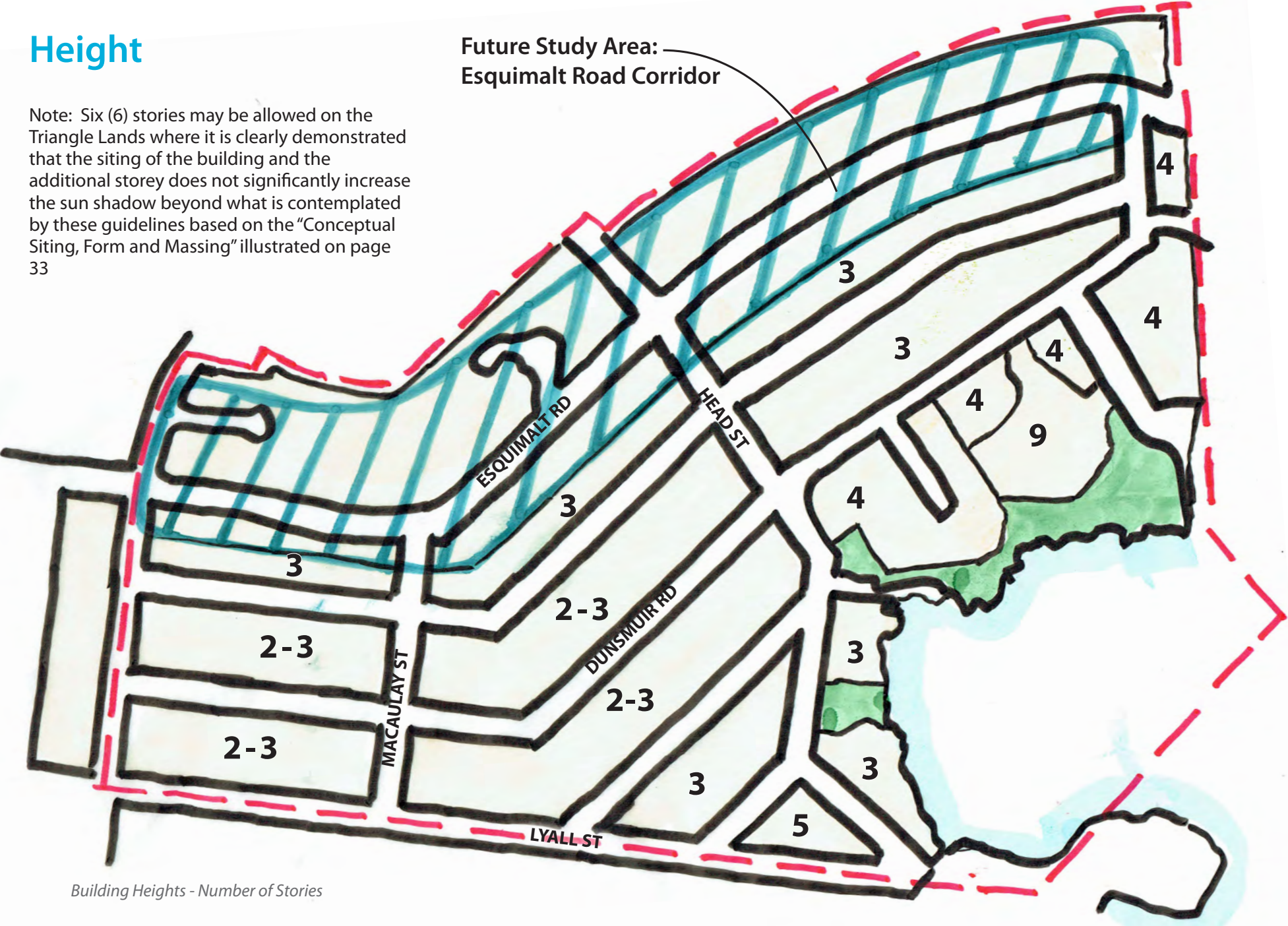
Design entrances to distinguish between individual commercial and residential.

- Locate and design entrances to create building identity and to distinguish between individual commercial and/or residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
- Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
- Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
- Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - » Are vertically proportioned and approximately the size and proportion of a traditional window
 - » Include substantial trim or molding
 - » Are separated from adjacent windows by a vertical element
 - » Are made up of small panes of glass
 - » Are separated with moldings or jambs but grouped together to form larger areas of glazing
- The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
- In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
- Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.

Height

Note: Six (6) stories may be allowed on the Triangle Lands where it is clearly demonstrated that the siting of the building and the additional storey does not significantly increase the sun shadow beyond what is contemplated by these guidelines based on the "Conceptual Siting, Form and Massing" illustrated on page 33

Future Study Area:
Esquimalt Road Corridor



Building Heights - Number of Stories

Green Healthy Buildings & Open Spaces

- Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.
- Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
- Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.

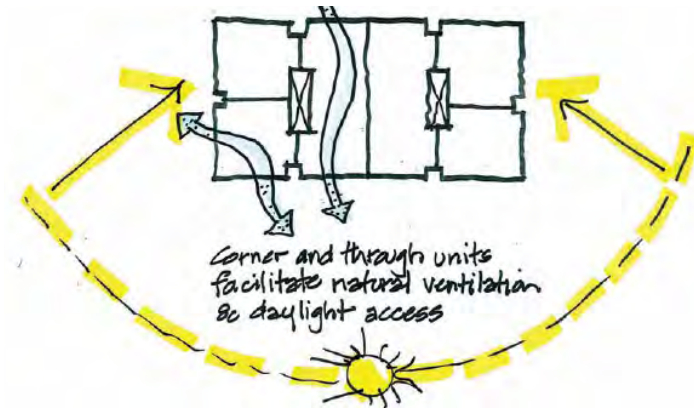
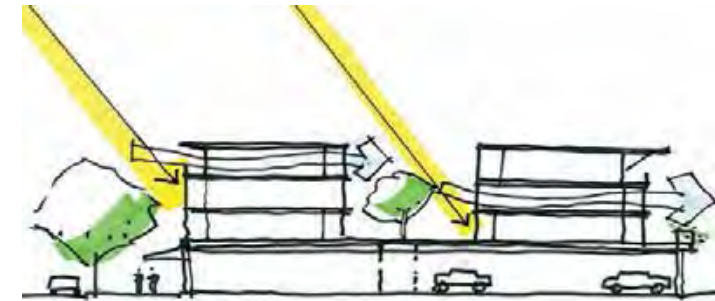
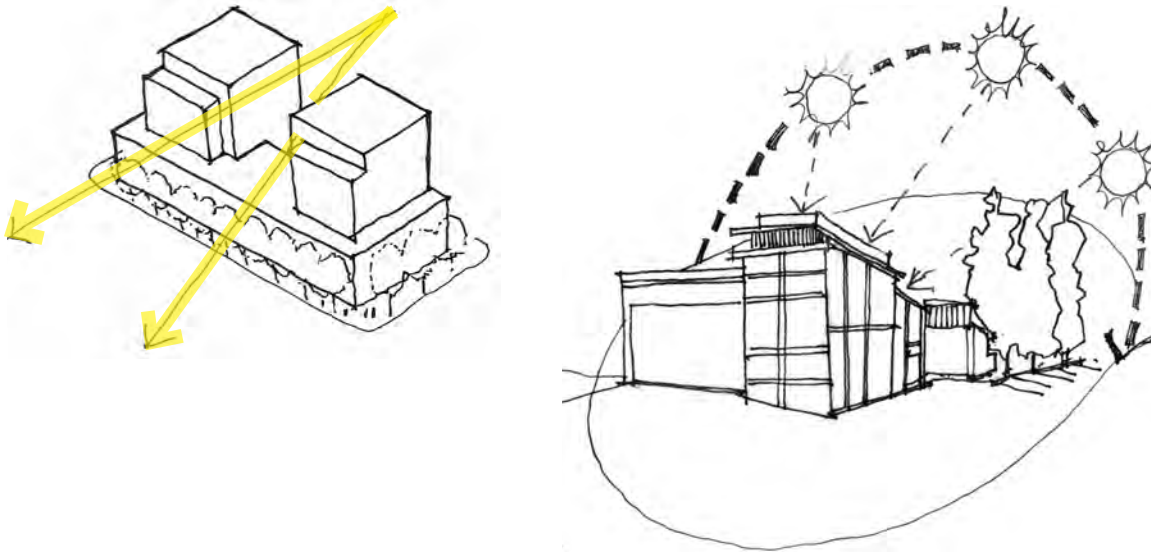


Photo: Joaquin Karakas

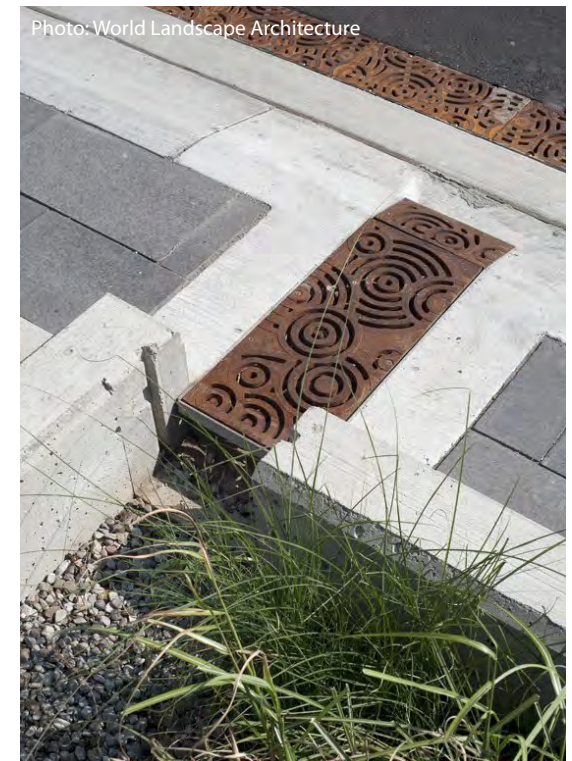
Incorporate common areas as defining elements of projects.



A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.



- New buildings should not block significant views or solar access to adjacent buildings and open spaces.
- Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
- Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
- Retention and infiltration best management practices for rainwater should be used as appropriate.
- Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.



Use best management practices for rainwater where appropriate.

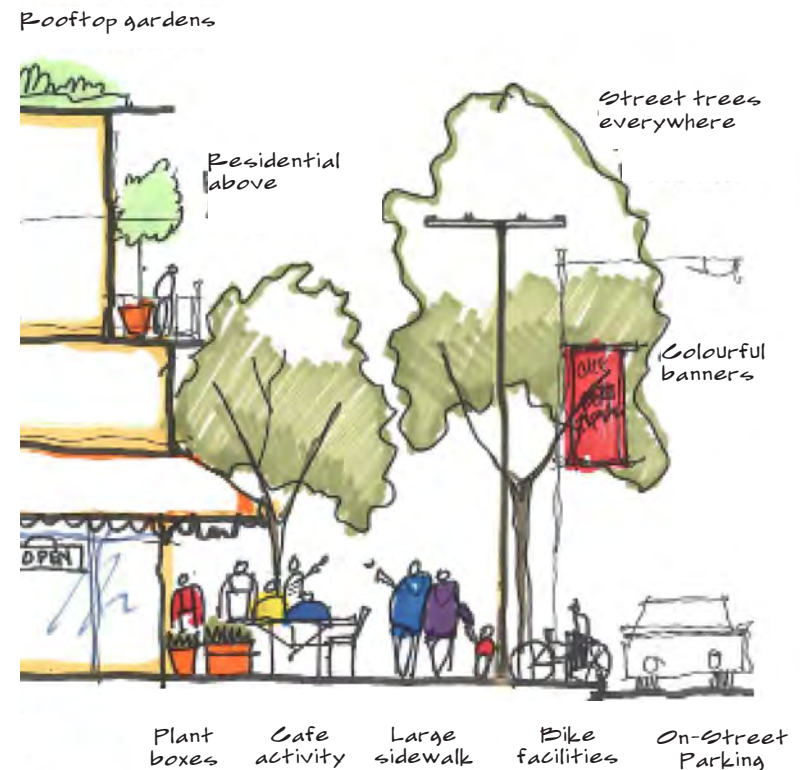
Public Realm Design: Pedestrian Comfort & Safety

INTENT

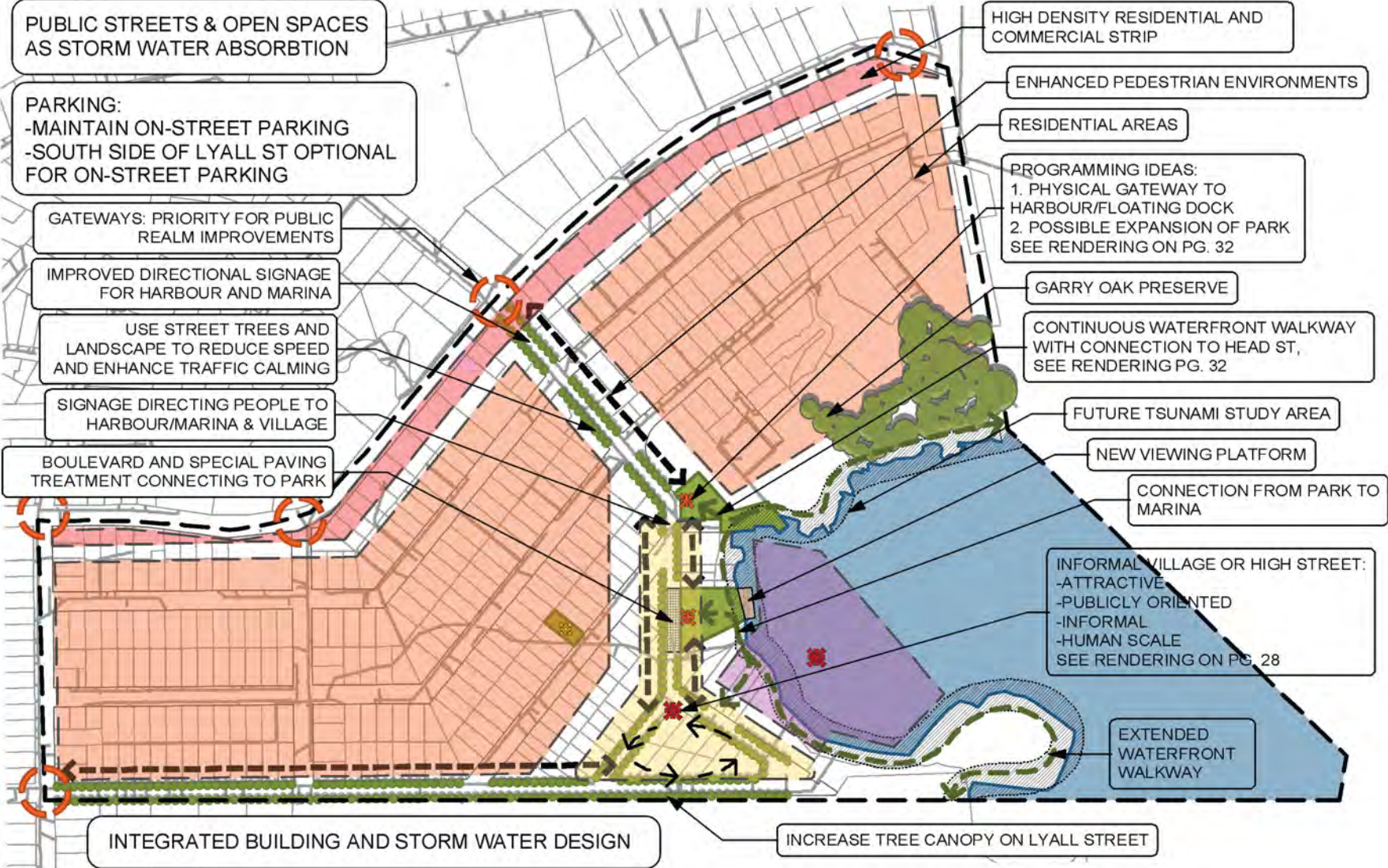
To ensure that the design of streets and open spaces creates visual interest, comfort, and safety for pedestrians and contributes to a unique local identity and sense of place for West Bay.

GUIDELINES

- Provide a continuous planting of street trees along both sides of neighbourhood streets, with priority for the Head Street South Village High Street and Esquimalt Road as indicated in the Recommended Public Realm Improvements map on page 25.
- Provide zebra- or ladder-painted crosswalks, or crosswalks made of special paving materials, at all key pedestrian crossings to increase driver awareness.
- Incorporate corner bulges into streetscape design in key pedestrian areas to enhance pedestrian crossings and provide space for landscaping, seating, rain gardens and public art.
- Provide adequate public streetscape amenities including benches, planters, garbage receptacles, bike racks, and distinctive bus shelters in areas with high pedestrian activity.
- Café tables are permitted and encouraged on public sidewalks in retail areas
- Pedestrian-oriented lighting should be provided with priority for high-activity pedestrian areas such as along Head Street and Esquimalt Road.
- Small plazas and squares should be incorporated in prominent locations with good solar gain, such as at Lyall and Head Street.
- Public art should be incorporated into parks, plazas and other key activity areas, especially at gateways and entry points, to enhance the sense of identity and entry into the neighbourhood.



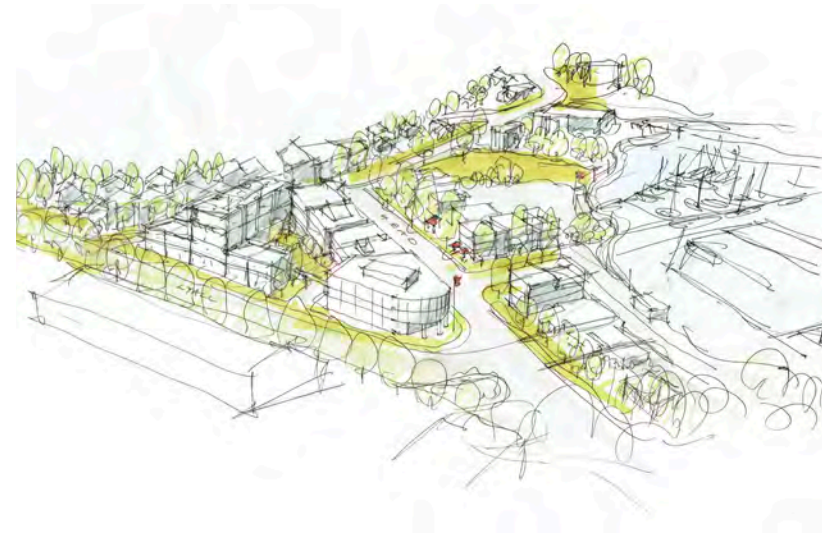
Recommended Public Realm Improvements



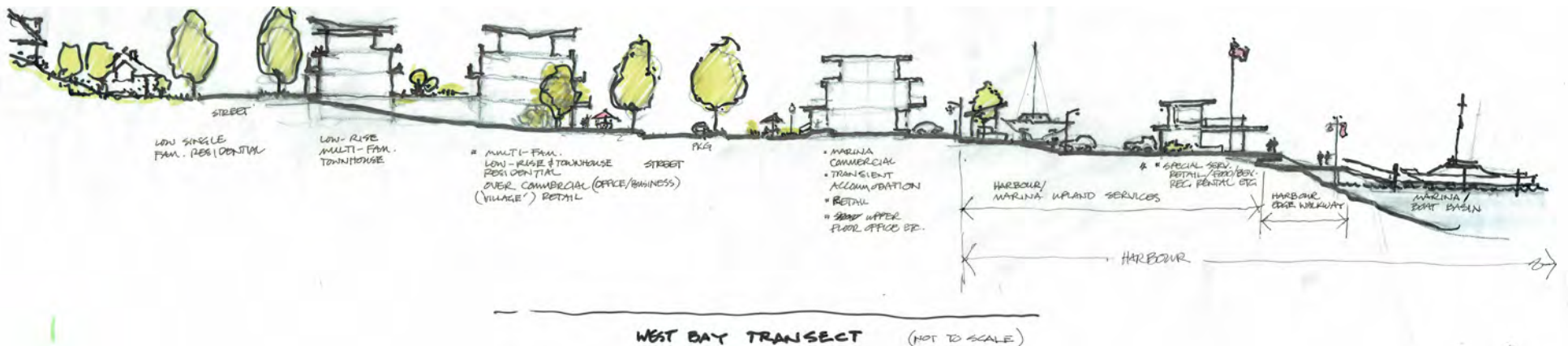
4. Site Specific Guidelines: Harbourfront Urban Village High Street

An important objective in the development of the design guidelines to address local concerns about density, height and massing, shadowing and view impacts, as well as the interface between federal lands and local ecosystems, specifically, in the harbourfront urban village area centred on Head Street South between Lyall and Paradise Streets. To this end, these guidelines were developed with input and feedback from neighbourhood stakeholders. This included an integrated design workshop (charrette) where design concepts and approaches for both public and private realms were developed. The sketch examples on this page were produced during this charrette.

This section is intended to provide site planning and design guidance and recommendations for this part of the West Bay Neighbourhood, based on stakeholder input. It is important to note that the concepts and illustrations on the following pages do not represent a development proposal, but rather a conceptual implementation of the preceding West Bay Neighbourhood Design Vision, Principles and Guidelines.



Sketches from the West Bay Design Workshop (above and below)



URBAN VILLAGE HIGH STREET LOOKING NORTH-WEST AT CORNER OF HEAD AND LYALL STREET

Maximum 5 storey building height



Urban Village high street defined by low scale buildings with active fronts and lush street tree canopy

Harbourfront retail, food and beverage uses

New public connection to harbourfront and Hidden Harbour Marina

Landmark corner plaza located at south gateway to urban village with views to harbourfront

URBAN VILLAGE HIGH STREET (HEAD STREET SOUTH) LOOKING SOUTH

Street trees, pedestrian oriented lighting and banners line either side of the street.

Continuous planting of street trees along south-west side of Head Street

Maximum 3 storey street wall along Head Street south

A rhythm of pedestrian oriented shop fronts and entryways line the street with sidewalk cafes and other uses spilling out onto the sidewalk

Landscaped bulb-outs and parking bays on north-east side of Head Street



Marked (zebra striped) pedestrian crossings at Gore and Lyall Streets

LOOKING NORTH TO HARBOUR AND URBAN VILLAGE HIGH STREET

*Publicly oriented active uses
activate the harbourfront*



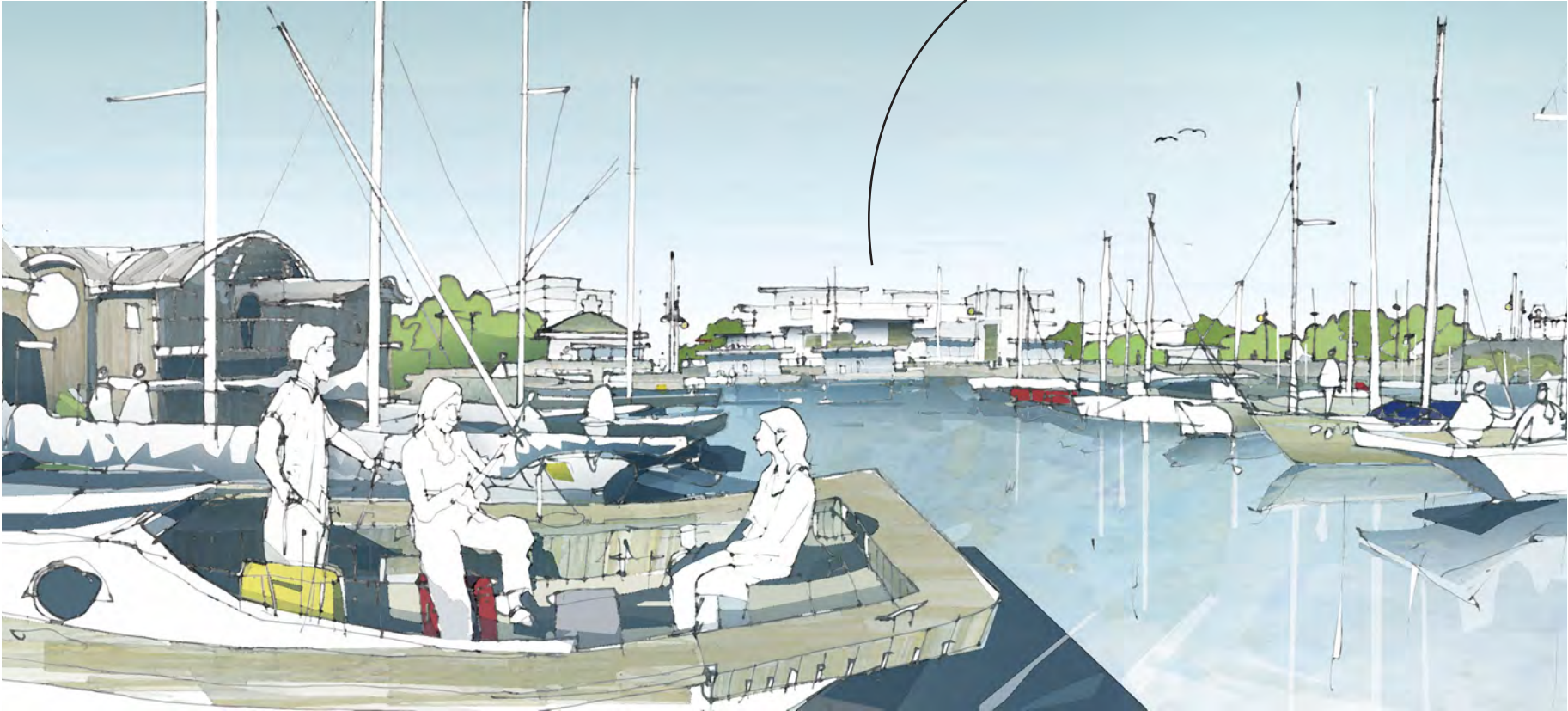
*Parking areas along the
harbourfront represent great
redevelopment opportunities where
appropriate*

*Continuous walking connecting
Songhees to West Bay Urban
village/marina and beyond*

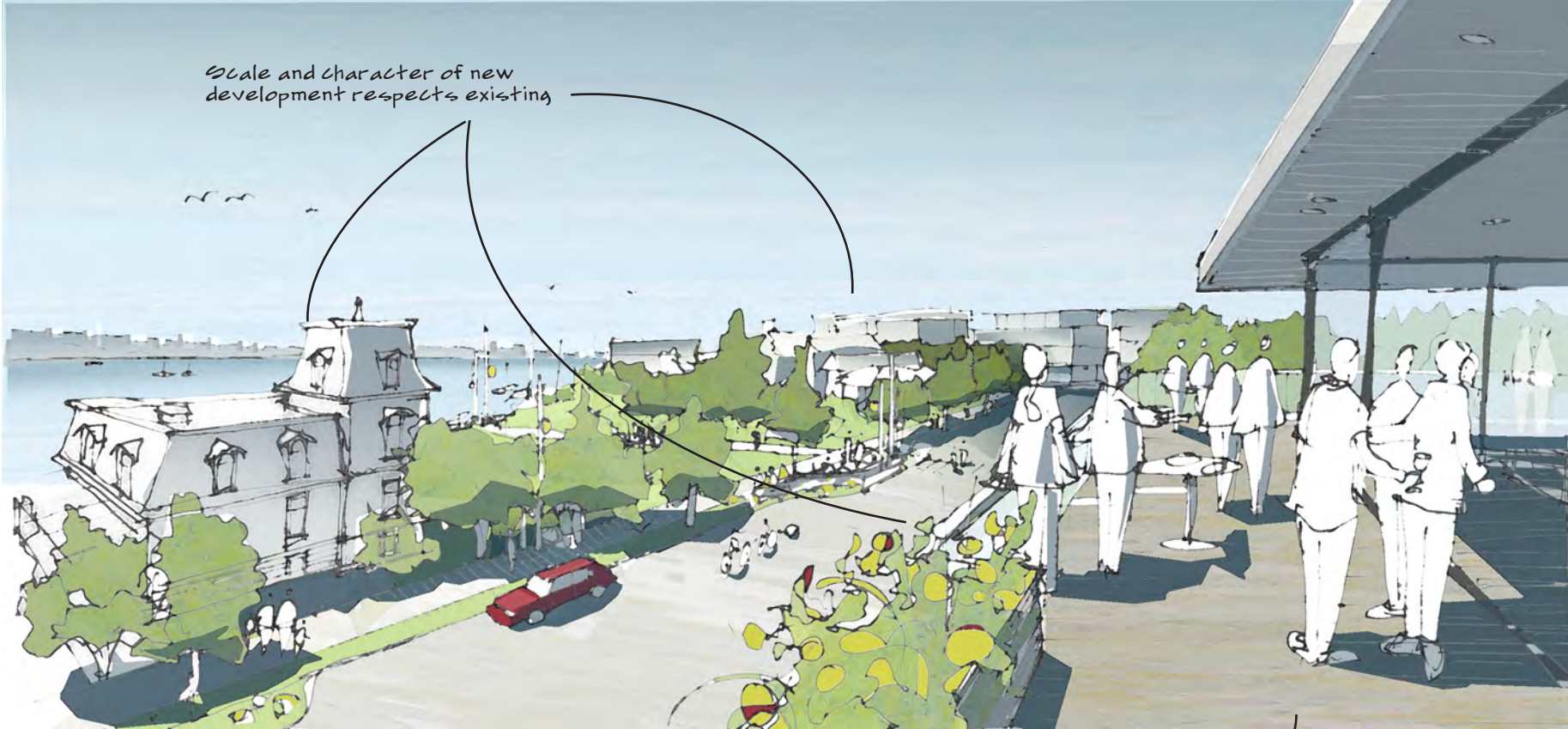
VIEW TO TOWARDS URBAN VILLAGE HIGH STREET FROM THE HARBOUR LOOKING WEST

Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity.

New development scaled to be compatible with, and not overwhelm, harbourfront character and identity. Also exhibits the transition massing from existing 7 storey on DND land.



VIEW FROM FUTURE BUILDING LOOKING SOUTH-EAST TO HARBOUR ACROSS URBAN VILLAGE HIGH STREET AND CAPTAIN JACOBSON PARK



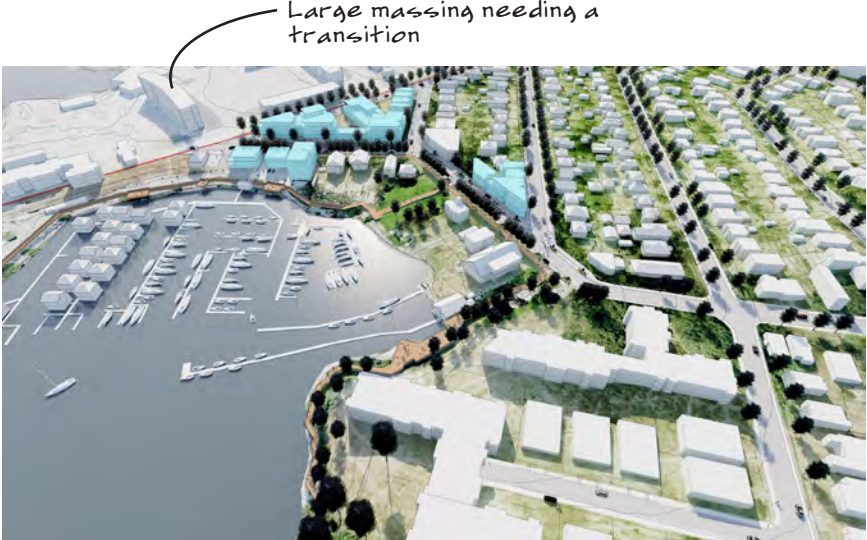
Scale and character of new development respects existing

Upper storeys step back to let sun onto urban village high street

HEAD STREET LOOKING SOUTH TOWARDS GORE AND LYALL

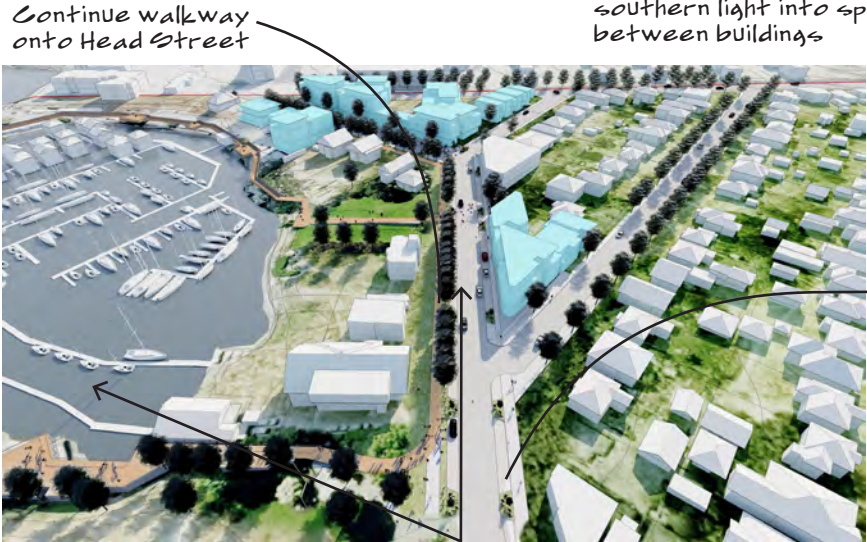


URBAN VILLAGE HIGH STREET: CONCEPTUAL SITING, FORM AND MASSING



Large massing needing a transition

- Legend**
- Conceptual massing
 - Continuous Boardwalk/Walkway



Continue walkway onto Head Street

Masses oriented to let southern light into spaces between buildings



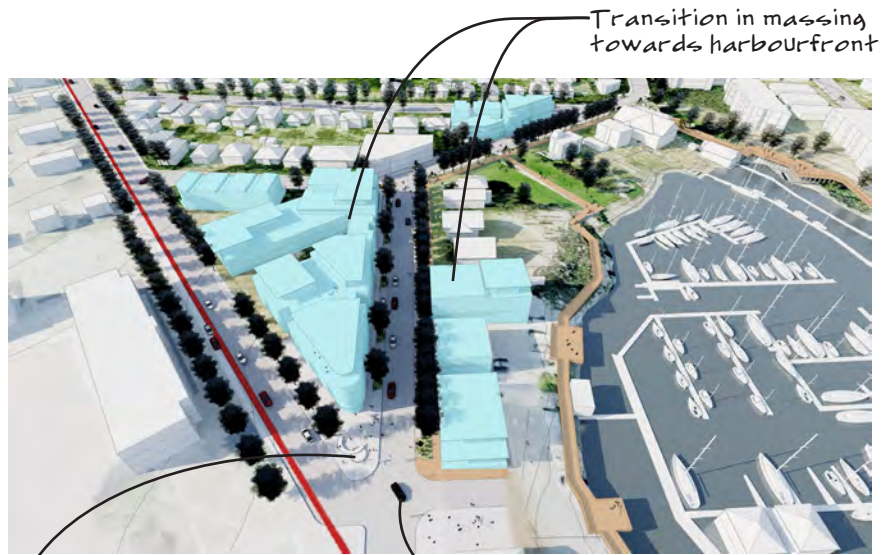
Continuous walkway

Public realm enhancements opportunity areas

New intersection

Improved view corridor

View on page 32.



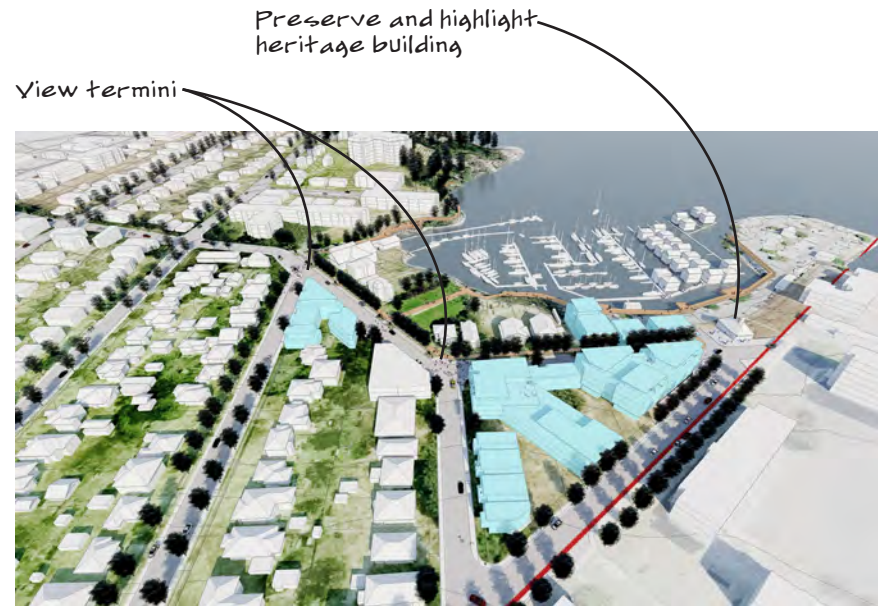
Transition in massing towards harbourfront

Landmark corner plaza located at South gateway to urban village, with views to harbourfront. See sketch on page 27.

New intersection



View on page 28



Preserve and highlight heritage building

View termini

Streetscape Guidelines

OVERVIEW

The design, construction and maintenance of streetscapes in the West Bay Neighbourhood should serve to reinforce its unique identity by incorporating:

1. Strong connection to the harbourfront;
2. A walkable neighbourhood;
3. Human scale;
4. Harbourfront ecology;
5. Harbourfront community & identity, and
6. Neighbourliness.

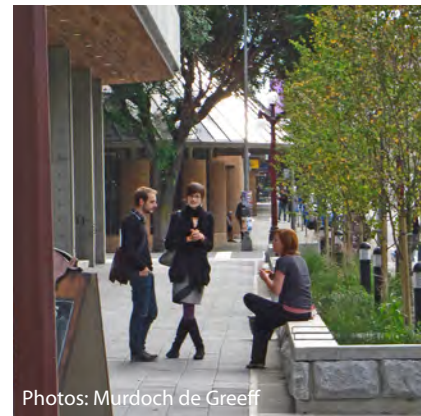
To illustrate the integration of these principles four typical streetscape sections and plans are presented below. These include Head Street North, Head Street South, Lyall Street West, and Lyall Street East. The sections focus on Public Realm improvements within two distinct areas of the West Bay Neighbourhood; West Bay Urban Village Gateways and West Bay Urban Village.

WEST BAY URBAN VILLAGE GATEWAY

West Bay Urban Village is accessible by Lyall Street and Head Street. Both streets currently have 18m wide Right of Ways (R.O.W) and accommodate two way traffic in standard 4.25m wide lanes. Objectives for the Lyall Street and Head Street Gateways include: improving the pedestrian environment, maintaining on-street parking, improved wayfinding, increased tree canopy, and integrated stormwater management. The Lyall Street West section shows a typical treatment through an existing residential lot on Lyall across from the Department of Defense (DND) lands. The Head Street North section shows a typical treatment near the intersection of Head and Esquimalt.

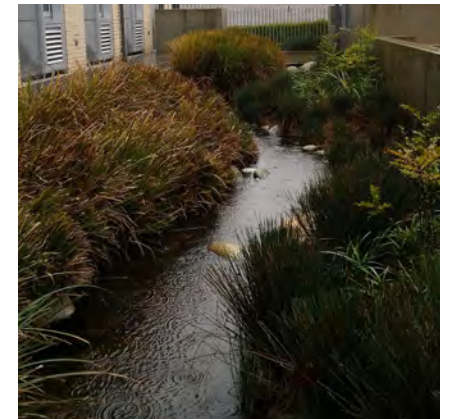
WEST BAY URBAN VILLAGE

West Bay Urban Village is located on the harbour front adjacent to the Marina. The area is identified for potential future development with a primary focus on the 'Triangle Property'. Some of the objectives for the Urban Village include: increased density, creation of places to gather, an inviting and active public realm, and reduced speed/traffic calming. Both the Lyall Street East section and Head Street South section show typical treatments at the triangle property and suggest a 'Skinny Streets' approach with reduce travel lane widths (3.0m)



Photos: Murdoch de Greeff

Integrated Storm Water Management, The Atrium building, Victoria BC



Integrated Storm Water Management, Reliable Controls building, Victoria BC

Integrated Storm Water Management

Integrate rain gardens, swales, tree planting, and other suitable BMP's to capture, slow, and treat storm water before it enters the harbour.



Photos: Murdoch de Greeff

Storm Water Plaza,
Cecelia Road, Victoria BC



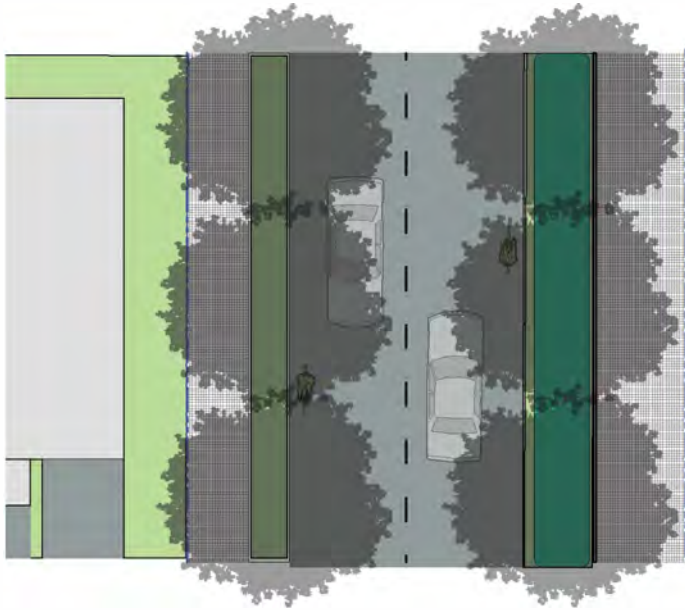
Photo: Paul de Greeff

Storm Water Streetscape,
Portland, Oregon



Photo: Murdoch de Greeff

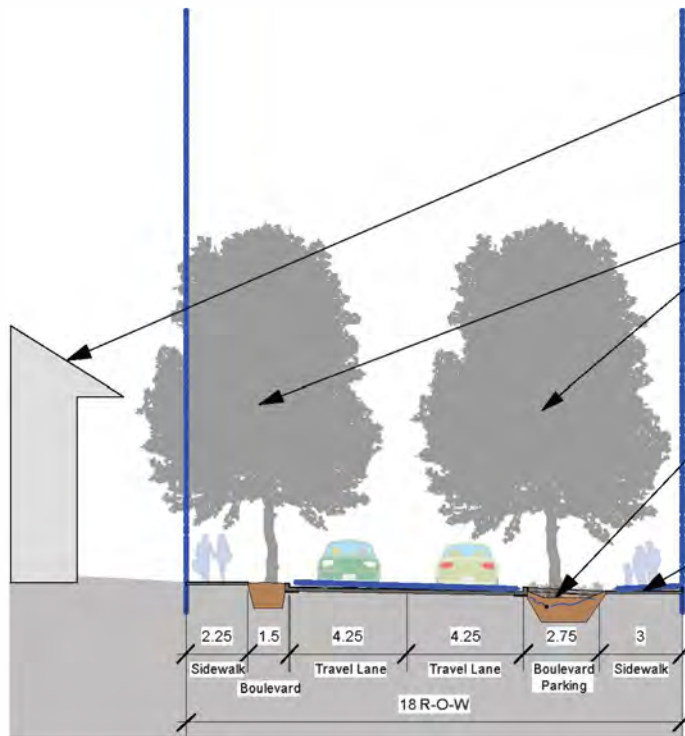
Storm Water Traffic Bulge,
Trent St., Victoria BC



Head Street North

Head street North, near the intersection of Head and Esquimalt Road, is intended to act as a Gateway to the Village, Harbour, and Marina. The intent on this section of Head is to increase the appeal of the pedestrian environment, employ clear wayfinding strategies to direct traffic towards the Village and/or Harbour/Marina.

Strategies to achieve this include: Increased sidewalk width to encourage walking, a double row of trees to accentuate Head as a linear corridor to the harbour/village, source controls such as onstreet rain gardens to treat stormwater, and similar building scales to reinforce the street end/harbour as a focal point. Development fronting onto the east side of this section of Head street should maintain physical and visual permeability to the waterfront by integrating paseos and achieving a minimal building separation of 6 metres



Neighbourliness-

Transition building heights from shoreline to upland areas to minimize shadow and view impacts.

Human Scale-

Utilize a double row of trees to accentuate the linearity of Head Street as a Gateway and the Village as a destination at the terminus.

Upland Ecology-

Install onstreet rain gardens to capture, treat and slow street run off before it enters the harbour downstream.

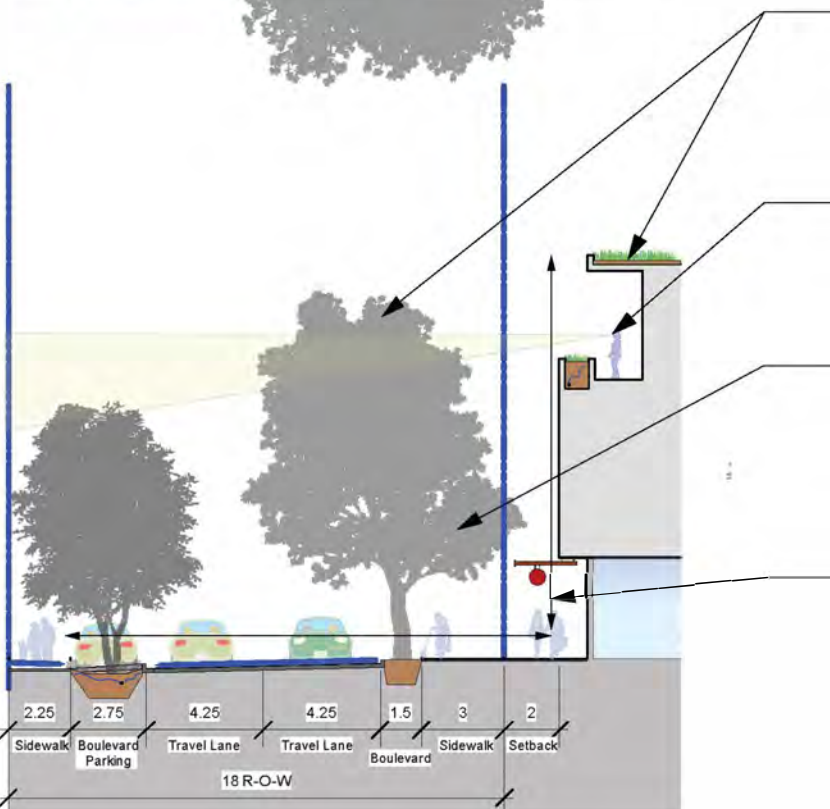
Walkable Neighbourhood-

Utilise vegetated boulevard strips to provide separation from vehicular traffic and increase sidewalk widths on gateway streets to promote wayfinding while contributing to attractive and inviting pedestrian environments.

Head Street South

Head street South, is intended to act as the communities Village or High Street. The intent on this section of Head is to provide public open spaces that contribute to a sense of being on the waterfront while supporting the commercial viability of the area. The *West Bay Village* public space should include but is not limited to waterfront access, space for gathering and festivals, and an attractive pedestrian oriented environment.

Strategies to achieve this include: *Distance to Building Height Ratios* of between 1:1 and 3:1, use of green building techniques to preserve the health and visual aesthetic of the shoreline, and transitions in scale to perserve upslope and adjacent views.



Waterfront Community and Identity-

Green Roofs, street trees and building heights support each other to reinforce the identity of West Bay as a Green Waterfront Community. Designs acknowledge views both to and from the inner harbour.

Strong Connection to the Waterfront/Neighbourliness-

Buildings step back and transition in scale to maximize potential for harbour views while minimizing shadow impacts on adjacent properties.

Waterfront Ecology-

Tree plantings intercept rainwater, reduce storm water flows through evapotranspiration, and provide valuable bird habitat contributing significantly to the shoreline ecology.

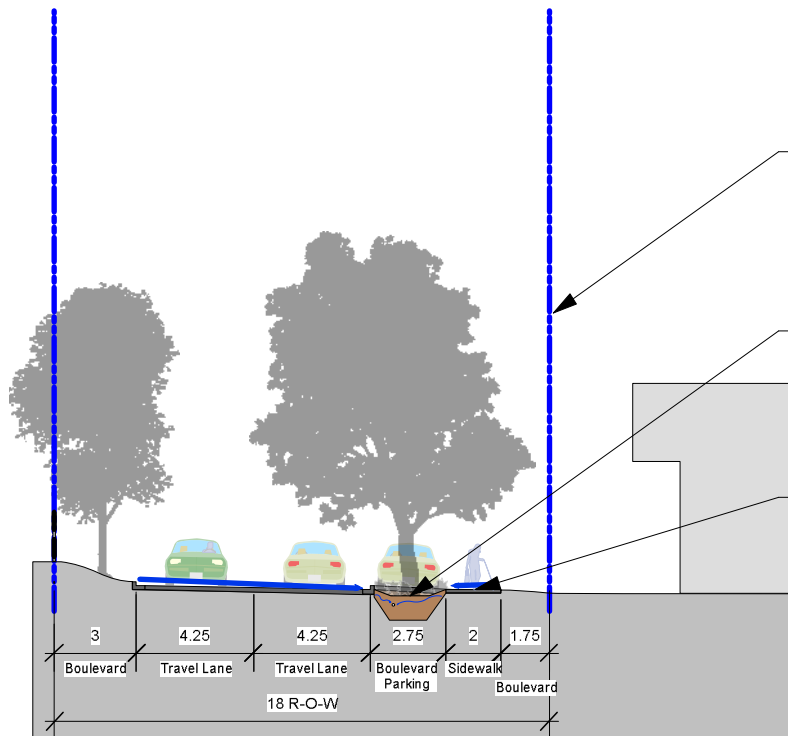
Human Scale-

Buildings should be between 1:1 and 3:1 *Distance to Building Height Ratio* to create intimate and inviting public open spaces.



Lyall Street West- Existing Residential

West Lyall Street acts as an important connection from the harbour to the surrounding residences, Esquimalt village, and the Esquimalt Rec. Center. Future developments along this corridor should focus on improving the pedestrian environment to increase the walkability of the neighbourhood. Strategies to achieve this include: Tree planting to reduce the visual scale of the street and provide shade, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour downstream, and separated boulevards to increase the attractiveness of the pedestrian environment.



Human Scale-

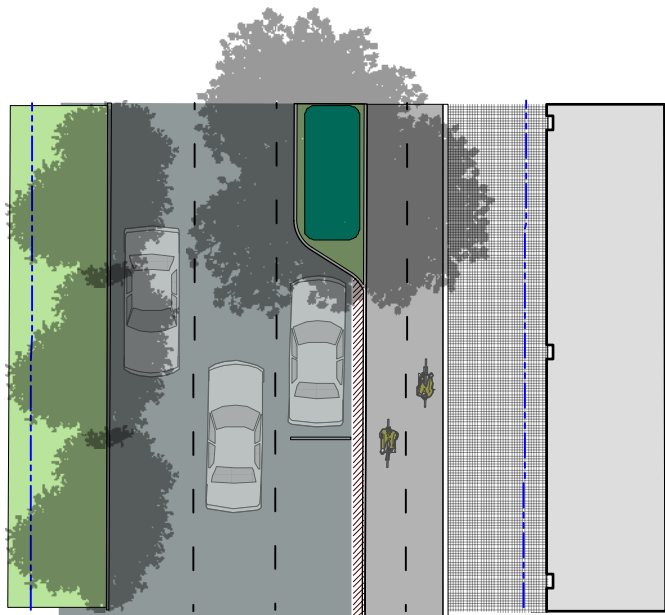
Tree planting can reduce the visual scale of the street while offering summer shade for the street and sidewalk.

Waterfront Ecology-

Integrated rain gardens to treat stormwater and provide habitat for birds and pollinators.

A Walkable Neighbourhood-

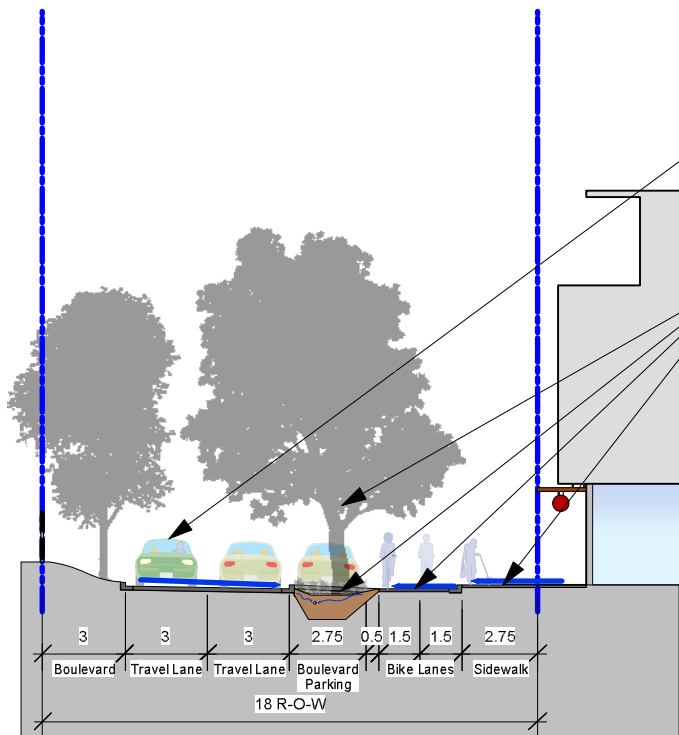
Use separated boulevards to remove sidewalks from vehicular traffic and increase the attractiveness of the pedestrian environment.



Lyall Street East- West Bay Village

East Lyall Street at the harbour interface is meant to act as part of the *West Bay Village*. The intent here is to contribute to the sense of a waterfront village while providing an attractive pedestrian environment with places for festivals and gatherings. Located adjacent to the DND lands Lyall street, in general, should focus on pedestrian improvements on the North side which is more heavily used by the public.

Strategies to achieve this include: Tree planting to reduce the visual scale of the street, vehicular travel lane reductions to provide space for bike lanes and encourage alternative modes of transportation, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour, and seperated boulevards to increase the attractiveness of the pedestrian environment.



Walkable Neighbourhood

Skinny Streets help to maximize pedestrian oriented space and allow for increased sidewalk widths as well as seperated bike lanes.

Waterfront Ecology-

Significant tree planting, green infrastructure, bike lanes, access corridors to waterfront recreation, clear wayfinding, and attractive pedestrian spaces should be layered and integrated to create a vibrant community, ecologically healthy shoreline, and healthy upland ecology.

20 January 2017

Mayor and Council
c/o Trevor Parkes, Senior Planner
Development Services
Township of Esquimalt

RE: Westbay Triangle Development Permit
Redevelopment of 468 Head Street, Westbay, Esquimalt, BC

Hillel
architecture



101 1831 Oak Bay Avenue
Victoria BC V8R 1C3

phone: 250 . 592 . 9198
fax: 250 . 592 . 9178

Mayor and Council, c/o Trevor Parkes, Senior Planner

It is a pleasure for Hillel Architecture to enclose herein a Development Permit Application for the Westbay Triangle for the following independent land titles being amalgamated under this Development proposal into a single land holding:

468 Head Street [Triangle Estates],

PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];

PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];

PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];

PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];

PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];

PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];

PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];

PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street];

and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].

Enclosures

3 copies 24x36" copy of The Triangle at Westbay

1 copy 11x17" copy of The Triangle at Westbay

digital copies of same

digital copies of a Green Building Checklist (original submission reproduced)

digital copies of this DP application cover letter.



Hillel Architecture was retained in August of 2014 to chart a new course for the Development of the Westbay Triangle. Hillel Architecture engaged in a comprehensive community consultation process, involving neighbouring residents, Esquimalt Planning and Development staff, Esquimalt Engineering staff, the Esquimalt Chamber of Commerce, and numerous one on one interviews which defined an outcome for the Westbay Triangle as a whole. This consultation process resulted in a program document and an illustration of a concept recorded in our Westbay Triangle Development Guidelines. From this body of work, and within that consultation process, a rezoning and development proposal was illustrated for one portion of the land holdings known as the Westbay Triangle, and partially developed for the remaining half to be submitted at a later date, now known as Westbay Marinaview.

Westbay Triangle Rezoning Application

The Rezoning Application received broad based support from residents, local businesses, local off shore residents and patrons of the Westbay Marina, Esquimalt Planning and Economic Development Staff. At that same time Esquimalt engaged in a separate and independent neighbourhood consultation to create the Westbay Community Development Guidelines for the community as a whole. As a new document, it was expected that the Westbay Triangle Development Proposal would also conform to that independently developed guideline. Planning Staff noted in their project analysis that the Rezoning Submission did conform to that document expressing the desires of the neighbourhood.

Through its own consultation with Neighbours, Esquimalt Staff, and later with the members of APC, DRC, the proposal for the Westbay Triangle evolved subtly, each time improving, and each time not wavering from its original form as developed with, and supported by those earlier participants: the neighbours. The Westbay Triangle went on to receive its Public Hearing in the summer of 2016, without one voice of objection.

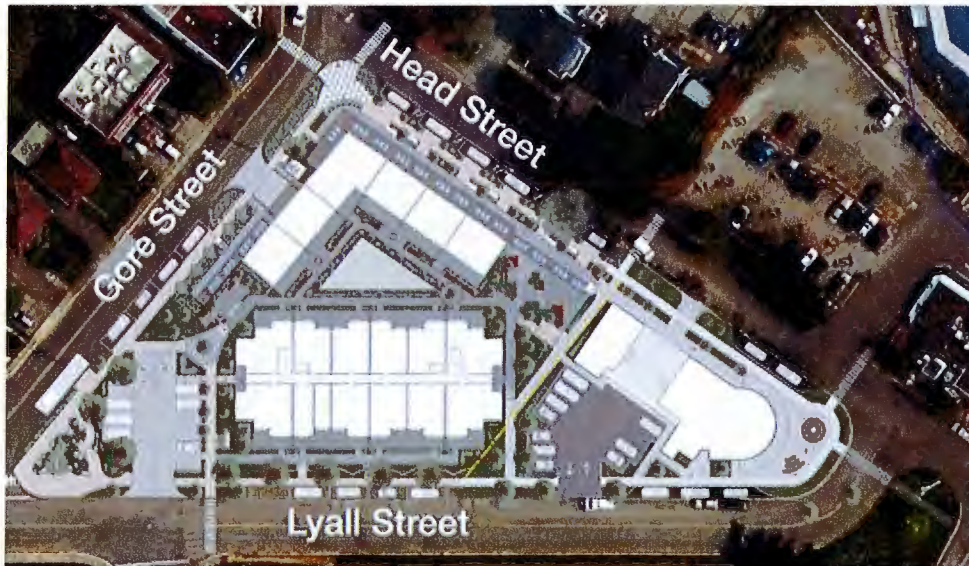
As that Rezoning successfully approached its conclusion, the second half of that same land holding, known as Westbay Marinaview, was developed further to meet the requirements of its own independent rezoning application. This submission is in keeping with - and conforming with - those same principals established in the Westbay Community Development Guidelines produced by Esquimalt, and through Hillel Architecture's own consultation process. The owners of Westbay Marinaview applied for this rezoning in January 2017.



Westbay Triangle Development Permit Defining Character and Role

The Westbay Triangle Development Permit application drawings present a building unchanged from its initial rezoning application drawings. It was paramount to the consultant and the owners, that the building submitted for Development Permit honoured that broad community support, staff support, respected that it received thoughtful review from the DRC, Esquimalt Staff, and concluded with Council approval at its rezoning stage.

The Triangle also can now be reviewed with the total composition for the Triangle Lands. Marinaview and the Triangle play differing roles in the definition of the new Westbay Village core. The Triangle defined the dynamic village centre. The approximate length of this village core aligns with the locations of heritage style buildings on the opposing side of this village streetscape.



The project's most visible corner, that at the intersection of Gore and Head Street - the new "main street" - is designed to stimulate growth along this portion of Head Street, and re-invigorate, re-energize these buildings and businesses further along Head Street towards Captain Jacobson Park and onwards to the terminus of the Songhees Walkway. This public space, with its integral compass rose, is intended to attract pedestrians, provide seating and visual cues this this is the center of the village core.

Subtle design cues create a perception that the village setting does not extend up Gore Street. The exterior stairwell, the vehicle entry, and some significant planting beds serve to protect Gore from the dynamics of the village propagating up Gore Street. These aid in keeping this a more quiet residential side street as it is preferred.

Further along Head street towards the Marina, the character changes. The village setting captured between the Triangle and the existing heritage style buildings comes to a natural conclusion at the transition between the Triangle and Marinaview.

In the Triangle's village area the focus is internal, on the dynamics of a shopping setting. People watching. Pedestrians shopping. Socializing and perusing. Interesting buildings. Interesting people. Interesting things. The character changes to one of a more open view towards the water. An ocean view. A Marina view. This is not the end of the village, it is a change in character and perspective.

As you walk past the Triangle, the shopping character changes, canopies lower in scale, and projection lessens. In front of the second phase of the Triangle lands the enclosed village evolves into the perspective of open, bright, ocean views and marina views. This is the home of Marinaview. Marinaview is slightly more modern, a little more crisp, its finishes slightly more refined in tone. Where the Triangle needs to be comfortable with the existing heritage style buildings, Marinaview adds another layer of fit and finish and relates to the open sightlines, the modern lines of boats, the decidedly more contemporary float homes visible in the background. Although further description of Marinaview's roles should not be outlined here. It is important that the reader know, that the same level of thought that defined the character and finishes of the triangle are equally at work in Marinaview, only now just entering its rezoning phase; the younger sibling. Its more contemporary lines and use of materials permits the next contributions to the neighbourhood's composition to be truly modern if that is those designers intent. The stage has been set.

Defining Site and Setting

The total composition, of both Phase I; the Triangle, and phase II; Marinaview contribute three distinctly differing corner public spaces at the road intersections. The dynamic center village intersection of Gore and Head Street with its very public setting combined with people, seating, shop front displays, and attractive landscaping. The sites and sounds of the village core.



Along Head Street the village setting at Lyall has a bright open public space, with long sightlines over the water, plants and seating in the open all day long, sun washed, public social space. Here another food and beverage company gets to offer a fuller menu, a licensed eatery, for longer conversations. This portion developed within Westbay's Marinaview



Away from the water, further up Lyall Street, the Triangle concludes with a quiet, restful, lush garden setting encouraging of quieter reading, quieter conversation, and protection from the on shore breezes. Here the location is a quiet residential neighbourhood. Here the last orchard tree of the original local farm is protected, for it to serve out its years in an organic composition and natural setting.



Along each of these three street fronts, each street has its own character respected, each is deserving of being a front yard, and each has a substantial front yard setback now incorporated in the zoning description. These set backs provide the physical distance to achieve an outdoor dynamic market place one side, privacy protected residences on the other two street fronts. Each generously landscaped to suit their differing roles and characters



The greenspaces

The Triangle design creates three distinct elevated private greenspaces, 3 differing characters, distributed to permit some in sun and some in shade at all times.

One with views out over the water and suitable for larger social gatherings;



One breeze protected and internal in focus, linear in nature with small seating elements; and finally,



one overlooking a quiet residential community with a more plant oriented, green and natural setting. Surrounding the property, planting areas contribute purposefully differing settings.



There are areas designed for the enjoyment of people and socializing, while others exclude people to increase privacy, increase the abundance of, and diversity of flora and fauna around the Triangle

Defining a neighbourhood

The Triangle offers a range of residential unit sizes, views, and features. Residences are designed to suit a range of incomes, a range of lifestyles, a range of family sizes, and to suit a range in ages. This diversity aids in creating a full neighbourhood, a complete community. This extends to the business units that are small in scale, ensuring their shop fronts match the scale of those already present, and their stage set invigorates the streetscape. The outside realm encourages uses, permits retail product display and / or patrons seating, all essential to a dynamic village setting.

Architectural Expression – The village

A palette of materials, and building details have been considered that define the character and aesthetics of the Triangle, both the village setting, and above, for the residences. The materials are used in a manner that is honest, practical, and consistent with hardworking marine buildings along our coastlines.

On the ground floor the doors and entryways of the commercial shop fronts are serviced by openings configured to serve this location appropriately: pedestrian doors for patrons, and larger sliding wood door assemblies permitting the passage of larger retail items, and for openness on fair weather days. These larger wood panel, glazed doors are defined with surface mounted hardware of a scale appropriate to these marine based buildings. The DRC could discuss the use of black framing to these commercial openings, which would also differentiate the commercial building portion from its lighter residential character above.



Overhead canopies, significant in span (3m), and scale, protect outdoor display and furnishings space. That interstitial space between the public realm and the shop itself. A dynamic multi-use space providing protection from heat, sun, rain, sound from travelling vertically to residences above, and to define that stage setting for the shop front. These canopies change in scale and span to communicate when the village setting is changing as you approach Marinaview along Head Street.



The village core features real stone column facings, heavy timber bracketing supporting exposed heavy timber framing above. Solid wood decking is visible on the underside of the canopy deck, and cedar siding finishes the upper portion of the shop front wall. This also ensures that wood finishes mostly removed from the affects of rain, increasing their longevity, and minimizing their maintenance.



Steel detailing, structural supports, and exposed structural brackets are fashioned from hot dipped galvanized steel for a maintenance free, finish free, industrial aesthetic. This is matched with light fixture enclosures and mountings.

Whereas in the finer detail of signage and its mounting, lettering, and building addressing, these items can be marine fittings and with finishes appropriate to the marine industry and including polished stainless steel.

Door hardware and hinges that occupy the scale between these two extremes are chosen on grounds of robust design, and corrosion free materials, and forged steel aesthetic.



Architectural Expression – The Residences

On the upper floors, the residential building layers are enclosed with the same materials executed in a lighter, more contemporary form. Where the Shop fronts below define an industrial like character rich village core at home with heritage style buildings of the streetscape, the upper floors communicate a slightly more contemporary form. Not modern, but sufficient to permit the next buildings added to this community to be so. The Triangle bridges that character change. The Triangle forever being the center of the village core, and the older sibling. The Lyall Street frontage of the residential building contains similar structural members, in a similar but more refined surface and scale. Here clear anodized framing defines entries and window systems.



Spandrel glass in appearance, glazed panels between windows aid in breaking exterior finishes into pairs of floors, as does stucco, and stone appointments – each running in differing twin storey arrangements. Together the materials brighten with height, lightening the visual nature of the building, lessening the perception of individual floors. The residential building concludes with a well setback, less visible penthouse floor.

This penthouse, when visible from distance is a distinct richer colour palette, complete with its own significant canopies from overhanging cedar tone finished soffits. Here where distance permits, and codes require, surfaces are non-combustible finishes appearing as cedar from below and matching in tone and character actual cedar used in the lower stories of the village level. The overhangs of canopies in this upper most floor tie the character to that of the lowest floors.

Summary

The enclosed building conforms with statements within Comprehensive Development District No. 98 [CD NO. 98] without variances. The development permit application is an extension of the rezoning process without building redefinition to suit a new preference or new intent. The Building represents that which was previewed by the neighbourhood, and in commitment to that neighbourhood's support, it is submitted here unchanged in form, character, or materials.

Throughout the rezoning process of this project, these building elements have received thoughtful commentary from the DRC, Planning, members of the Chamber of Commerce and those members of the community, and support by Mayor and Council. This submission honours that input and the rezoning that resulted. The drawings being only being more comprehensively labelled as is required by an escalating level of detail on its way towards construction documents.

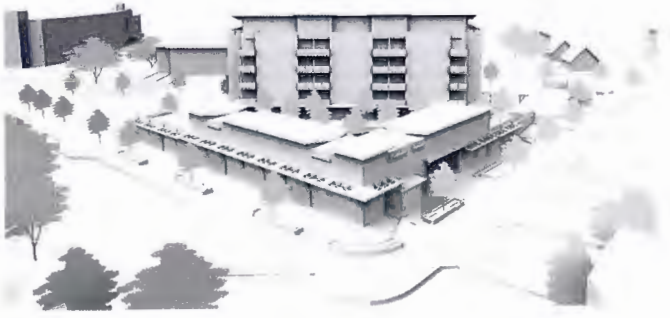
Regards
Peter Hardcastle

Hillel Architecture Inc.



Page 8 of 8

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 OF ESQUIMALT
 DEVELOPMENT SERVICES



DRAWING LIST

Architectural Drawings Enclosed

- A1.1 Existing Site and Site Data
- A1.2 Graphic Project Summary
- A1.3 Site Development Layout
- A1.4 Site Development Finishes
- A1.5 Graphic Summary of Setbacks

- A2.0 Underground Parkade Layout
- A2.1 Bldg 1 - Ground Floor @ Head Street Layout
- A2.2 Bldg 1 - Ground Floor @ Head Street Finishes
- A2.3 Bldg 1 & 2 Ground Floor @ Lyall Street Layout
- A2.4 Bldg 1 & 2 Ground Floor @ Lyall Street Finishes
- A2.5 Bldg 2 Floors 2nd through 5th floors
- A2.6 Bldg 2 Penthouse (Sixth) Floor Plan

- A2.7 Building 1 - Suite Floor Plans
- A2.8 Building 1 - Suite Floor Plans
- A2.9 Building 2 - Suite Floor Plans
- A2.10 Building 2 - Suite Floor Plans
- A2.11 Building 2 - Suite Floor Plans

- A3.0 Proposed Project Perspectives
- A3.1 Neighbourhood Perspectives
- A3.2 Building 1 - Head Street (northeast) & Gore Street (northwest) Elevations
- A3.3 Building 1 - (southeast) & (southwest) Elevations
- A3.4 Building 1 - Detail Elevations and Finishes

- A3.5 Building 2 - Lyall Street (south) Elevation
- A3.6 Building 2 - North Elevations
- A3.7 Building 2 - Gore Street (west) & East Elevations
- A3.8 Building 2 - Detail Elevations and Finishes

- A4.1 Building 1 & 2 - Section Facing (northwest) Gore Street
- A4.2 Building 1 - Section Facing (south) Lyall Street
- A4.3 Building 2 - Section Facing (northeast) Head Street

- A4.4 Building 1 - Colour & Material Board
- A4.5 Building 2 - Colour & Material Board

Landscape Drawings Enclosed

- L1 Landscape Concept
- L2 Landscape Sections & Planting

CONTACT LIST

Prime Consultant & Architect of Record
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 Fax: (250) 592.9178
 Email: peter@hillelarch.ca

Property Owner
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 7081 Central Saanich Road, Victoria, British Columbia, Canada V8M 1Y3
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 Fax: (250) 383.8585
 Email: mark.lindholm@lindholmiland.ca

Landscape Architect
Small & Rossell Landscape Architects Inc.
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 Phone: (250) 250-642-6967
 Email: carole@smallandrossell.com

THE TRIANGLE AT WESTBAY

ESQUIMALT, BRITISH COLUMBIA



1 Existing Site
A1.1 Scale 1:500

CIVIC ADDRESS

CIVIC ADDRESS
472, 470, 768 Head Street 515, 509 Gore Street 922, 920, 918, 912 Lyall Street.
Please note; 464, 460 Head Street are not included in this submission. Separate ownership.

PROJECT DATA

PROPOSED PROJECT

ZONING:

DENSITY

Site Area (m²) 4,075m² (43,865 R2)
Lot Coverage % 54.8% (2235.3 m² measured to outside face, buildings & all canopies)
63% (2572.7m² as measured above and including central park)
Floor Space Ratio 1.34 (6420 m² developed / 4075 site m² / January 2017)
Building Footprint Building 1 and Building 2 combined: 2235.3 m²
• areas based off exterior face of exterior walls
• includes all exterior decks, canopies, & cantilevers
Building 1 and Building 2 combined: 2572.7 m²
• includes all noted above plus interior elevated parks / terraces
Total Floor Area Building 1: Equimall definition for floor space ratio calculations
Ground Floor: 692.1 m²
2nd Floor: 400.0 m²
Total Floor Area: 1,092.1m²
See diagrams A1.2

Building 1 Ground Floor breakdown: G101: 62m² cl / 59.5m² inside Group C, D (live work) 2 bedroom residential suite
CRU1: 102m² cl / 97m² inside Group A, Group D, Group E
CRU2: 62m² cl / 59.5m² inside Group D, Group E
CRU3: 62m² cl / 59.5m² inside Group D, Group E
CRU4: 62m² cl / 59.5m² inside Group D, Group E
CRU5: 62m² cl / 59.5m² inside Group D, Group E, Group C Live or work unit
CRU6: 62m² cl / 59.5m² inside Group D, Group E, Group C Live or work unit
Building 1 Second Floor breakdown: P201: 59.5m² cl / 59.5m² inside Group C, Group D
P202: 97m² cl / 59.5m² inside Group C, Group D
P203: 97m² cl / 59.5m² inside Group C, Group D
P204: 97m² cl / 59.5m² inside Group C, Group D
P205: 97m² cl / 59.5m² inside Group C, Group D, Live or work unit

Building 2:
Level 1 714.8m²
Level 2 - 5 756.7m² Each of 4 floors
Level 6 565.9m²

Total Floor Area: 4327.5 m²
(areas based off definition: inside face of exterior walls + deductions)

Maximum Height Building 1: 21.75m ± above average grade 7.4m (7.3875m geodetic)
Building 2: 7.6m ± above average grade 7.4m (7.3875m geodetic)
Maximum Storeys Building 1: 2 storeys
Building 2: 6 storeys

SITE SETBACKS

Head Street +2.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 38
Drawings demonstrate ±5.8m to bldg face (±6m to grid)
Gore Street -illustrated similar to 2.0m page 38 of development guidelines
Drawings demonstrate ±5.8m to bldg face (±6m to grid), 3.0m to Exit Stairs
Lynn Street -1.0 m illustrated by Westbay Neighbourhood Development Guidelines, pg 40
Drawings demonstrate ±5.8m
Interior Side Yard -0.5m demonstrated to internal sideyard

RESIDENTIAL UNITS

No. of Residential Units **73 total residential units**
Building 1:
Ground Floor: 2 units Level 6.2m facing Gore Street
2nd Floor: 6 units Level 9.2m facing Park Terrace
8 total
Building 2:
Ground Floor: 11 units
2nd Floor: 12 units
3rd Floor: 12 units
4th Floor: 12 units
5th Floor: 12 units
6th Floor: 6 units
65 total
Building 1:
8 - two bedroom residential units
Building 2:
17 - two bedroom residential units
46 - one bedroom residential units

LEGAL ADDRESS

468 Head Street (Triangle Estates),
PID 001-843-891 Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (468 Head Street);
PID 009-720-430, Lot 5, Block H, Section 11, Esquimalt District, Plan 252 (470 Head Street);
PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 (472 Head Street);
PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 (515 Gore Street);
PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 (509 Gore Street);
PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (922 Lyall Street);
PID 000-704-588, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (920 Lyall Street);
PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (918 Lyall Street);
and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 (912 Lyall Street).

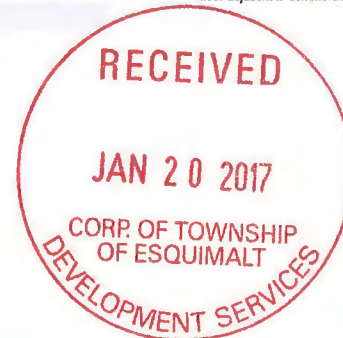
PARKING (assignment of parking allocations shown sheet A2.0)

parking stalls on site
CRU 1 @ 14m² / 1 stall / 5 seats 12 stalls (inside + outside seating)
CRU 2 @ 25m² 2.38 stalls (highest parking use assumed)
CRU 3 @ 25m² 2.38 stalls (highest parking use assumed)
CRU 4 @ 25m² 2.38 stalls (highest parking use assumed)
CRU 5 @ 25m² 2.38 stalls (highest parking use assumed)
CRU 6 @ 25m² 2.38 stalls (highest parking use assumed)
Total commercial: 23.9 stalls
Parking Bylaw Standards referenced:
Cafe @ 14m² or 5 seats per stall
Grocer @ 35m²
Retail @ 25m²
Office @ 30m²
15% of required commercial parking can be met by use of residential guest parking stalls during business hours
Residential 1.3 each 25% to be assigned as visitors stalls
Total Residential 73 x 1.3 each: 94.9 stalls req'd - 97 stalls provided.
Residential Guest Parking 25% assigned as visitors - 24 stalls
2 spaces in excess of Bylaw
Total Project provided: 119 stalls provided (5 in excess of Bylaw requirements)
(assignment of parking allocations shown sheet A2.0)

Commercial Loading Stall Not required / less than 700 m² commercial space
parking stalls off site 15 parallel street parking stalls provided on municipal property and not counted

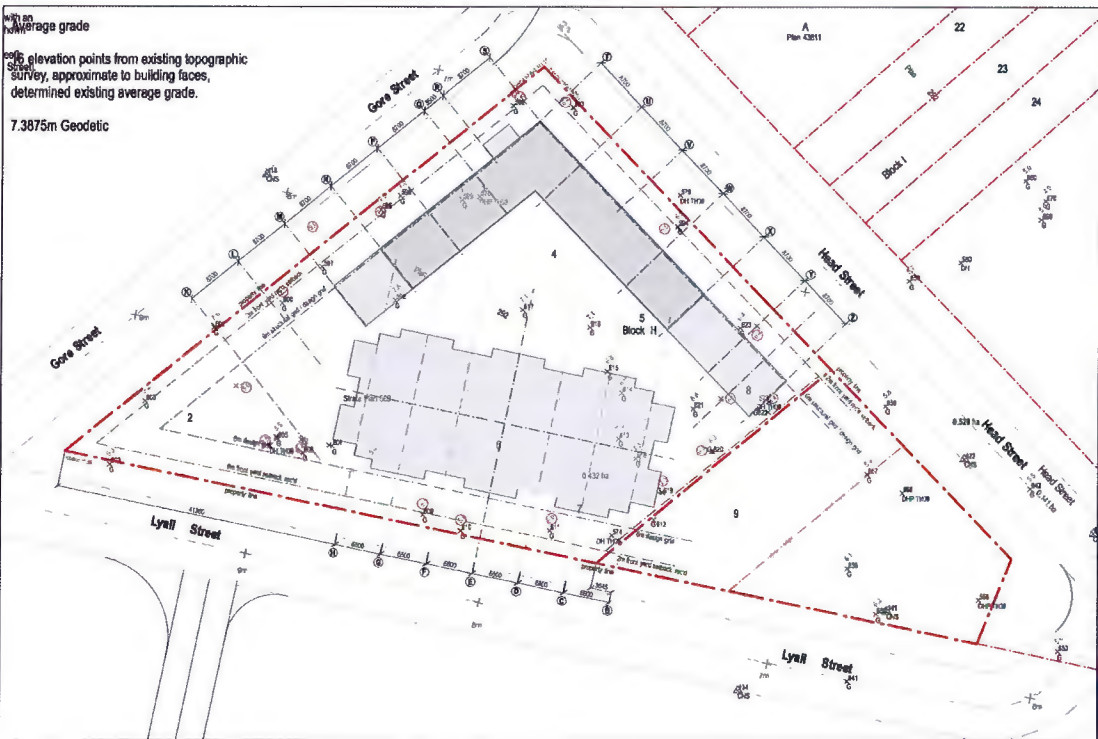
BIKE PARKING

Bicycle parking 110 min req'd (73 x 1.5 per residential unit) 116 shown. Exceeds: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1, page 16
Class I (enclosed in bldg)
Commercial bicycle parking 9 provided (1.5 per commercial tenant) exceeds: Esquimalt OCP, Section 2. Managed growth, 2.3.1.1, page 18
Class I (enclosed in bldg)
Electric bicycle charging 2 locations provided for residents, exceeds Bylaw
Class I (enclosed in bldg)
Motorcycle / scooter parking (gas fueled scooters / motorcycles) • covered or enclosed
.9m x 1.8m stall: 19 stalls (exceeds Bylaw requirement)
surface public parking 4 stalls
parkade level P1: 8 stalls
parkade level P2: 7 stalls
total: 19 stalls
Electric motorcycle / scooter charging (rechargeable battery operated) 3 locations provided, exceeds Bylaw
Guest bicycle parking: Residential Class II (outdoor racks) 6 guest bikes at residential entry. Ex: Esquimalt OCP, Section 2. Managed growth, 2.2.4.1, page 16
Guest bicycle parking: Commercial Class II (outdoor racks) 6 guest bikes at Head and Gore intersection. Exceeds: Esquimalt OCP, Section 2. Managed growth, 2.3.1.1, page 18
Bike and Motorcycle user facilities (showers, laundry) Bikers and Mariners shared laundry and showering facilities (commercial employees, residents, marine lease holders only)
BUILDING STORAGE
Residential Storage 75 residential storage lockers provided (min 1 per residential unit)
Additional Storage Rooms - 6 units
23.5m² storage room 1 (parkade P2 level 2.9m grade)
23.5m² storage room 2 (parkade P2 level 2.9m grade)
(revenue producing storage units, for 6 commercial tenants on ground floor adjacent to commercial use spaces)
28.5m² storage room 3 (parkade P1 level 5.9m grade)
28.5m² storage room 4 (parkade P1 level 5.9m grade)
28.5m² storage room 5 (parkade P1 level 5.9m grade)
28.5m² storage room 6 (parkade P1 level 5.9m grade)



468 Head Street (Triangle Estates),
PID 001-843-891 Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (468 Head Street);
PID 009-720-430, Lot 5, Block H, Section 11, Esquimalt District, Plan 252 (470 Head Street);
PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 (472 Head Street);
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PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 (509 Gore Street);
PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (922 Lyall Street);
PID 000-704-588, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 (920 Lyall Street);
PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 (918 Lyall Street);
and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 (912 Lyall Street).

Original Survey Source
McIlvaney Riley Land Surveying Inc.
#113 - 2244 Sooke Road
Victoria, B.C. V8B 1X1
(250) 474-5538
www.mrls.ca



1 Existing Site
A1.1 Scale 1:500

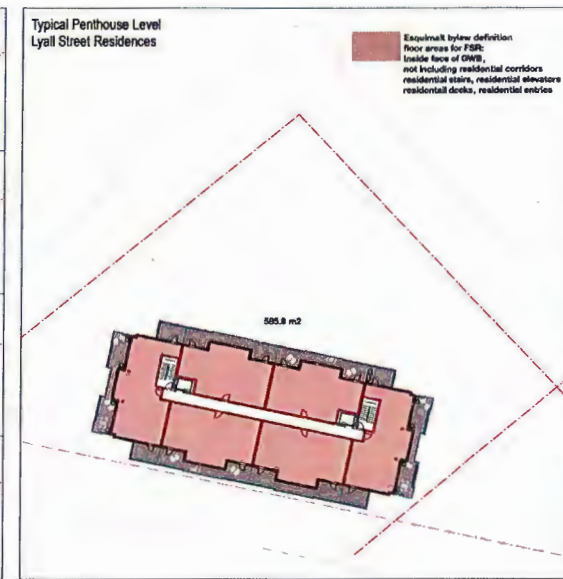
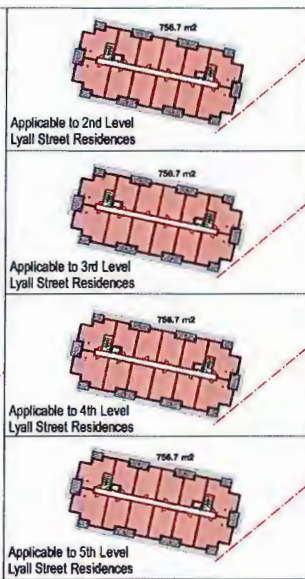
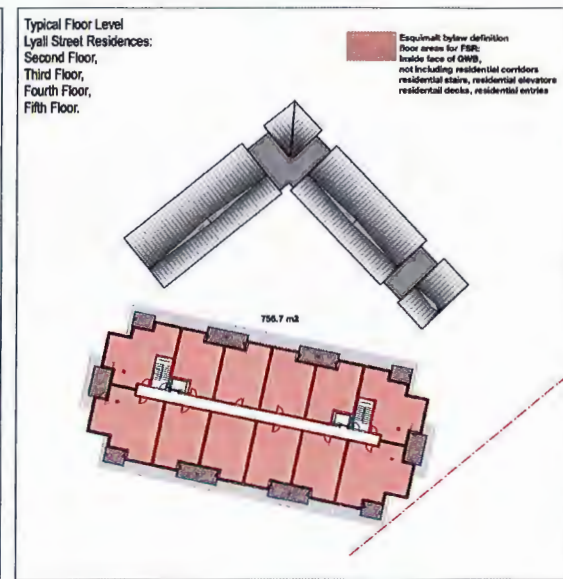
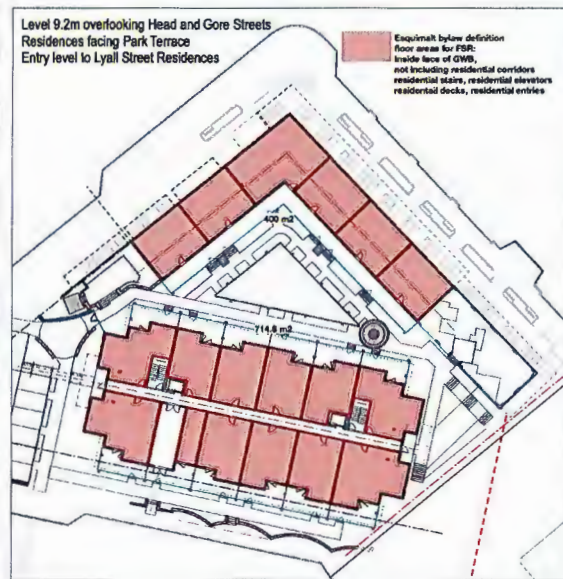
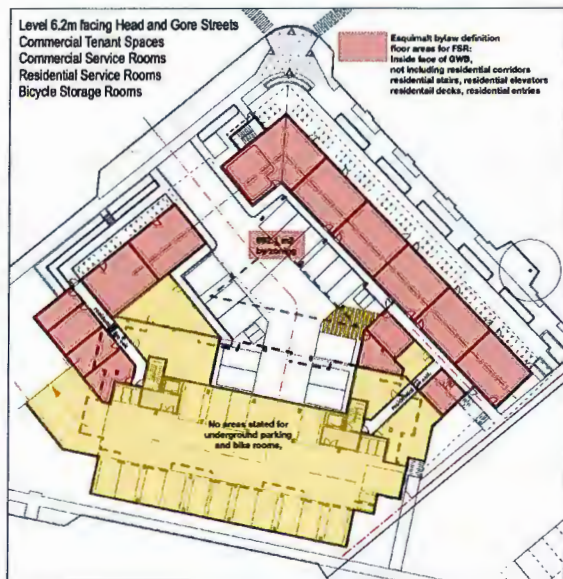
THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA



Hillel architecture
137 15th Ave, Esquimalt, BC V8R 1K5
phone: 250-592-9990
fax: 250-592-9978

Project: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

Existing Site & Site Data
A1.1



ZONING BYLAW 1992, NO. 2050
Amendment Bylaw No. 2861

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area" when used in reference to a floor, Storey or Building means the entire area which is plan enclosed by the interior face of the exterior walls of the Storey or floor level, calculated in accordance with Section 14.

(9) of PART 4 - GENERAL REGULATIONS Section 14.

NEW DEFINITION USED IN THESE DOCUMENTS:
14. CALCULATION OF FLOOR AREA AND FLOOR AREA RATIO

(1) The following shall not be included as Floor Area:
(a) balconies,
(b) open decks and sun decks,
(c) unenclosed porches and verandas,
(d) chimneys forming part of an exterior wall,
(e) projecting bay windows where the bottom of the window is located a minimum 0.3 metres above the floor, and no area of the bay window in plan exceeds 1.8 square metres.

(2) In addition, the following shall not be included as Floor Area in Multiple Family Dwellings and in residential portions of Mixed Commercial Residential Buildings:
(a) the area of the floor of any and all portions of the Building used or intended to be used for the parking or movement of vehicles,
(b) secure bicycle storage facilities (bike lockers),
(c) the area of the floor of any and all portions of the Building intended to be used by all residents of the building, including but not limited to the following:
(i) mezzanines,
(ii) corridors and hallways,
(iii) landings and foyers,
(iv) staircases and stairwells, and
(v) elevator shafts.

(3) In addition, the following shall not be included as Floor Area for the purposes of computing Floor Area Ratio:
(a) Any portion of a penthouse containing elevator or ventilating machinery,
(b) Any portion of a Building where the ceiling height is less than 1.2 metres above the floor.

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area Ratio" means the figure obtained when the Floor Area of all floors and Storeys in all Buildings on a Parcel is divided by the Area of the Parcel, calculated in accordance with Section 14.

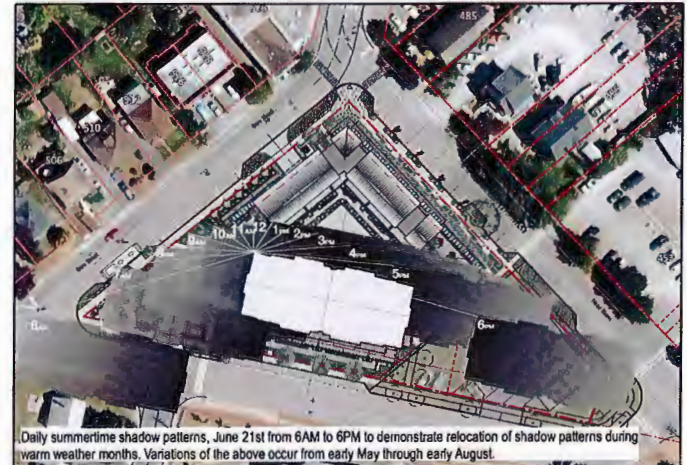
1 Floor Area Graphic Summary
Scale 1:500
A1.2



2 Site Coverage Graphic Summary
Scale 1:500
A1.2



3 Daily Shading Summary: March, June, Sept.
Scale 1:2500
A1.2



4 Shadows / 9AM and 3PM, March 21st, Sept. 21st
Scale 1:2500
A1.2



DATE	14 January 2017	DESCRIPTION	Development Permit Application
DATE	01 April 2016	DESCRIPTION	Planning Committee
DATE	08 December 2015	DESCRIPTION	Planning Sub-committee
DATE	June 2014	DESCRIPTION	
DATE	08 October 2013	DESCRIPTION	
DATE	08 October 2013	DESCRIPTION	

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THE TRIANGLE @ WESTBAY
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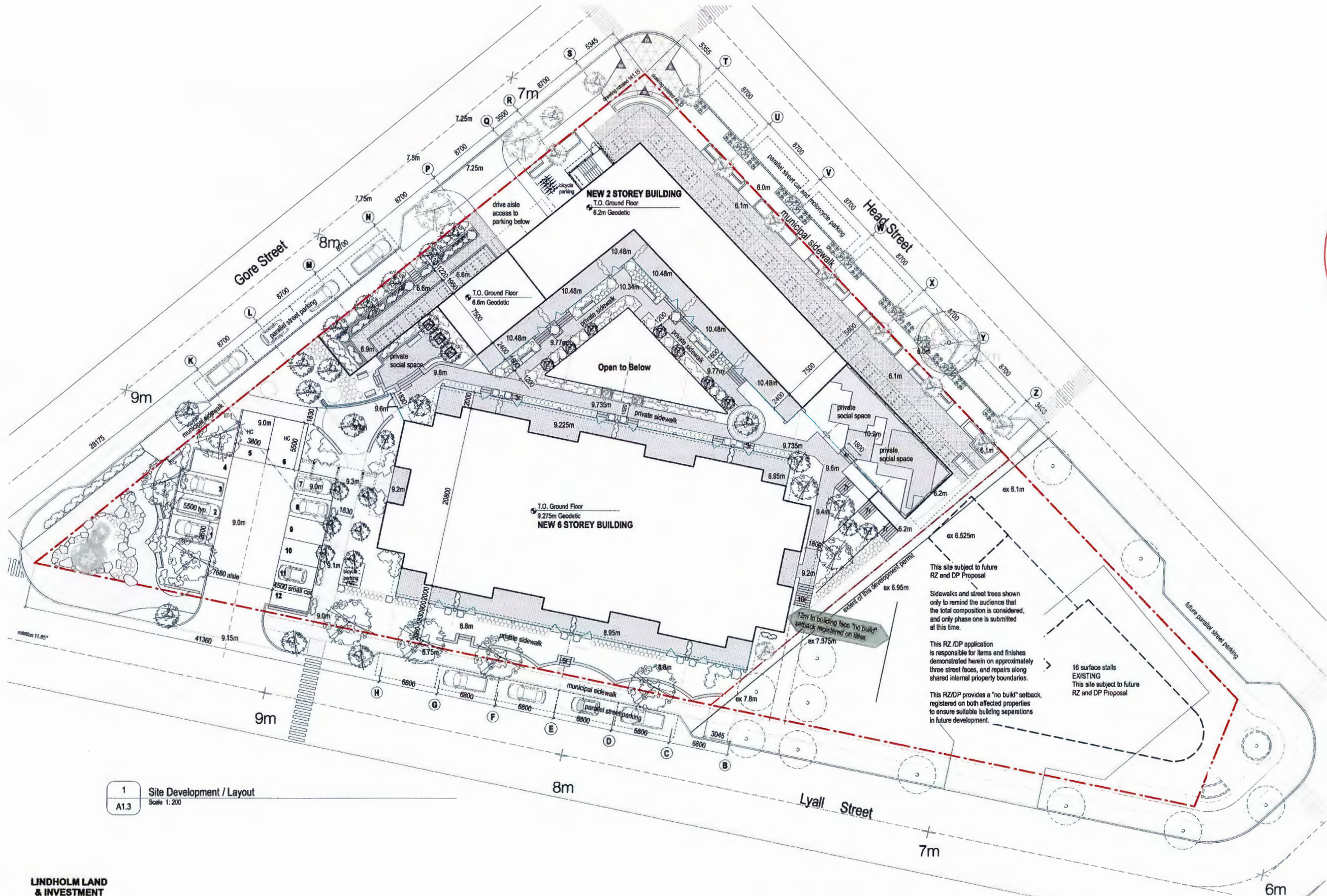
Hillel architecture

PROJECT: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

GRAPHIC PROJECT SUMMARY

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 OF ESQUIMALT
 DEVELOPMENT SERVICES



1 Site Development / Layout
 A1.3 Scale 1:200

16 January 2017	Development Permit Application
29 April 2016	Planning Consultancy
08 December 2015	Planning Consultancy
June 2014	Site Plan
2014.05	Site Plan
2014.05	Site Plan

Remission is granted for these drawings to be registered by Lindholm Land & Investment Corporation

Hillel architecture

project
THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
Site Development Layout

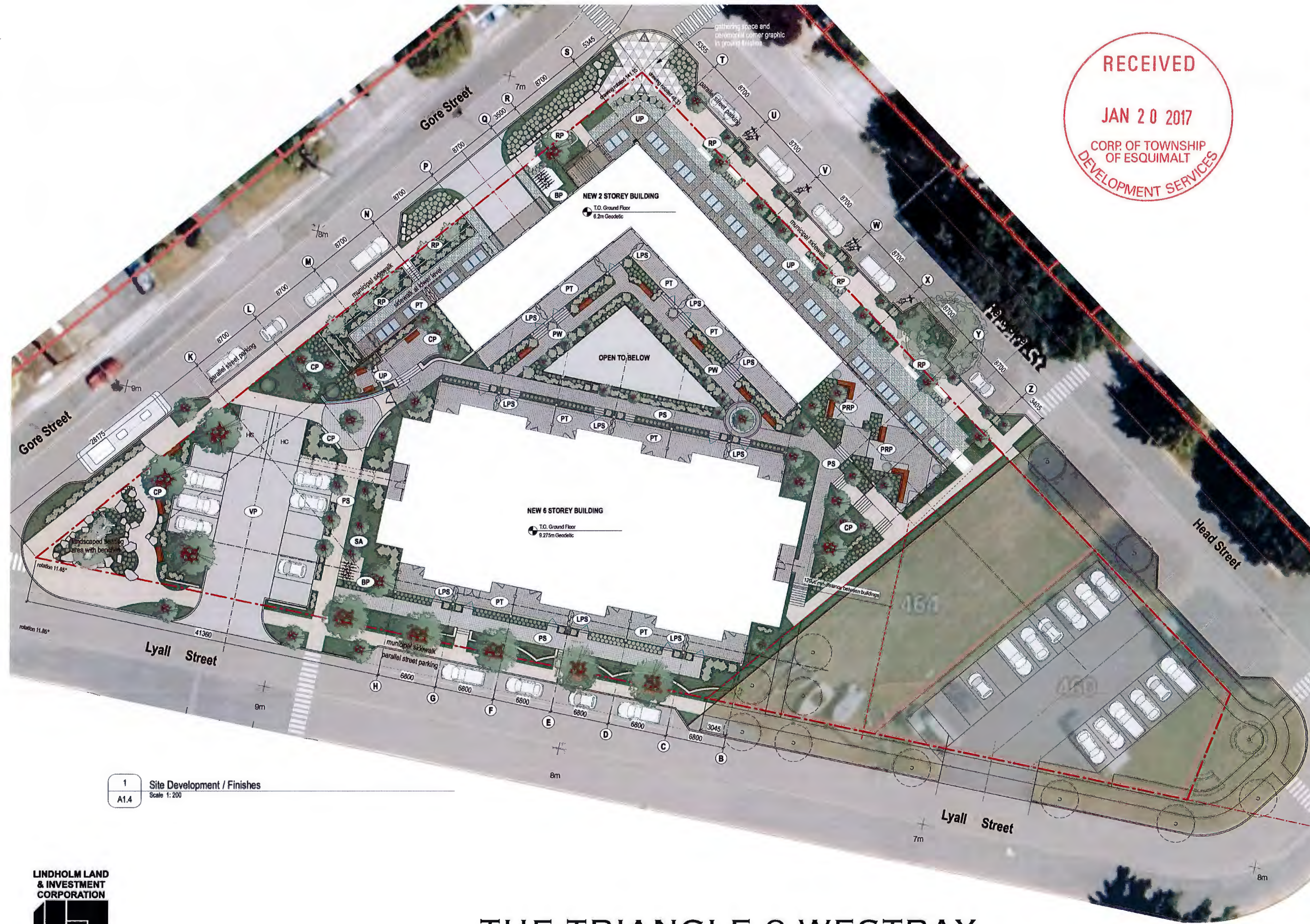
phone 250-592-7978
 fax 250-592-7978

revision no. 1
 drawing no. A1.3

LINDHOLM LAND & INVESTMENT CORPORATION

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 mark.lindholm@lindholm.ca

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Site Finishes Legend
 List of finishes typical of all elevations

- ET existing trees to be undisturbed during construction
- CP common planting area - see landscape plan for details
- SA seeded or sodded grass area - see landscape plan for details
- NT approximate location of new tree planting area - see landscape plan for details
- RF rock feature - see landscape plan for details
- RP raised concrete planter - see landscape plan for details

- UP unit paver area - pathways and patios
- APS new asphalt surface
- MS municipal sidewalk - poured concrete finish
- PS private sidewalk - coloured poured concrete finish
- PW private walkways / suite entry features - coloured poured concrete finish

- PT private terrace - unit pavers finish
- RT roof top terrace - unit pavers finish
- PRP private residents park - unit pavers finish with bench seating

- SC site privacy screening - cedar fence - see landscape plan for details
- LPS landscape privacy screening - see landscape plan for details

- RW cultured stone veneer on concrete retaining wall with a concrete cap
- CW concrete retaining wall with a concrete cap

- BP six bike class 2 visitors bike parking / lockable
- VP asphalt surface vehicle parking stalls
- PB precast concrete parking blocks, typical all parking stalls

- ART public art
- PL parking / site lighting

1 Site Development / Finishes
 A1.4 Scale 1:200

LINDHOLM LAND & INVESTMENT CORPORATION

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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

DATE	15 January 2017	DESCRIPTION	Development Permit Application
DATE	03 April 2016	DESCRIPTION	Planning Consultancy
DATE	05 December 2015	DESCRIPTION	Security Submission
DATE	June 2014	DESCRIPTION	Site Plan

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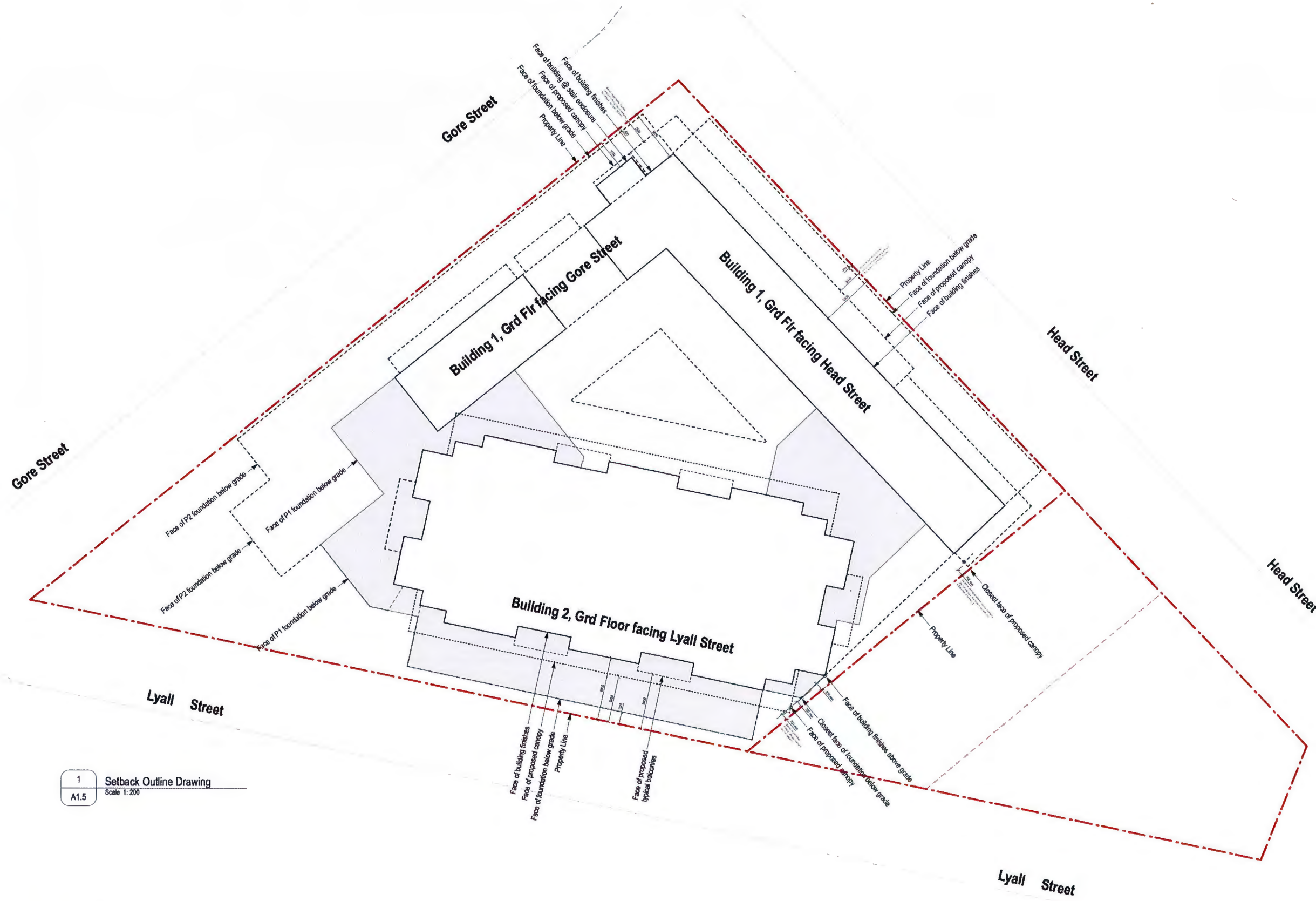
Hillel architecture

 101 15th Oak Bay Avenue
 Victoria BC V8R 1C5
 phone 250-922-9978
 fax 250-922-9979

project
THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
Site Development Finishes

drawing no.
A1.4



1 Setback Outline Drawing
A1.5 Scale 1:200

DATE	19 January 2017	DESCRIPTION	Development Permit Application
DATE	05 April 2016	DESCRIPTION	Planning Commentary
DATE	08 December 2014	DESCRIPTION	Resolving Subdivision
DATE	June 2014	DESCRIPTION	Site Plan
DATE	2014.01	DESCRIPTION	Site Plan
DATE	2014.01	DESCRIPTION	Site Plan

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project
THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
Graphic Summary of Setbacks

phone 250-992-9998
fax 250-992-9978

drawing no.
A1.5



19 comm total min required
 +3 beyond bylaw req's
 22 illustrated under
 commercial strata ownership

95 residential min. required
 24 stalls for visitors
 71 stalls for owners
 +2 beyond bylaw req's
 97 illustrated under
 residential strata ownership

- RG 4 residential short term guest stalls
- CP 8 commercial public parking stalls
- 14 street side parallel parking spaces not counted

Note: 3 residential guest stalls permitted to be commercial use daytime as per bylaw. Commercial parking becomes residential guest stalls after hours

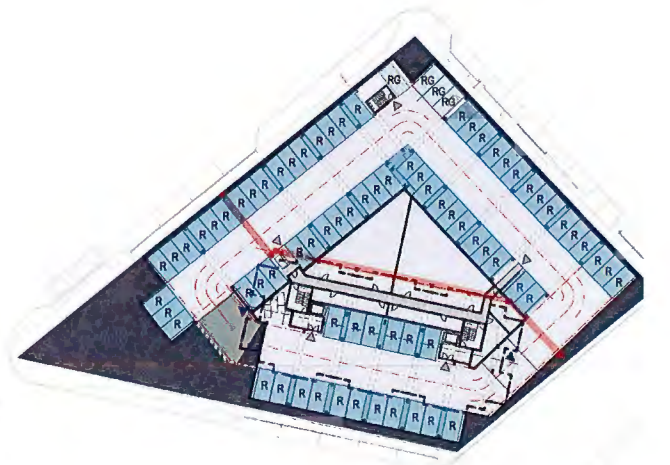
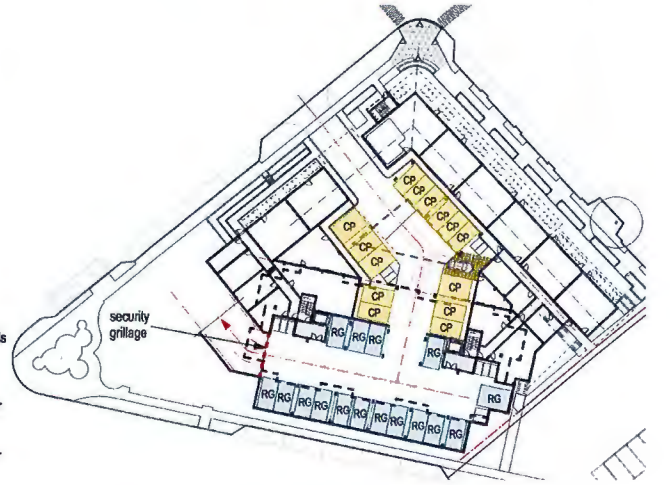
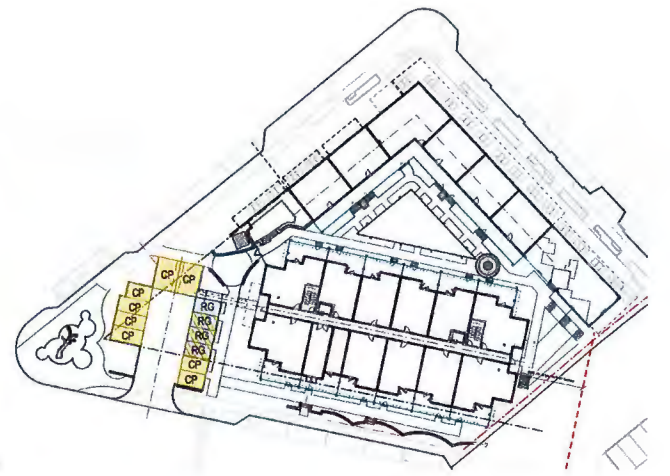
Note motorcycle, scooter, electric motorcycle, electric scooter not coloured in these diagrams, however these items are distributed throughout parking facilities to similarly provide these items in each parking category.

long term residential guest, short term residential guest, residential homeowner, comm public parking, comm leaseholder parking

- RG 16 Residential Guest stalls
- CP 14 commercial public parking stalls

Note: Residential guest stall area includes guest motorcycle / scooter parking in excess of bylaw. Commercial public stall area includes guest motorcycle / scooter parking in excess of bylaw.

- RG 4 long term residential guest stalls, entry permit / entry card req'd
- R 73 Residential stalls for 73 units



1 Underground Parkade
 Scale 1:200

2 Parking Stall Assignments
 Scale 1:200



THE TRIANGLE @ WESTBAY

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no.	date	description
1	19 January 2017	Development Permit Application
2	05 April 2016	Parking Commentary
3	08 December 2015	Recording Submission
4	June 2014	Planning File
5		Other
6		Other
7		Other
8		Other
9		Other
10		Other

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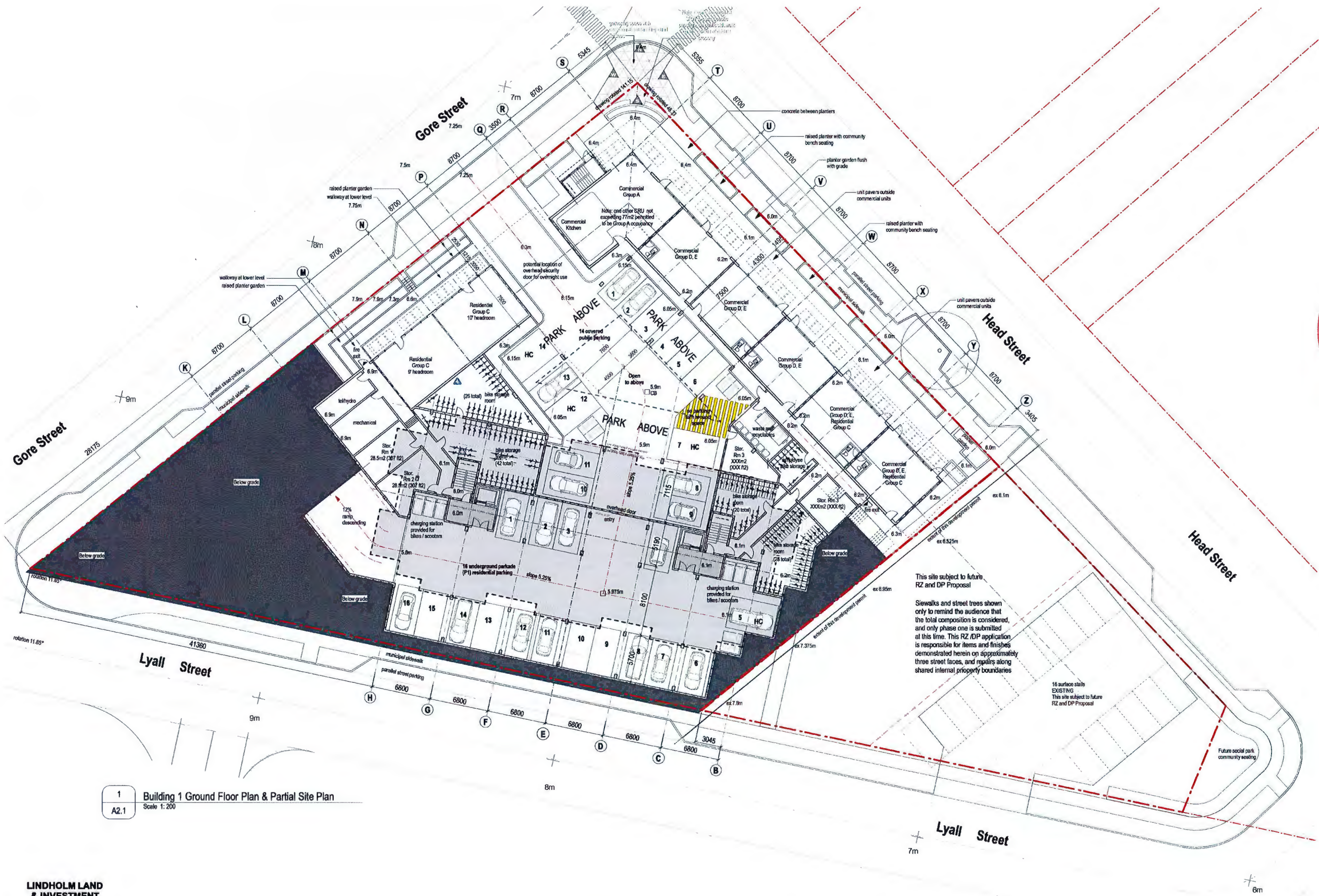
1011 15th Street, Suite 101
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phone 250-592-9978
 fax 250-592-9978

project: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing file: Underground Parkade Layout

drawing no: A2.0



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1 Building 1 Ground Floor Plan & Partial Site Plan
 A2.1 Scale 1:200



THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	08 April 2016	DESCRIPTION	Planning Commentary
DATE	18 December 2015	DESCRIPTION	Resolving Submission
DATE	June 2014	DESCRIPTION	Phase 1
DATE	June 2014	DESCRIPTION	Phase 2
DATE	June 2014	DESCRIPTION	Phase 3
DATE	June 2014	DESCRIPTION	Phase 4
DATE	June 2014	DESCRIPTION	Phase 5
DATE	June 2014	DESCRIPTION	Phase 6
DATE	June 2014	DESCRIPTION	Phase 7
DATE	June 2014	DESCRIPTION	Phase 8
DATE	June 2014	DESCRIPTION	Phase 9
DATE	June 2014	DESCRIPTION	Phase 10
DATE	June 2014	DESCRIPTION	Phase 11
DATE	June 2014	DESCRIPTION	Phase 12
DATE	June 2014	DESCRIPTION	Phase 13
DATE	June 2014	DESCRIPTION	Phase 14
DATE	June 2014	DESCRIPTION	Phase 15
DATE	June 2014	DESCRIPTION	Phase 16
DATE	June 2014	DESCRIPTION	Phase 17
DATE	June 2014	DESCRIPTION	Phase 18
DATE	June 2014	DESCRIPTION	Phase 19
DATE	June 2014	DESCRIPTION	Phase 20

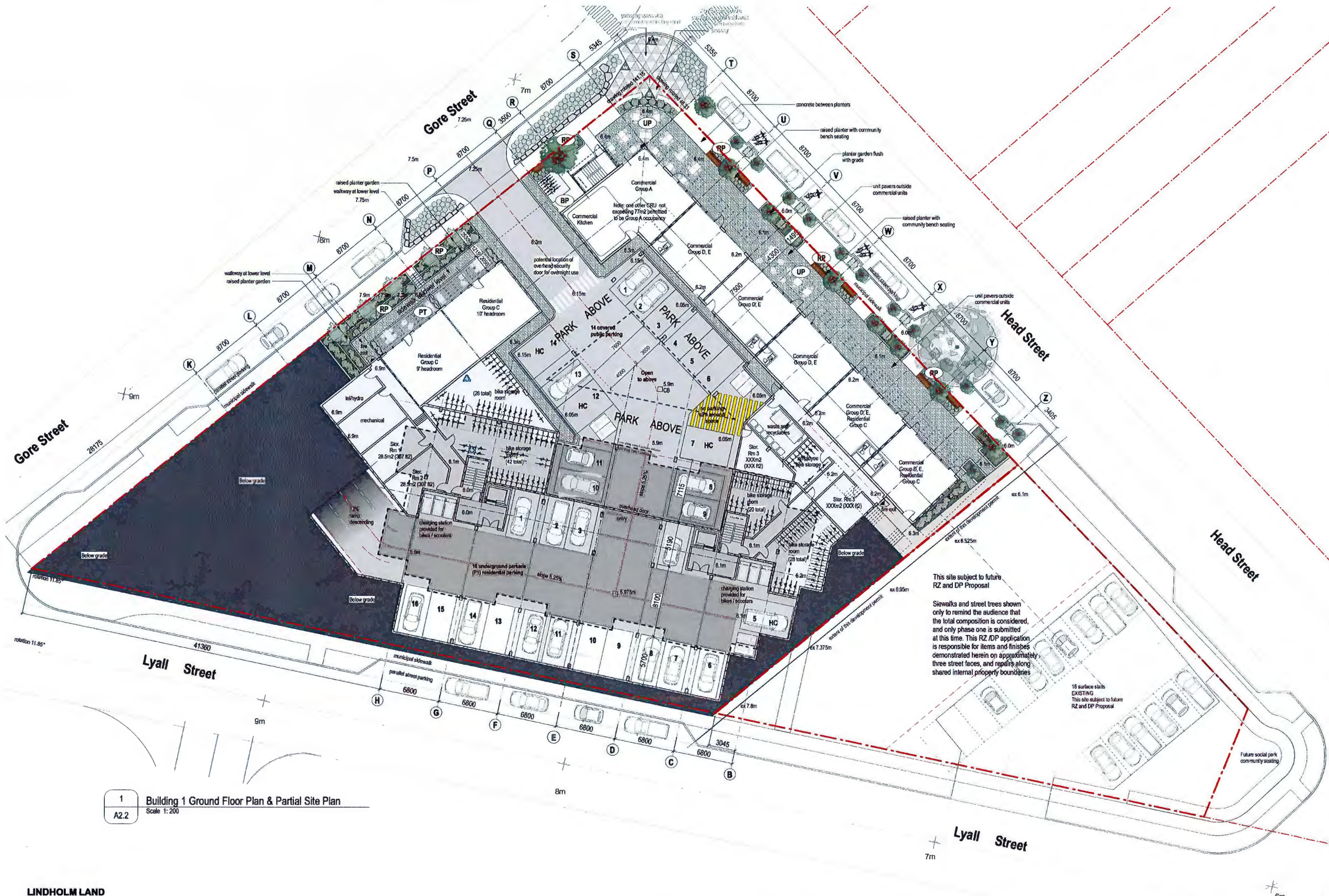
Hillel architecture

project
THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
Building 1 - Ground Floor @ Head Street - Layout

phone 250-992-9998
 fax 250-992-9978

DATE: 2014.03
 PERMITTED BY: LINDHOLM LAND & INVESTMENT CORPORATION
 DRAWING NO.: A2.1



Site Finishes Legend
List of Finishes typical of all elevations

- (ET) existing trees to be undisturbed during construction
- (CP) common planting area - see landscape plan for details
- (SA) seeded or sodded grass area - see landscape plan for details
- (NT) approximate location of new tree planting area - see landscape plan for details
- (RF) rock feature - see landscape plan for details
- (RP) raised concrete planter - see landscape plan for details
- (UP) unit paver area - pathways and patios
- (APS) new asphalt surface
- (MS) municipal sidewalk - poured concrete finish
- (PS) private sidewalk - coloured poured concrete finish
- (PW) private walkways / suite entry features - coloured poured concrete finish
- (PT) private terrace - unit pavers finish
- (RT) roof top terrace - unit pavers finish
- (RRP) private residents park - unit pavers finish with bench seating
- (BC) site privacy screening - cedar fence - see landscape plan for details
- (LPS) landscape privacy screening - see landscape plan for details
- (RW) cultured stone veneer on concrete retaining wall with a concrete cap
- (CW) concrete retaining wall with a concrete cap
- (BP) six bike class 2 visitors bike parking / lockable
- (VP) asphalt surface vehicle parking stalls
- (PB) precast concrete parking blocks, typical all parking stalls
- (ART) public art
- (PL) parking / site lighting

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 DEVELOPMENT SERVICES

This site subject to future RZ and DP Proposal

Sidewalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ/DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

15 surface stalls EXISTING
This site subject to future RZ and DP Proposal

1 Building 1 Ground Floor Plan & Partial Site Plan
A2.2 Scale 1:200

Rev	18 January 2017	Development Permit Application
Rev	05 April 2016	Planning Consultation
Rev	05 December 2015	Revisory Submission
Rev	June 2014	Site Plan
Rev	10/2014	Site Plan
Rev	10/2014	Site Plan

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THE TRIANGLE @ WESTBAY

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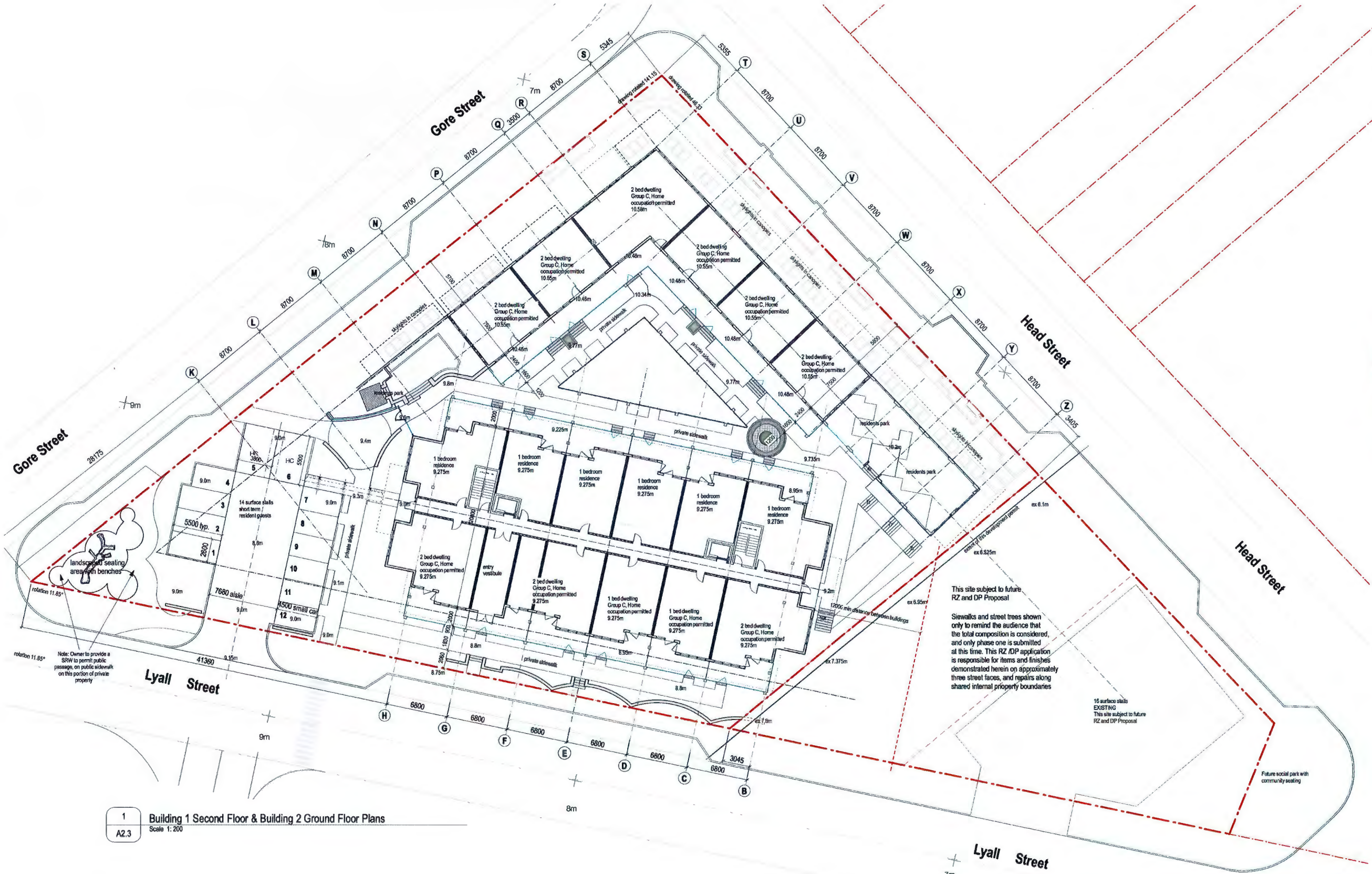
Hillel architecture

project
THE WESTBAY TRIANGLE
HEAD, GORE, & LYALL STREETS, ESQUIMALT

drawing title
Building 1 - Ground Floor @ Head Street - Finishes

phone 250-992-9998
fax 250-992-9979

sheeting no. A2.2



1 Building 1 Second Floor & Building 2 Ground Floor Plans
A2.3 Scale 1:200

LINDHOLM LAND & INVESTMENT CORPORATION

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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Rev	Date	Description
1	19 January 2017	Development Permit Application
2	02 April 2018	Planning Commentary
3	09 December 2018	Neighbour Submission
4	June 2024	Revising

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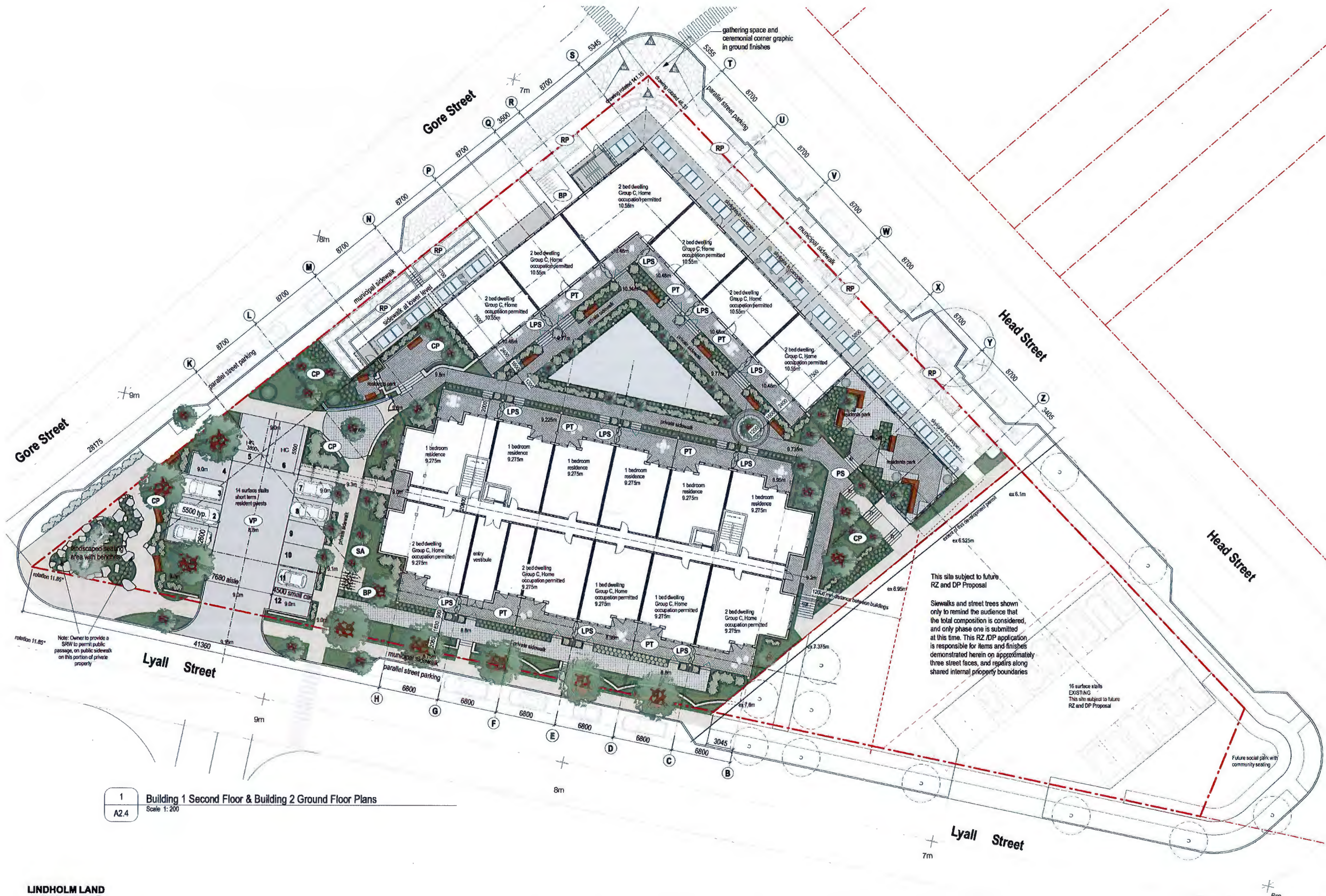
Hillel architecture

 101 15th Clark Bay Avenue
 Victoria BC V8R 1K5
 phone 250-592-9978
 fax 250-592-9978

project: **THE WESTBAY TRIANGLE**
 HEAD GORE & LYALL STREETS, ESQUIMALT

drawing No: **Building 1 & 2 Ground Floor @ Lyall Street - Layout**

drawing No: **A2.3**



Site Finishes Legend
List of finishes typical of all elevations

- (ET) existing trees to be undisturbed during construction
- (CP) common planting area - see landscape plan for details
- (SA) seeded or sodded grass area - see landscape plan for details
- (NT) approximate location of new tree planting area - see landscape plan for details
- (RF) rock feature - see landscape plan for details
- (RP) raised concrete planter - see landscape plan for details
- (UP) unit paver area - pathways and patios
- (APS) new asphalt surface
- (MS) municipal sidewalk - poured concrete finish
- (PS) private sidewalk - coloured poured concrete finish
- (PW) private walkways / suite entry features - coloured poured concrete finish
- (PT) private terrace - unit pavers finish
- (RT) roof top terrace - unit pavers finish
- (PRP) private residents park - unit pavers finish with bench seating
- (SC) site privacy screening - cedar fence - see landscape plan for details
- (LPS) landscape privacy screening - see landscape plan for details
- (RW) cultured stone veneer on concrete retaining wall with a concrete cap
- (CW) concrete retaining wall with a concrete cap
- (BP) six bike class 2 visitors bike parking / lockable
- (VP) asphalt surface vehicle parking stalls
- (PB) precast concrete parking blocks, typical all parking stalls
- (ART) public art
- (PL) parking / site lighting

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 DEVELOPMENT SERVICES

This site subject to future RZ and DP Proposal

Sidewalks and street trees shown only to remind the audience that the total composition is considered, and only phase one is submitted at this time. This RZ/DP application is responsible for items and finishes demonstrated herein on approximately three street faces, and repairs along shared internal property boundaries

16 surface voids EXISTING
This site subject to future RZ and DP Proposal

Future social park with community seating

1 Building 1 Second Floor & Building 2 Ground Floor Plans
A2.4 Scale 1:200



THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

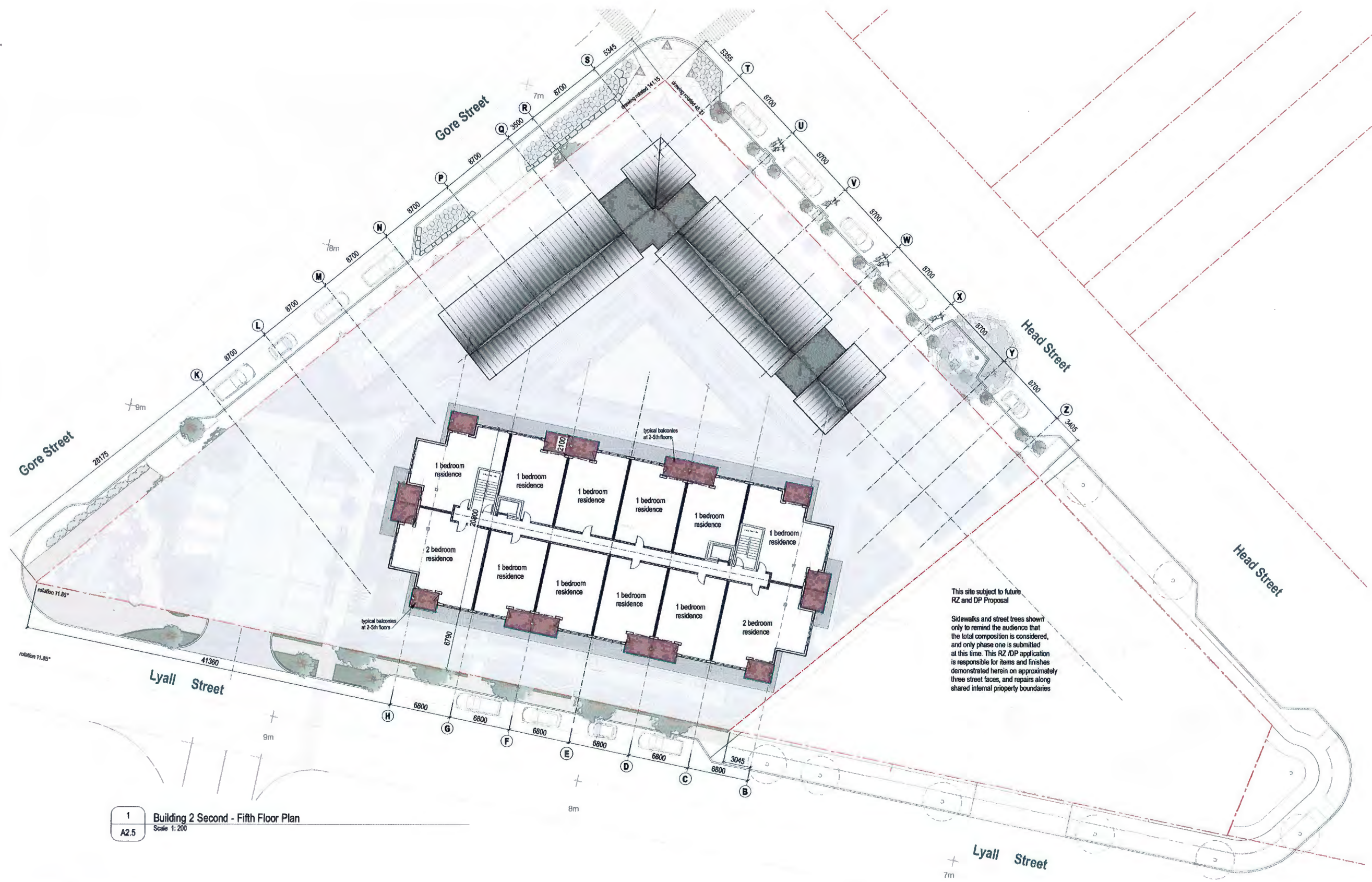
No.	Date	Description	By
1	19 January 2017	Development Permit Application	
2	20 April 2016	Planning Commentary	
3	18 December 2015	Revisory Submission	
4	July 2014	Planning	
5	2013	Planning	
6	2013	Planning	
7	2013	Planning	
8	2013	Planning	
9	2013	Planning	
10	2013	Planning	

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THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 Drawing No: **Building 1 & 2 Ground Floor @ Lyall Street - Finishes**
 revision no. drawing no. **A2.4**



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1 Building 2 Second - Fifth Floor Plan
A2.5 Scale 1:200

DATE	18 January 2017	ISSUED FOR	Development Permit Application
DATE	08 April 2016	ISSUED FOR	Planning Commentary
DATE	08 December 2015	ISSUED FOR	Accessy Submission
DATE	June 2014	ISSUED FOR	Final Review
DATE	2014.04	ISSUED FOR	Final Review
DATE	2014.03	ISSUED FOR	Final Review

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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Hillel architecture

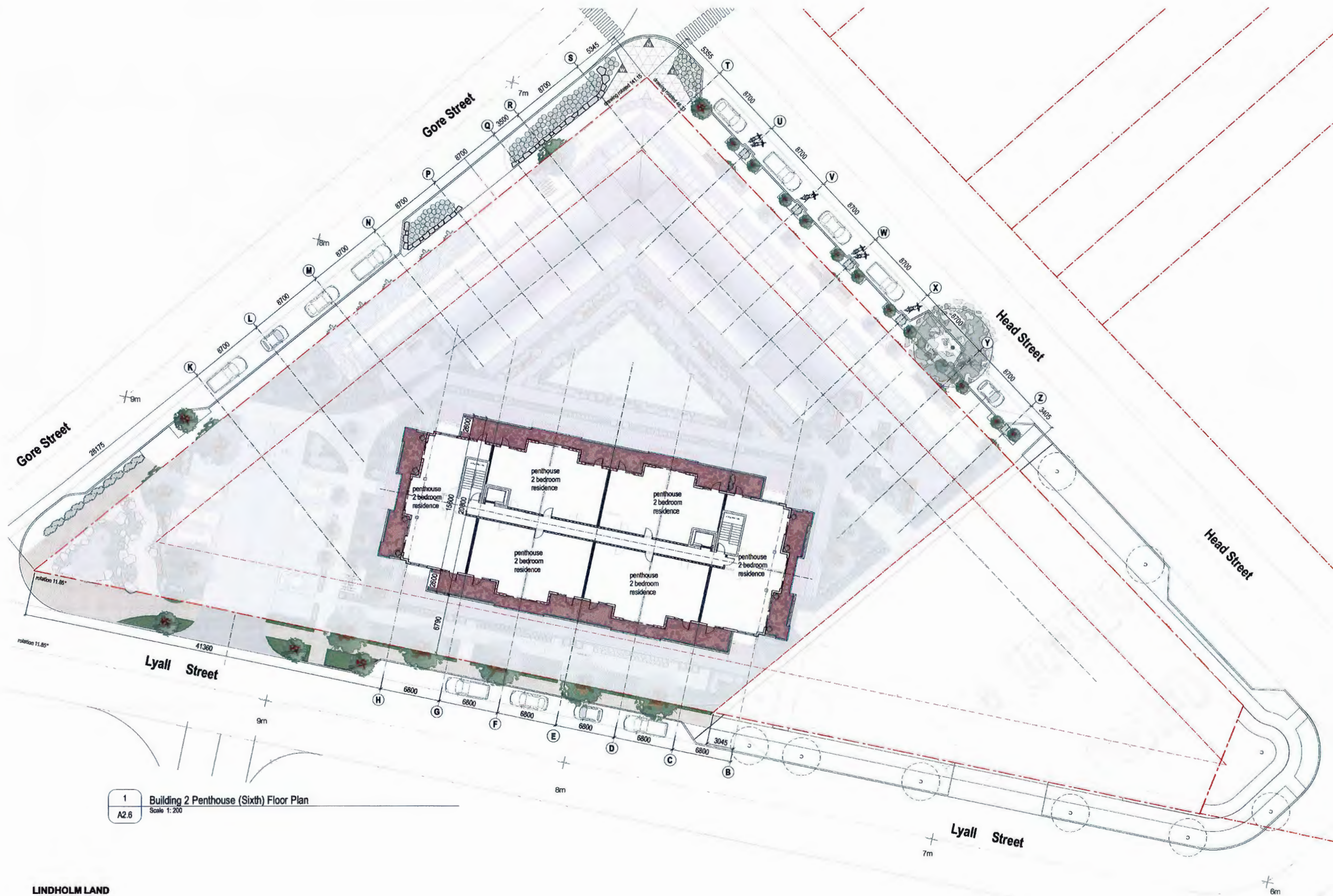


project
THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
**Building 2 - Typical Floor Layout
2nd through 5th Floor Plans**

phone 250-592-9978
fax 250-592-9978

sheet no. **A2.5**



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1 Building 2 Penthouse (Sixth) Floor Plan
 A2.6 Scale 1:200

no.	19 January 2017	Development Permit Application
no.	08 April 2016	Planning Commentary
no.	08 December 2015	Neighbour Submission
no.	June 2014	Final Plan
no.	2014.05	2014.05


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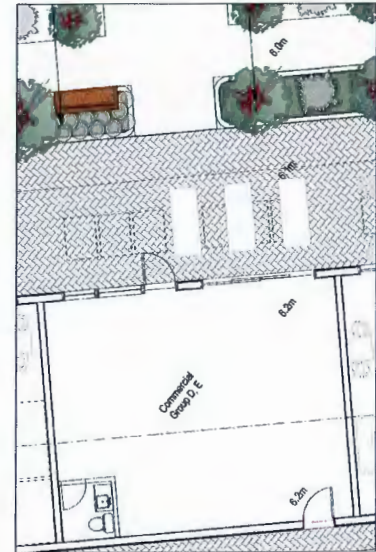
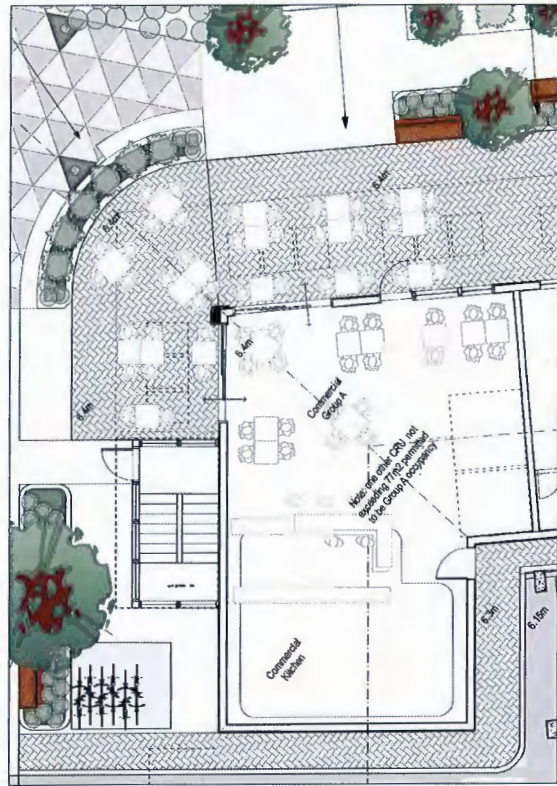
 7081 Central Saanich Road
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THE TRIANGLE @ WESTBAY

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Hillel architecture

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Project: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 Drawing title: Bldg 2 - Penthouse (Sixth) Floor Plan
 Drawing no.: A2.6

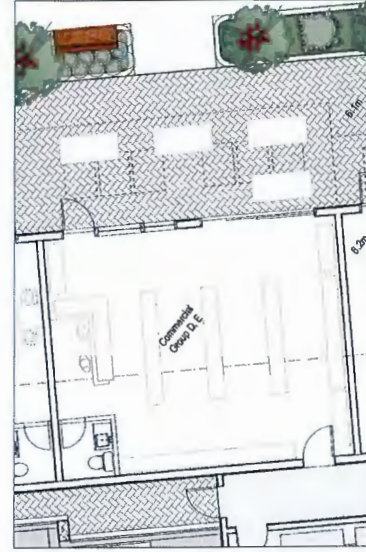


Suite CRU2 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 R2
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38

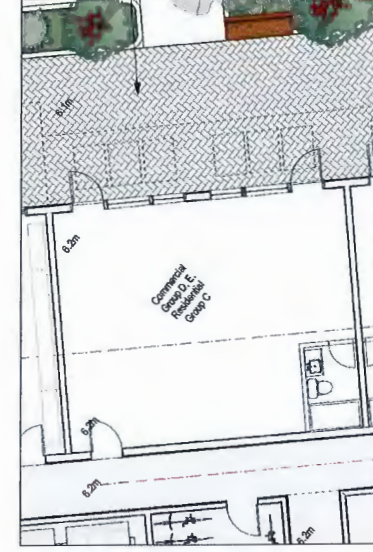
Suite CRU1
96.1m² inside GWB / 100.9 m² to cl / 1086 R2
Commercial suite
Group A (less than 10% gross floor area, minor occupancy),
Group D, Group E
Parking 96.1m² @ 14m²/st = 6.8 stalls minimum
Parking @ 1 stall per 5 seats: 60 seats / 12 stalls req'd



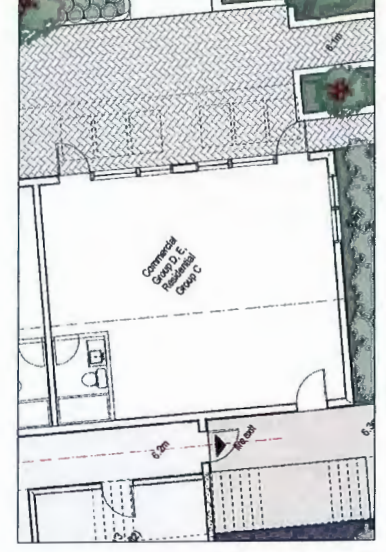
Suite CRU3 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 R2
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38



Suite CRU4 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 R2
Group D, Group E
Parking 59.5m² @ 25m²/st = 2.38



Suite CRU5 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 R2
Commercial / Res suite
Group D, Group E;
Group C = Live or work unit
Parking 59.5m² @ 25m²/st = 2.38
As residence: 1.3 parking stalls



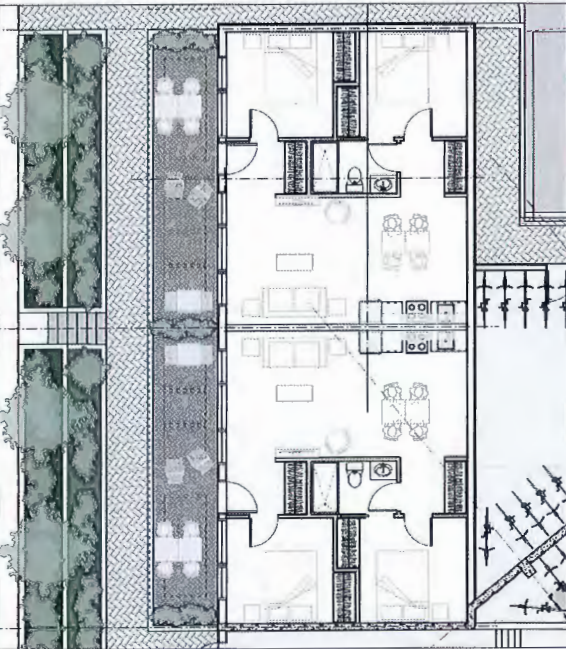
Suite CRU6 Commercial suite
59.5 m² inside GWB / 62.9m² to cl walls / 677 R2
Commercial / Res suite
Group D, Group E;
Group C = Live or work unit
Parking 59.5m² @ 25m²/st = 2.38
As residence: 1.3 parking stalls

Please note:
Greatest horizontal building area by code definition, outside face of walls: 1731.2m² gross area.
Minor occupancies can not exceed 173.12m².

Building Code assumptions
CRU 1 - Group A Public Assembly Use unit. 96.1m²
This unit less than 10% greatest horizontal building area - minor occupancy: permitted.

CRU 2 to CRU 6 permitted to be Group A Public Assembly Use provided
that the total of all public assembly use spaces combined do not exceed 173.12m².
Each CRU @ ±59.5m² permits one more commercial space to be assigned Group A use.
Project parking calculations permit this increase (5 stalls in excess of Bylaw demonstrated)

2 Building 1 Commercial Suite Plans - Ground Floor
Scale 1:100
A2.7



Suite G102 Residential suite
59.3 m² inside GWB / 62.9m² cl of walls / 677 R2
2 bedroom residential dwelling
Group C
Home occupancy permitted
Parking: 1.3 stalls per dwelling

Suite G101 Residential suite
59.3 m² inside GWB / 62.9m² cl of walls / 677 R2
2 bedroom residential dwelling
Group C
Home occupancy permitted
Parking: 1.3 stalls per dwelling

1 Building 1 Residential Suite Plans - Ground Floor
Scale 1:100
A2.7



no.	date	description
1	15 January 2017	Development Permit Application
2	05 April 2016	Parking Commentary
3	05 December 2015	Recording Submission
4	June 2014	Permit Fee
5	2014.03	2014.03

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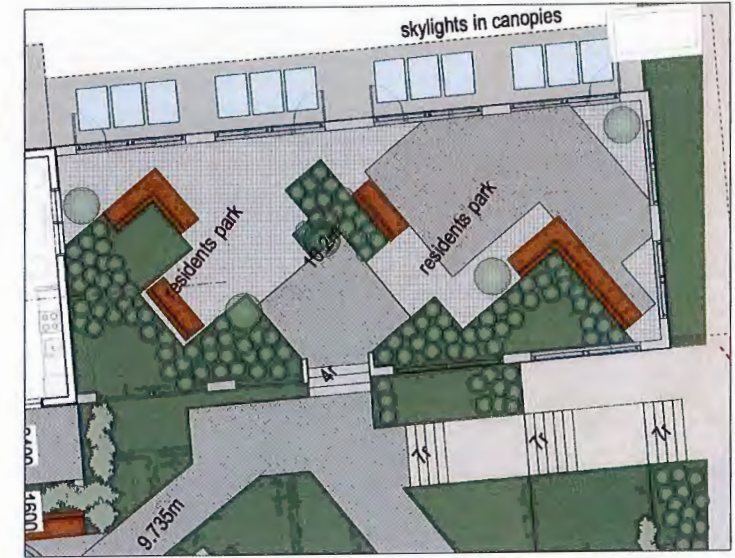
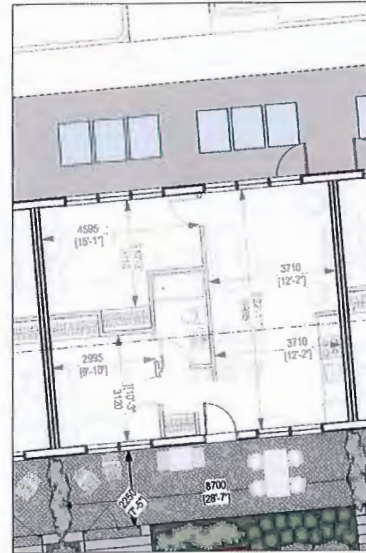
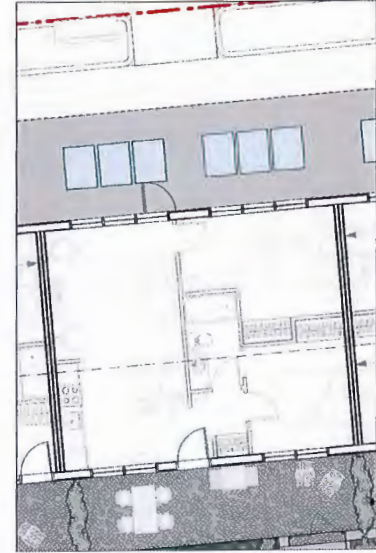
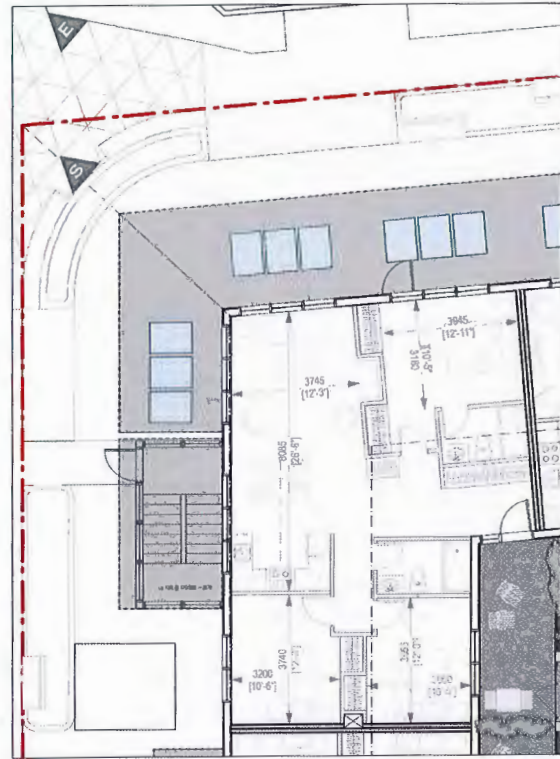
Hillel architecture

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phone 250-592-9798
fax 250-592-9770

project
THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title
Building 1 - Suite Floor Plans

drawing no.
A2.7



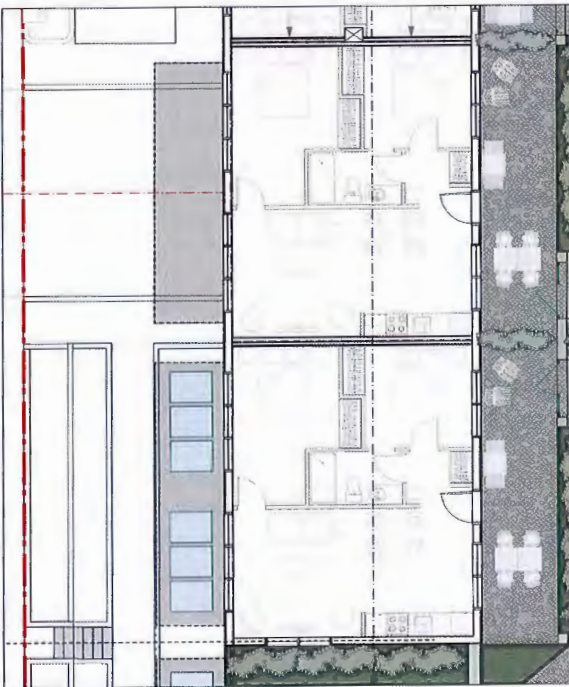
Suite P204 Residential Suite
63 m2 to cl walls / 678 ft2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P203 Residential Suite
101.1 m2 to cl walls / ±1088 ft2
3 bedroom dwelling
Group C Residential use only
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P205 Residential Suite
63 m2 to cl walls / 678 ft2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P206 Residential Suite
63 m2 to cl walls / 678 ft2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Roof Top Ocean view / Marina view Park
1280 ft2 raised gardens, and container trees.
Fixed tables and chairs
Residential Amenity / residents only



Suite P202 Residential Suite
63 m2 to cl walls / 678 ft2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite P201 Residential Suite
63 m2 to cl walls / 678 ft2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

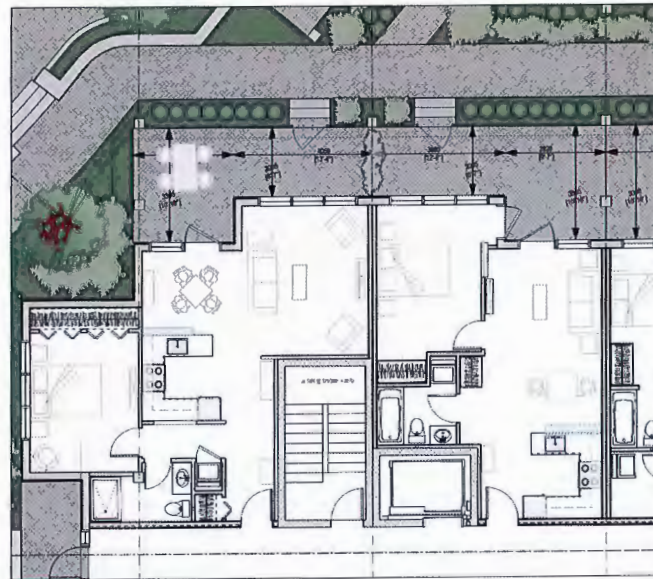
2 Building 1 Suite Plan - Second Floor
Scale 1:100

1 Building 1 Suite Plan - Second Floor
Scale 1:100



rev	18 January 2017	description	Development Permit Application
rev	06 April 2016	description	Planning Comments
rev	06 December 2015	description	Reporting Submission
rev	June 2015	description	
rev		description	
rev		description	
rev		description	
rev		description	

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Suite 112 Residential Suite
65.6 m2 / 708 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite 111 Residential Suite
53.5 m2 / 576 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Suite 110 Residential Suite
59.6 m2 / 642 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted

Suite 109 Residential Suite
59.6 m2 / 642 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted



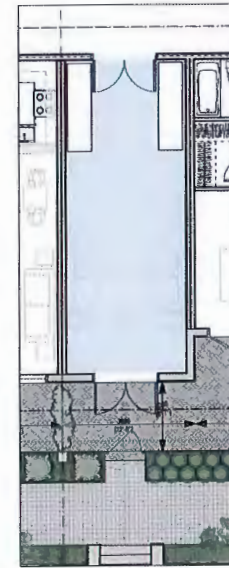
Suite 108 53.5 m2 / 576 R2
1 bedroom Residential suite
Group C
Residential use only

Suite 107 65.6 m2 / 708 R2
1 bedroom Residential suite
Group C
Residential use only

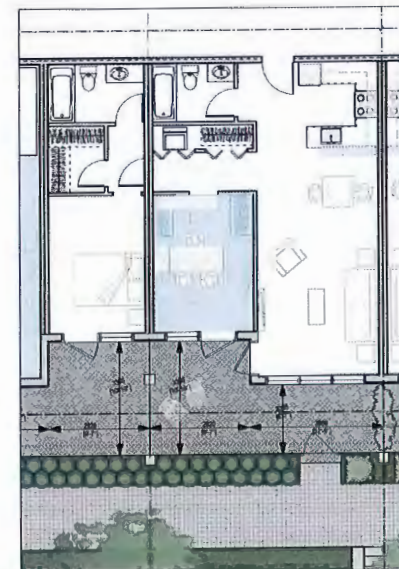
1 Building 2 Residential Suite Plans - Ground Floor
Scale 1:100
A2.9



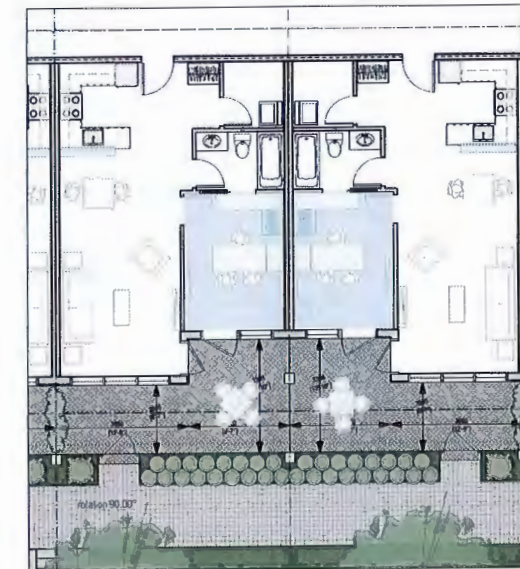
Suite 101 Residential Suite
78.1 m2 / 840 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Residential Entry Foyer
Room 102



Suite 103 84 m2 / 904 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Suite 104 59.6 m2 / 642 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

Suite 105 59.6 m2 / 642 R2
1 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling



Suite 106 78.1 m2 / 840 R2
2 bedroom dwelling
Group C Residential use
Home occupancy permitted
Parking : 1.3 stalls per dwelling

2 Building 2 Suite Plans - Entry Vestibule
Scale 1:100
A2.9



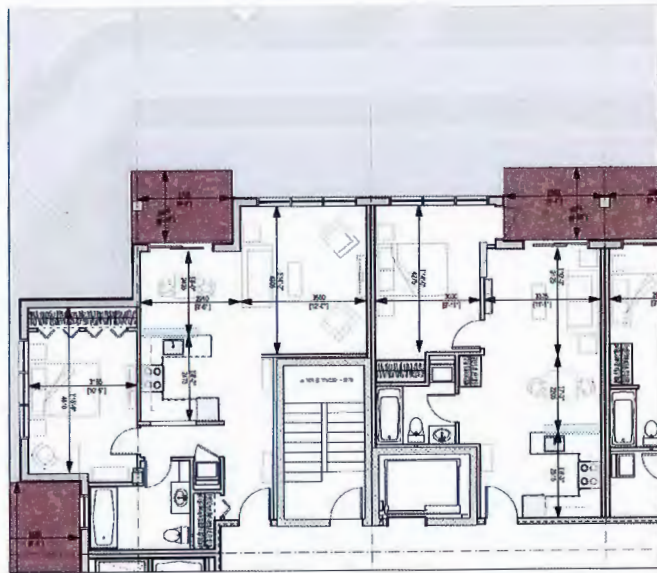
THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA



no.	18	18 January 2017	Development Permit Application
no.	09	09 April 2016	Planning Commentary
no.	08	08 October 2015	Permitting Submission
date	June 2014	10/11/14	
drawn by			
checked by			
scale	AS SHOWN		314.75

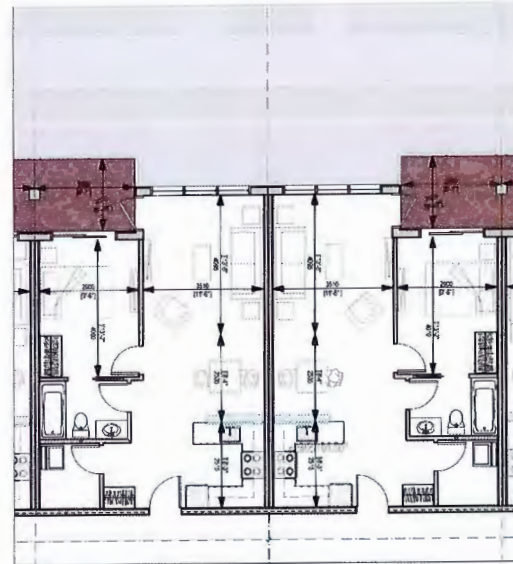
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THE WESTBAY TRIANGLE HEAD, CORN. OF LYALL STREET, ESQUIMALT	
Building 2 - Suite Floor Plans	
drawing no. A2.9	sheet no. A2.9



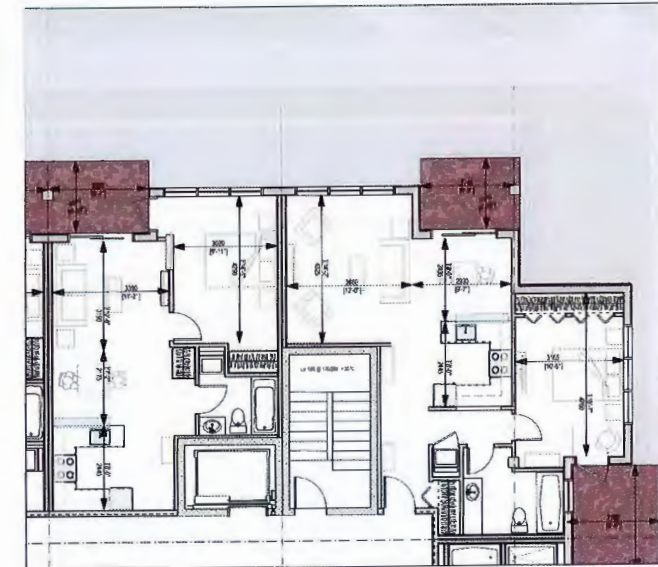
Suite 212 68.4 m2 / 736 ft2
1 bedroom Residential suite

Suite 211 53.5 m2 / 576 ft2
1 bedroom Residential suite



Suite 210 59.6 m2 / 642 ft2
1 bedroom Residential suite

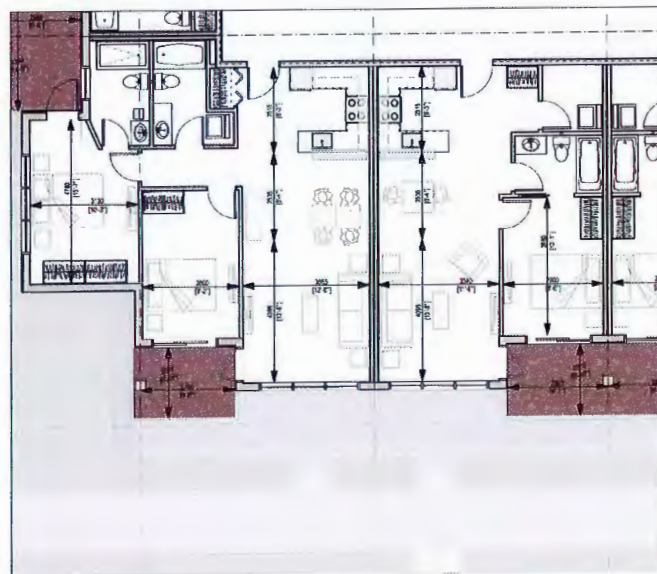
Suite 209 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 208 53.5 m2 / 576 ft2
1 bedroom Residential suite

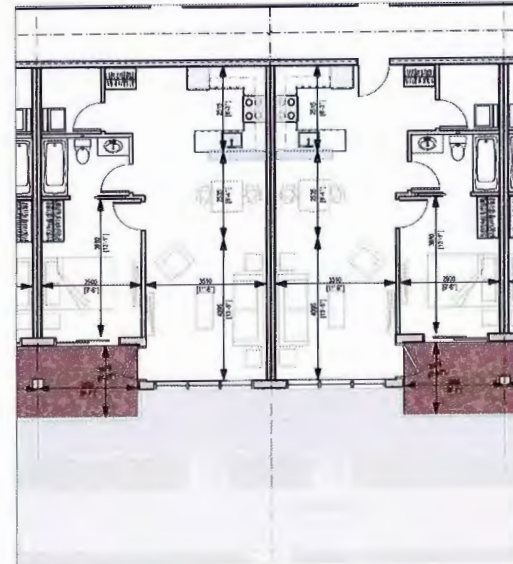
Suite 207 68.4 m2 / 736 ft2
1 bedroom Residential suite

1 Building 2 Suite Plan - 2nd to 5th Flrs
Scale 1:100
A2.10



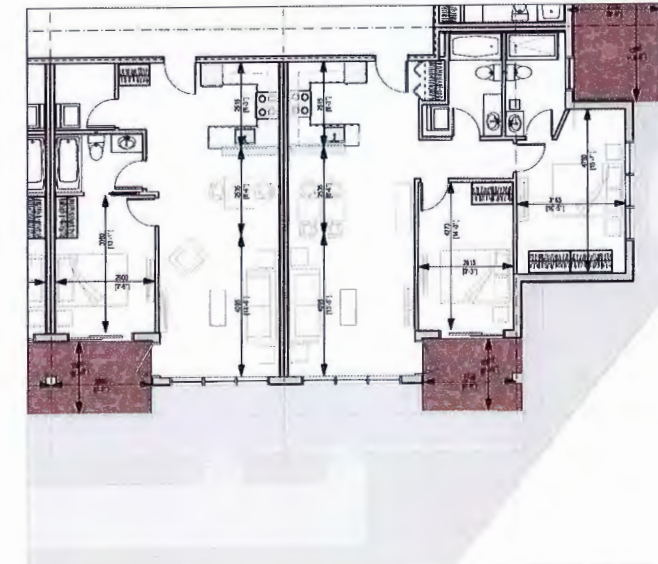
Suite 201 81.4 m2 / 876 ft2
2 bedroom Residential suite

Suite 202 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 203 59.6 m2 / 642 ft2
1 bedroom Residential suite

Suite 204 59.6 m2 / 642 ft2
1 bedroom Residential suite



Suite 205 59.6 m2 / 642 ft2
1 bedroom Residential suite

Suite 206 81.4 m2 / 876 ft2
2 bedroom Residential suite

2 Building 2 Suite Plan - 2nd to 5th flrs
Scale 1:100
A2.10



THE TRIANGLE @ WESTBAY

ESQUIMALT + BRITISH COLUMBIA

no.	date	description
1	18 January 2017	Development Permit Application
2	08 April 2016	Planning Commentary
3	08 December 2015	Rezoning Submission
4	June 2014	Party 14
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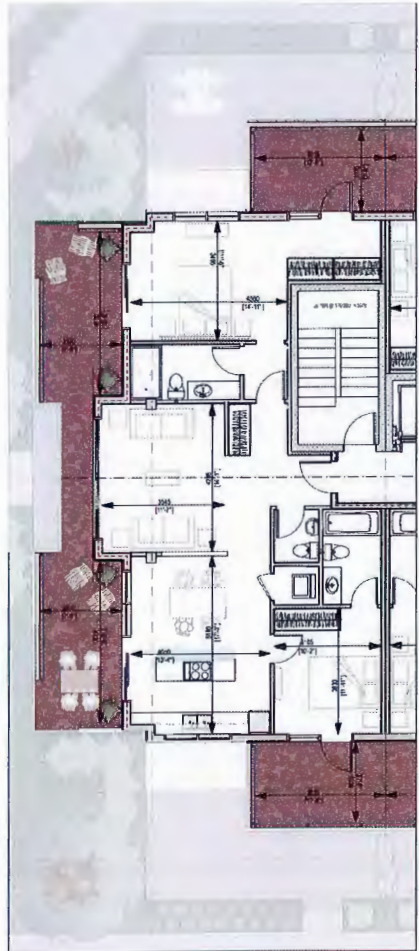
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project: THE WESTBAY TRIANGLE
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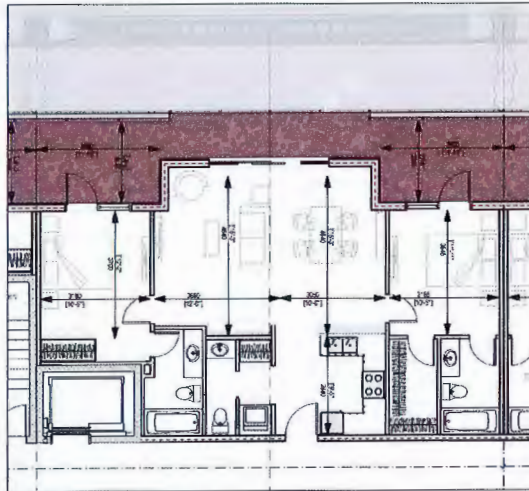
drawing title: Building 2 - Suite Floor Plans

drawing no.: A2.10

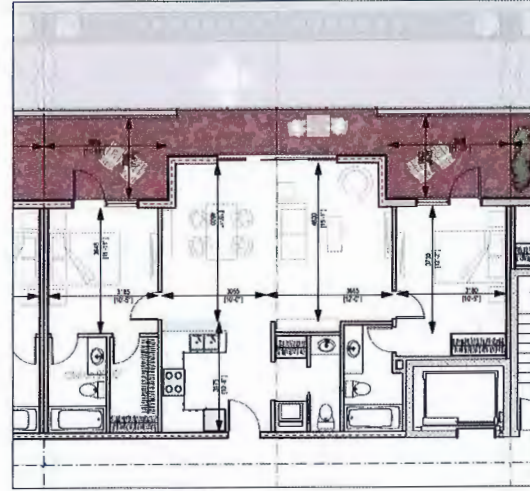
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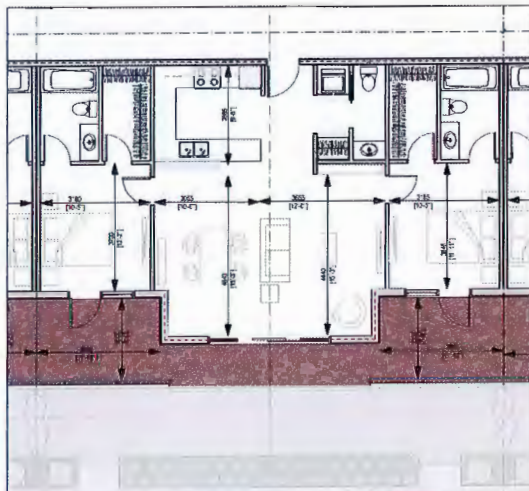
Penthouse Suite 601 97.5 m² / 1050 ft²
 2 master bedroom Residential suite



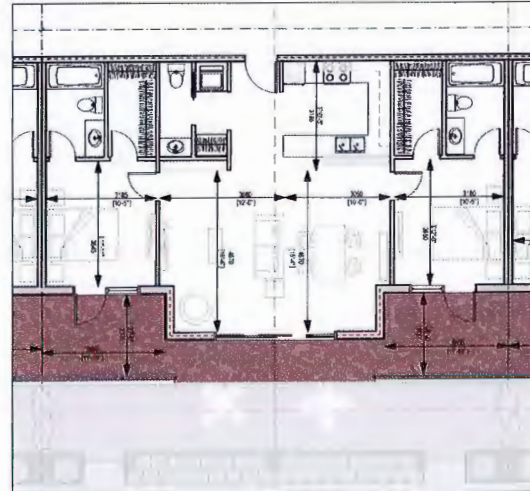
Penthouse Suite 606 91.3 m² / 982 ft²
 2 master bedroom Residential suite



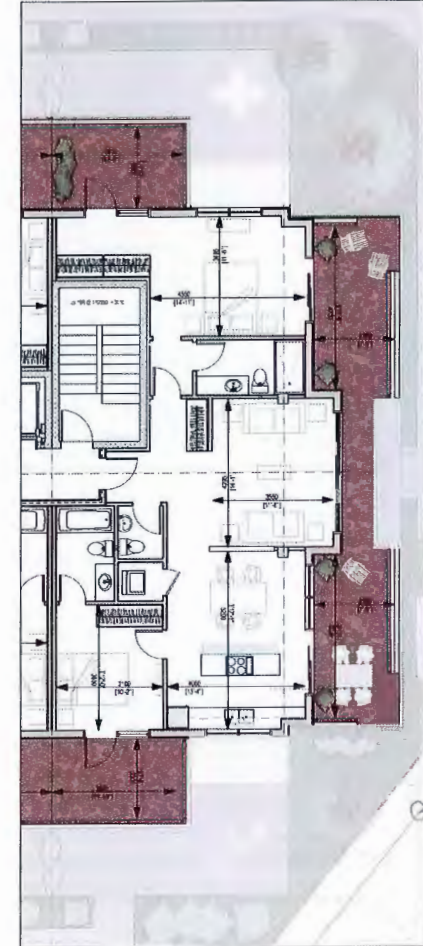
Penthouse Suite 605 91.3 m² / 982 ft²
 2 master bedroom Residential suite



Penthouse Suite 602 97.9 m² / 1053 ft²
 2 master bedroom Residential suite



Penthouse Suite 603 97.9 m² / 1053 ft²
 2 master bedroom Residential suite



Penthouse Suite 604 97.5 m² / 1050 ft²
 2 master bedroom Residential suite

1 Building 2 Suite Plan - Penthouse
 A2.11 Scale 1:100

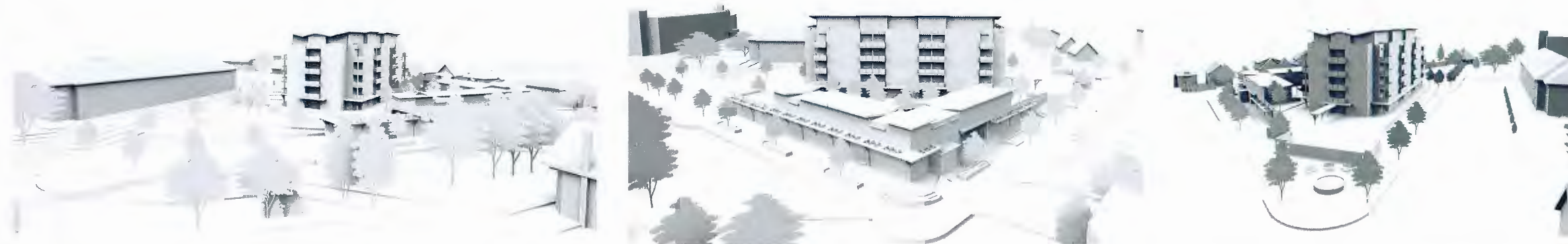
THE TRIANGLE @ WESTBAY
 ESQUIMALT + BRITISH COLUMBIA

rev	18 January 2017	description	Development Permit Application
rev	05 April 2016	description	Planning Commentary
rev	05 December 2015	description	Revising Submission
rev	June 2014	description	
date	2014.30	project	

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Note: This sheet shows artistic renderings only. For exterior finishes, dimensions, elevations, and the relationship to neighboring buildings see elevation sheets A3.2 - A3.8



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THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

NO.	DATE	DESCRIPTION
1	18 January 2017	Issue Development Permit Application
2	06 April 2016	Issue Planning Commentary
3	09 December 2015	Issue Rezoning Submittal
4	June 2014	Final Design
5	2014	Final Design
6	2014	Final Design

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Hillel
 architecture
 101 15th Street, West Vancouver
 BC V8R 1K3
 phone 250-992-9998
 fax 250-992-9999

project: **THE TRIANGLE @ WESTBAY**
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 drawing title: **Project Perspectives**
 drawing no: **A3.0**

As viewed when approaching from Head Street



As viewed from Head Street



As viewed when approaching from the far side of Lyall Street



As viewed from along Lyall Street



As viewed from end of Lyall Street, near Westbay Marina



As viewed from a boat on the water in the Marina



Note: This sheet shows artistic renderings only. For exterior finishes, dimensioned elevations, and their relationship to neighboring buildings see elevation sheets A3.2 - A3.8

THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA



no.	18 January 2017	Development Permit Application
no.	08 April 2016	Planning Consultation
no.	09 December 2015	Planning Submission
no.	June 2014	
no.		
no.	2014 30	

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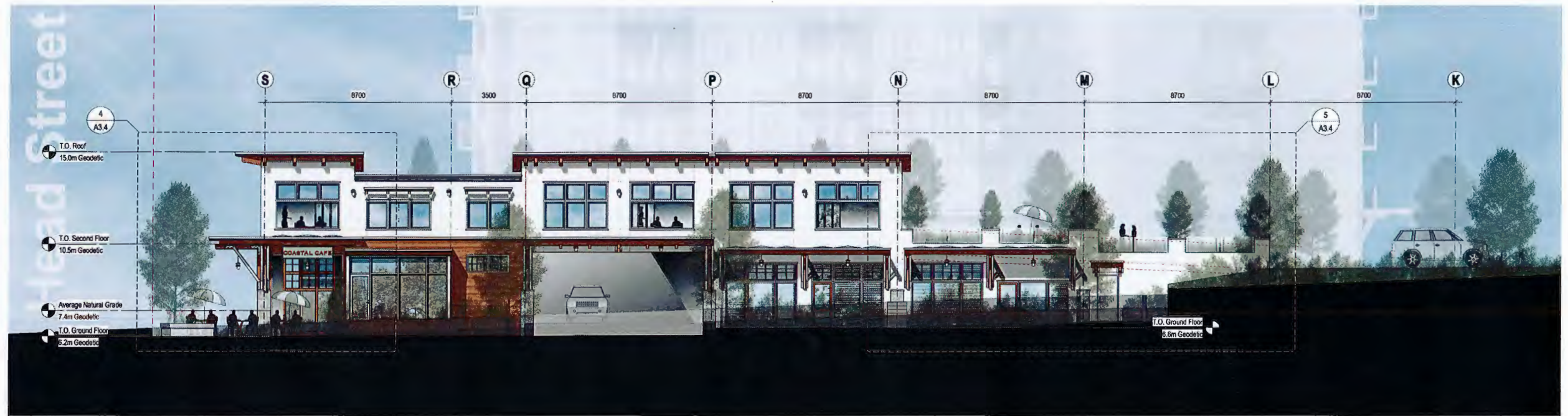
project
THE TRIANGLE AT WESTBAY
HEAD, GURZ & LYALL STREETS, ESQUIMALT

drawing title
Neighbourhood Perspectives

revision no. 01/16/16
drawing no. A3.1



1 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:100
A3.2



2 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:100
A3.2



DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	08 December 2015	DESCRIPTION	Perching Submission
DATE	25 September 2014	DESCRIPTION	Preliminary Review
DATE	June 2014	DESCRIPTION	Site Plan
DATE		DESCRIPTION	
DATE		DESCRIPTION	

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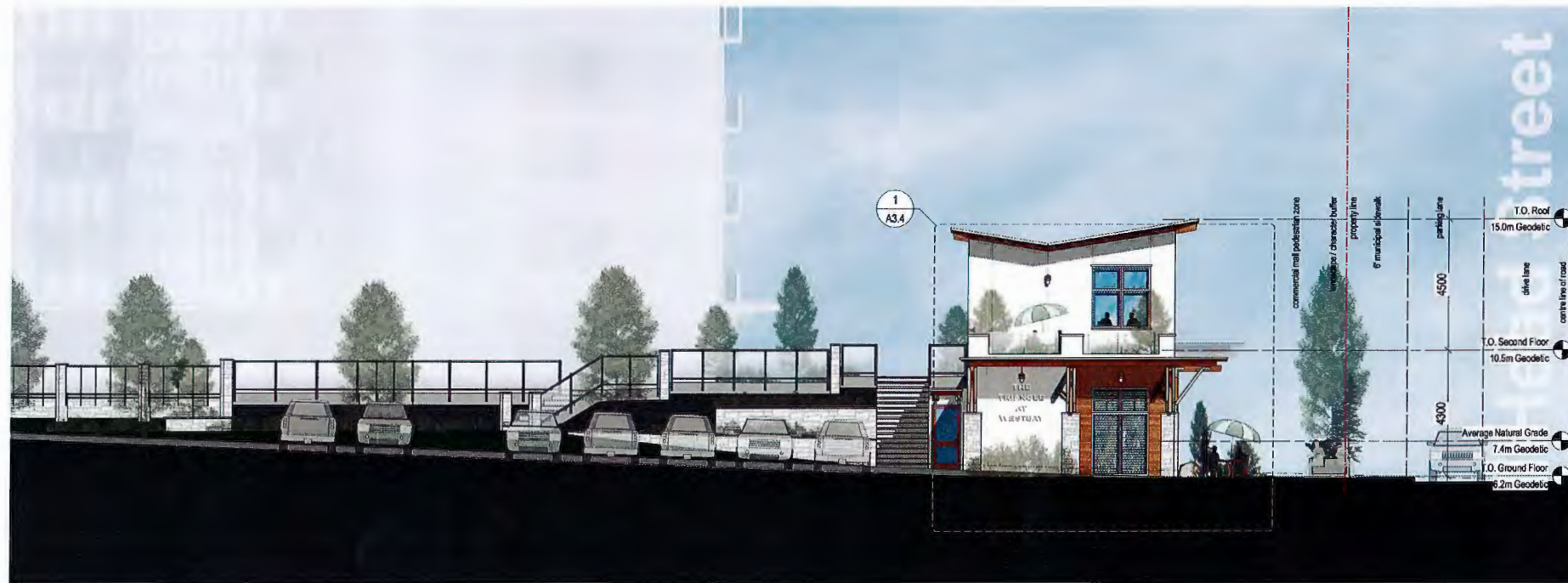
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project: THE TRIANGLE AT WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT

Building 1 - Head Street (northeast) & Gore Street (northwest) Elevations

drawing no. A3.2



1 Building 1 [southeast] Exterior Elevation
Scale 1:100
A3.3

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikkers Cetol finished exposed roof timber framing
- 03 Wood tone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikkers Cetol finished fascia boards
- 05 Wood tone finished cementitious fascia boards as non combustible finish matching cedar finishes of building 1
- 06 Sikkers Cetol finished exposed cedar soffit
- 07 Wood tone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikkers Cetol stain finish. Timber columns extend down to stone veneer pilasters
- 09 Framed sunshade canopy with matching roof flashing. Sikkers Cetol finished soffit over residential windows

Exterior Finishes

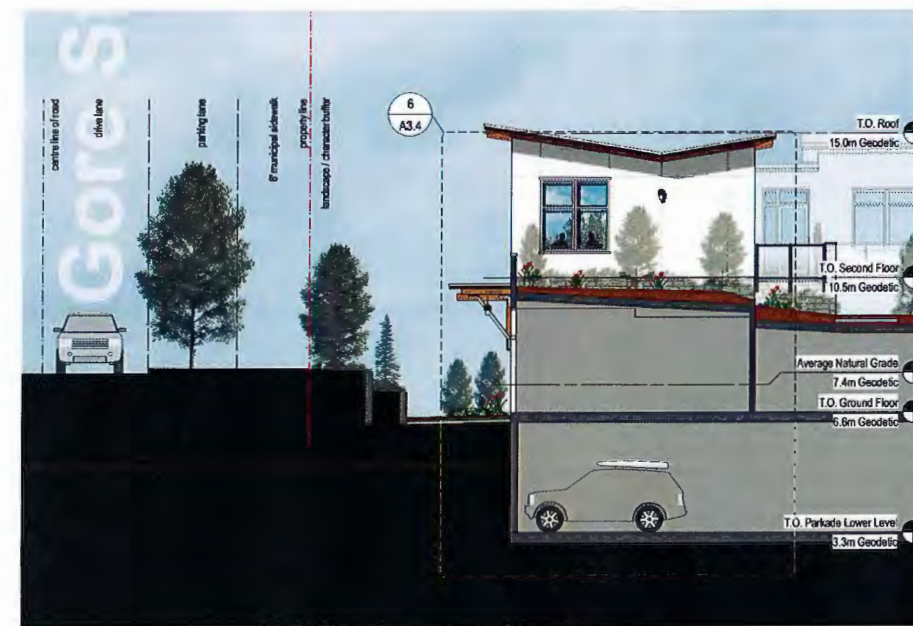
- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray. Colour matched to item 11 above.
- 14 Sikkers Cetol finished 1x4 T & Groove Cedar siding installed square edge out, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding (hard product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations.

Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite gray painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikkers Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight

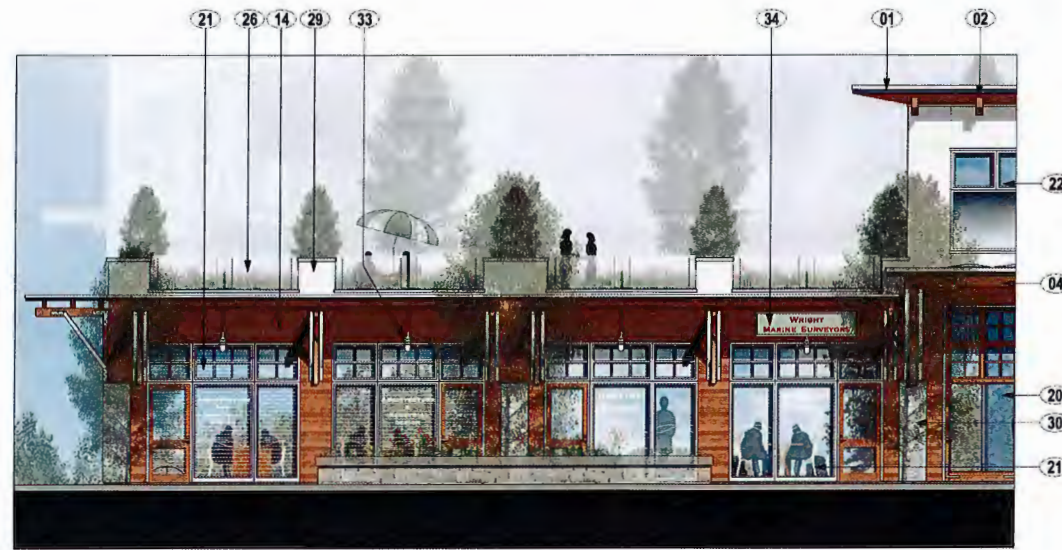


2 Building 1 [southwest] Exterior Elevation
Scale 1:100
A3.3





1 Building 1 [southeast] Exterior Elevation
Scale 1:75
A3.4



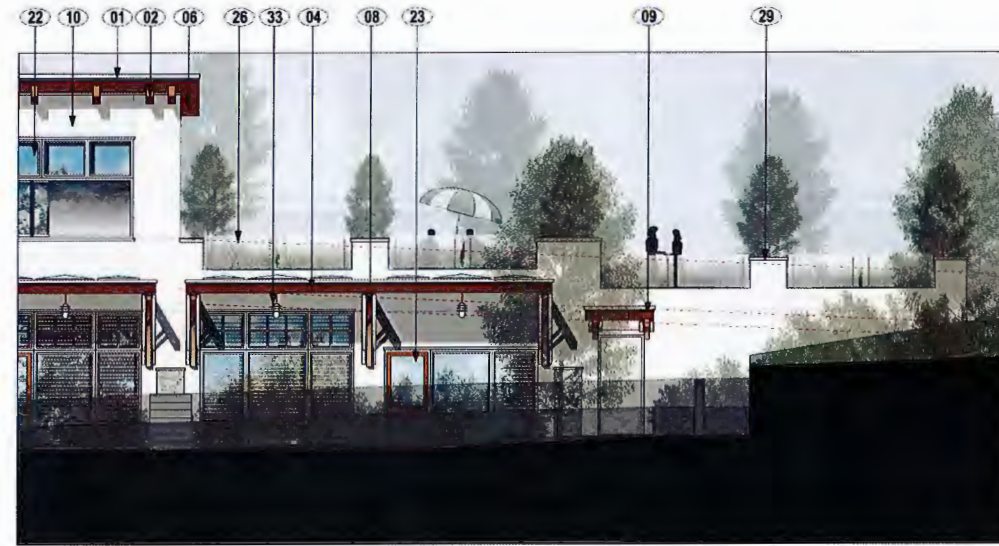
2 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:75
A3.4



3 Building 1 Head Street [northeast] Exterior Elevation
Scale 1:75
A3.4



4 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:75
A3.4



5 Building 1 Gore Street [northwest] Exterior Elevation
Scale 1:75
A3.4



6 Building 1 [southwest] Exterior Elevation
Scale 1:75
A3.4



19 January 2017	Development Permit Application
08 December 2016	Meeting Submission
25 September 2014	Public Review
June 2014	

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project: THE TRIANGLE AT WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT
drawing title: Building 1 - Detail Elevations and Finishes
drawing no: A3.4



1 Building 2 - Lyall Street [south] Exterior Elevation
Scale 1:100
A3.5

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DATE	18 January 2017	DESCRIPTION	Development Permit Application
DATE	06 April 2016	DESCRIPTION	Planning Commentary
DATE	08 December 2015	DESCRIPTION	Mapping Submission
DATE	April 2014	DESCRIPTION	
DATE	2013.33	DESCRIPTION	

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project	THE TRIANGLE AT WESTBAY HEAD, GORE & LYALL STREETS, ESQUIMALT
drawing title	Building 2 - Lyall Street [south] Elevation
drawing no.	A3.5



1 Building 2 - (north) Exterior Elevation
 A3.6 Scale 1:100



no.	16	16 January 2017	owner	Development Permit Application
no.	17	08 April 2018	owner	Planning Commentary
no.	18	08 December 2018	owner	Rezoning Submission
no.	19	April 2019	owner	City 14
no.	20	2019	owner	2019-20
no.	21	2019	owner	2019-21

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	project THE TRIANGLE AT WESTBAY HEAD, GORE & L'ALLIÈRE STREETS, ESQUIMALT
drawing title Building 2 - (north) Elevations	revision no. drawing no. A3.6



1 Building 2 - (east) Exterior Elevation
A3.7 Scale 1:100



1 Building 2 - Gore Street (west) Exterior Elevation
A3.7 Scale 1:100



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no.	rev.	date	description
1	01	16 January 2017	Development Permit Application
1	02	08 April 2018	Planning Committee
1	03	08 December 2018	Planning Submission
1	04	June 2019	Final
1	05	2019.11	Final
1	06	2019.11	Final

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PROJECT:		THE TRIANGLE AT WESTBAY HEAD, GORE & L'YALL STREETS, ESQUIMALT	
DRAWING NO.:		Building 2 - Gore Street (west) & East Elevations	
DATE:		2019.11	A3.7



1 Building 2 - Lyall Street (south) Exterior Elevation
A3.8 Scale 1:75



2 Building 2 - Gore Street (east) Exterior Elevation
A3.8 Scale 1:75



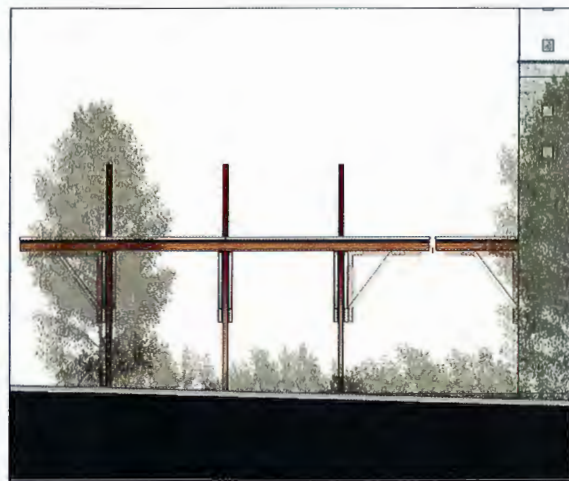
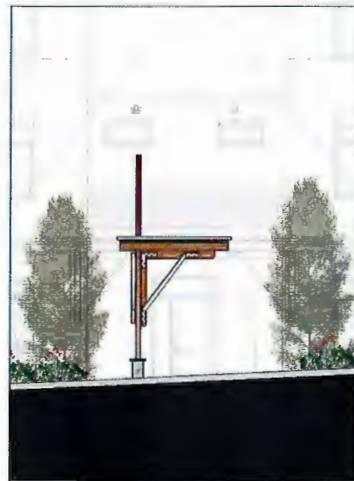
3 Building 2 - Lyall Street (south) Exterior Elevation
A3.8 Scale 1:75



4 Building 2 - Lyall Street (south) Exterior Elevation
A3.8 Scale 1:75



5 Building 2 - Gore Street (east) Exterior Elevation
A3.8 Scale 1:75



6 Building 2 - Side Entry Roof
A3.8 Scale 1:75

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikksens Cetol finished exposed roof timber framing
- 03 Wood lone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikksens Cetol finished fascia boards
- 05 Wood lone finished cementitious fascia boards as non combustible finish matching cedar finishes of building 1
- 06 Sikksens Cetol finished exposed cedar soffit
- 07 Wood lone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikksens Cetol stain finish. Timber columns extend down to stone veneer pilasters
- 09 Framed sunshade canopy with matching roof flashing, Sikksens Cetol finished soffit over residential windows

Exterior Finishes

- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray, Colour matched to item 11 above.
- 14 Sikksens Cetol finished 1x4 T & Groove Cedar siding installed square edge cut, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood lone 1x4 ship lap siding (hardi product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations.

Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite gray painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikksens Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Uprand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight



DATE	BY	FOR	DESCRIPTION
19 January 2017	Development	Permit Application	
05 April 2016	Planning	Conceptual	
05 December 2015	Planning	Conceptual	
June 2014	Planning	Submission	
2014.03			

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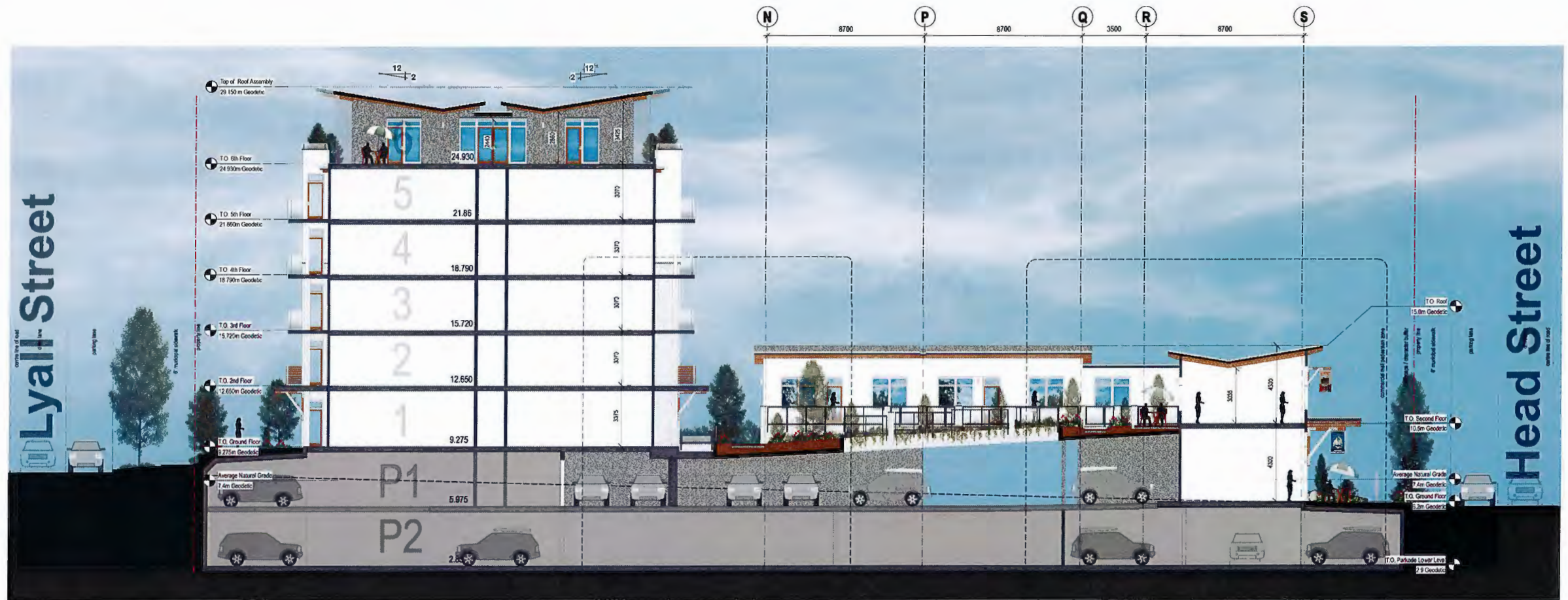
Hillel architecture

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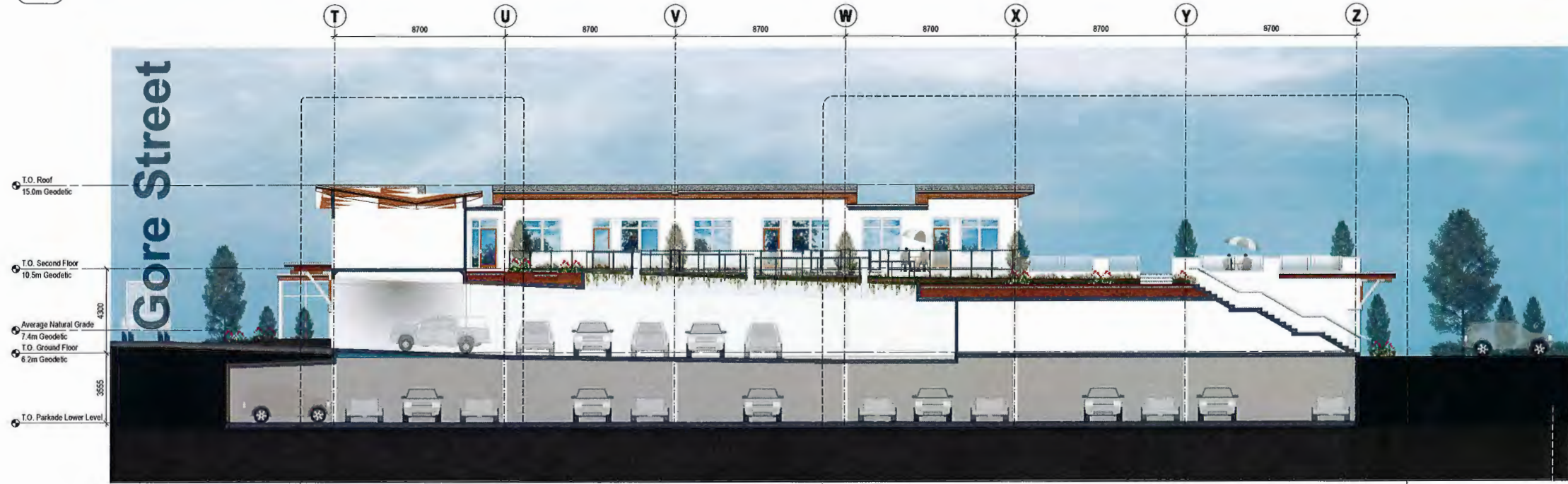
PROJECT: THE TRIANGLE @ WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT

Drawn by: Building 2 - Detail Elevations and Finishes

Scale: A3.8



1 Buildings 1 & 2 - Section Facing [northwest] Gore Street
A4.1 Scale: 1:125



1 Building 1 - Section Facing [northeast] Head Street
A4.1 Scale: 1:125



15 January 2017	15 January 2017	Issue of Development Permit Application
05 April 2016	05 April 2016	Reviewing Commentary
08 December 2015	08 December 2015	Receiving Submission
June 2014	June 2014	Preparation
2014/20	2014/20	Project Number

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THE TRIANGLE @ WESTBAY

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Project	THE TRIANGLE AT WESTBAY HEAD, GORE & LYALL STREETS, ESQUIMALT
Drawing No.	Building 1 & 2 - Section Facing [northwest] Gore Street
Sheet No.	A4.1

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1 Building 2 - Section Facing [south] Lyall Street
 A4.2 Scale 1:100

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THE TRIANGLE @ WESTBAY
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REV	DATE	DESCRIPTION
1	14 January 2017	Development Permit Application
2	05 April 2016	Planning Committee
3	18 December 2015	Planning Submission
4	June 2014	PRELIM

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project: THE TRIANGLE AT WESTBAY
 HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title: Building 2 - Section Facing [south] Lyall Street

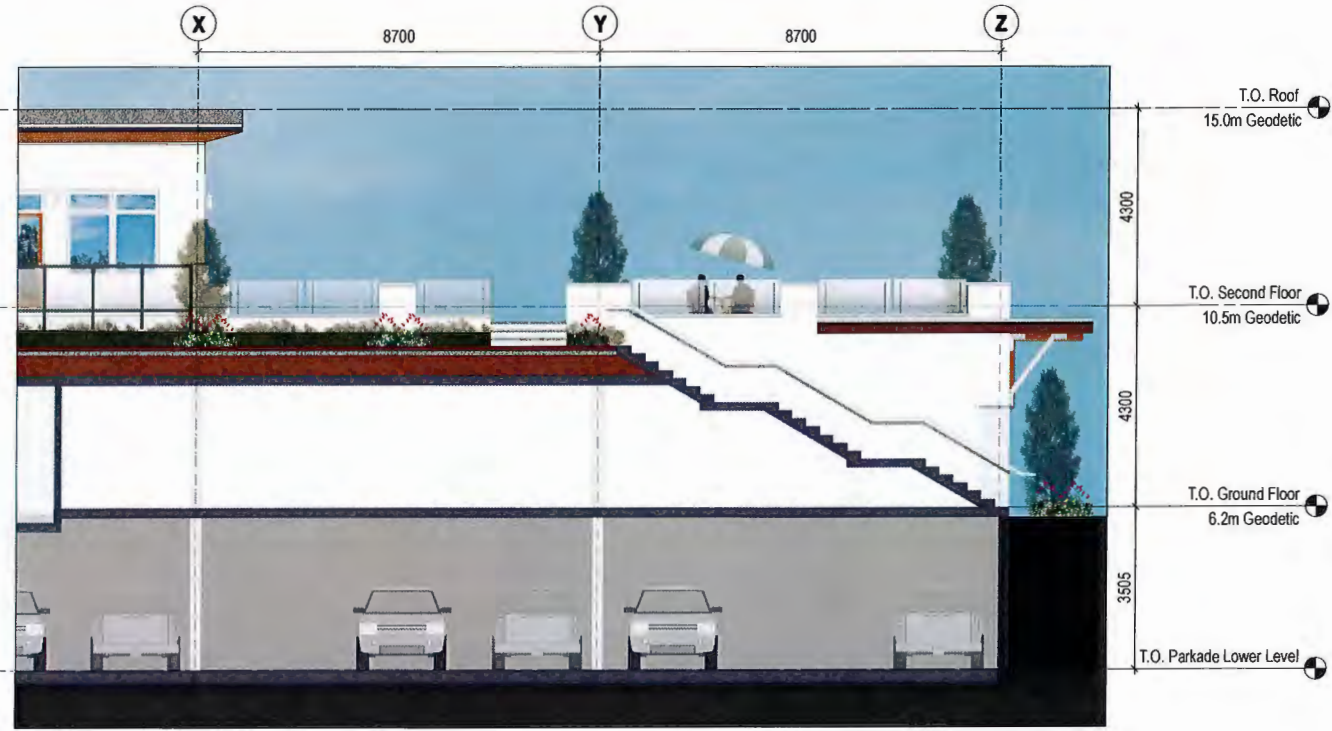
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sheet no.: A4.2

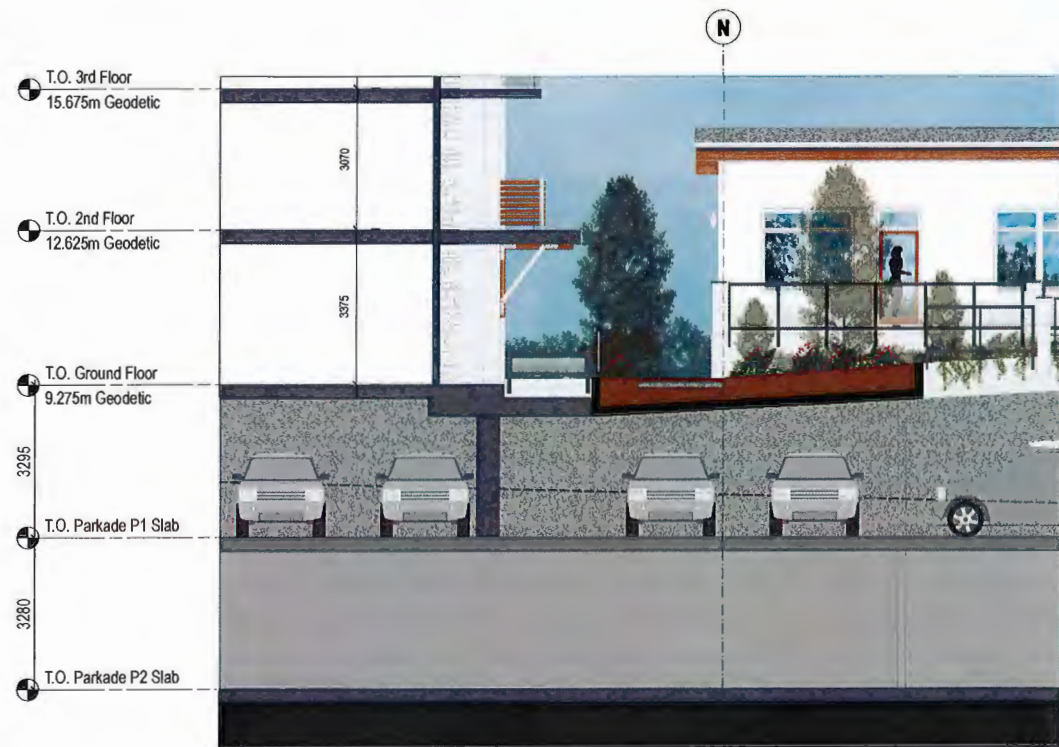
101 45th Oak Bay Avenue
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 fax 250.592.9998



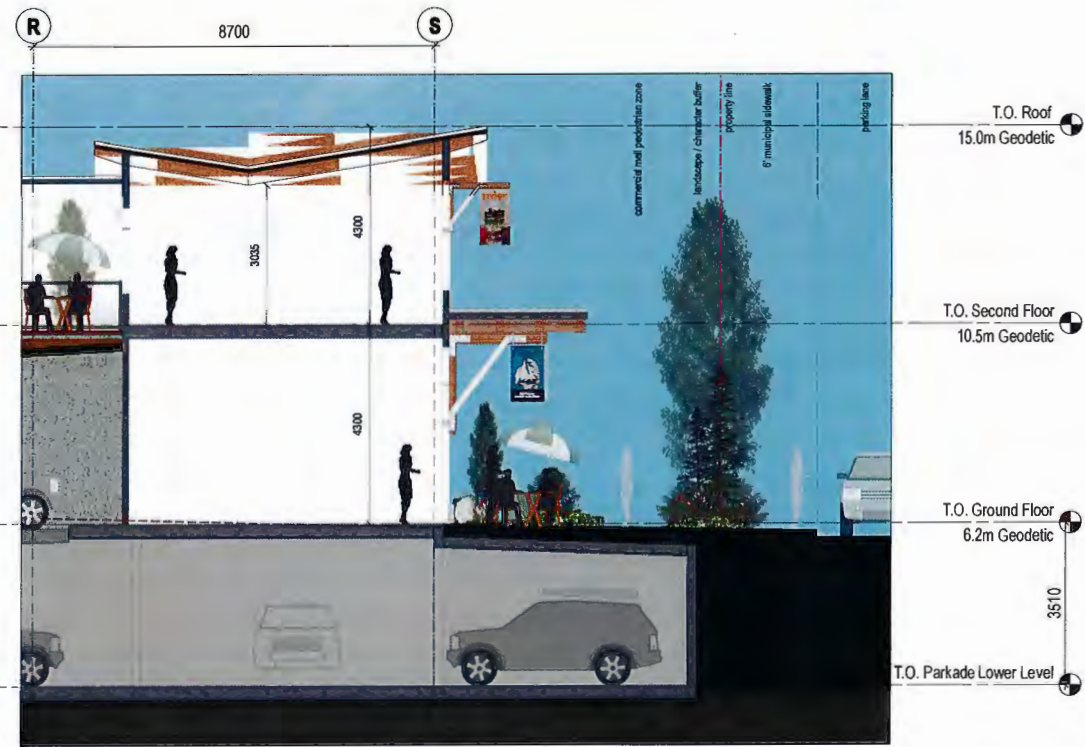
1 Buildings 1 - Detail Section
Scale 1:75
A4.3



2 Building 1 - Detail Section
Scale 1:75
A4.3



3 Buildings 1 & 2 - Detail Section
Scale 1:75
A4.3



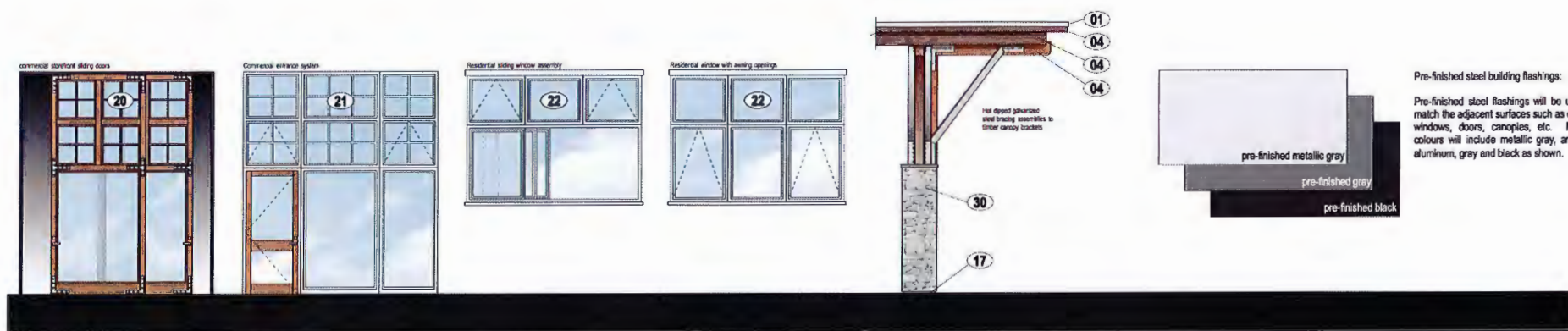
4 Building 1 - Detail Section
Scale 1:75
A4.3



THE TRIANGLE @ WESTBAY

ESQUIMALT + BRITISH COLUMBIA

Colour And Materials Palette - Building 1



Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikkers Cetol finished exposed roof timber framing
- 03 Wood tone finished cementitious board finish as non combustible finish matching cedar finishes of building 1
- 04 Clear Sikkers Cetol finished fascia boards
- 05 Wood tone finished cementitious fascia boards as non combustible finish matching cedar finishes of building 1
- 06 Sikkers Cetol finished exposed cedar soffit
- 07 Wood tone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikkers Cetol stain finish. Timber columns extend down to stone veneer pilasters
- 09 Framed sunshade canopy with matching roof flashing. Sikkers Cetol finished soffit over residential windows

Exterior Finishes

- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray. Colour matched to item 11 above.
- 14 Sikkers Cetol finished 1x4 T & Groove Cedar siding installed square edge out, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding (hardi product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
- 17 Dark gray elastomeric paint finish to exposed concrete foundations,

Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite grey painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikkers Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Upland balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

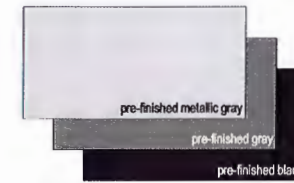
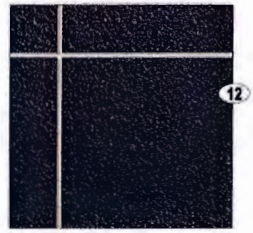
- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight



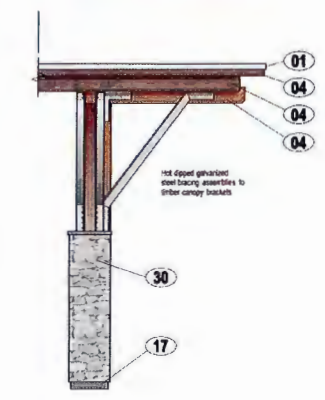
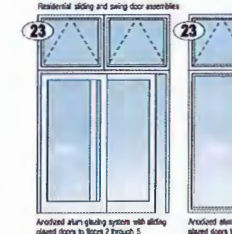
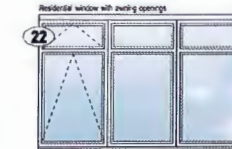
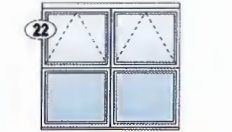
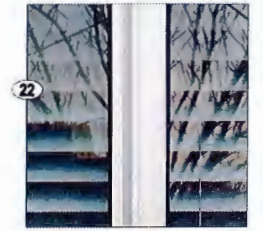
THE TRIANGLE AT WESTBAY

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Colour And Materials Palette - Building 2



Pre-finished steel building flashings:
Pre-finished steel flashings will be used to match the adjacent surfaces such as exterior windows, doors, canopies, etc. Multiple colours will include metallic gray, anodized aluminum, gray and black as shown.



Roofing

- 01 Pre-finished metallic gray steel roof flashing.
- 02 Clear Sikkens Cetol finished exposed roof timber framing
- 03 Wood tone finished cementitious board finish as non-combustible finish matching cedar finishes of building 1
- 04 Clear Sikkens Cetol finished fascia boards
- 05 Wood tone finished cementitious fascia boards as non-combustible finish matching cedar finishes of building 1
- 06 Sikkens Cetol finished exposed cedar soffit
- 07 Wood tone finished cementitiousboards to soffit to match building 1
- 08 Structural timber framed roof canopy with Sikkens Cetol stain finish. Timber columns extend down to stone veneer pilasters
- 09 Framed sunshade canopy with matching roof flashing, Sikkens Cetol finished soffit over residential windows

Exterior Finishes

- 10 Panelized, smooth stucco c/w metallic gray steel reveals Colour 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals Colour 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals Colour 3: Graphite
- 13 Stucco accent band, colour: Gray, Colour matched to item 11 above.
- 14 Sikkens Cetol finished 1x4 T & Groove Cedar siding installed square edge out, bandsawn face, tight joint, uniform surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding (hardi product or woodtone rustic series)
- 16 Natural stone veneer: Eldorado Natural Stone, Chisel Cut Pebble Gray
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Exterior Components

- 20 Style and rail framed glazed solid wood sliding storefront door system. Concealed overhead rollers / track.
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Contrasting coloured aluminum glazed balcony doors within clear anodized aluminum frame. Sliding and swing door configurations.
- 24 Opaque spandrel glazing panels, surface mounted with clear anodized veneering aluminum frame sections or equivalent.
- 25 Graphite gray painted insulated hollow core steel door.
- 26 Side mounted balcony railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels.
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with Sikkens Cetol finished 1x4 cedar screening members
- 28 Ground floor terrace railing of powder coated posts and pickets at terraces and central open triangle. Clear tempered glazed accent panels.
- 29 Upside balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals and pre-finished metallic gray steel flashing cap

Exterior Features

- 30 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete column with a concrete cap
- 31 Natural stone veneer (Eldorado Natural Stone, Chisel Cut Pebble Gray) on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 Aluminum framed skylight



THE TRIANGLE AT WESTBAY

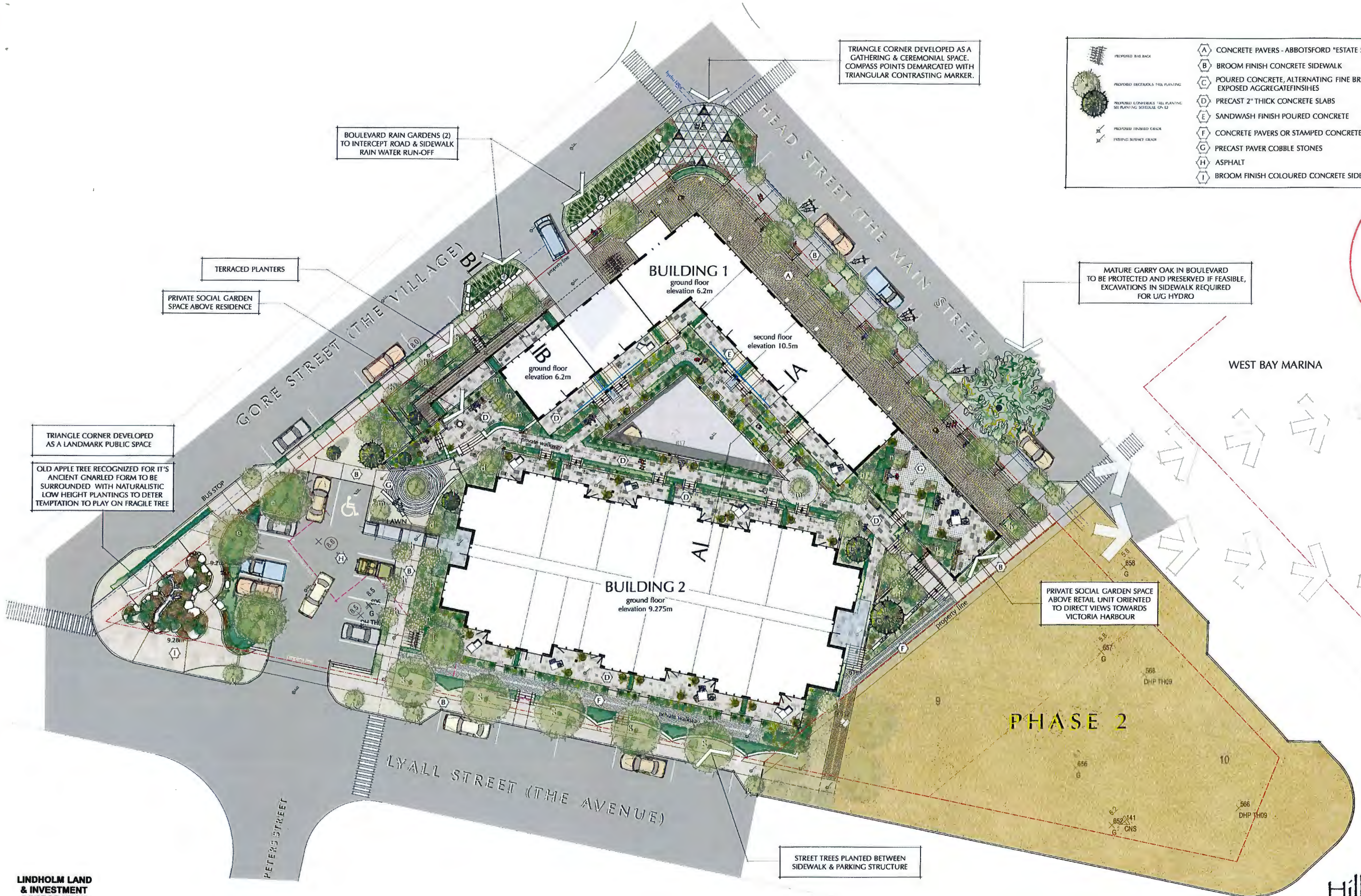
ESQUIMALT • BRITISH COLUMBIA

	A CONCRETE PAVERS - ABBOTSFORD "ESTATE STONE"
	B BROOM FINISH CONCRETE SIDEWALK
	C POURED CONCRETE, ALTERNATING FINE BROOM & EXPOSED AGGREGATE FINISHES
	D PRECAST 2" THICK CONCRETE SLABS
	E SANDWASH FINISH POURED CONCRETE
	F CONCRETE PAVERS OR STAMPED CONCRETE
	G PRECAST PAVER COBBLE STONES
	H ASPHALT
	I BROOM FINISH COLOURED CONCRETE SIDEWALK

RECEIVED

JAN 20 2017

CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES



TRIANGLE CORNER DEVELOPED AS A GATHERING & CEREMONIAL SPACE. COMPASS POINTS DEMARCATED WITH TRIANGULAR CONTRASTING MARKER.

BOULEVARD RAIN GARDENS (2) TO INTERCEPT ROAD & SIDEWALK RAIN WATER RUN-OFF

TERRACED PLANTERS

PRIVATE SOCIAL GARDEN SPACE ABOVE RESIDENCE

MATURE GARRY OAK IN BOULEVARD TO BE PROTECTED AND PRESERVED IF FEASIBLE. EXCAVATIONS IN SIDEWALK REQUIRED FOR U/G HYDRO

TRIANGLE CORNER DEVELOPED AS A LANDMARK PUBLIC SPACE

OLD APPLE TREE RECOGNIZED FOR IT'S ANCIENT GNARLED FORM TO BE SURROUNDED WITH NATURALISTIC LOW HEIGHT PLANTINGS TO DETER TEMPTATION TO PLAY ON FRAGILE TREE

PRIVATE SOCIAL GARDEN SPACE ABOVE RETAIL UNIT ORIENTED TO DIRECT VIEWS TOWARDS VICTORIA HARBOUR

STREET TREES PLANTED BETWEEN SIDEWALK & PARKING STRUCTURE

THE TRIANGLE @ WESTBAY
 ESQUIMALT + BRITISH COLUMBIA

DATE	15 Jan 2017	DESCRIPTION	Development Permit Application
DATE	18 Jan 2017	PROJECT	
DATE	ASDC 28	PROJECT	CAF
DATE	1.2.20	PROJECT	

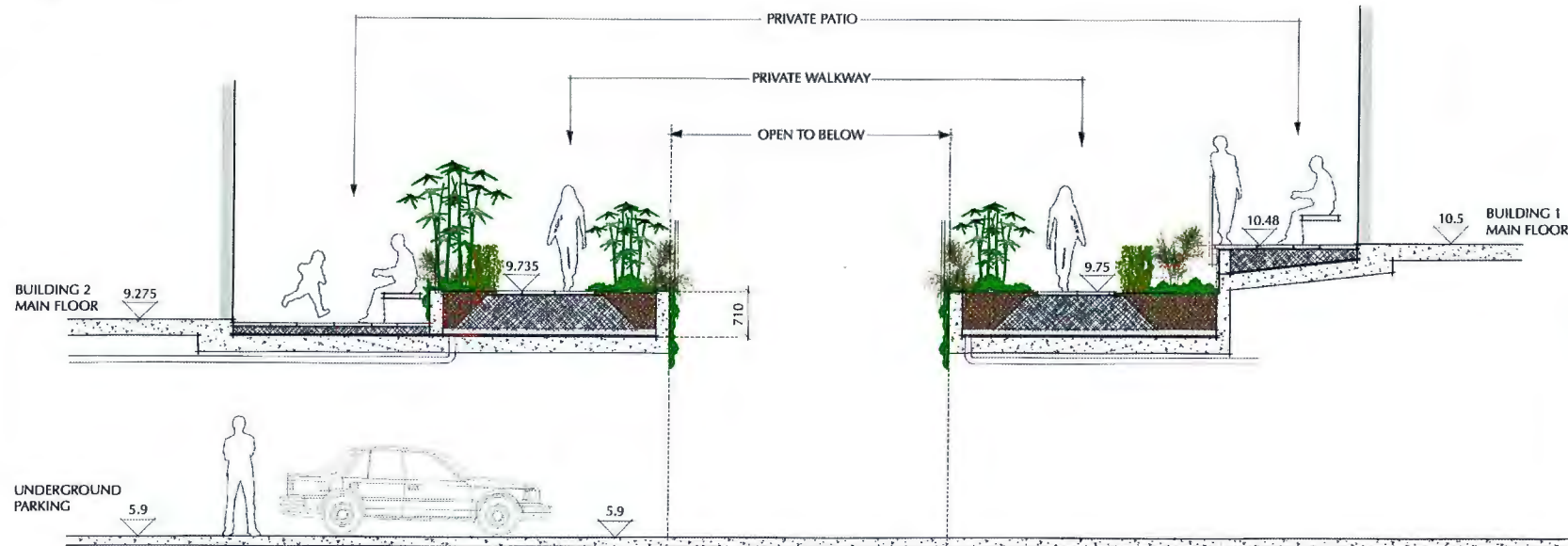
Permission is granted for these drawings to be reprinted by Lindholm Land and Investment Corporation

Hillel
 architecture

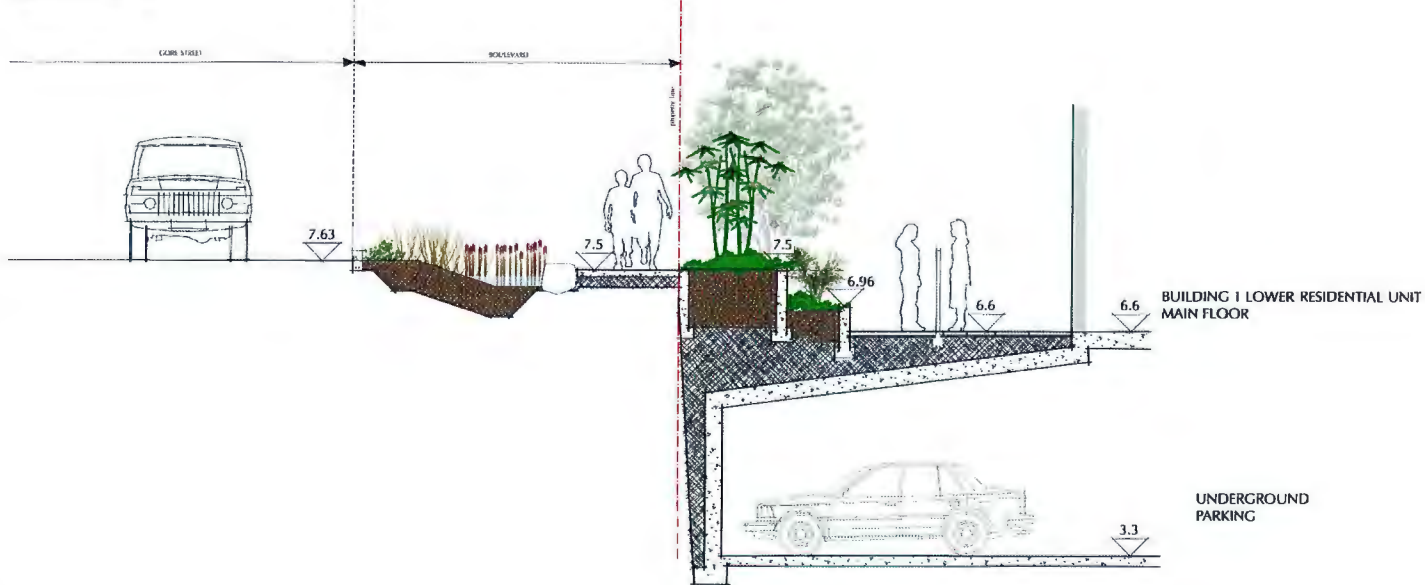
101 15th Oak Bay Avenue
 Victoria BC V8K 1K3
 phone 250-592-9998
 fax 250-592-9978

PROJECT	THE WESTBAY TRIANGLE HEAD, GORE & LYALL STREETS, ESQUIMALT
DRAWING TITLE	LANDSCAPE CONCEPT
DATE	1.2.20
SCALE	
DESIGNER	
PROJECT NO.	
DRAWING NO.	L1

SECTION A - A



SECTION B - B



PLANT SCHEDULE	BOTANICAL NAME	COMMON NAME	HEIGHT	CALIPER	ROOTBALL & BRANCHING	CONTAINER SIZE	SPACING	QUANTITY	QUALITY	INSTRUCTIONS
TREES										
a	ACER CIRCINATUM	VINE MAPLE	250-300 CM	4 CM	B&B, 60 CM DIAM. BALL, 3 STRONG STEMS		AS SHOWN	4	CNLA NO.1 QUALITY	STAKED
b	CORNUS KOUSA SATOMI	SATOMI FLOWERING DOGWOOD	250-300 CM	4 CM	B&B, 60 CM DIAM. BALL, 3 STRONG STEMS		AS SHOWN	6	CNLA NO.1 QUALITY	STAKED
c	CHAMAECYPARIS NODOSKATENSIS GREEN ARROW	GREEN ARROW NOOTKA CYPRESS	300 CM		B&B, 120 CM DIAM. BALL		AS SHOWN	2	CNLA NO.1 QUALITY	STAKED
d	CERCIS CANADENSIS TRISING SUN	TRISING SUN REDBUD	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	2	CNLA NO.1 QUALITY	STAKED
e	ACER RUBRUM 'OCTOBER GLORY'	RED MAPLE	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	3	CNLA NO.1 QUALITY	STAKED
f	CORNUS EDOGUS WHITE WONDER	FLOWERING DOGWOOD	300-425 CM	5 CM	B&B, 60 CM DIAM. BALL		AS SHOWN	3	CNLA NO.1 QUALITY	STAKED
g	QUERCUS BILBOA 'AUTUMN GOLD'	QUERCUS	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	3	CNLA NO.1 QUALITY	STAKED
h	MAGNOLIA GALAXY	GALAXY MAGNOLIA	300-425 CM	6 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	3	CNLA NO.1 QUALITY	STAKED
i	CHAMAECYPARIS OBTUSA GRACILIS	SLENDER HINOKI FALSE CYPRESS	200 CM		CONTAINER	#15 POT	AS SHOWN	5	CNLA NO.1 QUALITY	STAKED
j	PRUNUS X YEDOENSIS 'AKERONU'	JAPANESE FLOWERING CHERRY	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	6	CNLA NO.1 QUALITY	STAKED
k	CERCIS CANADENSIS 'APPALACHIAN RED'	CANADIAN REDBUD	300-425 CM	5 CM	B&B, 70 CM DIAM. BALL, 10 BRANCHES		AS SHOWN	6	CNLA NO.1 QUALITY	STAKED
SHRUBS - TOTAL AREA, 230SQ.M. AVERAGE SPACING - 1 PLANT / SQ.M. = 230 SHRUBS.										
SHRUBS - NATIVE										
l	ARBITUS UNEDO	STRAWBERRY TREE	250-300 CM	MULTI	B&B, 60 CM DIAM. BALL		AS SHOWN	19	CNLA NO.1 QUALITY	STAKED
m	MAGNOLIA X 'YULCAN'	RED FL. MAGNOLIA	175-200 CM		B&B, 55 CM DIAM. BALL, 4 STRONG STEMS		AS SHOWN	5	CNLA NO.1 QUALITY	STAKED
n	COTINUS COGGYGRIA	AMERICAN SMOKE TREE	60-100 CM		CONTAINER	#5 POT	AS SHOWN	4	CNLA NO.1 QUALITY	
SHRUBS - ORNAMENTAL										
	HYDRANGEA BLUE BIRD	LACECAP HYDRANGEA	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO.1 QUALITY	
	LEYCESTERIA GOLDEN LANTERNS	GOLDEN PHEASANT BERRY	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO.1 QUALITY	
	MAGNOLIA X 'CHARITY'	MAGNOLIA	60-100 CM		3 CANES	#5 POT	AS SHOWN		CNLA NO.1 QUALITY	
	MAGNOLIA DOMESTICA	HEAVENLY BAMBOO	60-100 CM		SPREAD 35 CM	#5 POT		1000	CNLA NO.1 QUALITY	
	PHILADELPHUS LIMONEL	MOCK ORANGE	40-100 CM		5 CANES	#5 POT		1000	CNLA NO.1 QUALITY	
	PRUNUS MUGO 'PUMILIO'	MOUNTAIN PINE	50 CM		SPREAD 50 CM	#5 POT		1000	CNLA NO.1 QUALITY	
	RUBUS CALYCINOIDES	CREeping RASPBERRY	15-30 CM		3 CANES	#1 POT		500	CNLA NO.1 QUALITY	
	VACCINIUM CORYMBOSUM 'DUKE'	'DUKE' EDIBLE BLUEBERRY	25-60 CM		4 CANES	#2 POT		1000	CNLA NO.1 QUALITY	
PERENNIALS - TOTAL AREA, 235 SQ.M. AVERAGE SPACING - 3 PLANTS / SQ.M. = 705 SHRUBS.										
PERENNIALS & ORNAMENTAL GRASSES										
	DIERAMA PULCHERRIMA	ANGEL'S FISHING ROD	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		600	CNLA NO.1 QUALITY	
	EPIMEDIUM RUBRUM	BARRENWORT	15-18 CM			#1 POT		600	CNLA NO.1 QUALITY	
	GERANIUM MACROBOTHYDUM	CRANESBILL	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		600	CNLA NO.1 QUALITY	
	HEMEROCALLIS 'STELLA D'ORO'	DAY LILY YELLOW FL.	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		600	CNLA NO.1 QUALITY	
	IRIS SANSATA 'ORIENTAL LADY'	PINK JAPANESE IRIS	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		600	CNLA NO.1 QUALITY	
	MISCANTHUS 'GRACILLIMUS'	JAPANESE SILVER GRASS				#2 POT		1000	CNLA NO.1 QUALITY	
HEDGE PLANTS										
	BUXUS SEMPERVIRENS	BOX	25-60 CM			#2 POT		800	400	CNLA NO.1 QUALITY
	LONICERA NITIDA 'RED TIPS'	CHINESE PRIVET	25-60 CM			#2 POT		750	9	CNLA NO.1 QUALITY
	ARBITUS UNEDO	STRAWBERRY TREE	60-100 CM			#5 POT		1000	18	CNLA NO.1 QUALITY
	TAXUS X 'HICKSII'	HICKS'S YEW	60-100 CM			#5 POT		750	28	CNLA NO.1 QUALITY
	TSUGA HETEROPHYLLA	HEMLOCK	60-100 CM			#5 POT		1000	27	CNLA NO.1 QUALITY
RAIN GARDEN PLANTS										
	CAREX ELATA 'AUREA'	GOLDEN SEDGE	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		500	80	CNLA NO.1 QUALITY
	CAREX OBNOPTA	SLOUGH SEDGE	15-18 CM		2-FAN, BLOOMING SIZE	#1 POT		500	80	CNLA NO.1 QUALITY
	CORNUS STOLONIFERA 'KELSEYI'	DWARF RED OSIER DOGWOOD	50 CM		SPREAD 50 CM	#2 POT		1000	35	CNLA NO.1 QUALITY

NOTE: PLANT SPECIES, CULTIVARS, SIZES AND NUMBERS TO BE REFINED AT DETAIL DESIGN STAGE



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THE TRIANGLE @ WESTBAY
ESQUIMALT + BRITISH COLUMBIA

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project: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing no: LANDSCAPE SECTIONS & PLANTING
sheet no: L2

Plan Showing Proposed Building Location
 Situated on Proposed Consolidation of:
 - 1, 2, 4, 5, 6, 7, 8, Block H, Plan 292;
 - Strata Plan 509;
 all of Section 11, Esquimalt District.

Township of Esquimalt



The intended plot size of this plan is 864 mm in width by 559 mm in height (D size) when plotted at a scale of 1:250

Legend:

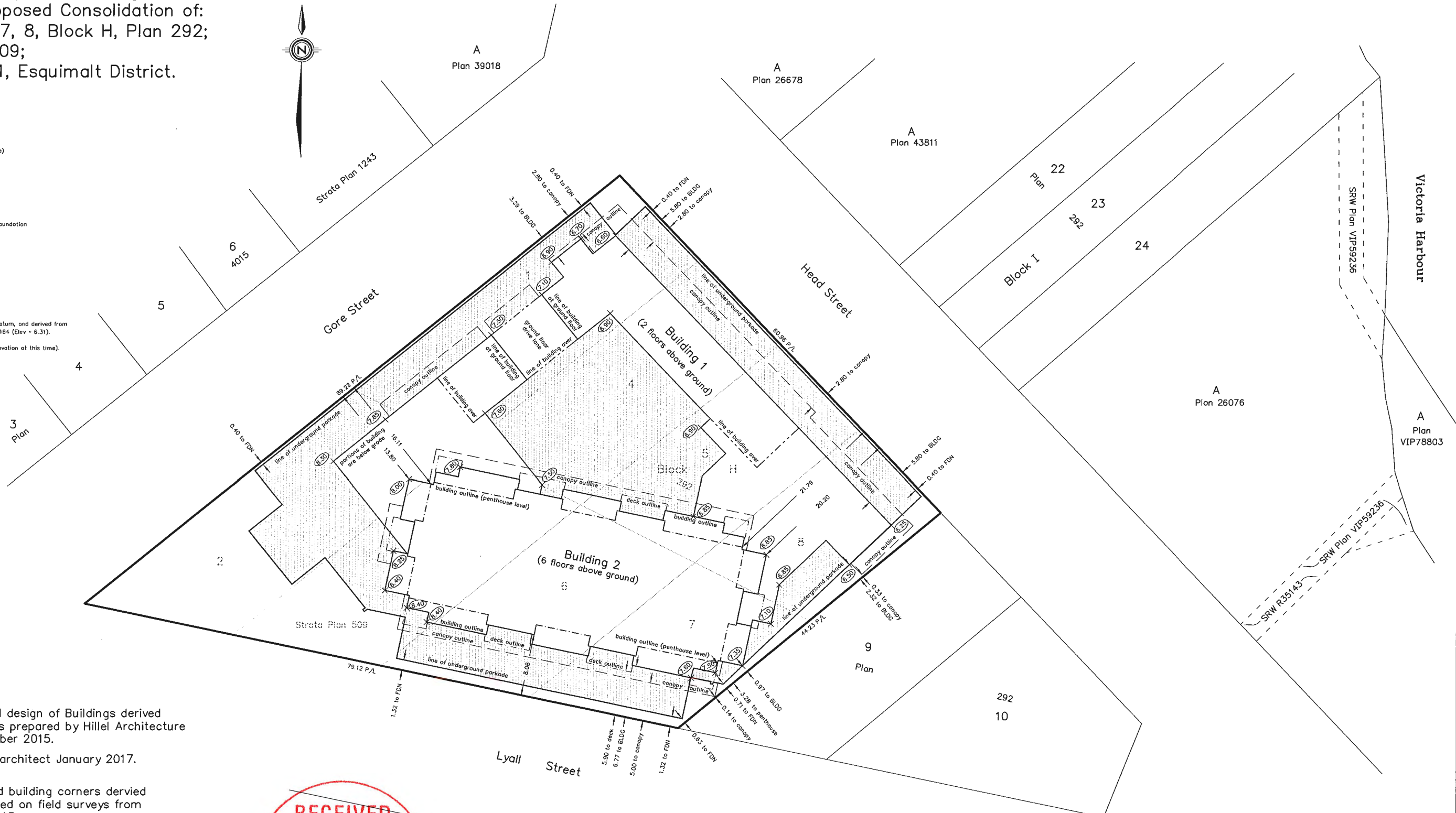
All distances are in metres and decimals thereof unless otherwise indicated.

- denotes parkade and underground foundation
- building footprint
- P/L denotes property line
- FDN denotes foundation underground
- BLDG denotes building above ground

Total Site Area = 0.408 ha

Elevations are in metres, based on geodetic datum, and derived from Bench Marks B4H0163 (Elev = 9.98) and B4H0164 (Elev = 6.31).

Elevation at Original Ground (no excavation at this time).



Note:
 Proposed location and design of Buildings derived from autocad drawings prepared by Hillel Architecture and received 7 December 2015.

Plan amended as per architect January 2017.

Elevations at proposed building corners derived from interpolation based on field surveys from July 2012 and April 2013.

